

Application number
94609/15



Development & Regeneration Dept
Development Management Section

Town Hall, Bolton, Lancashire, BL1 1RU
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Bolton
Council

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Date of Meeting: 22/10/2015

Application Reference: 94609/15

Type of Application: Full Planning Application

Registration Date: 15/07/2015

Decision Due By: 13/10/2015

Responsible Officer: Alex Allen

Location: EDGE FOLD INDUSTRIAL ESTATE, PLODDER LANE, BOLTON, BL4 0LW

Proposal: DEMOLITION OF EXISTING EMPLOYMENT & CONSTRUCTION WASTE TRANSFER PREMISES AND ERECTION OF 9NO. NEW EMPLOYMENT UNITS (B1C / B2 / B8) AND NEW CONSTRUCTION WASTE TRANSFER UNIT (SUI GENERIS) ALONG WITH ASSOCIATED PARKING AND LANDSCAPING

Ward: Hulton

Applicant: JCBS Properties Ltd

Agent : P4 Planning Limited

Officers Report

Recommendation: Approve subject to conditions

Proposal

The applicant is seeking the demolition of existing industrial buildings located on the northern side ('Top Deck') of Edgefold Industrial Estate including the removal of an existing Construction Industry Recycling Centre (Waste Transfer Station). The existing buildings on site are at the end of their useful life and replacement is necessary.

In terms of floorspace the existing site provides 3,331 sq.metres. The current proposal seeks the replacement of this floorspace with a total floor area of 3,641 sq.metres. In terms of the context of the wider industrial estate this represents an increase in floorspace of less than 2%. The new buildings would be used for B1c (general industrial compatible with residential use), B2 (general industrial) and B8 (storage and distribution) uses.

The overall height of the new units would vary between 7.7 metres and 10.2 metres to the ridge. This would ensure that each unit has clear internal heights of 6.5 metres to enable them to be appropriate for modern industrial use.

Site Characteristics

The application relates to the northern part of Edgefold Industrial Estate which forms part of the large Estate currently operated by the Bolton based construction company Seddon's. The application site currently provides 3,331 sq.m of floorspace with the existing buildings being constructed of bricks with profiled cladding in a mix of colours, in varying degrees of dilapidation. The land in between the buildings consists of large areas of hardstanding which is used for a mix of servicing, car

parking and vehicular access/egress. Vehicular access is from Smethurst Lane to the west.

Policy

National Planning Policy Framework (NPPF).

Core Strategy: P1 Employment land, P5 Accessibility, S1 Safe, CG1 Cleaner Greener, CG2 Sustainable Design and Construction, CG3 The Built Environment, CG4 Compatible Uses, OA4 West Bolton.

Bolton Allocations Plan: Policy CG7AP Green Belt, Appendix 2: Protected Employment Land - Site 14 P1.2 - Edge Fold.

Analysis

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- * principle of development;
- * impact on highway safety;
- * impact on adjoining land uses;
- * impact on drainage/sustainability;
- * impact on ecology; and
- * impact on ground stability / land contamination.

Principle of development

The application site is located within allocated Green Belt land and is also allocated as a Protected Employment area (Site 14 P1.2 Edge Fold).

Guidance contained within NPPF (Paragraphs 79 to 92) provides guidance on the consideration of development proposals within the Green Belt. The overarching aim of Green Belt policy is to protect Green Belt areas from inappropriate development to prevent urban sprawl by keeping land permanently open. This is replicated within Allocations Plan policy CG7AP. It is noted that paragraph 89 of NPPF does allow for '...the replacement of a building, provided the new building is in the same use and not materially larger than the one it replaces.

The site is also allocated as protected employment land. Core Strategy policy P1 seeks to ensure that the Council and its partners safeguard existing employment areas where they are compatible with residential amenity and contribute to the sustainability of communities in which they are situated.

As outlined above the proposal would result in the redevelopment of an existing large employment site whose buildings have become dilapidated and are not fit for purpose. The net increase in

floorspace would be 310 sq. metres which in the context of the application site and the wider Edgefold Industrial Estate is a modest addition. The proposed heights of the new buildings would be broadly comparable to the existing site ensuring the proposal would have a minimal impact on the openness of the Green Belt.

