

**Special Meeting  
Of  
Planning Committee  
15th March 2012**

**Bolton  
Council**

Bolton Council has approved a Guide to Good Practice for Members and Officers Involved in the Planning Process. Appendix 1 of the Guide sets down guidance on what should be included in Officer Reports to Committee on planning applications. This Report is written in accordance with that guidance. Copies of the Guide to Good Practice are available at [www.bolton.gov.uk](http://www.bolton.gov.uk)

Bolton Council also has a Statement of Community Involvement. As part of this statement, neighbour notification letters will have been sent to all owners and occupiers whose premises adjoin the site of these applications. In residential areas, or in areas where there are dwellings in the vicinity of these sites, letters will also have been sent to all owners and occupiers of residential land or premises, which directly overlook a proposed development. Copies of the Statement of Community Involvement are available at [www.bolton.gov.uk](http://www.bolton.gov.uk)

The plans in this report have been annotated with the symbol ● to show where a letter of objection has been received from an owner or occupier of a property shown on the Report Plan.

The plans in this report have been annotated with the symbol ▲ to show where a letter of support has been received from an owner or occupier of a property shown on the Report Plan.

The plans in the report are for location only and are not to scale. The application site will generally be in the centre of the plan edged with a bold line.

The following abbreviations are used within this report: -

CS	Bolton's Adopted Core Strategy
RSS	Regional Spatial Strategy for the North West of England 2008
PCPN	A Bolton Council Planning Control Policy Note
PPG	Department of Communities and Local Government Planning Policy Guidance Note
MPG	Department of Communities and Local Government Minerals Planning Guidance Note
SPG	Bolton Council Supplementary Planning Guidance
SPD	Bolton Council Supplementary Planning Document
PPS	Department of Communities and Local Government Planning Policy Statement
TPO	Tree Preservation Order
EA	Environment Agency
SBI	Site of Biological Importance
SSSI	Site of Special Scientific Interest
GMEU	The Greater Manchester Ecology Unit

The background documents for this Report are the respective planning application documents which can be found at:-

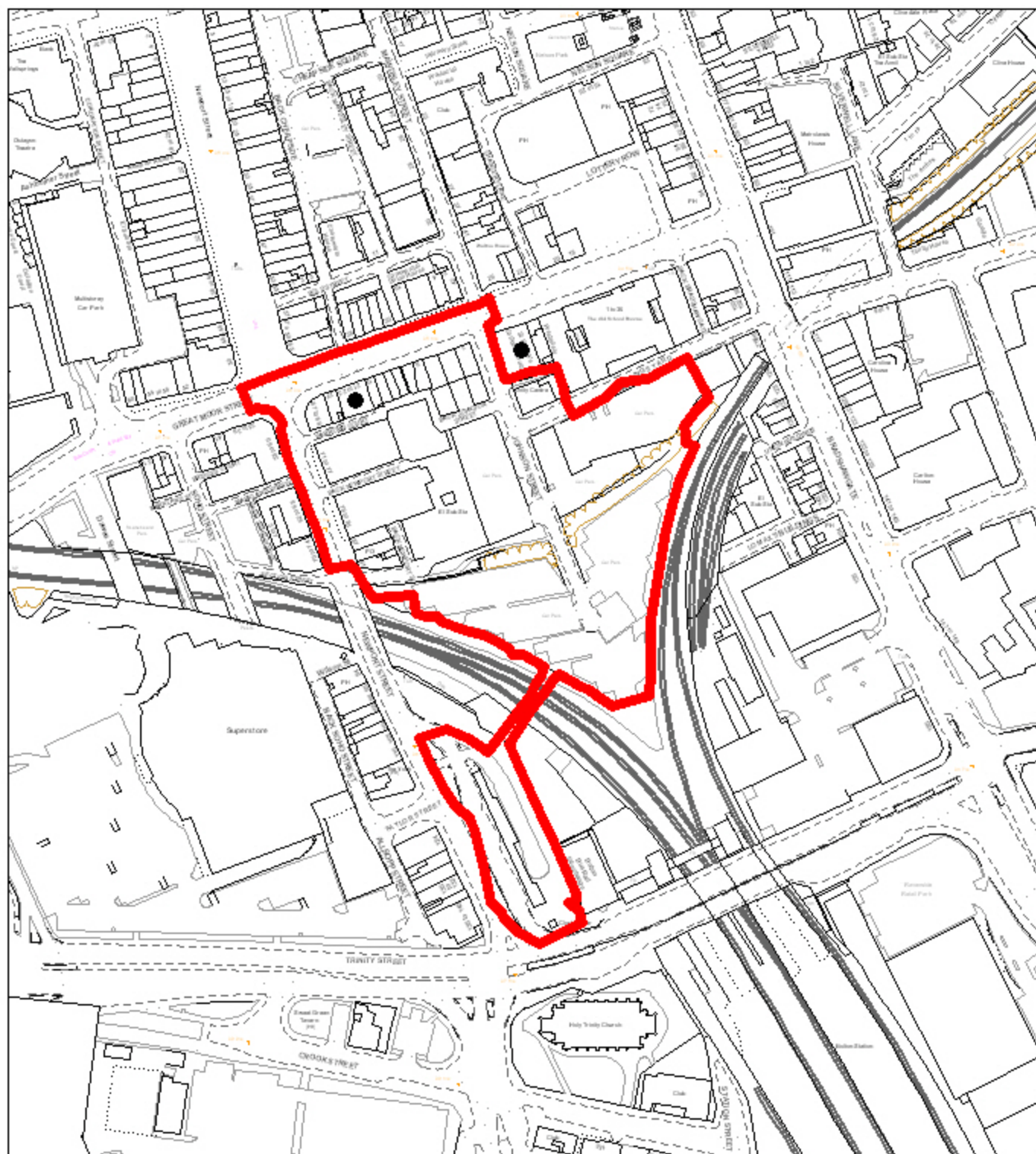
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Application No.

**87438/12**



**Development and Regeneration Department  
Planning Control Section**

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**Date of Meeting: 15/03/2012**

**Application Reference: 87438/12**

**Type of Application: Reserved Matters**  
**Registration Date: 30/01/2012**  
**Decision Due By: 29/04/2012**  
**Responsible Officer: Jon Berry**

**Location: LAND BOUNDED BY BOLLINGS YARD/JOHNSON STREET/NEWPORT STREET/GREAT MOOR STREET (INCLUDING THE RAILWAY TRIANGLE), BOLTON.**

**Proposal:** PART A - DEMOLITION OF UNITY CENTRE, NEW BUS STATION AND ANCILLARY FACILITIES, PEDESTRIAN BRIDGE TO THE RAIL STATION, 2 DETACHED OPERATORS BUILDINGS, CYCLE CENTRE, PUBLIC REALM, LANDSCAPING, HIGHWAY WORKS AND AMENDMENTS TO THE BOLTON INTERCHANGE ON NEWPORT ST.  
PART B - FUTURE COMMERCIAL DEVELOPMENT APPROVED UNDER OUTLINE APPLICATION 83971/10 WHICH WILL BE THE SUBJECT OF A FUTURE RESERVED MATTERS PLANNING APPLICATION

**Ward: Great Lever**

**Applicant: TfGM**  
**Agent : Aedas Architects Ltd**

### **Officers Report**

**Recommendation: Approve subject to conditions**

#### **Proposal**

This is the reserved matters planning application (Members will recall the previous grant of outline permission referenced 83971/10 in June 2010) to integrate Central Bolton's bus station with the main railway station, by relocating the existing facilities from Moor Lane (which is 650 metres away), together with ancillary development. This proposal is a key component of the adopted Town Centre Transport Strategy Bolton, which includes an anti clockwise gyratory system for buses and a clockwise circulation route for general traffic. The principle of an additional future commercial and retail scheme is again put forward, though this remains in outline form for a later phase of planning process and delivery.

The bus station has been designed to accommodate a theoretical maximum capacity of 210 bus movements per hour at peak weekday times, but the currently anticipated maximum is 155 per hour and the average is likely to be approximately 107 per hour. On Sundays the anticipated maximum is reduced to 52 bus movements per hour and the average will be around 38 per hour (within a reduced maximum peak capacity of 71 per hour pro rata). The reasons for the differences are because operational flexibility and scope for future growth have been factored in. The buses would be served by 18 drive in reverse out stands (DIROs), 4 single drive through stands, a double drive through stand

(23 in total) and 9 lay-over/out of service bays. The design life is for at least 40 years and is to a BREEAM "very good standard" (59%).

Within the bus station would be a new cafe, toilets, retail units, a supervisor's office (with 360 degree views) and a travel shop. The ancillary development also includes operators' office accommodation and crew facilities buildings and a cycle centre (50-80 spaces). The proposed operators' offices, 5 - 5.5 metres in height, have been relocated to the boundary of Bollings Yard, in order to provide screening against noise and light spillage between the bus route and the residential apartments in the Old School Rooms. An adjoining acoustic barrier, taking the form of a stone wall, would stretch from this operators' building around the boundary with St Patrick's Church, and this would vary in height between 5 metres, at the easternmost point, and 4.25 metres along the approach to Great Moor Street. A new curtilage of public realm, including access and servicing provision, would be created in an enhanced setting for the church. The crew facilities would be in an island between the bus routes in the north-eastern area of the site. The cycle centre would be created adjacent to the bus station entrance on the Newport Street side, fronting a public square with Yorkstone paving offering attractive pedestrian access from Central Bolton. Public Art is proposed in the bus station.

The predominant external materials would be as follows:

Bus station - steel, glazing, blue brick plinths;

Skylink - aluminium and glass;

Cycle Centre - reconstructed stone, glazing, blue brickwork;

Operators' buildings - reconstructed stone panels, glazed corridors, blue brickwork walls, and bio diverse green roofs.

The existing Bollings Yard car park is to be reduced as part of the scheme. Only 5 staff car parking spaces are proposed within the site with no provision for the public.

The proposed retail and commercial area, to be developed in a future phase, would be sited to the north of the interchange, in order to provide the context for the main approach from Central Bolton, around the public square. An enclosed skylink footbridge would also provide new pedestrian access across the railway lines from the bus station, and the commercial and retail core, to the arrival point at the recently remodelled railway ticket office.

In terms of highways arrangements, there are changes proposed to the roads within and surrounding the site and around the existing small scale bus interchange on Newport Street. Pedestrian facility improvements are proposed at what would be a signalised junction of the bus and church access lanes and Great Moor Street and a signalised crossing would also be provided on Newport Street, leading to the bus station entrance. All existing bus stops would be removed from Great Moor Street, which would be tabled between Newport Street and Mawdsley Street. Bus movements would circulate in an anti-clockwise direction. The site entrance would be from Newport Street and there would be a two-lane exit point on to Great Moor Street. Refuse vehicles would use the first DIRO three times a week.

The application is supported by an acoustic modelling report; an air quality assessment; a crime impact statement; a design and access statement; a sustainable urban drainage strategy; a sustainability, utilities and energy assessment; an underground drainage repair method statement; an environmental risk assessment and geotechnical interpretive report; an archaeological assessment; an ecology assessment; a transport assessment

and travel plan; and a public consultation report.

The application is accompanied by separate applications seeking conservation area consent for the demolition of the Unity Centre (87440/12) and listed building consent for the creation of a new area of public realm and an acoustic barrier wall adjacent to St Patrick's Church (87448/12)

### **Site Characteristics**

The application site consists of 2.3 hectares of urbanised land at the southern edge of Central Bolton, bordered by Great Moor Street, Newport Street, Johnson Street and Bollings Yard. The land is made up largely of surface car parks surrounded by Bolton Railway Station, commercial office, retail, leisure, residential and religious buildings. Within this area, the Preston and Blackburn to Bolton and Manchester railway lines diverge at the north end of Bolton station, creating a triangular junction. The only soft landscaping comes in the form of patches of trees and shrubs. In total, there are 59 individual trees and 6 groups of trees within the affected land and at the periphery.

To the north is the Mawdsley Street Conservation Area, which includes the Grade II listed St. Patrick's Church and Presbytery dating from 1861, fronting onto Great Moor Street. Next to the church is the former Bolton County Grammar School, which is also a Grade II listed building constructed of brick and terracotta in 1897. This building has been converted into apartments known as the Old School Rooms. Ground levels fall significantly from here through the application site by approximately 5 and 6 metres towards the railway station to the south.

The site lies within the Innovation Zone Trinity Gateway. Video footage of the salient features of the site and the surroundings will be presented to Members at the meeting of the Planning Committee on 15th March 2012.

### **Policy**

National Planning Policies:

Draft Planning Policy Statement: Planning for a Natural and Healthy Environment Planning Policy Statement 1:

Delivering Sustainable Development Planning Policy Statement: Planning and Climate Change - Supplement to Planning Policy Statement 1

4: Planning for Sustainable Economic Growth Planning Policy Statement

5: Planning for the Historic Environment Planning Policy Statement

9: Biodiversity and Geological Conservation Planning Policy Statement

10: Planning for Sustainable Waste Management Planning Policy Statement

12: Local Spatial Planning Planning Policy Guidance

13: Transport Planning Policy Guidance

17: Planning for Open Space, Sport and Recreation Planning Policy Guidance

19: Outdoor Advertisement Control Planning Policy Guidance

22: Renewable Energy Planning Policy Statement

23: Planning and Pollution Control Planning Policy Guidance 24: Planning and Noise Planning Policy Statement

25: Development and Flood Risk Planning Policy Statement.

Bolton Council's Local Development Framework; Core Strategy Development Plan Document Adopted 2nd March 2011; Shaping the Future of Bolton

Policies TC4 Innovation Zone, Bolton, Trinity Gateway; TC11 Design in the Town Centre; H1 Healthy; A1 Achieving; P1 Employment; P2 Retail and Leisure; P3 Waste; P5 Accessibility; S1 Safe; CG1 Cleaner and Greener; Sustainable Design and Construction; CG3 The Built Environment; CG4 Compatible Uses; SC2 Community Provision; IPC1 Infrastructure and Planning Contributions.

PCPNs 1 Health, Well Being and Quality of Life; 2 Space Around Dwellings; 6 Display of Signs and Advertisements; 7 Trees: Protection and Planting in New Developments; 9 Location of Restaurants, Cafes, Public Houses, Bars and Hot Food Takeaways in Urban Areas; 10 Planning Out Crime; 18 Provision for Cyclists; 19 Conservation Areas; 20 Listed Buildings; 21 Highways Considerations; and 22 Public Art.

Bolton Council Trinity Interchange Supplementary Planning Document June 2010

Bolton Council Building Bolton

Bolton Council Town Centre Transport Strategy

The Bolton Town Centre Public Transport Study

Bolton Council Public Realm Implementation Framework

### **Analysis**

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- \* impact on the Bolton Innovation Zone Trinity Gateway
- \* impact on the character and appearance of the locality, including the Mawdsley Street Conservation Area
- \* impact on the highway network
- \* impact on the neighbouring occupiers and the settings of the listed buildings



### Impact on the Innovation Zone, Bolton, Trinity Gateway

Bolton Council's Core Strategy, which was adopted on 2nd March 2011, specifically describes the Trinity Gateway of the Innovation Zone Bolton as "an area around Bolton railway station and bus interchange. It includes surface car parking to the north of the station and retailing along Newport Street. The surface car parking and the existence of some vacant sites gives opportunities for redevelopment, and the station is a key transport location for the whole borough".

Under the national framework of Planning Policy Statements and Guidance 1, 4 and 12, Core Strategy Policy TC4, at the local level, therefore states that "the council and its partners will:

1. Develop a high quality multi-modal interchange that will serve bus, rail and car borne passengers on the triangle of land between the Preston and Blackburn railway lines. This will replace the existing bus station at Moor Lane.
2. Redevelop the remainder of this site for new office, retail and leisure uses, and for a new multi-storey car park.
3. Ensure that there are good pedestrian links to the rest of the town centre.
4. Take opportunities to improve the commercial frontages along Newport Street".

The Bolton Town Centre Public Transport Study of March 2005 established the need to redevelop the bus station by removing it from the existing location at Moor Lane and creating a new facility within the vicinity of the rail station. There is an opportunity to deliver better waiting areas, accessibility, information displays, safety and security.

Bolton Council has set out a clear aspiration to improve the central core of the town by creating a genuine sustainable transport interchange and therefore a stronger gateway to the Borough. The initial work carried out by the Applicant to maximise the efficiency and effectiveness of the new bus station at the outline planning application stage, so that a large number of bus movements can be accommodated without blockage within the site, has now been complemented by detailed work to ensure that there would be high quality connectivity with the public realm of Central Bolton. The presentation of the new interchange to the context is key to ensure the scheme integrates with the pedestrian core.

The Places Matter! North-West Design Review Panel now consider that what has been *"created is more than simply a bus station and (this) should be seen and named as such"*. The public square and route to the site frontage is welcomed and the scheme provides the right balance between attractiveness and functionality. The layout would now benefit the pedestrian user of Central Bolton and there would be activity and surveillance all around the site, between the retail core and the southern part of the centre.

The new bus station would be a landmark new building providing a focus for views from and to the town centre. The steel, glazing and blue brick plinths would create elevations that would be sufficiently distinctive and the plentiful glass in the sky link would offer a safe and welcoming light space, thereby encouraging pedestrian activity. This is a great improvement on the existing approach to the train station where the current arrangements of the bus station and the taxi rank distorts the sense of arrival. The scale and design of the new public realm would be of the highest standard to form the

centrepiece and backdrop to the development and the use of durable and high quality natural finishes accords with Bolton Council's Public Realm Implementation Framework. Bolton Council's Strategic Development Officers advise that "*the proposed modern, high quality interchange will significantly improve the facilities, environment and public transport offer currently available to passengers in Bolton*".

It remains the aspiration to develop commercial, retail and leisure facilities around the public square link to the bus station entrance and a future phase of the overall scheme has been earmarked in this planning application accordingly. Ultimately, such uses would help to generate more pedestrian activity and add to the character of this crucial area between the existing town centre and the interchange. The Trinity Interchange Supplementary Planning Document states that "a comprehensive approach is to be adopted when proceeding with the development. Simply providing a transport interchange on the land would not make the most of this opportunity to substantially enhance the commercial, cultural and administrative heart of the town centre, together with providing a safe, modern transport hub.

The associated commercial development area would require the relocation of a number of existing businesses, currently fronting on to Great Moor Street and Newport Street. In terms of business support and relocation, the Strategic Development Officers have provided the following update:

*"The Council is acutely aware that the regeneration of the application site will inevitably involve the dispossession of businesses and the relocation of those businesses into alternative premises is a priority. In order to assist businesses, the Council is providing support which includes:-*

- \* assistance in finding a site/premises within the Bolton area;*
- \* assistance in finding space in other locations further afield, where this is the choice of the business;*
- \* access to support through local business support groups where appropriate;*
- and*
- \* access to training through the Skills Funding Agency".*

*Officers have contacted all business operators to arrange meetings to discuss current issues and ascertain future requirements".*

The reserved matters application is considered to accord with Planning Policy Statements and Guidance 1, 4 and 12 and Core Strategy Policy TC4.

#### Impact on the character and appearance of the locality, including the Mawdsley Street Conservation Area

Core Strategy Policy TC11 provides that "the council and its partners will:

1. Protect, strengthen and enhance the distinctive townscape qualities of Bolton town centre. Development should conserve and enhance the special nature of the conservation areas and listed buildings.
2. Ensure that development along the gateways to the town centre enhances the townscape through the use of high quality design and improves street frontage and pedestrian permeability.
3. Ensure a high standard of design of public realm.

4. Require development to respect and enhance existing vistas

5. Make efficient and effective use of land in the town centre due to the existing levels of high density development, ensuring that development provides adequate amenity space and privacy, or attractive public areas, where appropriate for the site".

Policy CG3 provides that the council will "conserve and enhance the heritage significance of heritage assets and heritage areas, recognising the importance of sites, areas and buildings of archaeological, historic, cultural and architectural interest and their settings".

The design of the bus station does, in the view of Officers, provide a visually interesting focus to frame views from the outside area. The creation of a contemporary, lightweight building here would create the right solution. The scale of the development fits well within the site and the locality, and the heights of the new buildings would not protrude above the properties that are to remain on the western side of Newport Street and the northern side of Great Moor Street, in the skyline. The supporting visual information submitted by the Applicant clearly demonstrates that the proposal would integrate with the surrounding streets and the wider area, whilst also improving the quality of the townscape.

One building, the Unity Centre, within the Mawdsley Street Conservation Area would be demolished under the proposal, in order to facilitate the improvement of Bollings Yard/Johnson Street to serve St Patrick's Church. The Council is obliged to protect the special nature of this area and two separate assessments have been carried out in the form of the Mawdsley Street Conservation Area Character Study and Building Bolton. The former identifies the Unity Centre as, not being a building of merit, but rather as a feature making a neutral impact. Building Bolton does not recognise this property as being of high value in terms of its contribution to local distinctiveness, nor as being a type characteristic of the area by way of its scale and materials choice. In the context of the benefits of the scheme, the loss of the Unity Centre is not considered to be a negative factor of significant weight.

The reserved matters application is considered to accord with Core Strategy Policies TC11 and CG3.

#### Impact on the highway network

Planning Policy Guidance 13 (PPG13) sets out the objectives to integrate planning and transport at the national, strategic and local level and to promote more sustainable transport choices for carrying people. For transport in Bolton, Core Strategy Policy P5 therefore states that "the council and its partners will ensure that developments take the following into account:

1. Accessibility by different types of transport, prioritising pedestrians, cyclists, public transport users over other motorised vehicle users.
2. The design of developments to enable accessibility by public transport.
3. Servicing arrangements.
4. Parking, including parking for cycles in accordance with parking standards.
5. The transport needs of people with disabilities.

6. The requirement for a Transport Assessment and Travel Plan with major trip generating developments".

The bus station has been designed to ensure it can handle the number of bus movements proposed. For example, if buses destined for the same stand happen to arrive together, there would always be nearby bays available. Negotiations have taken place for approximately four years between the Council's Highways Engineers, the Applicant, private transport consultants and the Greater Manchester Transportation Unit. Modelling has revealed that the proposed arrangements would be acceptable relative to the Central Bolton gyratory transport system and strategy. The intention in the application is for general traffic including buses to approach from the west and enter a one way highway between St Patrick's Church and Newport Street. Buses, black cab taxis and cyclists can also approach from the east to the junction between Great Moor Street and Newport Street and travel southbound towards the entrance to the interchange. The buses would then manoeuvre through the site, continuing in an anti-clockwise direction, and can turn either left or right back out onto Great Moor Street. General traffic would be able to travel northbound along Newport Street from Thynne Street, though there would be no turn in from Trinity Street (a condition is recommended to investigate improvements to the exiting arrangements so that this does become possible). The junctions around the site would be pedestrianised to emphasise that this scheme is to benefit public transport users connecting with Central Bolton. Vehicular access to the on-site car parking spaces at the Old School Rooms would be retained and the access and servicing arrangements for the church would be protected by demountable bollards operated by the occupiers.

The Council's Highways Engineers raise no objections, though they have proposed a number of minor technical amendments in terms of road markings; yellow box markings; button islands and signage near the bus only lanes; cycle lanes, boxes and routes; details of the "over run" area at the bus station entrance; improvements to the entrance to the interchange undercroft; and CCTV. The plans can be updated by the Applicant to show the inclusion of these requirements and further progress regarding this will be reported at the meeting.

The engineers also advise that planning conditions should be attached to a grant of permission to secure access to the railway track area through the undercroft for local authority vehicles involved in the inspection of bridges within Central Bolton; a street order closure for the area immediately to the west of St Patrick's Church; the maintenance of the permissive right of way between Great Moor Street and Bollins Yard; and the restricted circulation of construction traffic so that vehicles enter Central Bolton via Trinity Street and Bradshawgate, travel into the development site via Johnson Street and exit along Newport Street.

The negative factors of the proposed highways arrangements are that removable bollards or gates are required at Bollings Yard to the south of the church, in order to prevent a rat-run from Bradshawgate to Great Moor Street, and that traffic signals are necessary at the exit from the church yard into Great Moor Street. An alternative option has been considered by the Highways Engineers, the Applicant and transport consultants to make the route next to St Patrick's Church one way, running from Great Moor Street to Bollins Yard. This would have brought different benefits of reducing the need for signals at Great Moor Street (thereby improving network efficiency); allowing the creation of a church yard under the control of the occupiers of St Patrick's Church; and enabling multi-directional movements on exit from the church curtilage via Bollins Yard into Bradshawgate and/or Great Moor Street.

However, the Local Highway Authority recognises that Transport for Greater Manchester have gone to considerable lengths to meet the aspirations of the occupiers of St Patrick's Church in agreeing the access arrangements. The highway scheme as proposed is confirmed as being acceptable and the Diocese of Salford has written to Officers to say that the *"traffic flows meet concerns previously expressed by the Church and as such are appreciated"*. The alternative option is unsuitable for the representatives of the church, and funeral directors, as it would result in a change in operation, whereby vehicles associated with weddings and funerals would have to use a less dignified route by exiting onto Back Bradshawgate rather than Great Moor Street. The Applicant contends that the technical analysis carried out by the transport consultants demonstrates that the signal junction to the bus station exit, (which includes a pedestrian phase), would still be required to ensure the safe transfer of the anticipated volume and frequency of passenger flows across this, regardless of the traffic movements generated by St Patrick's Church.

The application conforms to PPG13, Core Strategy Policy P5 and PCPN 21.

#### Impact on the neighbouring occupiers and the settings of the listed buildings

Planning Policy Guidance 24 (PPG24) guides local authorities in England on the use of their planning powers to minimise the adverse impact of noise. It outlines the considerations to be taken into account in determining planning applications both for noise-sensitive developments and for those activities which generate noise.

It explains the concept of noise exposure categories for residential development and recommends appropriate levels for exposure to different sources of noise.

It also advises on the use of conditions to minimise the impact of noise. Six annexes contain noise exposure categories for dwellings, explain noise levels, give detailed guidance on the assessment of noise from different sources, gives examples of planning conditions, specify noise limits, and advise on insulation of buildings against external noise.

In light of this advice, Core Strategy Policy CG4 therefore states that "the council and its partners will:

1. Ensure that new development is compatible with surrounding land uses and occupiers, protecting amenity, privacy, safety and security;
2. Development should not generate unacceptable nuisance, odours, fumes, noise or light pollution, nor cause detrimental impacts upon water, ground or air quality".

Policies CG3 and TC11, as described earlier in the analysis section of this report, (under impact on the character and appearance of the locality, including the Mawdsley Street Conservation Area), is also relevant to this matter.

The setting of St Patrick's Church and the Old School Rooms residential apartments has been focused upon at this detailed design stage. The aim has been to create a curtilage and boundary treatment which defines significant separation between these existing historical buildings and this modern and transformational project. The new space, access and servicing arrangements for the church and the acoustic wall would mark the periphery of the Mawdsley Street Conservation Area and set the properties clearly within this by creating some protection from having unsympathetic heavily trafficked bus routes immediately adjacent. The sandstone and reconstituted materials of the barrier would be in keeping with the characteristics of the area and the new development. The enclosure

this would create would prevent views of the bus station development for the visitors to the church, thereby helping to maintain the perception of being within an historic religious environment. The principle of the Yorkstone paved yard, the new curtilage, the sound mitigation measures and the screening from the bus routes has been welcomed by the Places Matter! Design Review Panel and representatives of the church. However, what is troubling the latter is the sheer height of the wall, the impact of this on the setting of the listing building and the possible anti-social behaviour (particularly graffiti) which this could attract.

On the other hand, the main benefit of the acoustic wall is the contribution it would make towards noise control. Bolton Council's Environmental Health Officers have reviewed the assessments provided by the Applicant and have advised as follows:

*"The acoustic information provided for weekdays (anticipated and theoretical peaks) would indicate that 14 of the 16 flats at the Old School Rooms should benefit from a 'no worse than' situation once the new bus station is in operation. There are only 2 flats where there would be an exceedence of existing noise levels by more than 1dB but less than 3dB. This, therefore, would indicate there would not be a discernible increase in volume. However, the type of noise being generated would change, as reversing alarms would be being used by the buses in the new interchange and, as such, the noise is likely to be more noticeable.*

*With regard to the Sunday activities (anticipated and theoretical peaks), Officers would advise that the figures provided indicate that 4 flats would have a 'no worse than' situation, 7-8 would have no discernible increase in noise and 4-5 would have a noticeable rise. The Applicant though is proposing to only use stands furthest away from the residents, (in the reduced stand capacity for Sundays), in which case 7 flats would have a 'no worse than' situation, 6-7 would have no discernible increase and 3-4 (those in the upper floors in the south-west corner of the building), would have a noticeable rise.*

*Officers do not agree with the Applicant's presumption that the noise control code of practice can be assessed correctly with windows closed for properties which do not have mechanical acoustically treated ventilation with openable windows. The presumption must be that the occupiers of such properties would need to open the windows for fresh air.*

*It can therefore be reasonably concluded from the assessment that the residential properties do not achieve the reasonable standard of sound attenuation at the present time and that some of the current circumstances would be exacerbated as described above.*

*In the case of the church, as the stain glass windows will not be openable, then this presumption would be acceptable. As such, noise levels would not discernibly increase on weekdays but there are concerns with regard to those for Sundays, when, even with the acoustic works proposed, there would still be a significant rise. However, there does not appear to be any other reasonable acoustic treatment that the Applicant could propose within the site to provide additional mitigation.*

*This does not preclude the Applicant's ability to offer to carry out further works to the actual residential and church buildings, but it recognised that this would involve third party land that is outside of the controlling interest. Planning conditions are recommended to secure further land contamination testing; the noise control of fixed plant and machinery; acceptable lighting levels and the maximum use of the drive through stands, in order to restrict the need for bus reversing alarms, at night and on Sundays".*

The planning system is often about achieving the optimum balanced solution to land use

issues. Clearly the Council's Environmental Health Officers do not consider that the acoustic wall could be at all lowered without there being an additional adverse effect upon the neighbouring occupiers and it is acknowledged that the Applicant has gone as far as possible towards making the proposed environment acceptable. On the other hand, the nature of the wall remains a concern to the representatives of the church.

The Historic Churches Commission has suggested an alternative means of resolving the matter for the church. It has been put forward as follows that *"the acoustic barrier be deleted from the development scheme (there have been no representations from the interested parties at the Old School Rooms, who are protected by one of the proposed operators' buildings) and that instead the funding which would have been allocated to construct it be used to finance the design and installation of internal acoustic screens to the windows of the church.*

*The church already has polycarbonate screens fitted to the exterior of the windows. It is believed that these were installed before the ecclesiastical scheme came into existence as an anti-vandalism measure. Unfortunately they have been incorrectly fitted directly into the stonework of the windows. New screens should be designed and fixed appropriately to the exterior of the windows.*

*It is felt that if this was agreed to, these insulation measures should significantly reduce the increased noise levels that will occur when the new bus station is in use and the visual amenity would be protected if not enhanced when the buildings opposite the (liturgical) South elevation of the church are demolished.*

*The acoustic barrier should be replaced by railings for road safety reasons".*

The Applicant has agreed to investigate the potential for this with representatives of the church, though any additional noise impact for residents of the Old School Rooms, resulting from the deletion of the acoustic wall, would also need to be considered. Officers cannot recommend that any planning condition requires works to third party land and property, as this would be ultra vires, but instead a condition can be included by Members to require a report into the practicality of this alternative option. If it is viable, the condition could be discharged upon completion of the agreed works. If it is not, then the entire acoustic barrier would need to be implemented in accordance with the plans, as Officers consider that this presents the optimum balance between noise control and visual amenity within the application site. The Applicant would have to be responsible for maintaining and managing the condition of the wall.

The residents of the Old School Rooms would largely be protected from noise by the operators' building positioned at the boundary with Bollings Yard. The latter would be of a much smaller scale but, at a height of 5 - 5.5 metres, it would provide an effective screen from the new bus route within the station. The separation distance between the two properties would be 11.73 metres from the nearest ground floor facing residential window to the new elevation. PCPN 2, which offers guidance on space standards, requires new single storey buildings such as this, with no domestic windows, to be set back 9 metres from existing main openings. If the operators' block had two storeys the requirement would be for a separation of 13.5 metres. The proposed building is relatively tall for a single storey unit, and so, pro rata, this element of the scheme is considered to accord with policy guidelines. The development of the land with an attractive construction using sympathetic materials would improve the setting of the adjacent listed building and conservation area.

The air quality assessment is acceptable.

### **Value Added to the Development**

Consultations with Officers, Places Matter North-West, neighbouring occupants and the community have resulted in improved connectivity between the bus station and the core of the town centre, a landmark design for the building and a new curtilage, including access/servicing provision, and an improved setting for St Patrick's Church. The major investment of £48 million is now considered to represent value for money as the development would be an integrated part of a transport hub and Central Bolton, rather than being a functional but individual element of infrastructure.

### **Conclusion**

Officers conclude that this proposal would represent a high quality benefit to the town, rail and bus passengers, businesses and the Innovation Zone. A genuine, efficient, safe, secure and pleasant gateway interchange would be created that would really strengthen links between the railway station and the core of Central Bolton. The application site would be drastically transformed. A new setting to St Patrick's Church would be created and the Applicant is working with those representatives to try and achieve noise control measures within the listed building. If this does not prove to be viable, then the application proposes the optimum solution of the acoustic barrier, in terms of securing a balance between noise attenuation and visual amenity on the development site. The highways arrangements have been assessed in considerable detail and are considered to be acceptable subject to the conditions suggested by the Highways Engineers, as attached to this report. Planning decisions should be made in the interests of the greater good, which in this case is the regeneration of Central Bolton, and it is recommended that Members support the benefits of this scheme by issuing a grant of permission under the framework of national and local policies.



## **Representation and Consultation Annex**

### **Representations**

**Letters:-** 2 letters have been received from residents raising objections to the proposed development on the following grounds:

- \* impact on existing local businesses and services currently operating within the boundaries of the development site
- \* the Diocese of Salford has not been supplied by a copy of the final noise assessment by the Applicant
- \* the noise assessment submitted with the application displays a number of potentially significant snags which may have a material bearing on the predicted noise impacts taking into account the proposed 4m high noise barrier
- \* difficulty accessing documents online

Ladbrokes has objected as it is considered that the bus station development and public realm could be developed without the need for the outlet at 21-25 Great Moor Street and it is premature to include this for demolition in this application as it is an issue for the commercial development phase and The Bolton Council (Trinity Gateway) Compulsory Purchase Order 2011 (Officer Note: this is not a material planning consideration).

The Historic Churches Committee have raised the following concerns:

- \* the acoustic barrier could be subject to vandalism and may allow for anti-social behaviour within the church grounds
- \* the acoustic barrier would be detrimental to the visual amenity of the Grade II listed building and the street scene
- \* the acoustic barrier of the proposed height and length would be intrusive and detract from views from the church
- \* the acoustic barrier should be deleted and the funding should instead be put towards internal window screens, appropriately fixed
- \* railings are required instead of the acoustic barrier for road safety reasons.

The Diocese of Salford is broadly supportive of the scheme but has raised the following points:

- \* the acoustic barrier is welcomed in terms of noise control but not the setting of the listed building
- \* the acoustic barrier could attract anti-social behaviour and there are maintenance issues
- \* the public realm and paving detail would be beneficial
- \* traffic flow arrangements have been improved

- \* access is necessary to the external garage and toilet facility
- \* the development site encroaches upon the land ownership of the Diocese (Officer Note: this is not a material planning consideration).
- \* there is outstanding technical information in terms of the air quality and the noise pollution reports.

Bolton and District Civic Trust has commented on the application to request that the Applicant provides a parking facility for St Patrick's Church, particularly for elderly and disabled users. Although the plans indicate a provision for access to the church, separated from the access for buses, it would appear that the street parking and the car park that have existed on Bollings Yard for many years may be curtailed or cease to exist.

English Heritage has no comments.

**Petitions:-** none received.

**Elected Members:-** none received.

### **Consultations**

Advice was sought from the following consultees:

English Heritage

The Historic Churches Commission

Places Matter! North-West Design Review Panel

United Utilities

Network Rail

Bolton Council's Environmental Health Officers; Highways Engineers

### **Planning History**

Outline planning permission was granted under reference 83971/10 in June 2010 for the construction of a new bus station (to be relocated from Moor Lane) and ancillary facilities, pedestrian skylink bridge to railway station, detached operators' accommodation building, mixed use commercial development, public realm and landscaping (all matters reserved). The supporting information which was approved includes a flood risk assessment and a tree survey.

There is much further planning history in this area, but of most relevance to this current proposal is the construction of the existing transport interchange at the junction of Trinity Street and Newport Street in 1987, together with a new footbridge to connect the ticket office to the platforms. Improvement works including a new canopy over the interchange entrance, creation of a new taxi rank, conversion of the existing taxi rank into bus stands and renewal of lifts, stairways and overbridge were completed in 2006. In the same year a replacement bridge was completed to carry Newport Street over the railway.

In terms of the Old School Rooms, planning permission was granted in October 1999 for a change of use and conversion from an art college to 35 apartments on the basement,

ground and upper floors and food and drink use in the basement (55320/99). This was subject to a planning condition that 18 car parking spaces be provided for residents within the curtilage.

**Recommendation: Approve subject to conditions**

**Recommended Conditions and/or Reasons**

1. No development shall be commenced until samples of the facing materials to be used for the external walls and roofs have been submitted to and approved in writing by the Local Planning Authority.

Reason

To ensure the development either fits in visually with the existing building and safeguards the character and visual appearance of the locality or ensures the development safeguards the character and visual appearance of the locality.

2. Phase 1 Report

No development shall commence unless or until a Phase 1 Report (Preliminary Risk Assessment) to assess the actual and/or potential contamination risks at the site has been submitted to, and approved in writing by, the Local Planning Authority. The Report shall include a desk top study, site walk over, conceptual model, basic hazard assessment and recommendation regarding the need or otherwise for a Phase 2 Report.

Phase 2 Report

Should the Phase 1 Report recommend that a Phase 2 Report is required, then prior to commencement of any site investigation works, design of the Phase II site investigation shall be submitted to, and approved in writing by, the Local Planning Authority. Site investigations shall be carried out in accordance with the approved design and a Phase 2 Report shall then be submitted to, and approved in writing by, the Local Planning Authority prior to commencement of development. The Phase 2 Report shall include the site investigation data, generic quantitative risk assessment, detailed quantitative risk assessment (if required) and recommendations regarding the need or otherwise for remediation.

Should the Phase 1 Report recommend that a Phase 2 Report is not required, but during construction and prior to completion of the development hereby approved, contamination or gas migration is found or suspected, the developer shall contact the Local Planning Authority immediately and submit proposals for investigation and remediation of the contamination or gas migration within seven days from the date that it is found or suspected to the Local Planning Authority for approval in writing.

Options Appraisal

Should the Phase 2 Report recommend that remediation of the site is required then unless otherwise agreed in writing with the Local Planning Authority, no development shall commence unless or until an Options Appraisal has been submitted to, and approved in writing by, the Local Planning Authority. The

Options Appraisal shall include identification of feasible remediation options, evaluation of options and identification of an appropriate Remediation Strategy.

#### Implementation of Remediation Strategy

No development shall commence, unless otherwise agreed in writing with the Local Planning Authority, until the following information relating to the approved Remediation Strategy has been submitted and approved by the Local Planning Authority:

- I. Detailed remediation design, drawings and specification;
- II. Phasing and timescales of remediation;
- III. Verification Plan which should include sampling and testing criteria, and other records to be retained that will demonstrate that remediation objectives will be met; and
- IV. Monitoring and Maintenance Plan (if appropriate). This should include a protocol for long term monitoring, and response mechanisms in the event of non compliant monitoring results.

The approved Remediation Strategy shall be fully implemented in accordance with the approved phasing and timescales and the following reports shall then be submitted to the Local Planning Authority for approval in writing:

- I. A Verification Report which should include a record of all remediation activities, and data collected to demonstrate that the remediation objectives have been met; and
- II. A Monitoring and Maintenance Report (if appropriate). This should include monitoring data and reports, and maintenance records and reports to demonstrate that long term monitoring and maintenance objectives have been met.

#### Reason

To ensure that the development is safe for use.

3. All fixed plant and equipment requires to be designed to prevent noise disturbance to local residential property and should be designed to give a rating level, as defined in BS4142:1997, 5dB below the night time LAF90(5 min) or the daytime LAF90(1 hour) which ever is the most appropriate, as measured 4 metres from the nearest residential properties. The monitoring information and the acoustic calculations together with the proposed specifications of the fixed plant and equipment and any sound attenuation proposed should be submitted to the Local Planning Authority for approval prior to the commencement of installation.

#### Reason

To minimise the impact of noise on the general and residential amenity from the introduction of the development hereby approved.

4. Before development commences details of all external lighting equipment shall be submitted to and approved by the Local Planning Authority. The lighting in the scheme should be erected and directed so as to avoid nuisance to residential accommodation in close proximity. The lighting should be designed to provide a standard maintained illumination of no more than 5 LUX at the facades of the nearest residential properties. No other lighting equipment may then be used

within the development other than as approved by the Local Planning Authority.

Reason

To minimise light intrusion to the residential amenity of the area from the increase and alteration of commercial, industrial and recreational uses in the area.

5. The operational site managers should ensure the maximum use of the drive through stands for night services and the most frequent Sunday services (so the buses do not use reversing alarms).

Reason

In order to protect amenity for neighbouring residents.

6. No development shall be commenced unless and until an options appraisal report into the potential alternatives of providing the acoustic barrier at the boundary with St Patrick's Church or designing and installing internal acoustic screens to the windows has been submitted to and approved in writing by the Local Planning Authority. The scheme shall then be implemented entirely in accordance with the approved details before the development is first brought into use.

Reason

In the interests of the amenity of the occupiers of the church.

7. No development shall be commenced unless and until full details of the highway works at the Newport Street/Trinity Street junction; the Johnson Street/Great Moor Street junction and the Newport Street/Great Moor Street junction have been submitted to and approved by the Local Planning Authority, and none of the development shall be brought into use until such details as approved are implemented in full. Such works to be retained thereafter.

Reason

In the interests of highway safety.