The proposal would assist in providing modern industrial units with a modest increase in footprint without compromising the overall openness of the Green Belt in this location.

The proposal complies with policy.

Impact on highway safety

Core Strategy policy P5 seeks to ensure that new development proposals take into account accessibility, freight movement, servicing arrangements and parking.

The applicant has developed a scheme which provides 89 car parking spaces together with vehicle circulation areas.

The Council's Highways Engineers have commented that they have no objections to the proposal. They have concluded that the proposal provides sufficient car parking for the proposal.

The proposal would provide a redeveloped site which would provide a modern industrial facility providing sufficient parking, circulation and servicing space to meet the requirements of the end users of the new units.

The proposal complies with policy.

Impact on adjoining land uses

Core Strategy policy CG4 seeks to ensure that new development proposals are compatible with surrounding land uses and occupiers.

The site is located in the northern part of the wider Edgefold Industrial area, with the adjoining land use being in industrial use. There are some residential properties which front Plodder Lane some 90 metres away to the south. However, the wider Industrial area is located in between the application site and the existing industrial units which provides an effective buffer.

It is therefore considered that the proposed development would be complementary to the adjoining industrial use whilst not being detrimental to the closest residential properties.

The proposal complies with policy.

Impact on drainage/sustainability

Core Strategy policy CG1.5 seeks to ensure that new development proposals reduce the risk of flooding in Bolton by minimising water run-off from new development and ensuring a sequential approach is followed, concentrating new development in the areas of lowest flood risk. In addition, Core Strategy policy CG2 seeks to ensure that new development proposals achieve a BREEAM rating of very good together with the reduction in surface water run-off from sites by 50% for previously developed sites

It is noted that United Utilities currently advise a planning condition which makes provision for the above reduction in surface water run-off by 50%.

The applicant has stated that they are unable to reduce surface water run-off from the site for a

number of reasons:

-) the development proposal is the sequential replacement of existing buildings that are no longer fit for use. The existing development outfalls and connection routes to the public sewer network are being reused without replacement. There is no proposal to change to the existing rate or quantity of storm water discharge to the network. This is understood to be acceptable to UUpIc and the requirement restricting the surface water discharge may be removed.
-) the existing networks are separate foul and surface water drainage. This is compliant with your requirements and acceptable to UUpIc.
-) the site is built over historic landfill. This will preclude the use of sustainable drainage and infiltration methods. This is understood to be acceptable to UUpIc and supports the retention of the existing drainage serving the development.
-) the retention of the existing infrastructure, outfalls and rates of discharge combined with the need to avoid significant excavation that may open pathways to through the historic landfill will preclude the use of attenuation. This is understood to be acceptable to UUpIc and further supports our agreement to remove the obligation to restrict the surface water drainage.

The proposal represents a modest extension in floorspace than currently exists. It is considered that due to the site constraints the recommended policy requirement to reduce surface water run-off is not feasible. Therefore it is recommended, so as not to increase the risk of flooding from the proposed development, to condition that surface water flows from the site are not increased as a result of the proposed site redevelopment. This would be in compliance with policy CG1.

In planning policy terms the proposed development would have to comply with Core Strategy policy CG2 which seeks to ensure that buildings are constructed to a good standard including the aim to ensure new development proposals reduce carbon dioxide emissions by at least 10%. The applicant remains committed to build the proposed new buildings to the latest building regulations standard whilst ensuring the fabric of the building and associated insulation ensures that the required 10% reduction is provided for.

It is considered that the development proposal fully complies with Core Strategy policy CG1 and broadly complies with Core Strategy policy CG2.

Impact on ecology

Core Strategy policy CG1 seeks to ensure that new development proposals safeguard and enhance biodiversity in the borough by protecting sites of urban and rural biodiversity.

The proposal would not result in an increase in the existing operations taking place within the site in relation to ecology.

It is therefore considered that the proposal would not have a detrimental impact on the ecological value of the site and thus complies with policy.

Impact on ground stability / land contamination

Core Strategy policy CG4 seeks to ensure that new development proposals which are affected by contamination or ground instability must include an assessment of the extent of the issues and any possible risks.