8. Access shall be provided via the undercroft for local authority vehicles involved in the inspection of bridges within Central Bolton in accordance with details to be submitted to and approved in writing by the Local Planning Authority before the development is brought into use. The approved access arrangements shall be made available prior to the bus station first becoming operational and be so retained thereafter.

Reason

To facilitate the maintenance and management of the affected railway bridges.

9. No works shall be commenced to the new curtilage of St Patrick's Church until a street closure order has been secured from the Local Highway Authority.

Reason

To meet highway regulations.

10. The permissive right of way between Great Moor Street and Bollins Yard must be maintained at all times.

Reason

To meet highway regulations.

11. Construction traffic shall only enter Central Bolton via Trinity Street and Bradshawgate, the development site via Johnson Street and exit along Newport Street.

Reason

In the interests of highway safety.

1. The Council has granted planning permission, subject to the conditions listed above, because the proposed development is in accordance with all relevant policies of the local development framework, as is required by Section 38 of the Planning and Compulsory Purchase Act 2004. There are no material considerations, as specified in the Planning Officer Report, that outweigh this justification to support the grant of planning permission. A summary of the relevant Development Plan policies pursuant to Article 22 of the Town and Country Planning (General Development Procedure) (England) (Amendment) Order 2003 and how the proposed development relates to these policies is set out below.

Bolton Council's Local Development Framework; Core Strategy Development Plan Document Adopted 2nd March 2011; Shaping the Future of Bolton:

Policies TC4 Innovation Zone, Bolton, Trinity Gateway;  
TC11 Design in the Town Centre;  
H1 Healthy;  
A1 Achieving;  
P1 Employment;  
P2 Retail and Leisure;  
P3 Waste;  
P5 Accessibility;  
S1 Safe; CG1 Cleaner and Greener; Sustainable Design and Construction;  
CG3 The Built Environment;  
CG4 Compatible Uses;  
SC2 Community Provision;  
IPC1 Infrastructure and Planning Contributions.

Planning Control Policy Notes:

1 Health, Well Being and Quality of Life;  
2 Space Around Dwellings;  
6 Display of Signs and Advertisements;  
7 Trees: Protection and Planting in New Developments;  
9 Location of Restaurants, Cafes, Public Houses, Bars and Hot Food Takeaways in

Urban Areas;  
10 Planning Out Crime;  
18 Provision for Cyclists;  
19 Conservation Areas;  
20 Listed Buildings;  
21 Highways Considerations;  
and 22 Public Art.

Bolton Council Trinity Interchange Supplementary Planning Document June 2010

Bolton Council Building Bolton

Bolton Council Town Centre Transport Strategy

The Bolton Town Centre Public Transport Study

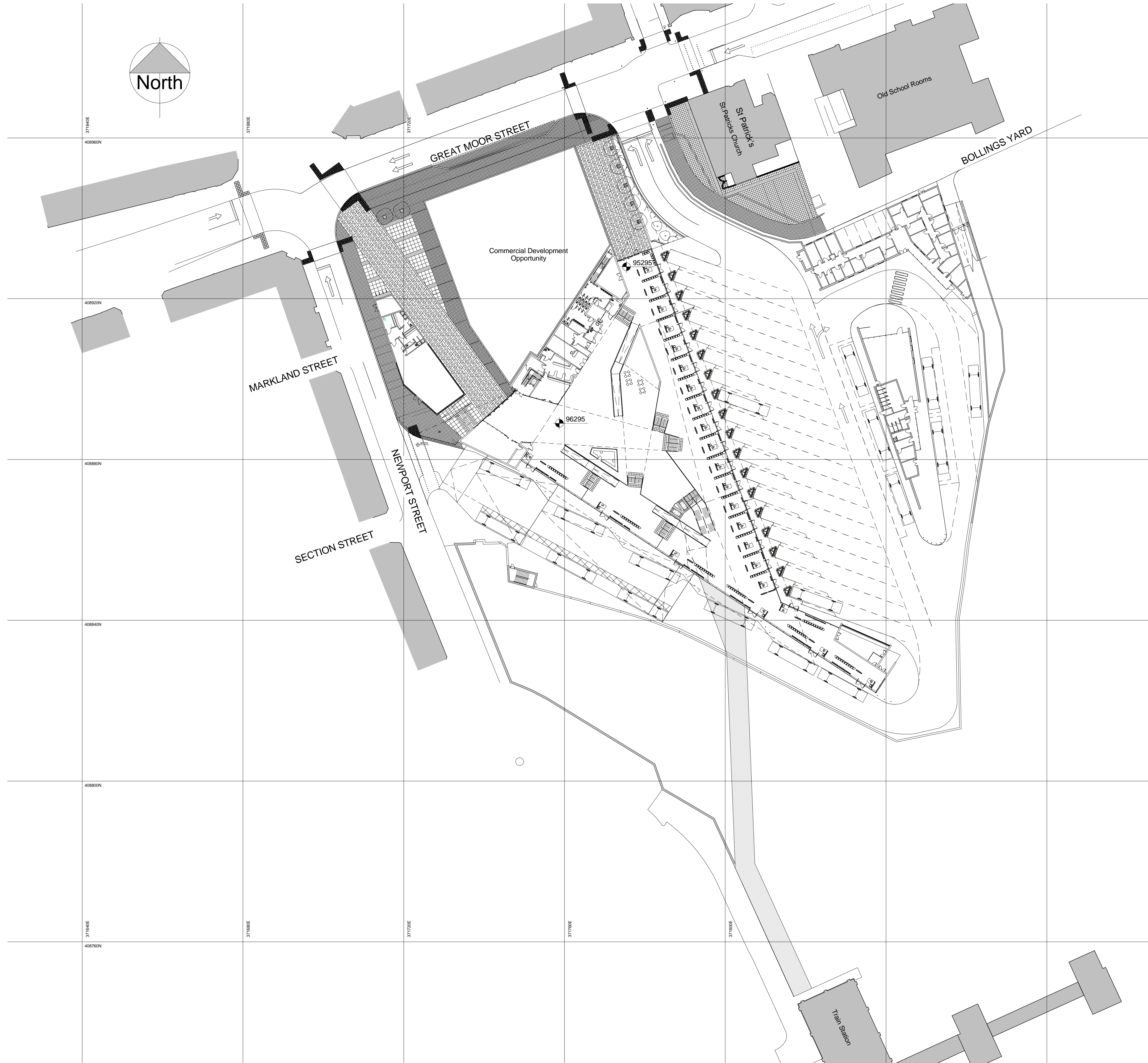
Bolton Council Public Realm Implementation Framework

2. Planning permission has been granted subject to the conditions listed above because the balance of the benefits accrued by the development outweigh all other material considerations, having had regard to the adopted Unitary Development Plan policies and Supplementary Planning Guidance detailed above.
3. The proposed development lies within a coal mining area, which could be subject to hazards resulting from past coal mining. Such hazards may currently exist, be caused as a result of the proposed development, or occur at some time in the future. Applicants must take account of these hazards, plus those beyond the application site, which could affect stability, health and safety or cause adverse environmental impacts during the carrying out of their proposal and seek specialist advice where required. Developers must also consider the potential of mine gases and gas prevention measures must be adopted during construction where there is such a risk. Developers must seek written permission from the Coal Authority before undertaking any intrusive activities that intersect, disturb or enter any coal seams, coal mine workings or coal mine shafts and adits. Failure to obtain Coal Authority permission for such activities is trespass, with the potential for court action. Property specific summary information on any past, current and proposed surface and underground coal mining activity and any other ground stability information must be obtained from the Coal Authority in order to make an assessment of the risk. This information can be obtained from the Coal Authority's Property Search Service on 0845 762 6848 or at [www.groundstability.com](http://www.groundstability.com).
4. The development hereby permitted is affected by public rights of way which must remain available to the public and must not be closed, diverted, disturbed or altered in any way until the appropriate statutory procedures have been completed. For more details, including likely costs and timescales, the Applicant is advised to contact immediately Asset Management, Highways and Engineering Delivery Services.
5. The development hereby permitted is affected by existing highways which must remain available to the public and must not be closed, diverted, disturbed or altered in any way until the appropriate statutory procedures have been completed. For more details including likely costs and timescales, the Applicant is advised to contact Asset Management, Highways and Engineering Delivery

Services.

6. Before the development is commenced a Legal Agreement must be entered into with the Highway Authority for the design and construction of highway improvement works. For more details, the applicant is advised to contact Highways Design, Highways and Engineering Development Services.
7. The development hereby permitted will require changes/additions to Traffic Regulation Orders on highways on or adjacent to the site. This will require the appropriate statutory procedures to be completed. For more details including likely costs and timescales, the Applicant is advised to contact Traffic Management, Highways and Engineering Development Services.





**SP** Proposed Site Plan  
1 : 500

Rev	Description	Date	Dr by	App by
original by	Edward Park	date created Jan. 2012	AH	

**Aedas**

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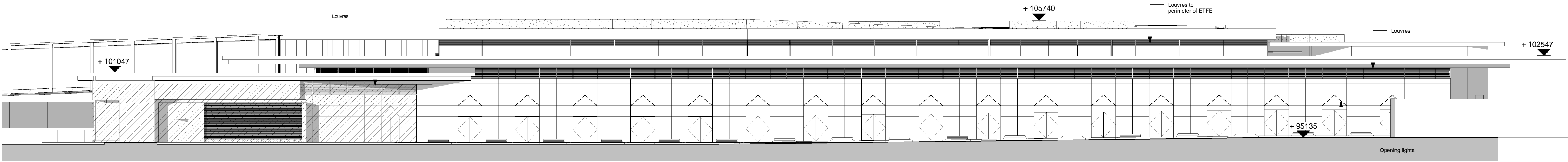
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client name TGM			
project Bolton Town Centre Transport Strategy			
drawing Proposed GA Site Plan			
computer file C:\flex Project\2008.01231 ADS S1-XX MG AR Site.damen.mccullagh.rvt		plot date	
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Notes

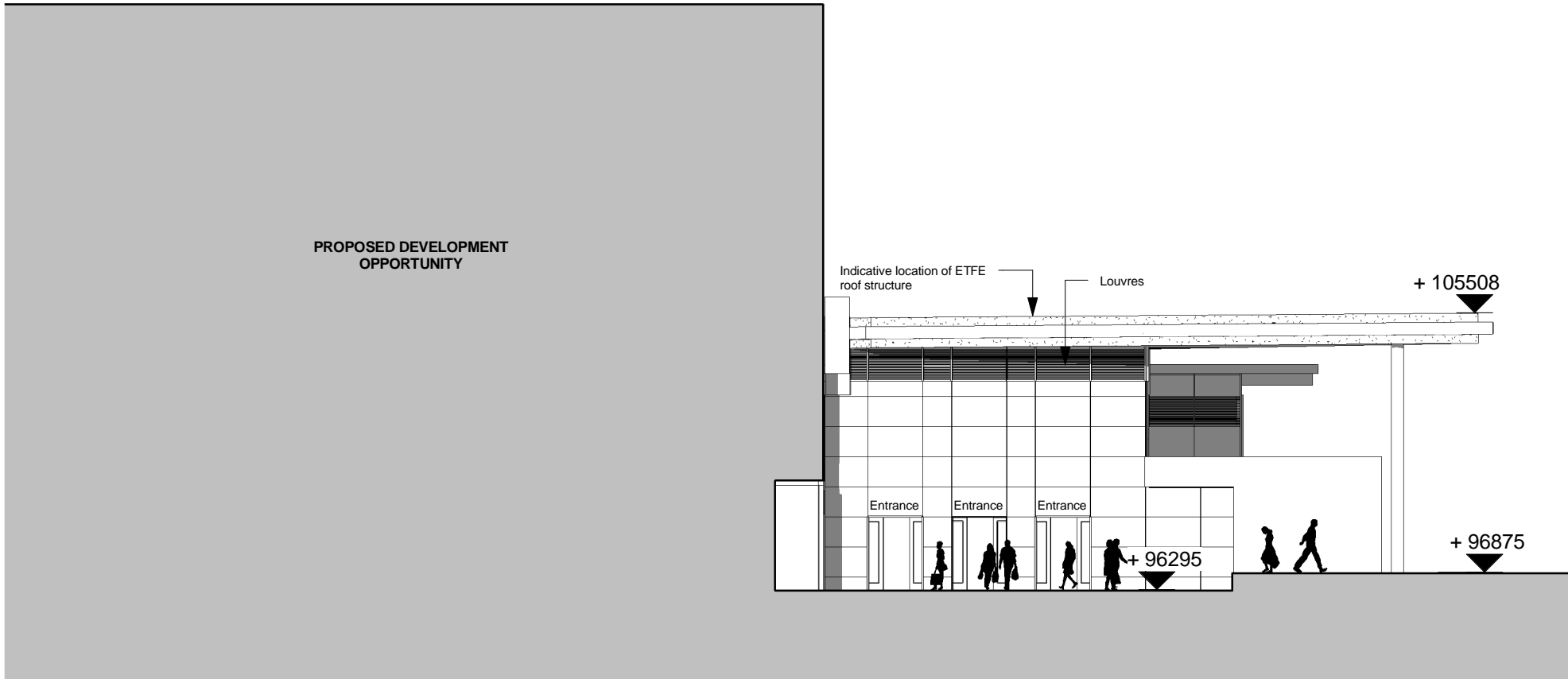
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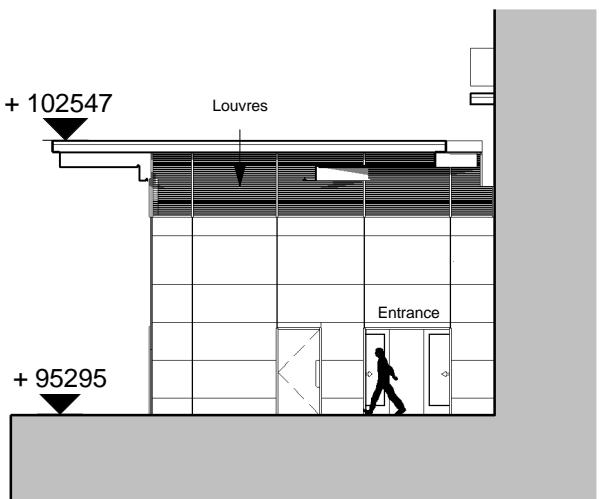
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**SW South West Elevation - Sawtooth Bays**  
1 : 200

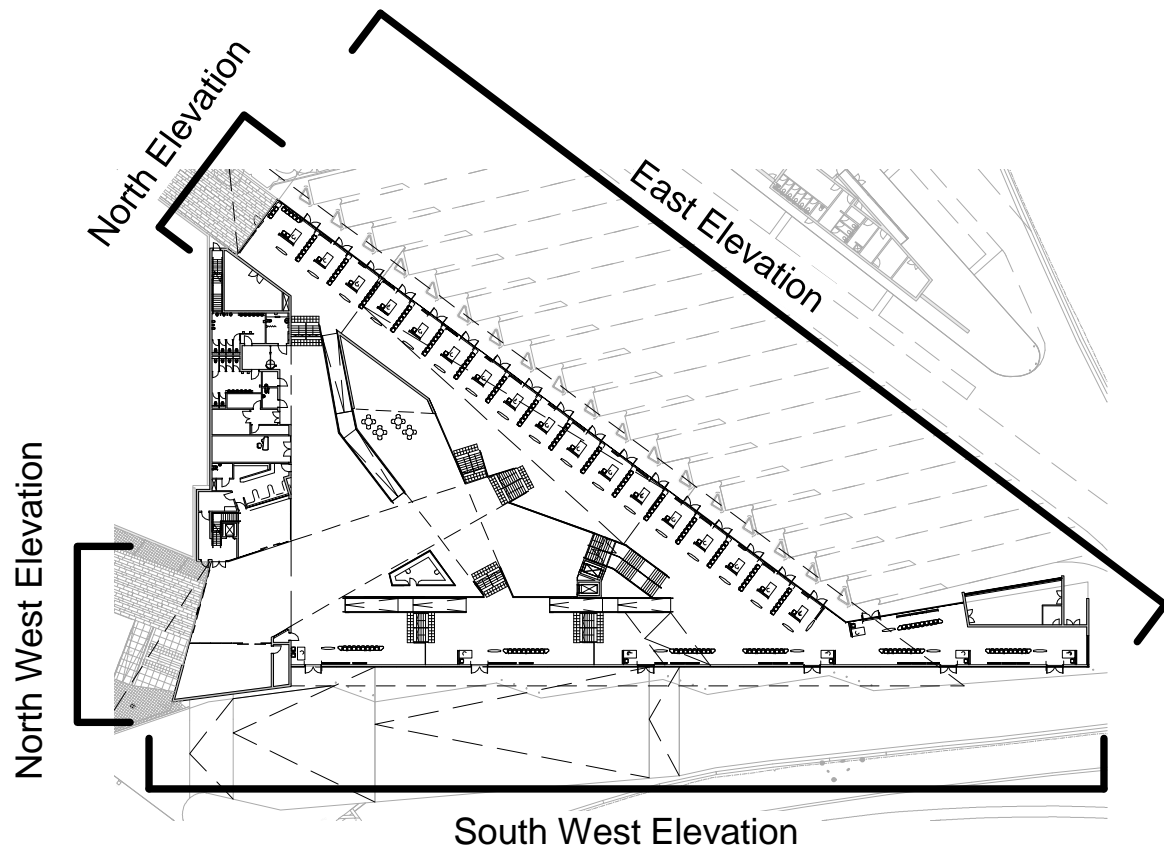


**NW North West Elevation - Primary Entrance**  
1 : 200



**NE North Elevation - Secondary Entrance**  
1 : 200

Materials Schedule		
Bus Station	Roof	Aluminium standing seam.
		ETFE pillows to roof light.
		Photovoltaic panels mounted on roof.
	Walls	Structural glazing, single glazed with PPC mullions.
		1000mm PPC louvers to facade perimeter at abutment with roof soffit for ventilation.
		Blue engineering brick plinth.
Crew Facilities and Operators Block	Roof	Bio-diverse roof.
		Single ply membrane.
		Structural glazing, single glazed with PPC mullions.
	Walls	Blue engineering brick.
		Architectural Recon Acid Etched, Natural Stone' cladding



**KP Key Plan**  
1 : 1000

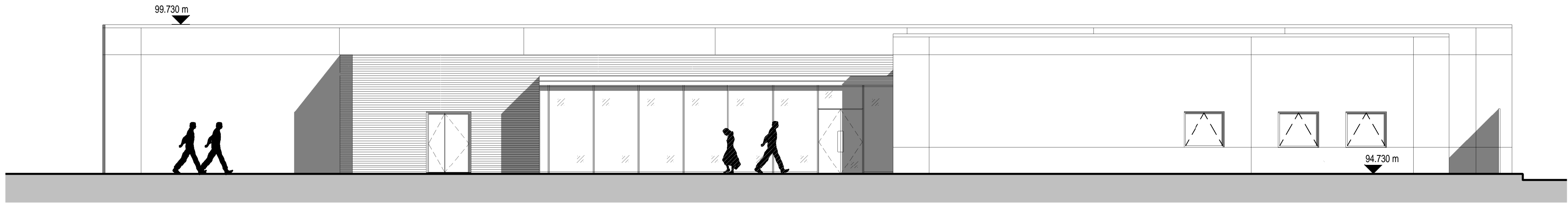
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Rev	Description	Date	Dr by	App by
original by	Edward Park	date created 01/12/12	A Hirst	

**Aedas**

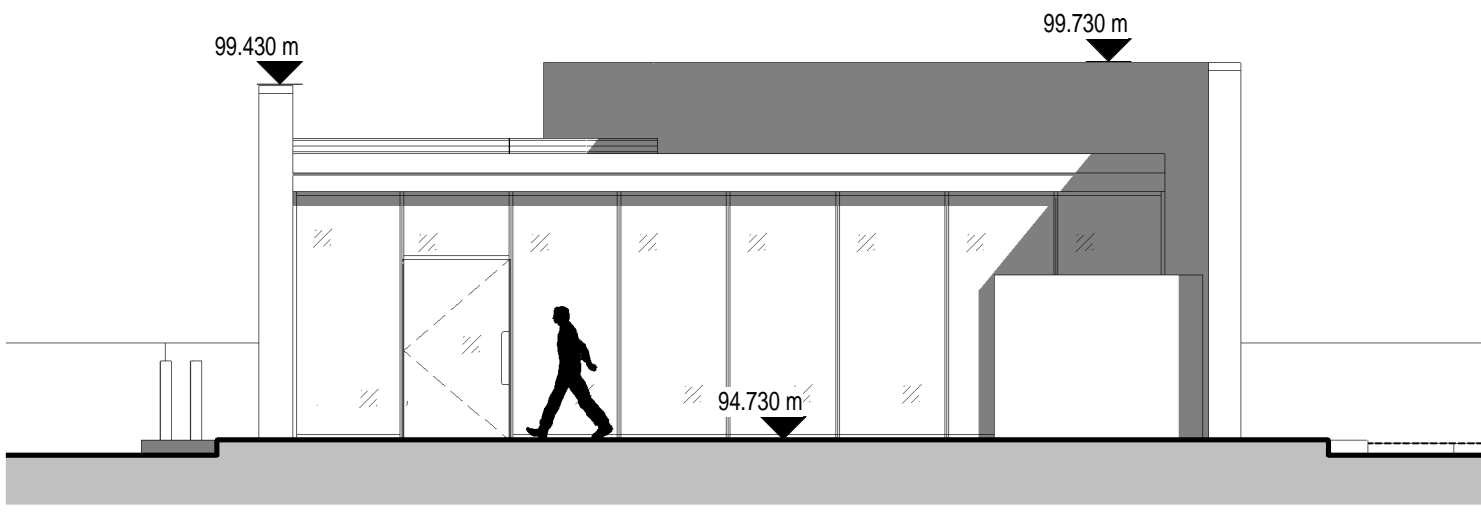
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client name TGM	
project Bolton Town Centre Transport Strategy	
drawing Bus Station - Proposed GA Elevations	
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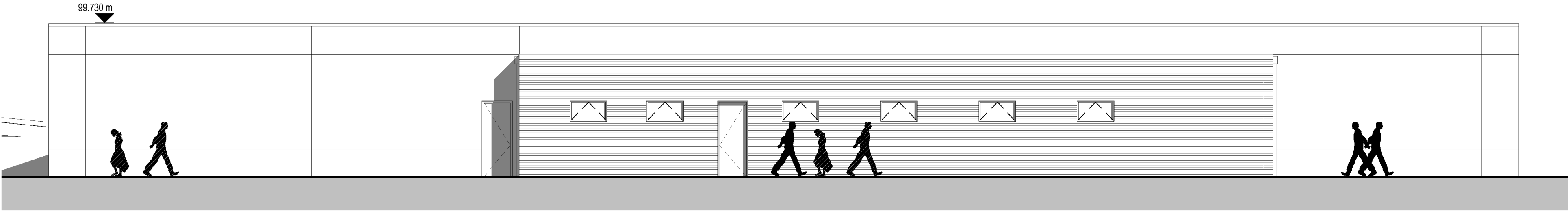
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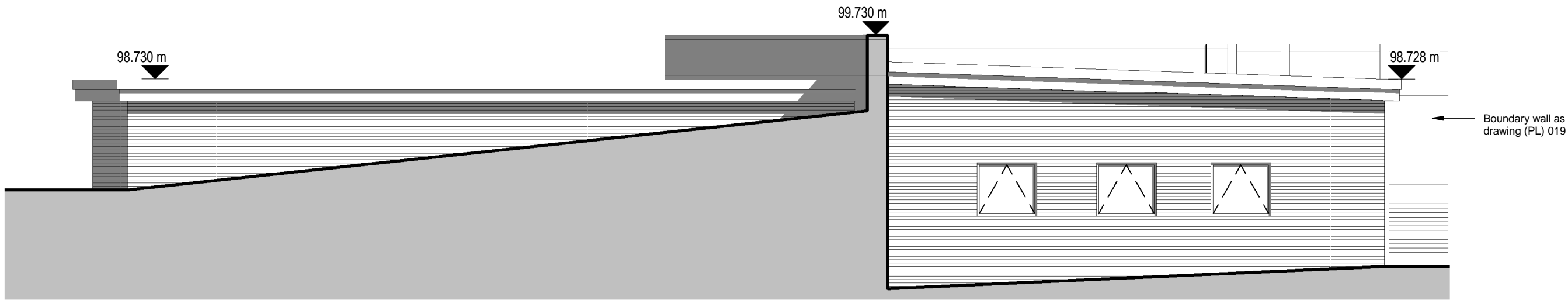
**CFN Crew Facilities Block - North Elevation**  
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**CFS Crew Facilities Block - South Elevation**  
1 : 100



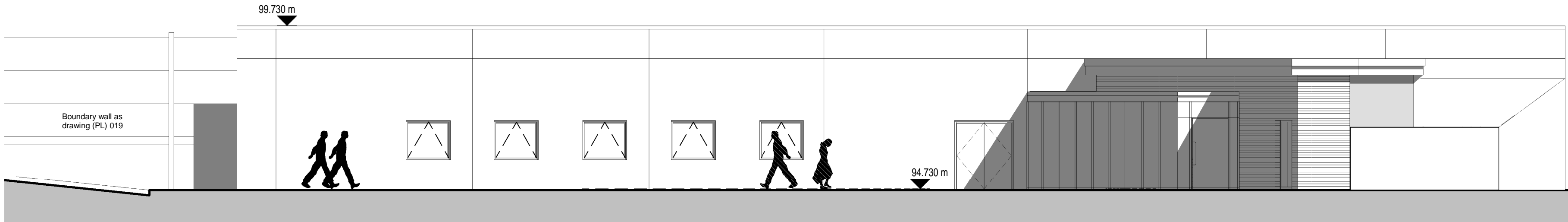
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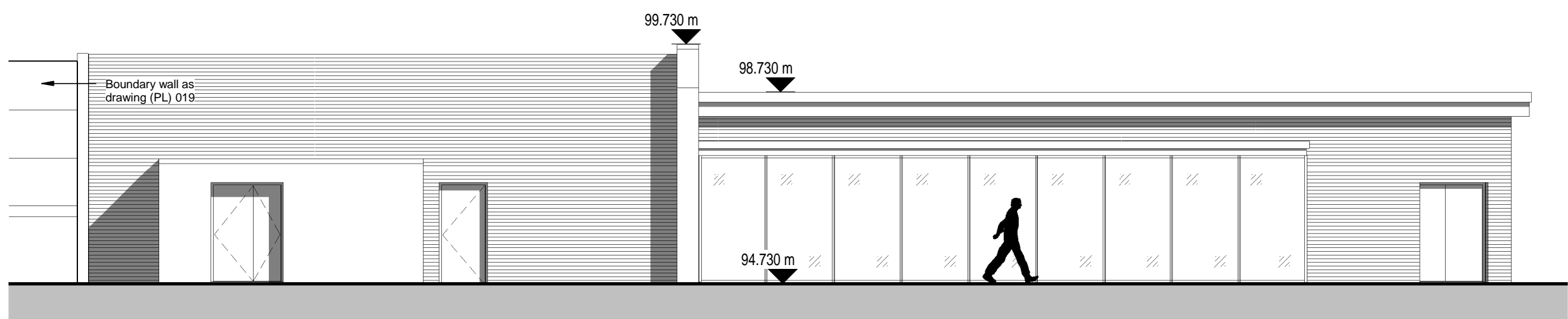
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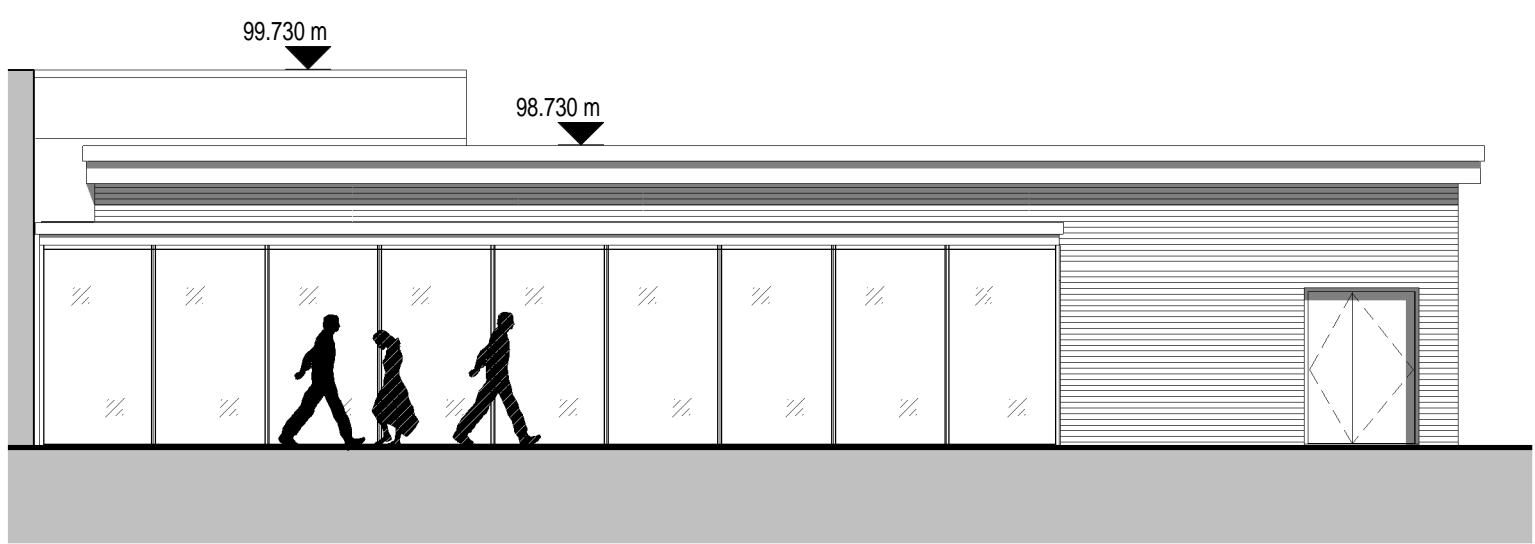
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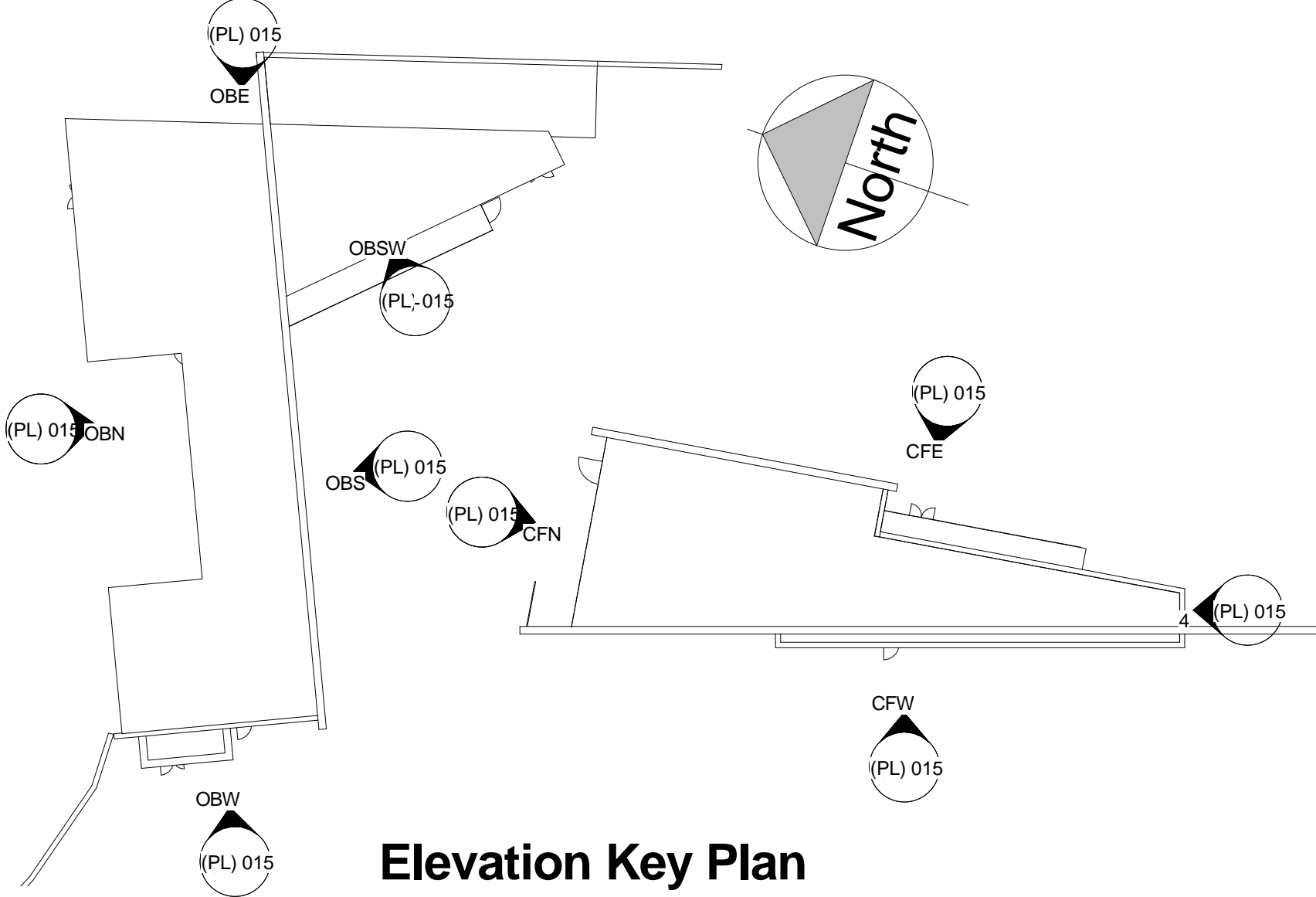
**OBS Operators Block -South Elevation**  
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**OBW Operators Block - West Elevation**  
1 : 100



**OBSW Operator's Block - South West Elevation**  
1 : 100



**Elevation Key Plan**  
1 : 350

Materials Schedule		
Bus Station	Roof	Aluminium standing seam. ETFE pillows to roof light. Photovoltaic panels mounted on roof.
	Walls	Structural glazing, single glazed with PPC mullions. 1000mm PPC louvers to facade perimeter at abutment with roof soffit for ventilation. Blue engineering brick plinth.
Crew Facilities and Operators Block	Roof	Bio-diverse roof. Single ply membrane.
	Walls	Structural glazing, single glazed with PPC mullions. Blue engineering brick. Architectural Recon Acid Etched, Natural Stone cladding

**Notes**  
All levels are indicative and will be finalised as working drawings are progressed.

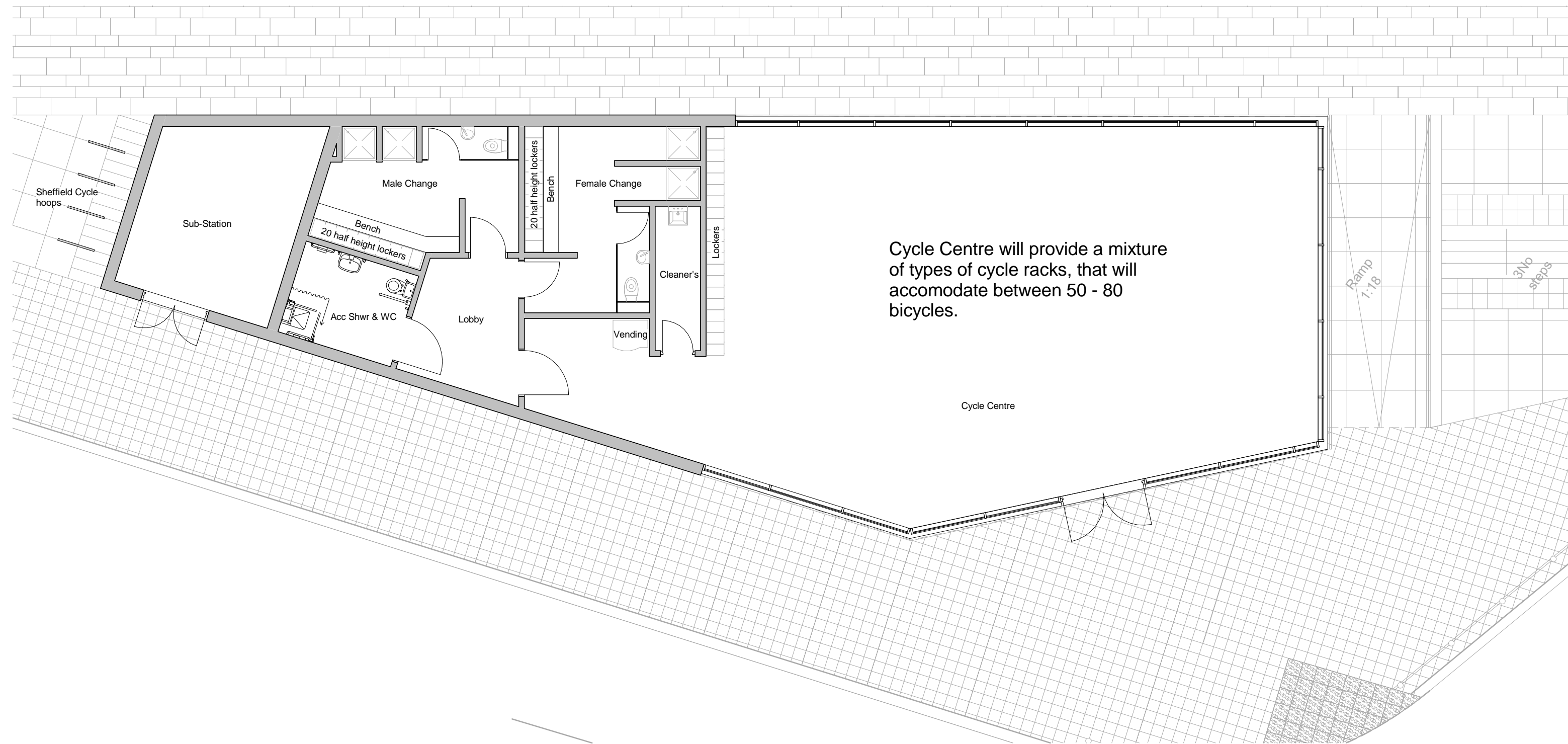
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Rev	Description	Date	Dr by	App by
original by	Edward Park	date created 01/13/12	approved by A Hirst	

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client name TGM	
project Bolton Town Centre Transport Strategy	
drawing Operators Block & Crew Facilities - Proposed GA Elevations	
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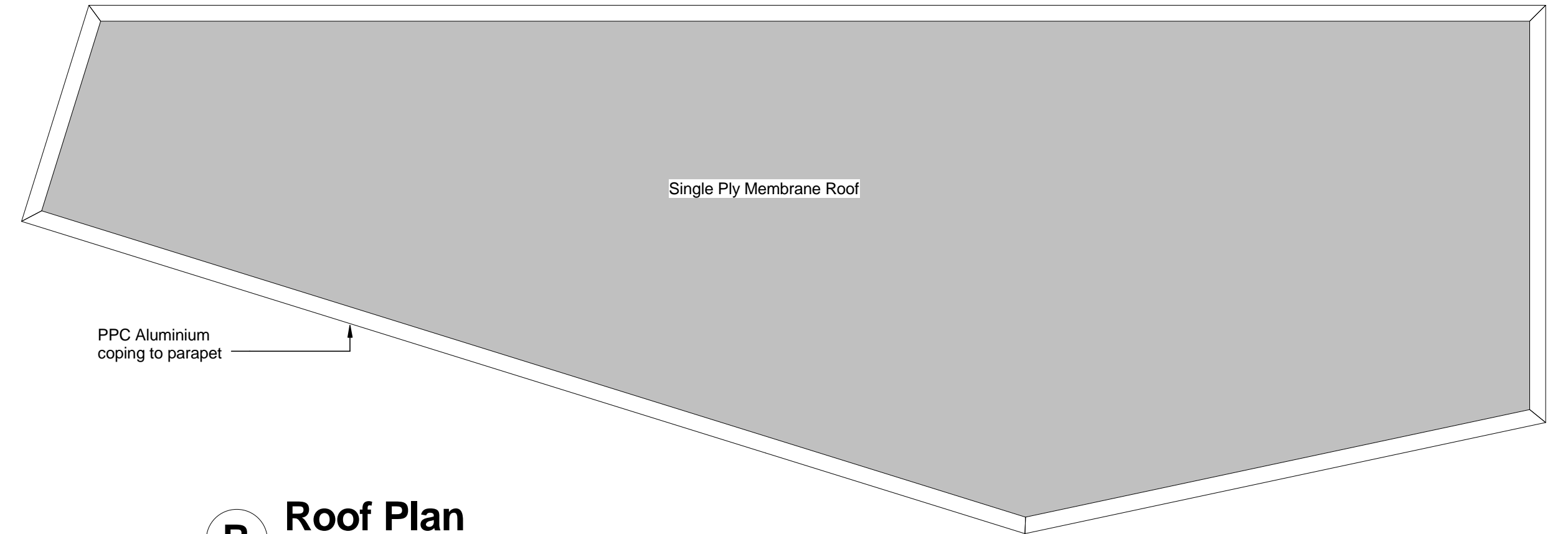
**GF** **Ground Floor**  
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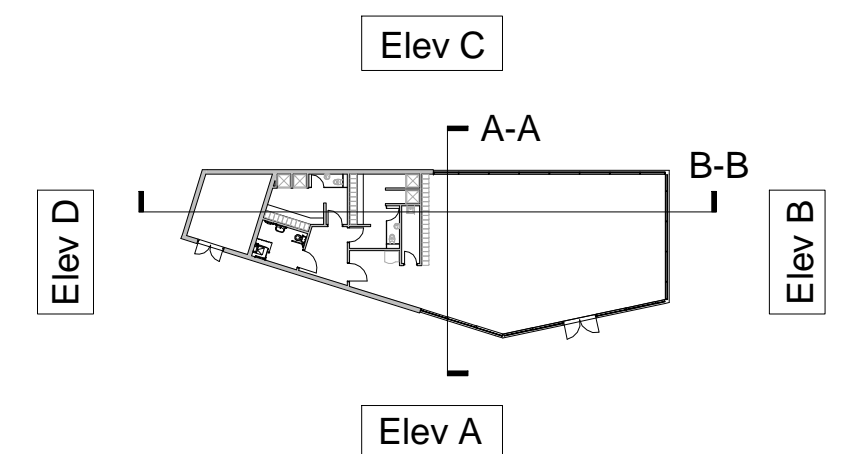
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**EC** **Elevation C**  
1 : 100



**R** **Roof Plan**  
1 : 100



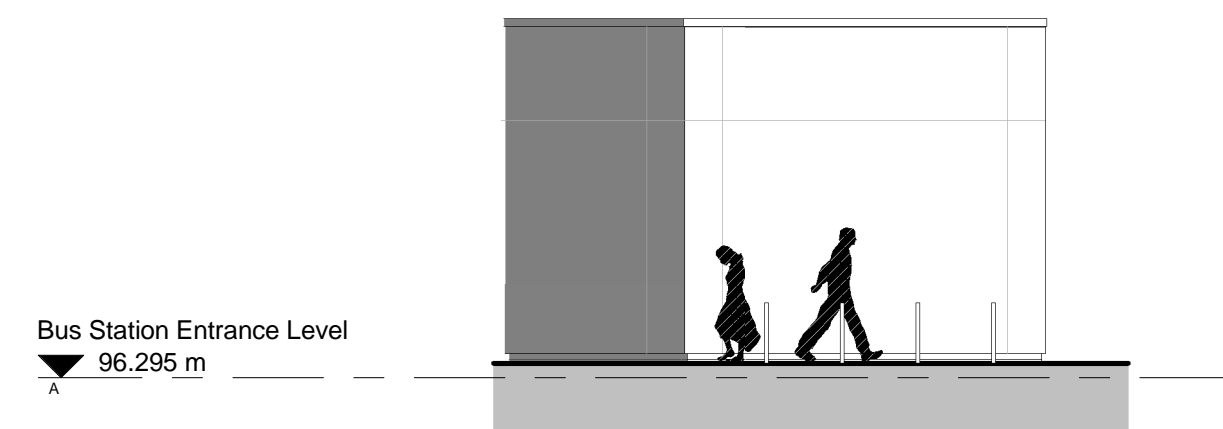
**KP** **Key Plan**  
1 : 500

#### Notes

All levels are indicative and will be finalised as working drawings are progressed.