The applicant has submitted a Phase I Geo Environmental Assessment which includes an assessment of potential contamination, geo technical risk assessment including land fill / ground gases, radon and coal mining/land stability. The report concludes that there is a requirement for further detailed site investigations to assess contamination and ground gas.

It is considered that sufficient information has been submitted with the application to ensure that the proposals can be built out. Further detail is required to inform the development. The provision and implementation of the approved remediation strategy would be conditional upon any approval.

Conclusion

The proposal would involve the redevelopment / regeneration of a well established industrial area, providing new industrial facilities which would meet the requirements of the existing and future business occupiers with a limited net increase in floorspace. The development would have a minimal impact on the openness of the Green Belt. The proposal would complement the existing use of the site, also complementing the adjoining use/wider industrial estate without compromising the living conditions of the closest residential occupiers. The development proposal would provide sufficient car parking whilst seeking to maintain surface water run off at existing discharge rates.

The proposal complies with policy and is recommended for approval subject to conditions.

Representation and Consultation Annex

Representations

Letters/petitions:- no representations received.

Elected Members:- no comments received.

Consultations

Advice was sought from the following consultees: the Coal Authority, the Council's Landscape Architects, Environmental Health officers, Highways Engineers, Economic Strategy, Strategic Development Unit, Drainage/Flood Risk officers, Environment Agency, Design for Security (Greater Manchester Police), Greater Manchester Waste Disposal Authority.

Planning History

The site has been the subject of a number of planning permissions for industrial development on the site which reflects the current layout.

Recommendation: **Approve subject to conditions**

Recommended Conditions and/or Reasons

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason

Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. Prior to the commencement of development the details (samples if required) of the type and colour of materials to be used for the external walls and roof to be agreed with the Local Planning Authority. The approved materials shall be implemented in full thereafter.

Reason

To ensure the development reflects local distinctiveness.

3. Within 3 months of the commencement of development, an energy assessment of the approved development shall be submitted to the Local Planning Authority. The submission shall include a scheme which details how either (i) renewable energy technology or (ii) an alternative scheme e.g. enhanced insulation shall reduce CO2 emissions of predicted energy use of the development by at least 10%. The approved scheme shall be installed, retained and maintained in perpetuity thereafter unless agreed by the Local Planning Authority.

Reason

To enhance the sustainability of the development and to reduce the reliance on fossil fuels in order to comply with Core Strategy policy CG2.

4. Development shall not commence until an investigation and risk assessment has been completed in accordance with a scheme to assess the nature and extent of any contamination on the site. The contents of the scheme shall be approved in writing by the Local Planning Authority. The investigation and risk assessment must be undertaken by competent persons in accordance with Model Procedures for the Management of Land Contamination (CLR 11) and a written report of the

findings must be produced. The written report shall be subject to the approval in writing of the Local Planning Authority and shall include:

- (i) a survey of the extent, scale and nature of contamination;
- (ii) an assessment of the potential risks to human health, property or the environment;
- (iii) an appraisal of remedial options and proposal for a preferred option.

Reason

To ensure the development is safe for use.

5. Development shall not commence until any remediation scheme required as a result of the investigation carried out in accordance with Condition 4 has been prepared, approved in writing by the Local Planning Authority, and implemented as approved. The scheme shall include provision for verifying that the remediation objectives (verification report) have been met and for any subsequent monitoring and maintenance in accordance with Model Procedures for the Management of Land Contamination (CLR 11), details of which should be submitted to and approved in writing by the Local Planning Authority before the development is first brought into use or first occupied.

Reason

To ensure the development is safe for use.

6. During the construction phase of the development hereby approved, should it arise that surface water run-off from the site is likely to increase then a drainage strategy shall be submitted to and approved in writing by the Local Planning Authority which makes provision for a reduction in surface water run-off levels which equates to the pre development surface water run-off levels. The approved scheme shall then be implemented in full prior to first use of the phase of development / development hereby approved and retained thereafter.

Reason:

To reduce the risk of contaminating surface water run off and reduce the risk of localised flooding and down stream flooding by ensuring the provision of a satisfactory means of surface water dispersion

7. Prior to the first occupation / use of Units 1 and 2 the redundant vehicular access point onto Smethurst Lane shall be closed to vehicles and the existing highway (kerbing and footway) made good to adoptable footway standards. There shall thereafter be no means of vehicular access to or from Smethurst Lane, other than as shown on drawing ref: P4053_0003 Rev. 1.