**EB** **Elevation B**  
1 : 100



**ED** **Elevation D**  
1 : 100

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Rev	Description	Date	Dr by	App by
original by		date created		approved by
JM		19/01/12		AH

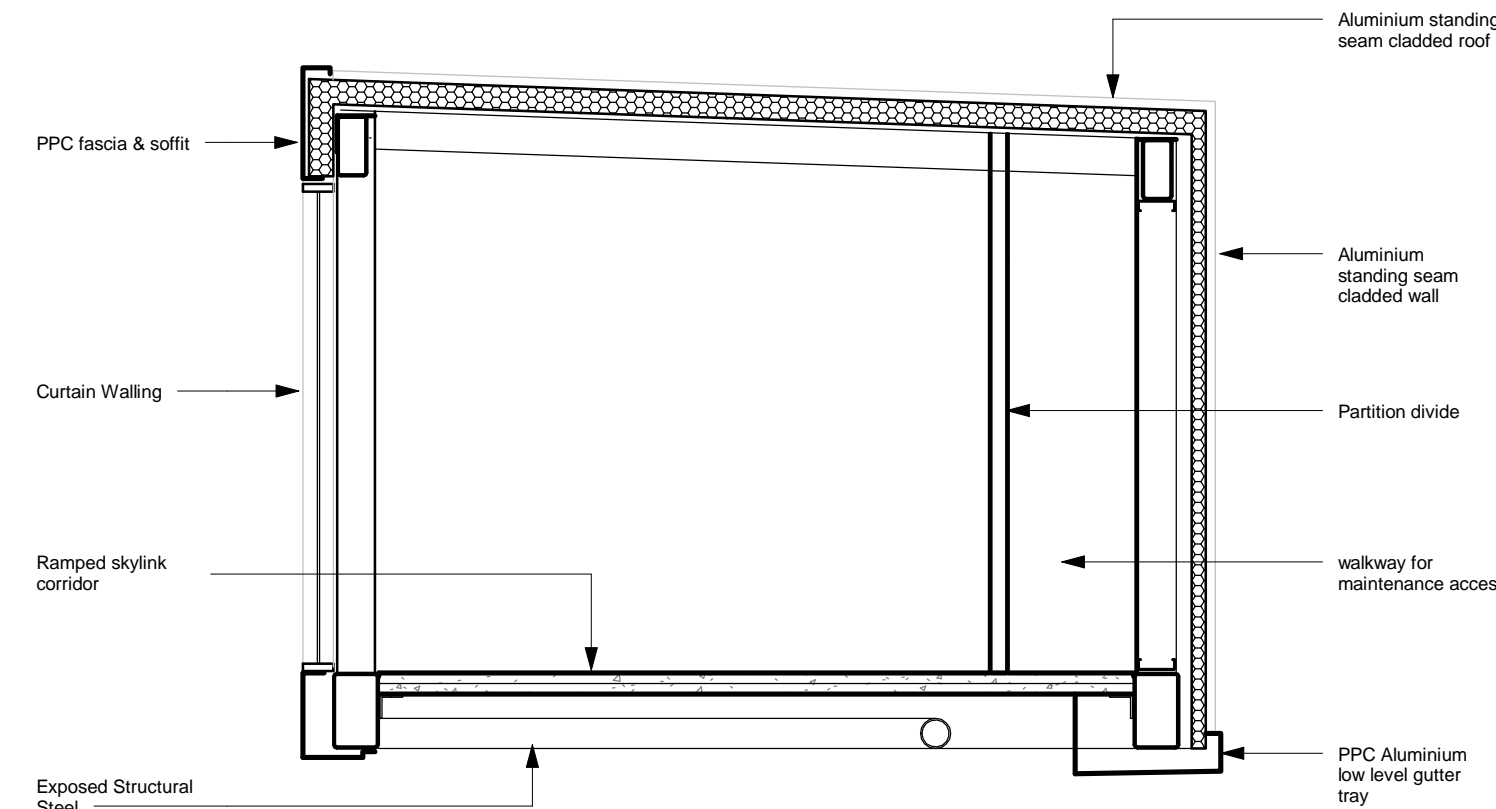
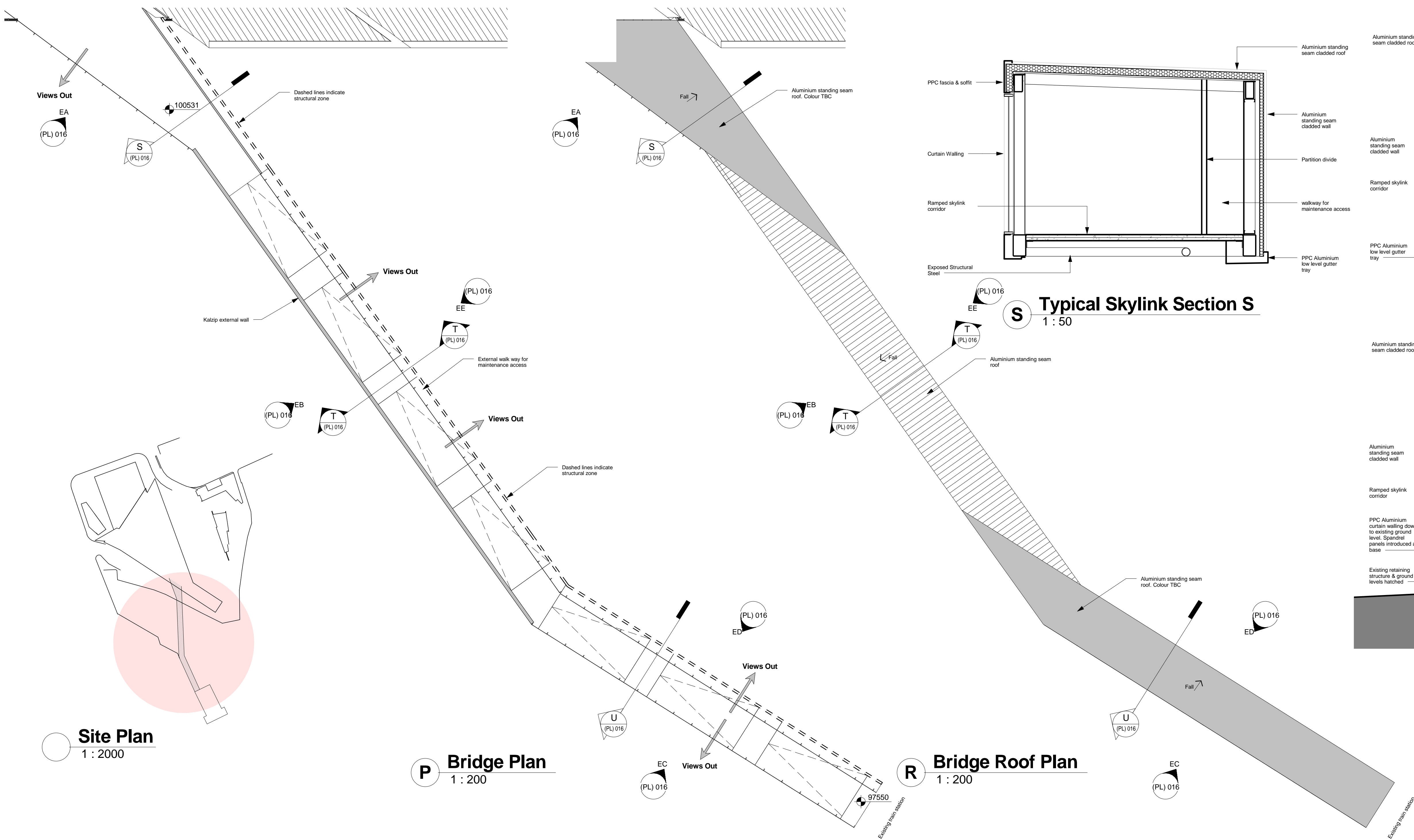
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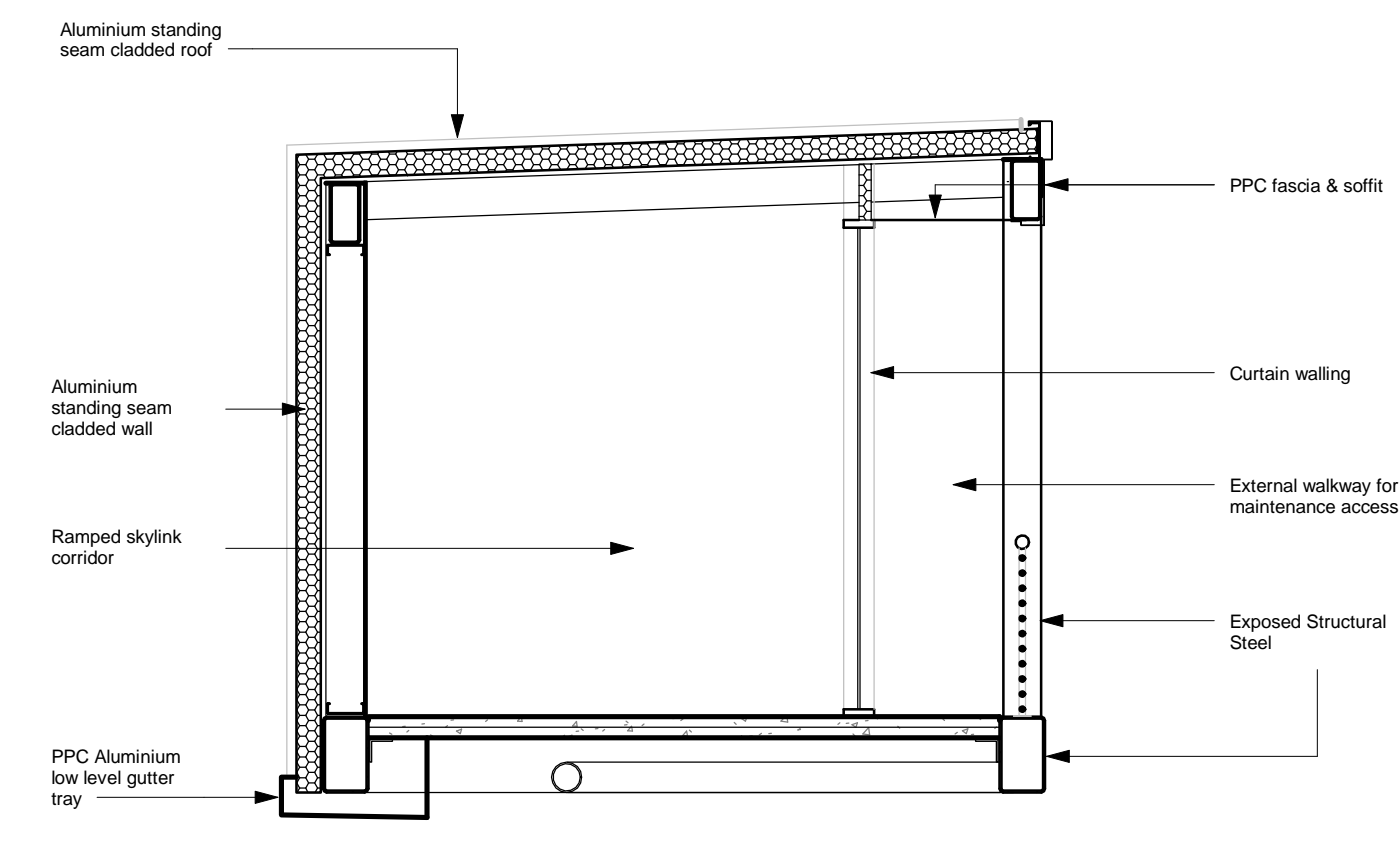
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project	Bolton Town Centre Transport Strategy
drawing	Cycle Centre - Proposed GA Plans & Elevations
computer file	C:\Revit
project number	2008.01231.000
drawing number	(PL)020
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rev	A
issue status	PLANNING
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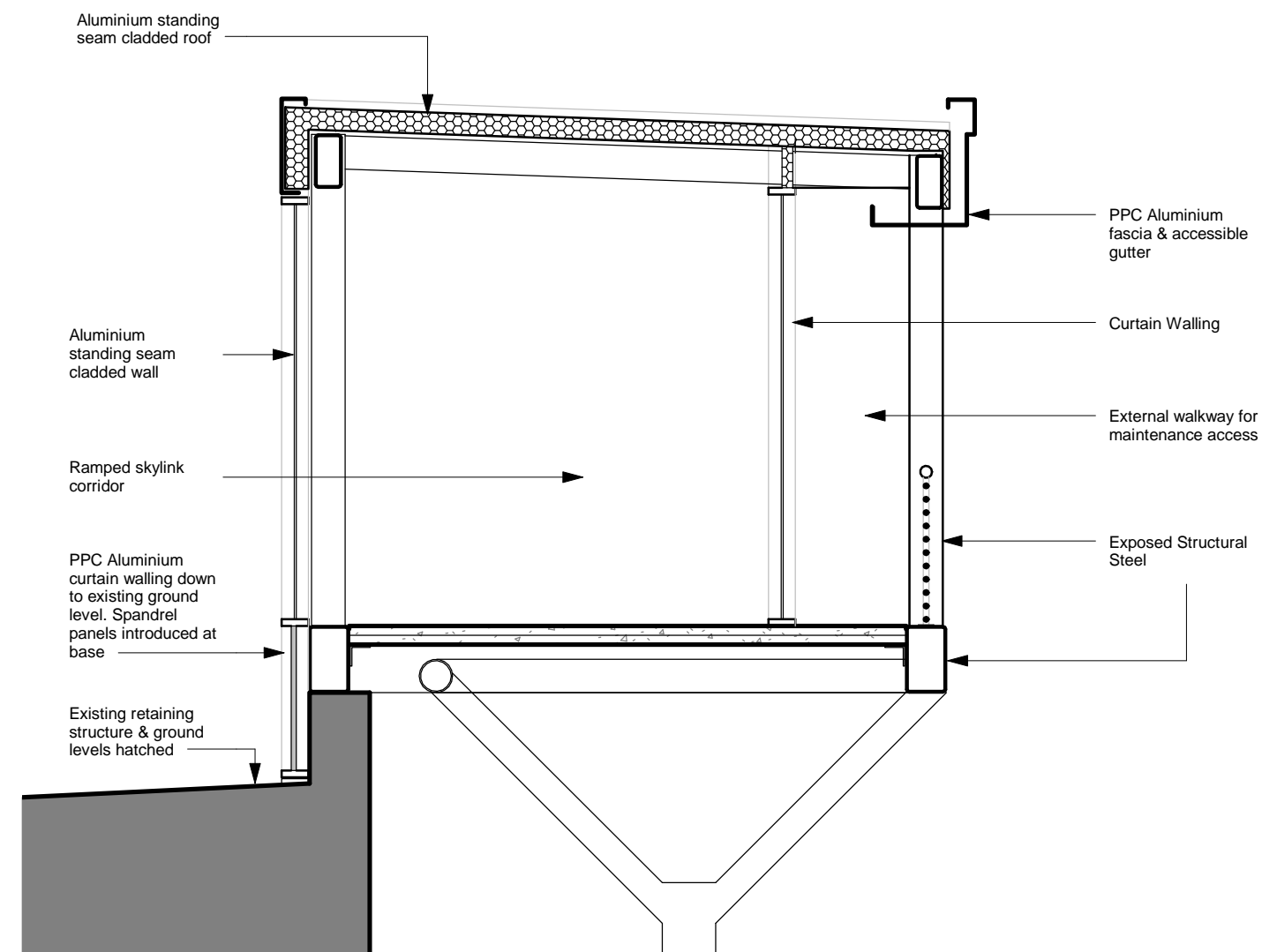




**S Typical Skylink Section S**  
1 : 50



**T Typical Skylink Section T**  
1 : 50

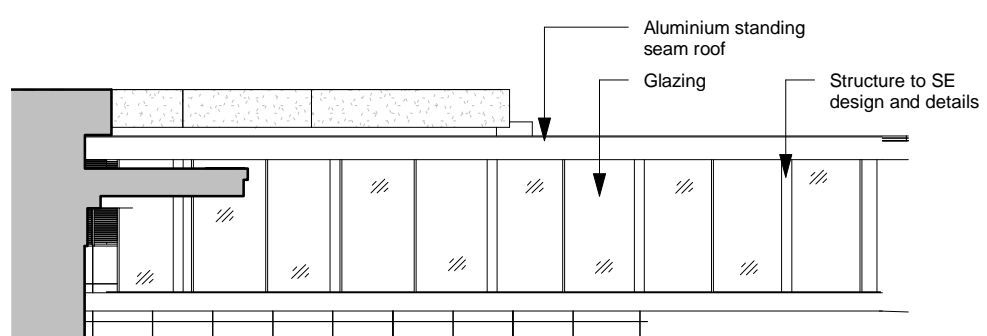


**U Typical Skylink Section U**  
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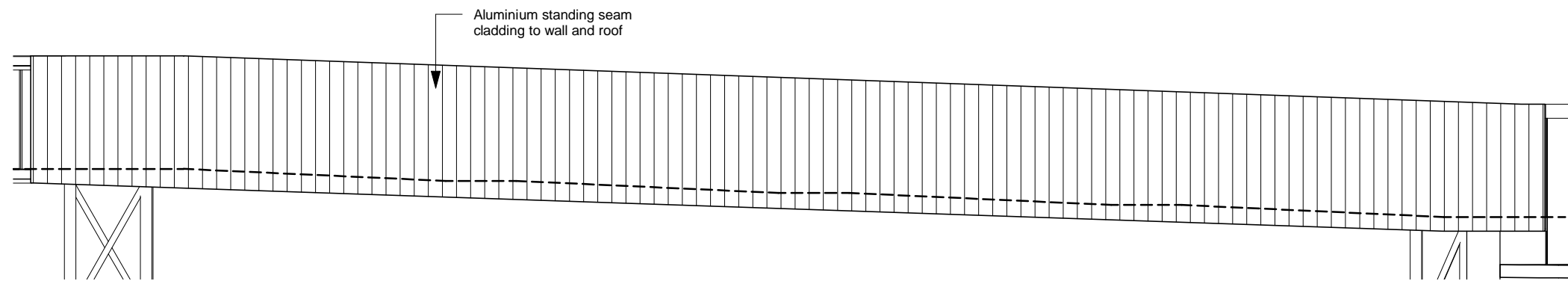
**Site Plan**  
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**P Bridge Plan**  
1 : 200

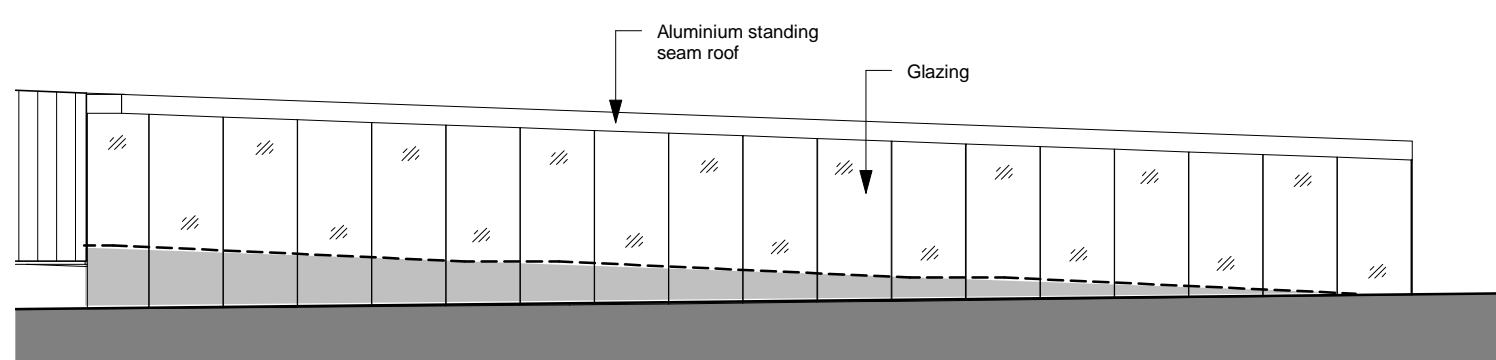
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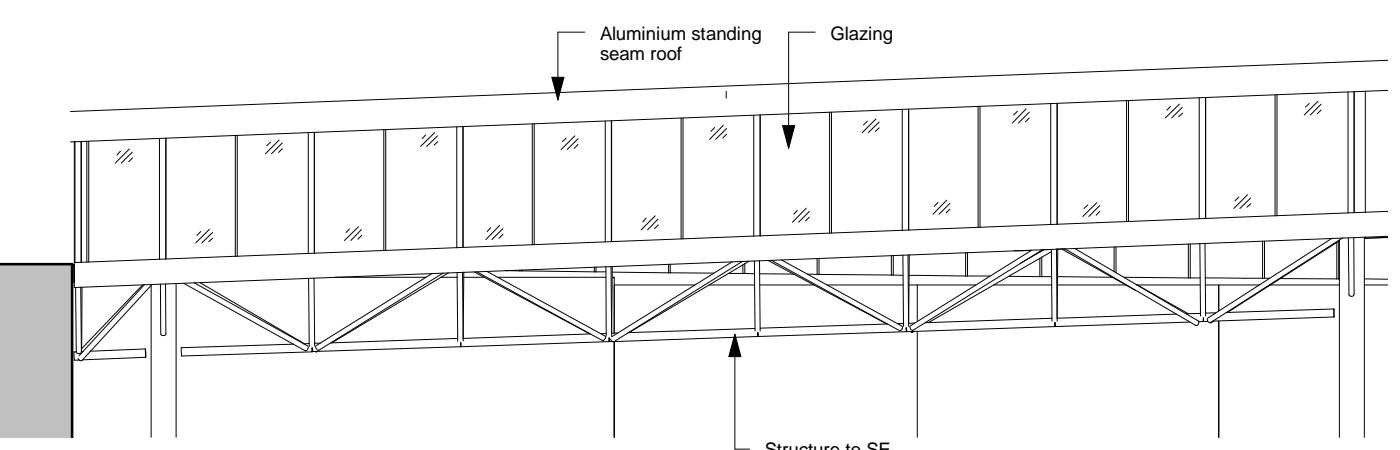
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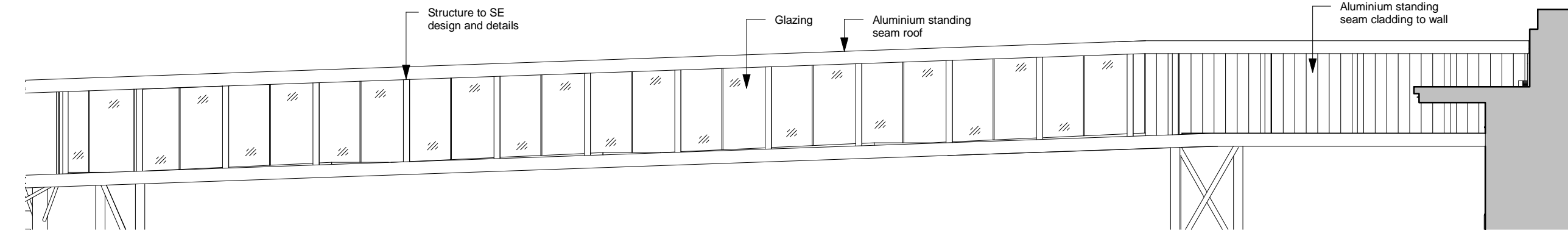
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**EC Elevation - C**  
1 : 200



**ED Elevation - D**  
1 : 200



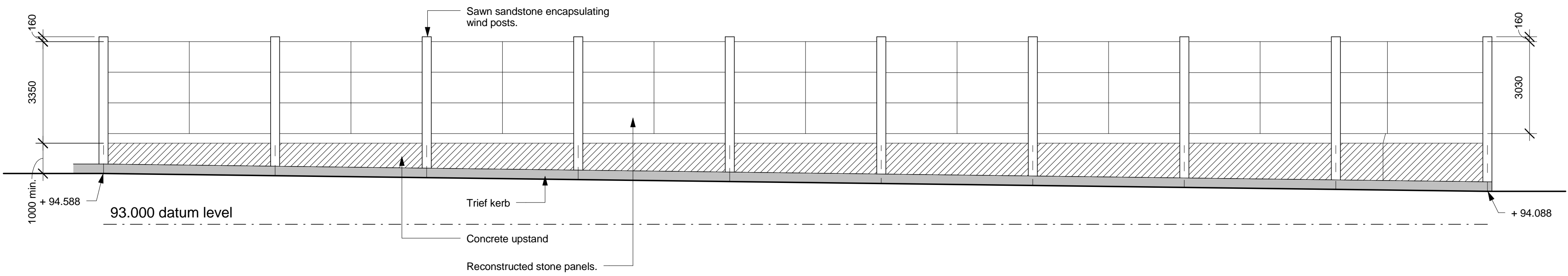
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Rev	Description	Date	Dr by	App by
original by	Edward Park	date created	01/13/12	A Hirst

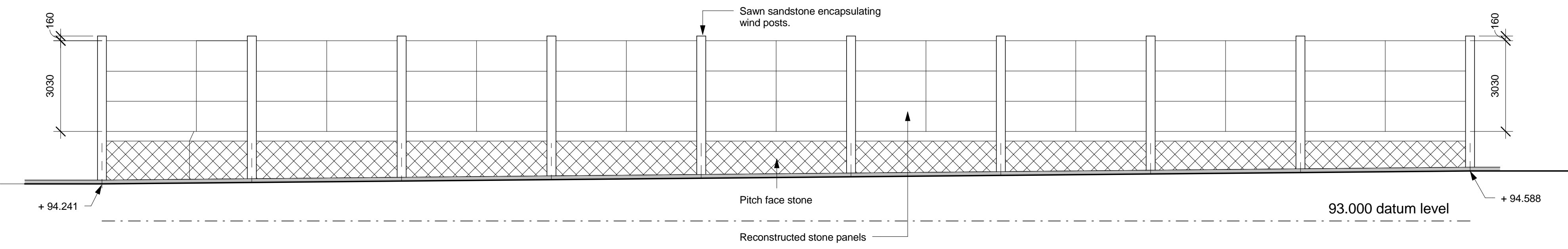
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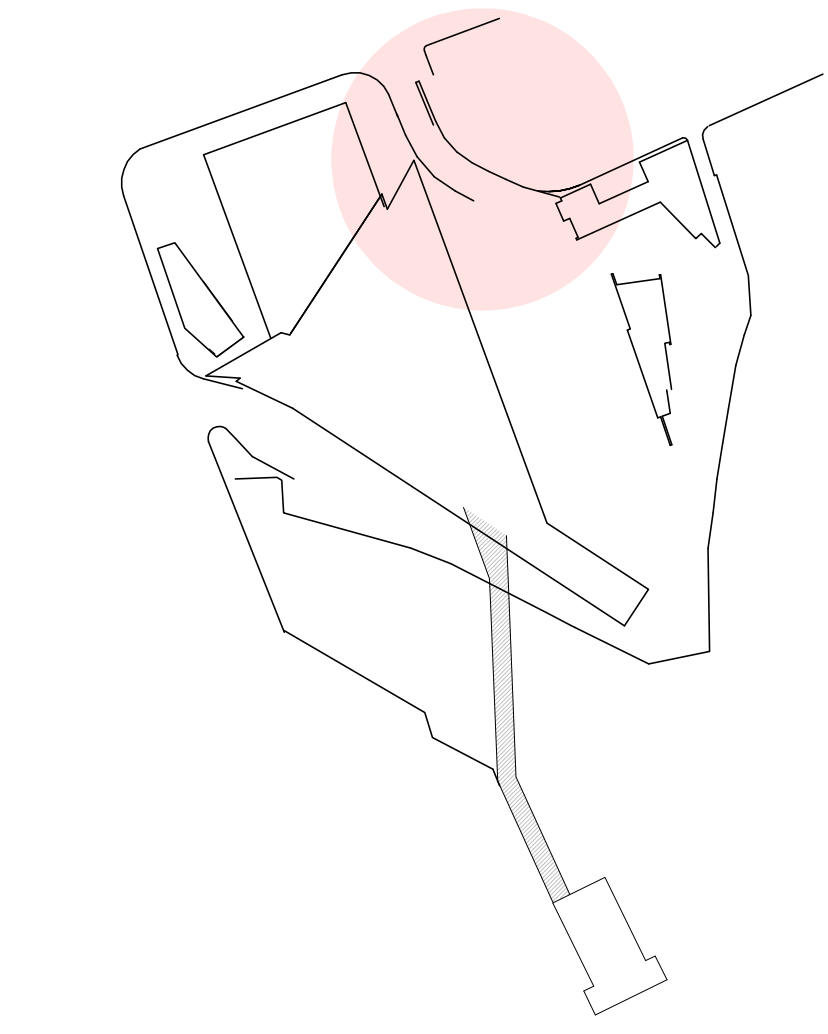
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project	Bolton Town Centre Transport Strategy		
drawing	Skylink - Proposed GA Plans, Sections & Elevations		
computer file	C:\Revit	plot date	
project number	2008.01231.000	scale	As indicated @A1
drawing number	(PL) 016	rev	issue status
			PLANNING
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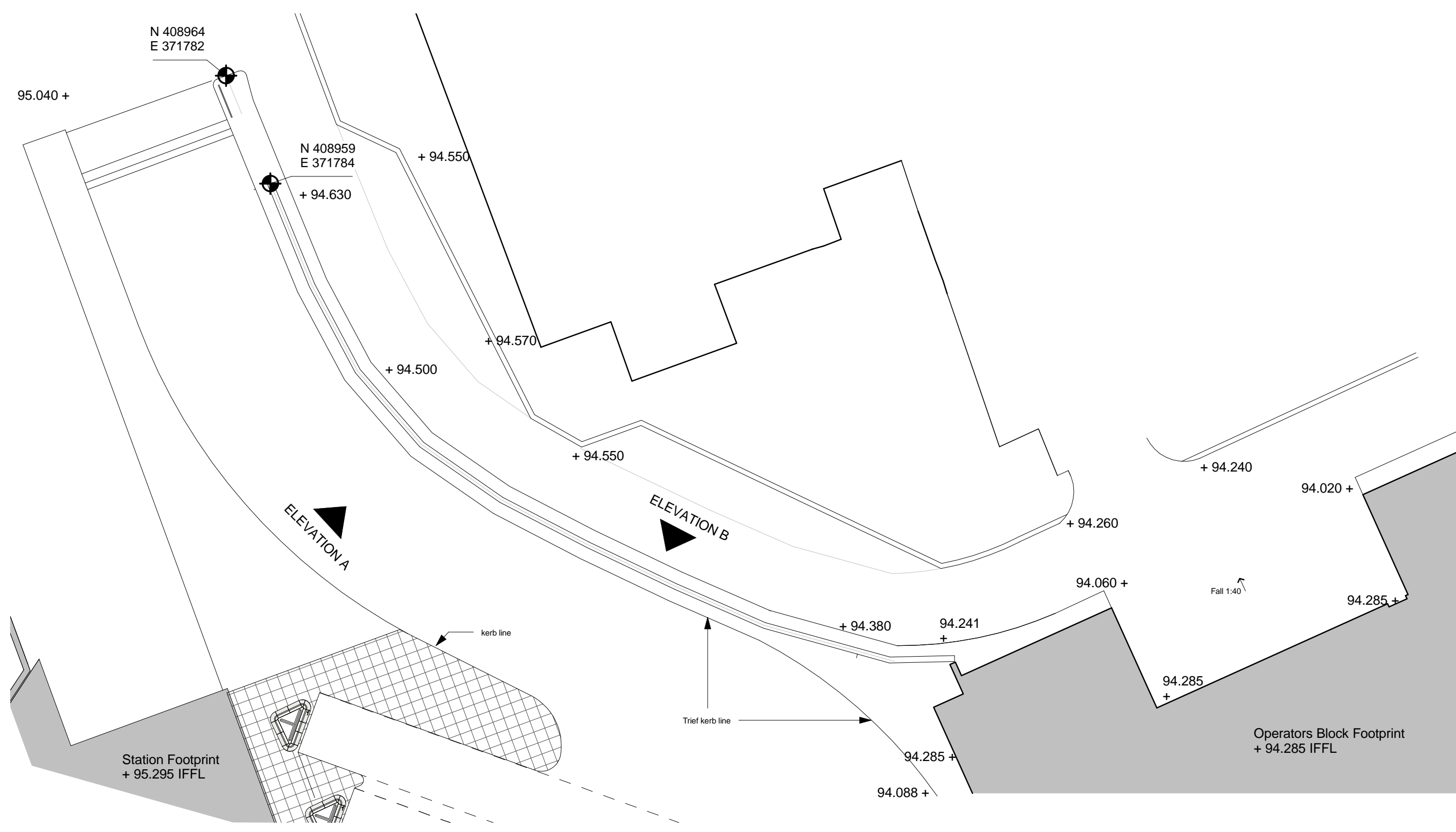
**3 Elevation - A**  
1 : 100



**2 Elevation - B**  
1 : 100



**Site Location Plan**  
1 : 2000



**1 Boundary Wall Plan Layout**  
1 : 200

Rev	Description	Date	Dr by/ App by
original by	DJM	date created	24/01/2012
			AH

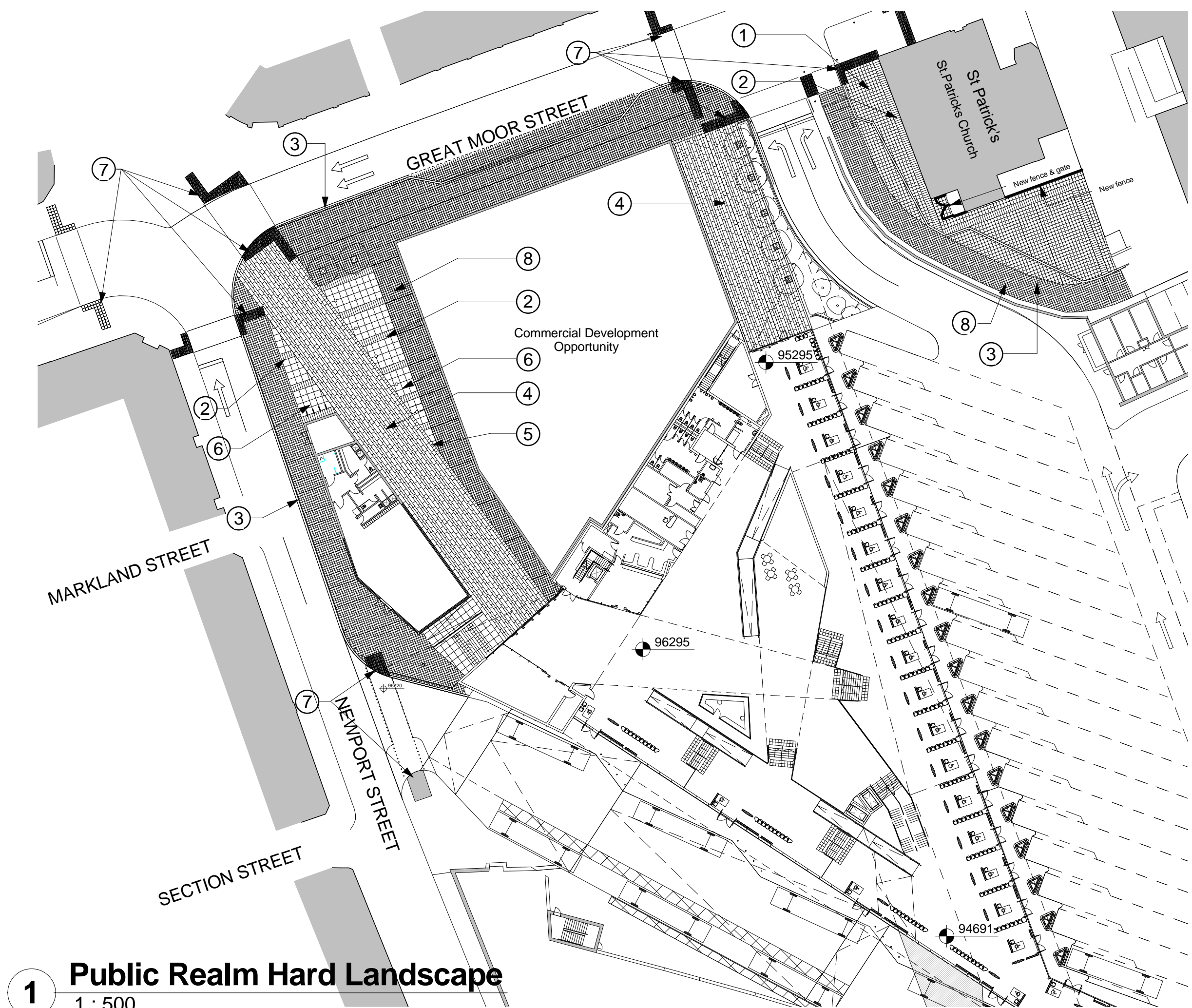
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client name		
TfGM		
project		
Bolton Town Centre Transport Strategy		
drawing		
Proposed Boundary Wall Details		
computer file	plot date	
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- Key:
- ① 450x450mm Marshals Scoutmoor Diamond Sawn Paving
  - ② 900x300mm Marshals Greenmoor Rustic Paving
  - ③ Marshals Granite Kerbs, Black GRA921, Fine Picked
  - ④ 300x600/900/1200/1500 and 450x600/900/1200/1500 Marshals Scoutmoor Diamond Sawn Paving
  - ⑤ 400x400mm Marshals Greenmoor Rustic Way Finding Strip
  - ⑥ 900x900mm Marshals Moselden Diamond Sawn Paving
  - ⑦ Marshals Granite Tactile Paving, Black GRA921, Fine Picked
  - ⑧ 300x300 Marshals Scoutmoor Diamond Sawn Paving

Rev	Description	Date	Dr by	App by
original by	date created			
Edward Park	01/12/12		Adam Hirst	

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client name		
TIGM		
project		
Bolton Town Centre Transport Strategy		
drawing		
Public Realm Hard Landscape Proposals		
computer file	plot date	
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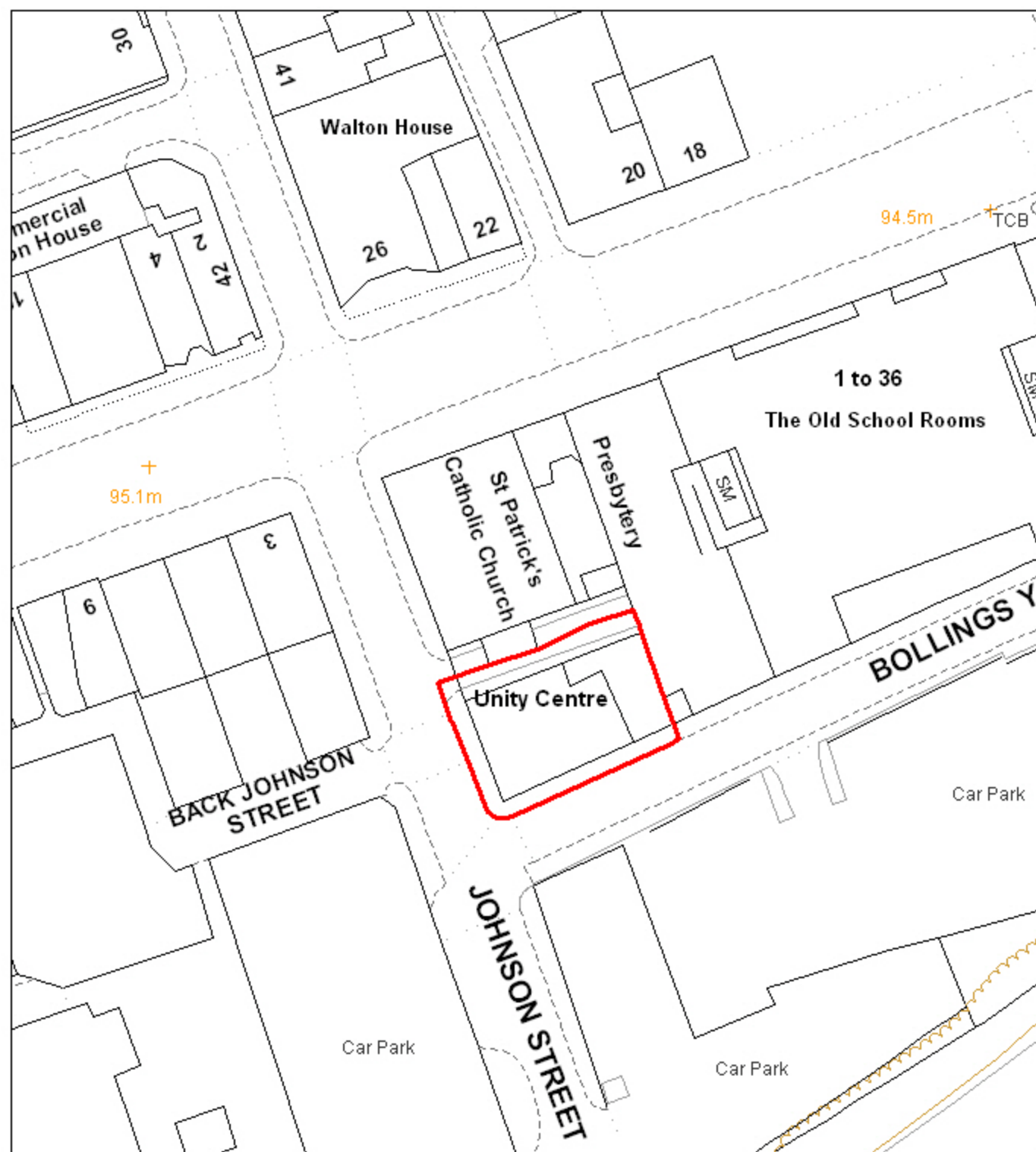
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# Public Realm Hard Landscape

1 : 500

Application No.