Reason

In the interests of highway safety.

8. Prior to first occupation or use of Units 1 and 2, full details of the proposed gabion wall and associated landscaping, including a timetable for implementation, shall be submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be implemented in full and retained thereafter.

Reason

To ensure the development reflects the landscape and townscape character of the area.

9. Before any unit hereby approved is first occupied or brought into use the associated access road, footway/footpath, car parking and HGV turning areas to be used in connection with that unit, as shown on Drawing No. P4053_0003 Rev 1, shall be provided in accordance with Drawing ref: P4053_0003 Rev. 1. Such infrastructure shall be made available at all times the unit(s) are in use.

Reason

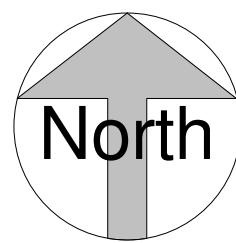
To ensure that adequate provision is made for vehicles to be left clear of the highway and to ensure the development is safe for use.

10. The development hereby permitted shall be carried out in complete accordance with the following approved plans:

**P4053_0003 - Proposed Site Plan, scanned to file 15/07/15;
P4053_0004 - WTS_ GA Ground Floor Plan, scanned to file 15/07/15;
P4053_0005 - WTS_ GA First Floor Plan, scanned to file 15/07/15;
P4053_0006 - WTS_ GA Roof Plan, scanned to file 15/07/15;
P4053_0007 - WTS_ GA Elevations, scanned to file 15/07/15;
P4053_0008 - Units 1 & 2_ GA Plans, scanned to file 15/07/15;
P4053_0009 - Units 1 & 2_ GA Elevations, scanned to file 15/07/15;
P4053_0010 - Units 3 to 6_ GA Plans, scanned to file 15/07/15;
P4053_0011 - Units 3 to 6_ GA Elevations, scanned to file 15/07/15;
P4053_0012 - Units 7 to 9_ GA Plans, scanned to file 15/07/15;
P4053_0013 - Units 7 to 9_ GA Elevations, scanned to file 15/07/15.**

Reason

For the avoidance of doubt and in the interests of proper planning.



Notes

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KEYPLAN

1

Updated in line with client comments

SPK

29.06.15

Rev

Description

By

Date

Client

Seddon

Job

Plodder Lane, Bolton

Drawing

Existing Site Plan

Ref

P4053_0002

Rev

1

Date

25/06/15

Scale

1 : 500

@ A1

2D Coord

-

3D Coord

SPK

Approved

JW

Feasibility

Tender

Construction

Design

Contract

As Built

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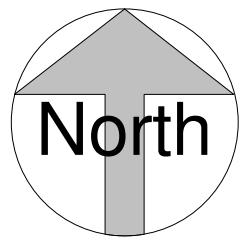
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Contractor
for details

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SCHEDULE OF APPROX AREAS

All areas GIA, unless stated

- UNIT 1** 413 sqm (4,445 sqft)
UNIT 2 413 sqm (4,445 sqft)
UNIT 3 140 sqm (1,507 sqft)
UNIT 4 140 sqm (1,507 sqft)
UNIT 5 140 sqm (1,507 sqft)
UNIT 6 140 sqm (1,507 sqft)
UNIT 7 140 sqm (1,507 sqft)
UNIT 8 140 sqm (1,507 sqft)
UNIT 9 225sqm (2,420 sqft)
WTS 1,750sqm (18,837 sqft)

TOTAL 3,641 sqm (39,189 sqft)



Site - Proposed
1 : 500

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KEYPLAN

1	Updated in line with client comments	SPK	29.06.15
Rev	Description	By	Date
Client	Seddon Developments		
Job	Edgefold, Plodder Lane, Bolton		
Drawing	Proposed Site Plan		
Ref	P4053_0003	Rev	1
Date	25/06/15	Scale	1 : 500
By	SPK	Approved	JW
Feasibility	Tender	Construction	
Design	Contract	As Built	