**87440/12**



**Development and Regeneration Department  
Planning Control Section**

Town Hall, Bolton, Lancashire, BL1 1RU.  
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**Date of Meeting: 15/03/2012**

**Application Reference: 87440/12**

**Type of Application: Conservation Area Consent**  
**Registration Date: 31/01/2012**  
**Decision Due By: 26/03/2012**  
**Responsible Officer: Jon Berry**

**Location: THE UNITY CENTRE, JOHNSON STREET, BOLTON, BL1 1NX**

**Proposal: CONSERVATION AREA CONSENT FOR DEMOLITION OF EXISTING BUILDING**

**Ward: Great Lever**

**Applicant: Transport for Greater Manchester**

**Agent : Aedas**

### **Officers Report**

**Recommendation: Approve subject to conditions**

### **Background**

Please see the report referenced 87438/12 for full details.

### **Site Characteristics**

The Unity Centre is a red brick, relatively modern building sited to the rear of St Patrick's Church at the periphery of the Mawdsley Street Conservation Area.

### **Policy**

PPS 5 Heritage

Core Strategy Policy CG3 The Built Environment and TC11 Design in the Town Centre

PCPN 19 Conservation Areas

### **Analysis**

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- \* impact on the character and appearance of the Mawdsley Street Conservation Area

Impact on the Character and Appearance of the Mawdsley Street Conservation Area

Core Strategy Policy CG3 states that the council will "conserve and enhance the heritage significance of heritage assets and heritage areas, recognising the importance of sites, areas and buildings of archaeological, historic, cultural and architectural interest and their settings" and Policy TC11 provides that "development should conserve and enhance the special nature of the conservation areas and listed buildings".

The demolition of the Unity Centre would facilitate the improvement of Bollings Yard/Johnson Street to serve St Patrick's Church. The Council is obliged to protect the special nature of this conservation area and two separate assessments have been carried out in the form of the Mawdsley Street Conservation Area Character Study and Building Bolton. The former identifies the Unity Centre as, not being a building of merit, but rather as a feature making a neutral impact. Building Bolton does not recognise this property as being of high value in terms of its contribution to local distinctiveness, nor as being a type characteristic of the area by way of its scale and materials choice. The Unity Centre is not considered to be a significant heritage asset.

**Conclusion**

The proposal does not conflict with Core Strategy Policies CG3 and TC11.

## **Representation and Consultation Annex**

### **Representations**

**Letters:-** none received

**Recommendation:**            **Approve subject to conditions**

### **Recommended Conditions and/or Reasons**

1. The works must be begun not later than the expiration of three years beginning with the date of this consent.

Reason

Required to be imposed pursuant to Section 18 of the Town and Country Planning (Listed Buildings and Conservation Areas) Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

1. The Council has granted planning permission, subject to the conditions listed above, because the proposed development is in accordance with all relevant policies of the local development framework, as is required by Section 38 of the Planning and Compulsory Purchase Act 2004. There are no material considerations, as specified in the Planning Officer Report, that outweigh this justification to support the grant of planning permission. A summary of the relevant Development Plan policies pursuant to Article 22 of the Town and Country Planning (General Development Procedure) (England) (Amendment) Order 2003 and how the proposed development relates to these policies is set out below.

Bolton Council's Local Development Framework; Core Strategy Development Plan Document Adopted 2nd March 2011; Shaping the Future of Bolton:

CG3 The Built Environment;  
TC11 Design in the town centre;

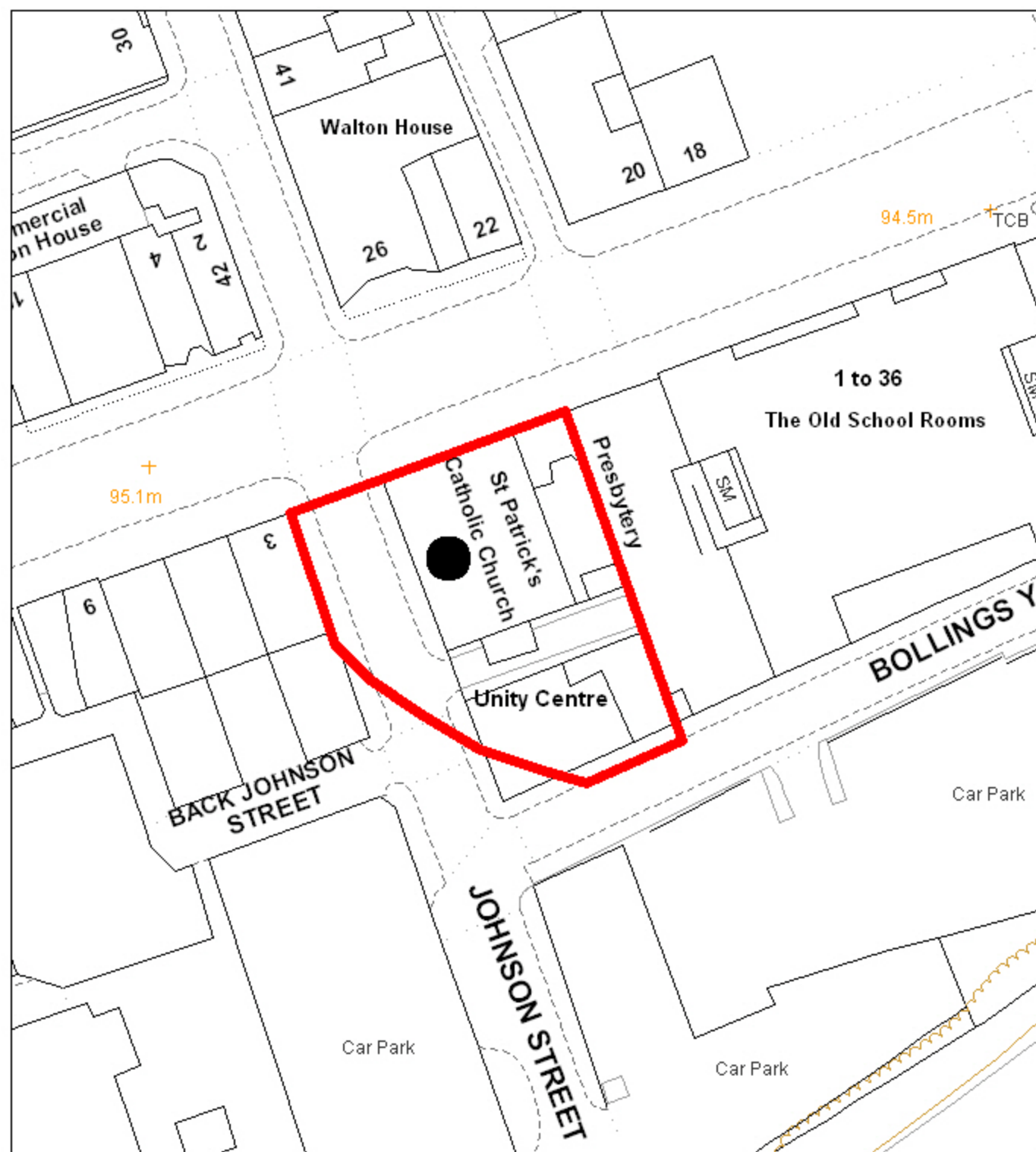
Planning Control Policy Notes:  
19 Conservation Areas.

2. Planning permission has been granted subject to the conditions listed above as being in compliance with Bolton's Unitary Development Plan, having had regard to the adopted and emerging policies and supplementary guidance detailed above.
3. The proposed development lies within a coal mining area, which could be subject to hazards resulting from past coal mining. Such hazards may currently exist, be caused as a result of the proposed development, or occur at some time in the future. Applicants must take account of these hazards, plus those beyond the application site, which could affect stability, health and safety or cause adverse environmental impacts during the carrying out of their proposal and seek specialist advice where required. Developers must also consider the potential of mine gases

and gas prevention measures must be adopted during construction where there is such a risk. Developers must seek written permission from the Coal Authority before undertaking any intrusive activities that intersect, disturb or enter any coal seams, coal mine workings or coal mine shafts and adits. Failure to obtain Coal Authority permission for such activities is trespass, with the potential for court action. Property specific summary information on any past, current and proposed surface and underground coal mining activity and any other ground stability information must be obtained from the Coal Authority in order to make an assessment of the risk. This information can be obtained from the Coal Authority's Property Search Service on 0845 762 6848 or at [www.groundstability.com](http://www.groundstability.com).

Application No.

**87448/12**



**Development and Regeneration Department  
Planning Control Section**

Town Hall, Bolton, Lancashire, BL1 1RU.  
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Fax (01204) 336399

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**Date of Meeting: 15/03/2012**

**Application Reference: 87448/12**

**Type of Application: Listed Building Consent**  
**Registration Date: 31/01/2012**  
**Decision Due By: 26/03/2012**  
**Responsible Officer: Jon Berry**

**Location: ST PATRICKS CHURCH, GREAT MOOR STREET, BOLTON, BL1 1NS**

**Proposal:** LISTED BUILDING CONSENT FOR CREATION OF A NEW AREA OF PUBLIC REALM ADJACENT ST. PATRICK'S CHURCH. INCLUDES NEW HARD LANDSCAPING, AND THE REPLACEMENT OF AN EXISTING STEEL BOUNDARY FENCE. NEW WALL TO PROVIDE ACOUSTIC ATTENUATION FROM ADJACENT PROPOSED BUS STATION SITE: THE WALL IS OUTSIDE THE CURTILAGE OF THE LISTED BUILDING BUT WILL AFFECT THE SETTING.

**Ward: Great Lever**

**Applicant: Transport for Greater Manchester (TfGM)**  
**Agent : Aedas Architects Ltd**

## **Officers Report**

**Recommendation: Approve subject to conditions**

### **Background**

Please see the report referenced 87438/12 for full details.

### **Proposal**

The barrier would take the form of a stone wall and this would stretch around the boundary with St Patrick's Church, varying in height between 5 metres at the easternmost point to 4.25 metres along the approach to Great Moor Street. A new curtilage of public realm, including access and servicing provision, would be created in an enhanced setting for the church

### **Site Characteristics**

The Grade II listed St. Patrick's Church and Presbytery dates from 1861 and it fronts onto Great Moor Street within the Mawdsley Street Conservation Area. Adjacent is the former Bolton County Grammar School, which is also a Grade II listed building constructed of brick and terracotta in 1897. This building has been converted into apartments known as the Old School Rooms.

### **Policy**

PPS 5 Heritage

### **Analysis**

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- \* impact on the neighbouring occupiers and the setting of the listed buildings

#### Impact on the Neighbouring Occupiers and the Setting of the Listed Buildings

Core Strategy Policy CG3 states that the council will "conserve and enhance the heritage significance of heritage assets and heritage areas, recognising the importance of sites, areas and buildings of archaeological, historic, cultural and architectural interest and their settings" and Policy TC11 provides that "development should conserve and enhance the special nature of the conservation areas and listed buildings".

Representatives of the church welcome the creation of the curtilage and the access and service arrangements. However, the height and nature of the proposed acoustic wall is a concern.

The Historic Churches Commission has suggested an alternative means of resolving noise issues for the church which would arise from the proposed interchange development operations. It has been put forward as follows that *"the acoustic barrier be deleted from the development scheme (there have been no representations from the interested parties at the Old School Rooms) and that instead the funding which would have been allocated to construct it be used to finance the design and installation of internal acoustic screens to the windows of the church.*

*The church already has polycarbonate screens fitted to the exterior of the windows. It is believed that these were installed before the ecclesiastical scheme came into existence as an anti-vandalism measure. Unfortunately they have been incorrectly fitted directly into the stonework of the windows. New screens should be designed and fixed appropriately to the exterior of the windows.*

*It is felt that if this was agreed to, these insulation measures should significantly reduce the increased noise levels that will occur when the new bus station is in use and the visual amenity would be protected if not enhanced when the buildings opposite the (liturgical) South elevation of the church are demolished.*

*The acoustic barrier should be replaced by railings for road safety reasons".*

The Applicant has agreed to investigate the potential for this with representatives of the church, though any additional noise impact for residents of the Old School Rooms, resulting from the deletion of part of the acoustic wall, would also need to be considered. Officers cannot recommend that any planning condition requires works to third party land and property, as this would be ultra vires, but instead a condition can be included by Members to require a report into the practicality of this alternative option. If it is viable, the condition could be discharged upon completion of the agreed works. If it is not, then the entire acoustic barrier would need to be implemented in accordance with the plans, as Officers consider that this presents the optimum balance between noise control and visual amenity within the application site. The Applicant would have to be responsible for maintaining and managing the condition of the wall.

### **Conclusion**

The creation of public realm for the church would enhance the setting of the listed building and the Applicant will accordingly work with representatives to further investigate noise mitigation options. Even if the acoustic barrier proves to be the only practical measure, the scheme is on balance considered to be acceptable.



## **Representation and Consultation Annex**

### **Representations**

**Letters:-** 2 letters have been received from the Salford Diocese and the Historic Churches Committee. The key details are included in the analysis section above.

### **Consultations**

Advice was sought from the following consultees: Historic Churches Committee; English Heritage.

**Recommendation:            Approve subject to conditions**

### **Recommended Conditions and/or Reasons**

1. The works must be begun not later than the expiration of three years beginning with the date of this consent.

Reason

Required to be imposed pursuant to Section 18 of the Town and Country Planning (Listed Buildings and Conservation Areas) Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. No development shall be commenced unless and until an options appraisal report into the potential alternatives of providing the acoustic barrier at the boundary with St Patrick's Church or designing and installing internal acoustic screens to the windows has been submitted to and approved in writing by the Local Planning Authority. The scheme shall then be implemented entirely in accordance with the approved details before the development is first brought into use.

Reason

In the interests of the amenity of the occupiers of the church.

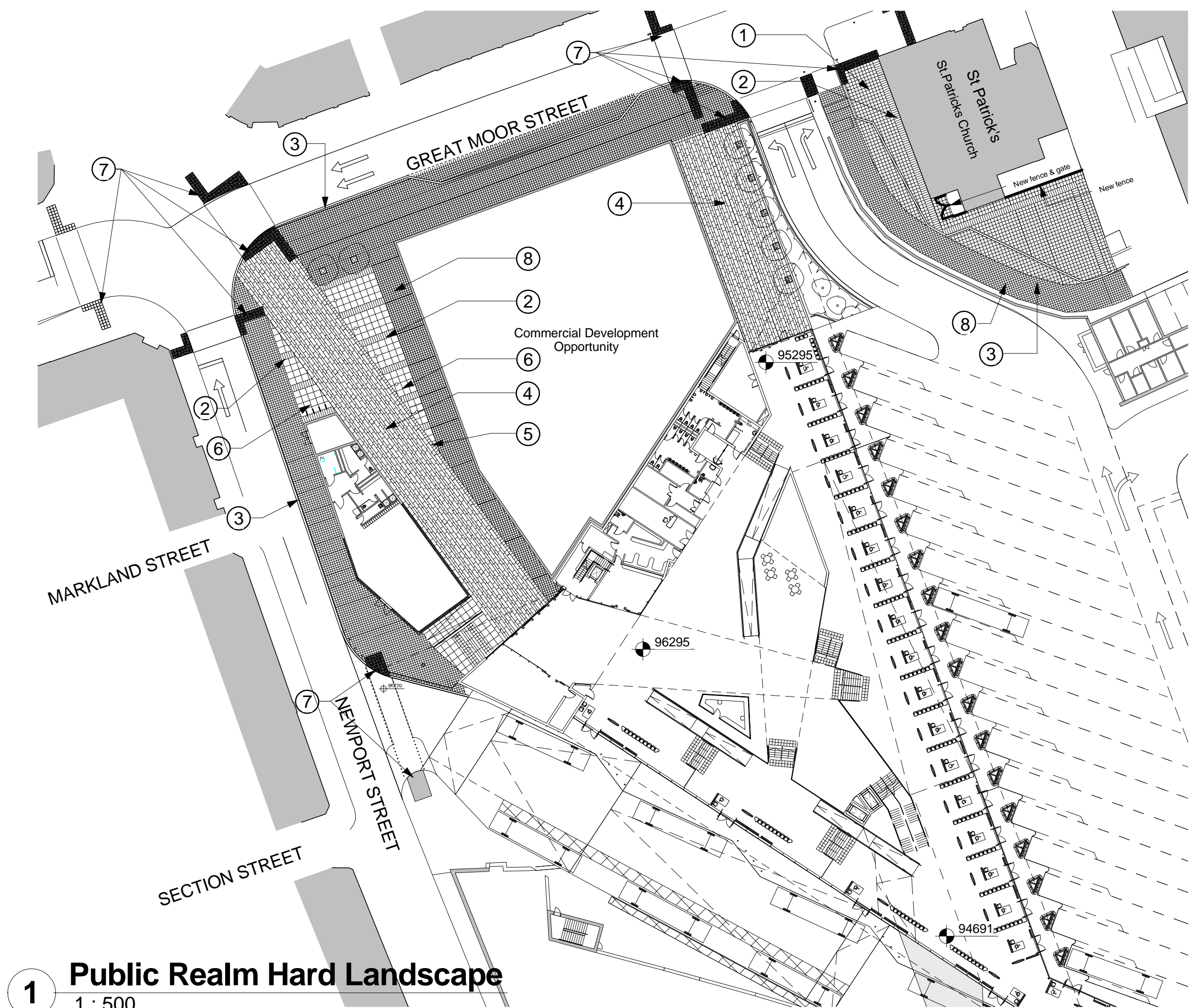
1. The Council has granted planning permission, subject to the conditions listed above, because the proposed development is in accordance with all relevant policies of the local development framework, as is required by Section 38 of the Planning and Compulsory Purchase Act 2004. There are no material considerations, as specified in the Planning Officer Report, that outweigh this justification to support the grant of planning permission. A summary of the relevant Development Plan policies pursuant to Article 22 of the Town and Country Planning (General Development Procedure) (England) (Amendment) Order 2003 and how the proposed development relates to these policies is set out below.

Bolton Council's Local Development Framework; Core Strategy Development Plan Document Adopted 2nd March 2011; Shaping the Future of Bolton:

CG3 The Built Environment;  
TC11 Design in the town centre;

Planning Control Policy Notes:  
19 Conservation Areas.

2. Planning permission has been granted subject to the conditions listed above because the balance of the benefits accrued by the development outweigh all other material considerations, having had regard to the adopted Unitary Development Plan policies and Supplementary Planning Guidance detailed above.
3. The proposed development lies within a coal mining area, which could be subject to hazards resulting from past coal mining. Such hazards may currently exist, be caused as a result of the proposed development, or occur at some time in the future. Applicants must take account of these hazards, plus those beyond the application site, which could affect stability, health and safety or cause adverse environmental impacts during the carrying out of their proposal and seek specialist advice where required. Developers must also consider the potential of mine gases and gas prevention measures must be adopted during construction where there is such a risk. Developers must seek written permission from the Coal Authority before undertaking any intrusive activities that intersect, disturb or enter any coal seams, coal mine workings or coal mine shafts and adits. Failure to obtain Coal Authority permission for such activities is trespass, with the potential for court action. Property specific summary information on any past, current and proposed surface and underground coal mining activity and any other ground stability information must be obtained from the Coal Authority in order to make an assessment of the risk. This information can be obtained from the Coal Authority's Property Search Service on 0845 762 6848 or at [www.groundstability.com](http://www.groundstability.com).



- Key:
- 1 450x450mm Marshals Scoutmoor Diamond Sawn Paving
  - 2 900x300mm Marshals Greenmoor Rustic Paving
  - 3 Marshals Granite Kerbs, Black GRA921, Fine Picked
  - 4 300x600/900/1200/1500 and 450x600/900/1200/1500 Marshals Scoutmoor Diamond Sawn Paving
  - 5 400x400mm Marshals Greenmoor Rustic Way Finding Strip
  - 6 900x900mm Marshals Moselden Diamond Sawn Paving
  - 7 Marshals Granite Tactile Paving, Black GRA921, Fine Picked
  - 8 300x300 Marshals Scoutmoor Diamond Sawn Paving

Rev	Description	Date	Dr by	App by
original by	date created			
Edward Park	01/12/12		Adam Hirst	

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client name		
TIGM		
project		
Bolton Town Centre Transport Strategy		
drawing		
Public Realm Hard Landscape Proposals		
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# 1 Public Realm Hard Landscape

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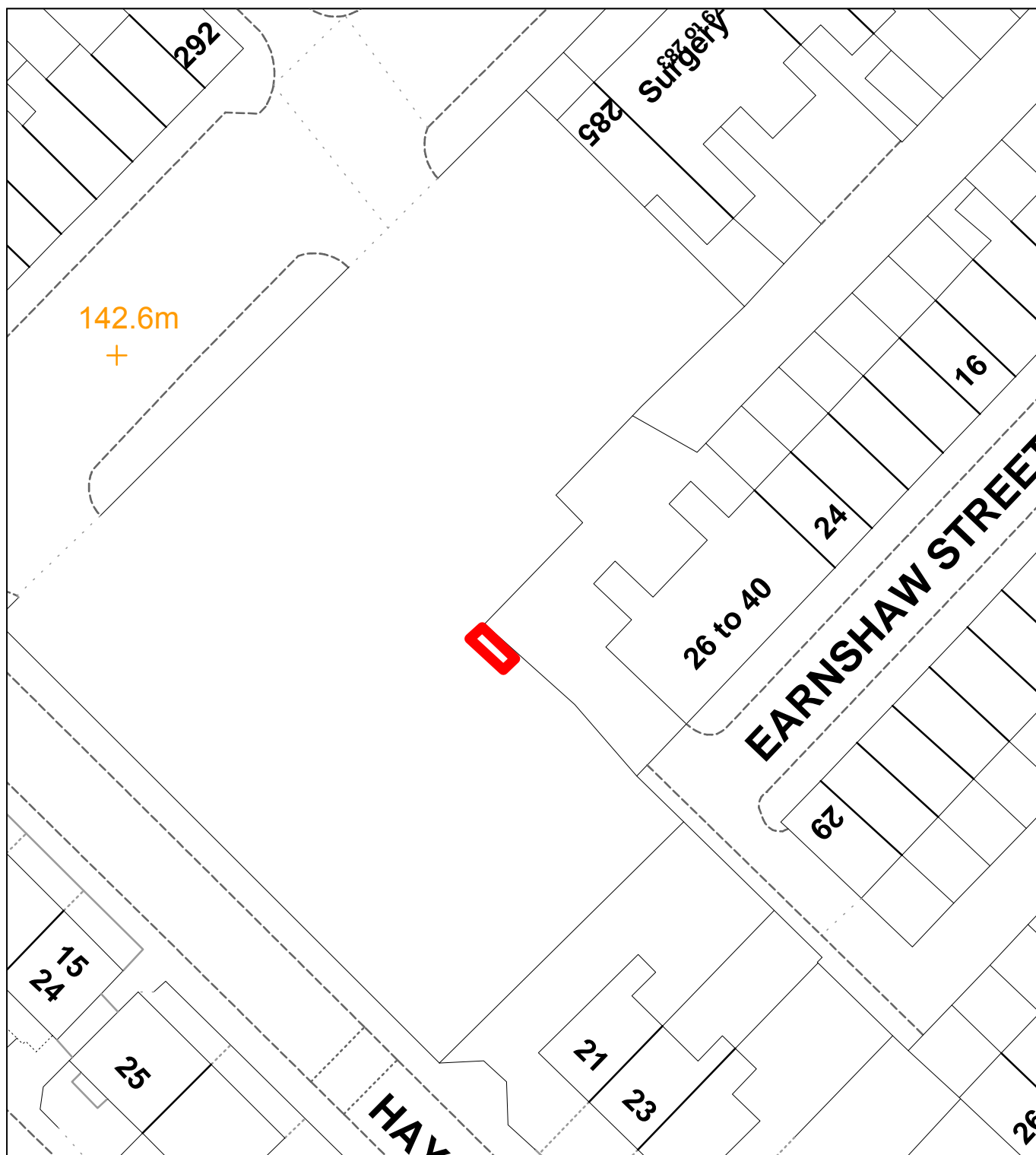




client name			
TIGM			
project			
Bolton Town Centre Transport Strategy			
drawing			
Proposed Boundary Wall Details			
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Application No.

87454/12



Development and Regeneration Department  
Planning Control Section

Town Hall, Bolton, Lancashire, BL1 1RU.  
Telephone (01204) 333333  
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**Bolton  
Council**

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11

**Date of Meeting: 15/03/2012**

**Application Reference: 87454/12**

**Type of Application: Prior Notification (all)**  
**Registration Date: 30/01/2012**  
**Decision Due By: 25/03/2012**  
**Responsible Officer: Andrew McGlone**

**Location: PYMGATE, JUNCTION OF ST HELENS ROAD AND HAYNES STREET, BOLTON, BL3 3QB**

**Proposal:** PRIOR NOTIFICATION FOR THE ERECTION OF A 14.8M HIGH TELECOMMUNICATIONS MONOPOLE WITH 6No ANTENNAE, 1No SHARED RADIO EQUIPMENT CABINET, 1No SHARED ELECTRICAL METER PILLAR AND ARMCO BARRIER.

**Ward: Hulton**

**Applicant: Vodafone UK Limited**  
**Agent : Mono Consultants**

## **Officers Report**

**Recommendation: Prior Approval Granted with Conditions**

### **Background**

The Planning Committee deferred this application on 1st March to enable Officers to check the site history. Officer's findings are as follows:

- Approval granted with conditions to use the land as a hand car wash/valeting service and car sales. Erection of a canopy and siting of 2 no. storage containers, together with 14 no. parking spaces. Ref: 83136/09.
- Approval granted with conditions to demolish the car dealership, erect 5 no. dwellings and 8 no. retail units with 16 no. apartments above. Ref: 77547/07. This permission was not implemented and has subsequently lapsed.

### **Proposal**

This is an application for prior approval seeking consent to erect a 14.8m high monopole mast with 6 no. antennae, 1 no. equipment cabinet, 1 no. electrical meter pillar and an armco barrier.

The proposed equipment forms part of the 3rd Generation network and is a shared installation between O2 and Vodafone. The mast is designed as a pole to blend in as much as possible with existing street furniture, such as street lights, within the urban area.

### **Site Characteristics**

This is a proposed new telecommunications site. The proposed installation is located in the south-east corner of the site, which is widely used as a car wash. St Helens Road is to the north-west, Haynes Street to the south-west.

Residential properties adjoin the site to the south and east, a number have views across the site.

### **Policy**

PPG8 (revised) Telecommunications.

Core Strategy Policies CG3 The Built Environment; RA1 Inner Bolton

Planning Control Policy Note No. 25 - Telecommunications.

### **Analysis**

PPG8 states that applications for Prior Approval must be considered on siting and appearance grounds only. Policies in the Development Plan relating to siting and appearance are relevant.

Applications which are acceptable in siting and appearance terms and comply with the provision of the Development Plan, should be granted Prior Approval.

Similarly applications which are unacceptable in siting and appearance terms, and which are contrary to the provisions of the Development Plan, should be refused Prior Approval.

It is therefore necessary to decide whether this application is acceptable in terms of siting and appearance, with due regard being had to relevant Development Plan policies.

The Government's policy is to facilitate the growth of new and existing telecommunications systems whilst keeping the environmental impact to a minimum.

The main impacts of the proposal are:-

- \* Consideration of Alternative Sites and Mast Sharing
- \* Design and Visual Amenity Issues
- \* Health Considerations
- \* Technical Justification

#### Consideration of Alternative Sites and Mast Sharing

PPG8 places strong emphasis on the use and reuse of existing sites, in particular the use of existing masts, buildings and other structures. The Government encourages applicants to hold pre-application discussions on with the Local Planning Authority and interested third parties regarding the consideration of alternative sites.

The lack of consideration of alternative sites can be a reason to refuse applications, but the Local Planning Authority must give clear and specific reasoning, having borne in mind the technical constraints of the Operators. The applicants have considered the following sites and masts prior to the submission of this application, all of which are considered to be unacceptable for the following reasons:-

- Pavement of St Helens Road, Outside Asda - An installation could not be built here due to the presence of underground services, leaving insufficient room for the installations foundations. The site has been discounted as Vodafone cannot implement a proposal of this nature.
- Pavement at junction of St Helens Road and Deane Church Lane - An installation could not be built here due to the presence of a disused railway line beneath the site, resulting in a large void beneath the pavement with no suitable ground to install the

masts foundation. The site has been discounted as Vodafone cannot implement this proposal.

- Mafredi, Deane Church Lane - Vodafone have made a number of approaches to the site owner with the intention of entering into an agreement on mutually acceptable terms, however despite these numerous approaches no response was forthcoming. Therefore, as Vodafone does not have the owner's permission, this site has been discounted.
- Pelham Street Garage, Pelham Street - The site provider has confirmed they are unwilling to accommodate the operators upon their land. Therefore the operators do not have the land owner's permission to develop the land.
- Hulton Motor, St Helens Road - The site provider has confirmed they are unwilling to accommodate the operators upon their land. Therefore the operators do not have the land owner's permission to develop the land.
- Pavement of St Helens Road/Haynes Street - This installation was subject of a withdrawn application. It cannot be built due to the presence of underground services, leaving insufficient room for the installations foundations. The site has therefore been discounted.

This is a shared mast between O2 and Vodafone. This will therefore remove the need for another installation in the area. It is considered the operators have highlighted and discounted alternative sites on valid grounds.

#### Design and Visual Amenity Issues

PPG8 requires that the siting, scale and external appearance of the apparatus, and any associated landscaping, have been designed to minimise, eliminate or mitigate negative impact on amenity, visual intrusion and, if applicable, the appearance of structures on which they are mounted. Core Strategy policy CG3 considers the built environment. Policy RA1 considers developments within Inner Bolton.

The mast will be positioned to the rear of the car wash site, adjacent to no. 26-40 Earnshaw Street. It will peer above the residential units on Earnshaw Street by some 6.7m as well as street lamps and other structures found on site.

Due to the siting and scale the mast will be visible in the local area. Due to the tight knit development, residents directly affected would be constricted to the end terraces on Earnshaw Street, Sloane Street, properties on Sloane Street between nos. 37-41 and residents on Rothbury Court facing Haynes Street. Except no. 26-40 Earnshaw Street (9m to nearest) a minimum interface distance of 26m would be upheld. The mast would be viewed from the nearest end terrace on Earnshaw Street - outlook from the rear window will be towards the car wash and St Helens Road, rather than the installation.

Residential uses on St Helens Road facing the site would view the mast in conjunction with established street lights.

The Council has allowed masts similar to the proposed monopole structure in residential areas and they are considered to generally be accepted as forming part of the urban fabric. The mast and equipment are considered to readily assimilate into the character and appearance of the urban area. It is not considered to be unreasonable or excessive. The siting and appearance of the mast is considered to be acceptable.

#### Health Considerations

The Central Government view is that if the output levels of the proposed equipment are below the guidelines recommended by the International Commission on Non-Ionizing



Radiation Protection, there should be no need for further consideration on health grounds.

PPG8 advises that Operators should provide Local Planning Authorities with ICNIRP compliance statements. The applicants have provided a valid ICNIRP compliance statement for this application, therefore fulfilling requirements of PPG8.

#### Technical Justification

PPG8 states that Operators may be expected to demonstrate the need for the proposal.

Previous applications in the local area clearly indicate there is a technical requirement. The mast is a shared facility and will remove the need for two separate masts within the area. Coverage plots show an enhanced O2 signal, in particular to the south-east. Plots demonstrate Vodafone will benefit significantly - signals will be improved to the south and north-east, particularly in residential areas and along St Helens Road.

#### **Conclusion**

It is considered that the siting and design of the proposed telecommunications equipment is acceptable and in accordance with national and local planning policy and guidance.

Members are recommended to approve the application for prior notification.

## **Representation and Consultation Annex**

### **Representations**

None received to date.

### **Consultations**

A site notice was posted on site on 8/2/2012.

Advice was sought from the following consultees: Highway Engineer and Telecommunications Consultant (ECS Limited).

St. Bede's Primary School is approx. 232m from the site. They have been consulted by letter.

### **Planning History**

Approval granted with conditions to use the land as a hand car wash/valeting service and car sales. Erection of a canopy and siting of 2 no. storage containers, together with 14 no. parking spaces. Ref: 83136/09.

Approval granted with conditions to demolish the car dealership, erect 5 no. dwellings and 8 no. retail units with 16 no. apartments above. Ref: 77547/07. This permission was not implemented and has subsequently lapsed.

Three applications submitted for pavement in front of Asda (later withdrawn) on St Helens Road, at the junction of St Helens Road and Haynes Street (later withdrawn) and at the junction of St Helens Road and Deane Church Lane (Approved) - Refs: 86436/11, 86962/11 & 85062/10.

## **Recommendation:        Prior Approval Granted with Conditions**

### **Recommended Conditions and/or Reasons**

1. The development hereby permitted shall be begun before the expiration of 3 years from the date of this permission.

Reason

Required to be imposed pursuant to section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. No development shall take place until details of the external finish and colour of the equipment cabinet has been submitted to an approved on writing by the Local Planning Authority. Development shall take place in accordance with the approved details prior to installation becoming operational and shall be maintained as approved.

Reason

To ensure the development safeguards the character and appearance of the locality.

1. The Council has granted planning permission, subject to the conditions listed above, because the proposed development is in accordance with all relevant policies of the Development Plan (the Core Strategy and the Regional Spatial Strategy Plan for the North-West), as is required by Section 38 of

the Planning and Compulsory Purchase Act 2004. There are no material considerations, as specified in the Planning Officer Report, that outweigh this justification to support the grant of planning permission. A summary of the relevant Development Plan policies pursuant to Article 22 of the Town and Country Planning (General Development Procedure) (England) (Amendment) Order 2003 and how the proposed development relates to these policies is set out below.

Core Strategy

CG3 The Built Environment

RA1 Inner Bolton

1 ALL DIMENSIONS ARE IN MM UNLESS NOTED OTHERWISE

N.G.R.	E: 369945 N: 407175
--------	---------------------

U/S OF 6NO. VODAFONE/02 ANTENNAS  
4-13.23m AGL

PROPOSED VODAFONE/02 VULCAN  
CABINET C/W ERICSSON RBS 6601 &  
NOKIA FLEXI BTS EQUIPMENT ON  
CONCRETE FOUNDATION.

EXISTING 3.0m WIDE x 10.0m LONG  
PORTACABIN (TO BE REMOVED BY MONO)

EXISTING 2.0m HIGH  
BRICK WALL

BRICK WALL WITH  
0.9m HIGH RAILING

[illegible]-1.2m B.G.L. 

TOP OF SECURITY LIGHT

+3.5m AGL

EXISTING 6.0m HIGH SECURITY LIGHT

EXISTING HAYNES STREET -  
ELECTRICITY SUB-STATION

The proposed column location shown on this drawing is subject to the receipt of Statutory Authority searches, a CAT scan and a trial dig of the proposed column location.

(1:100)

**The drawings comply with O2 & Vodafone Standard ICNIRP guidelines.**  
Designed in accordance with Cornerstone document: CORN/09/013

Checked:	Date:	Approved:	Date:
NJD	28.12.11	SRR	28.12.11

52



Notes:

1 ALL DIMENSIONS ARE IN MM UNLESS NOTED OTHERWISE

N.G.R.

E: 369945 N: 407175

30 JAN 2012

REV	MODIFICATION	BY	CH	DATE
B	Structure type amended/site revised	RW	NLD	09.01.12
A	Issued for Approval	RW	NLD	28.12.11

#

mono

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O<sub>2</sub>

Comerstone Project

Cell Name

PYMGATE – BOLTON

Opt

D

Cell ID No

Comerstone

Host

Sharer

(CS) 15678 (VF) 74716 (O2) 044615

Site Address / Contact Details

PYMGATE – BOLTON

ST HELENS ROAD

BOLTON

BL3 3QB

Drawing Title:

PROPOSED SITE PLAN

Purpose of issue:

PLANNING

Drawing Number:

200

Scale:

As Shown

Date:

28.12.11

Checked:

NLD

Date:

28.12.11

Approved:

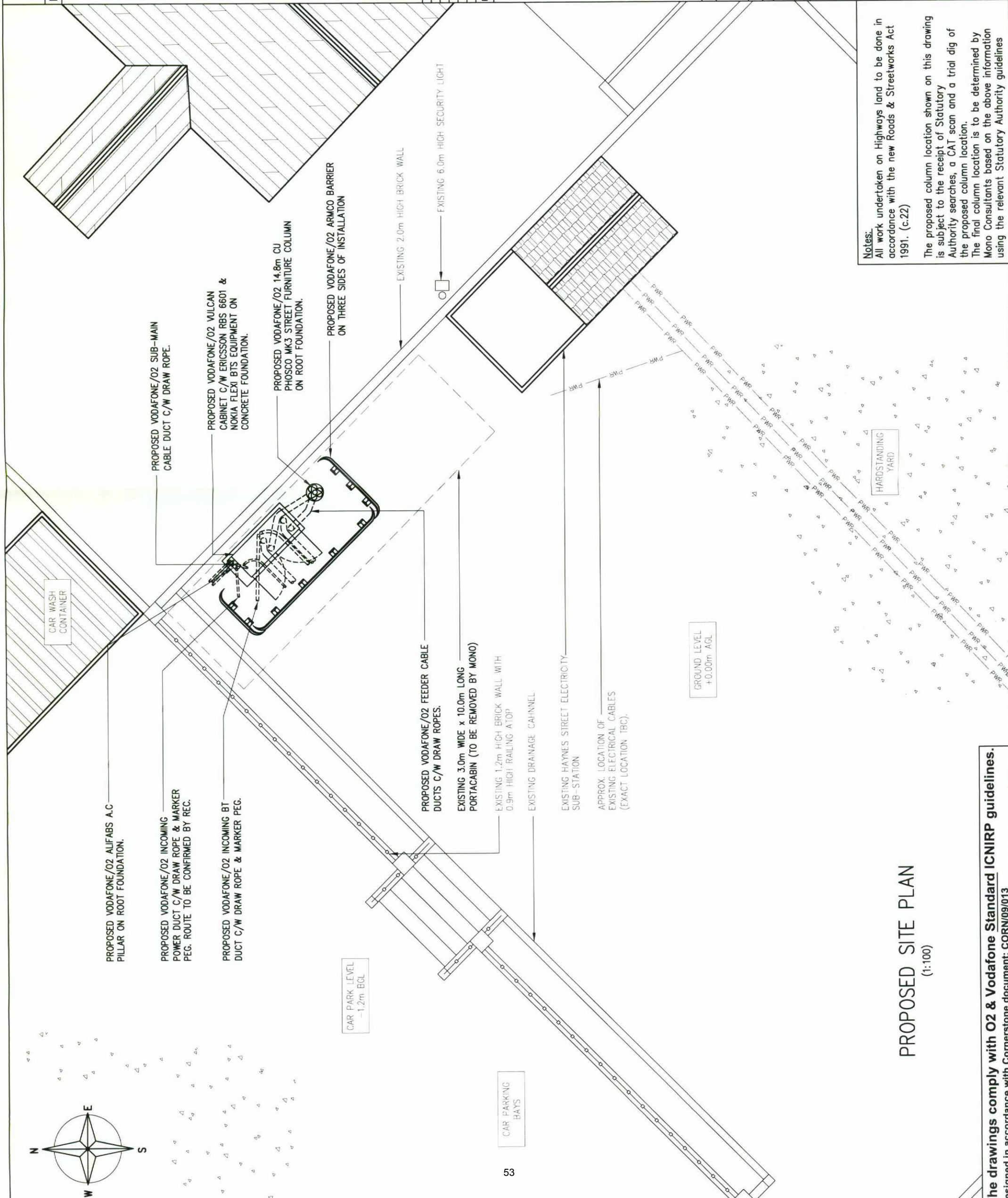
SRR

Date:

28.12.11

Issue:

B



Notes:  
 All work undertaken on Highways land to be done in accordance with the new Roads & Streetworks Act 1991. (c.22)  
 The proposed column location shown on this drawing is subject to the receipt of Statutory Authority searches, a CAT scan and a trial dig of the proposed column location.  
 The final column location is to be determined by Mono Consultants based on the above information using the relevant Statutory Authority guidelines

PROPOSED SITE PLAN  
 (1:100)