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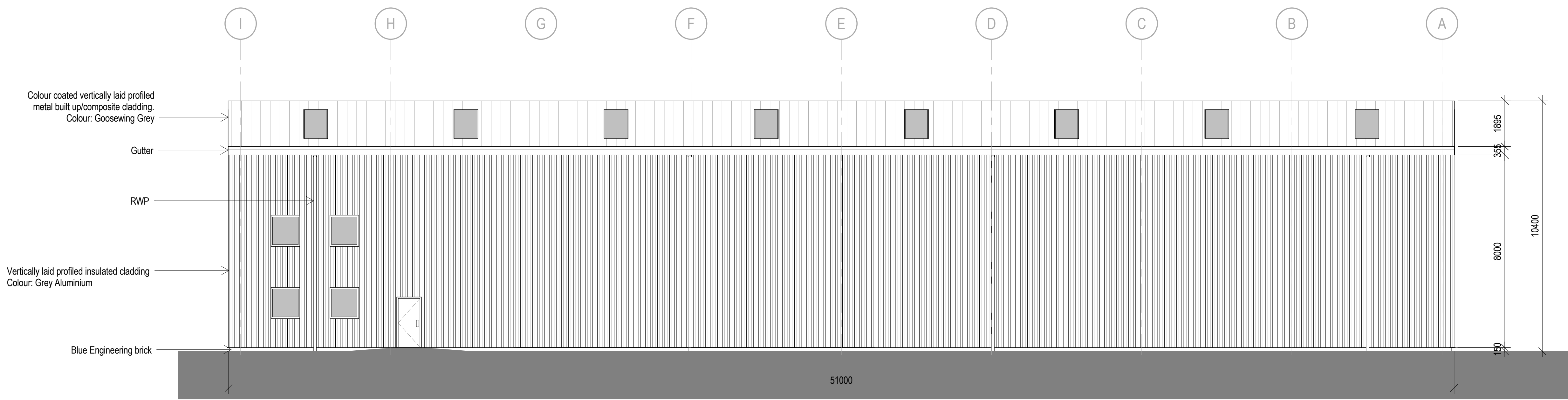
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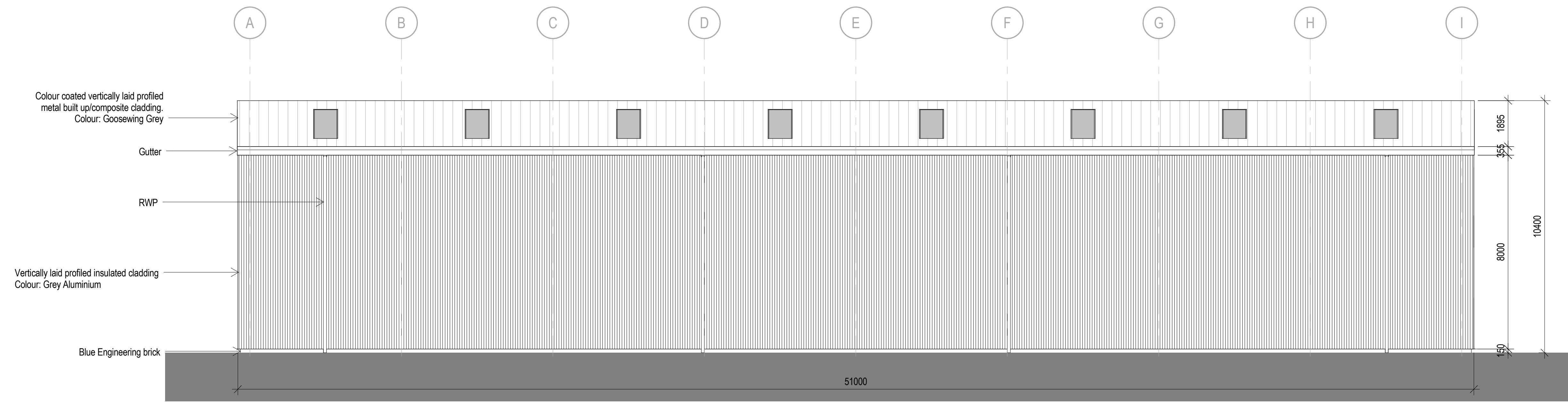
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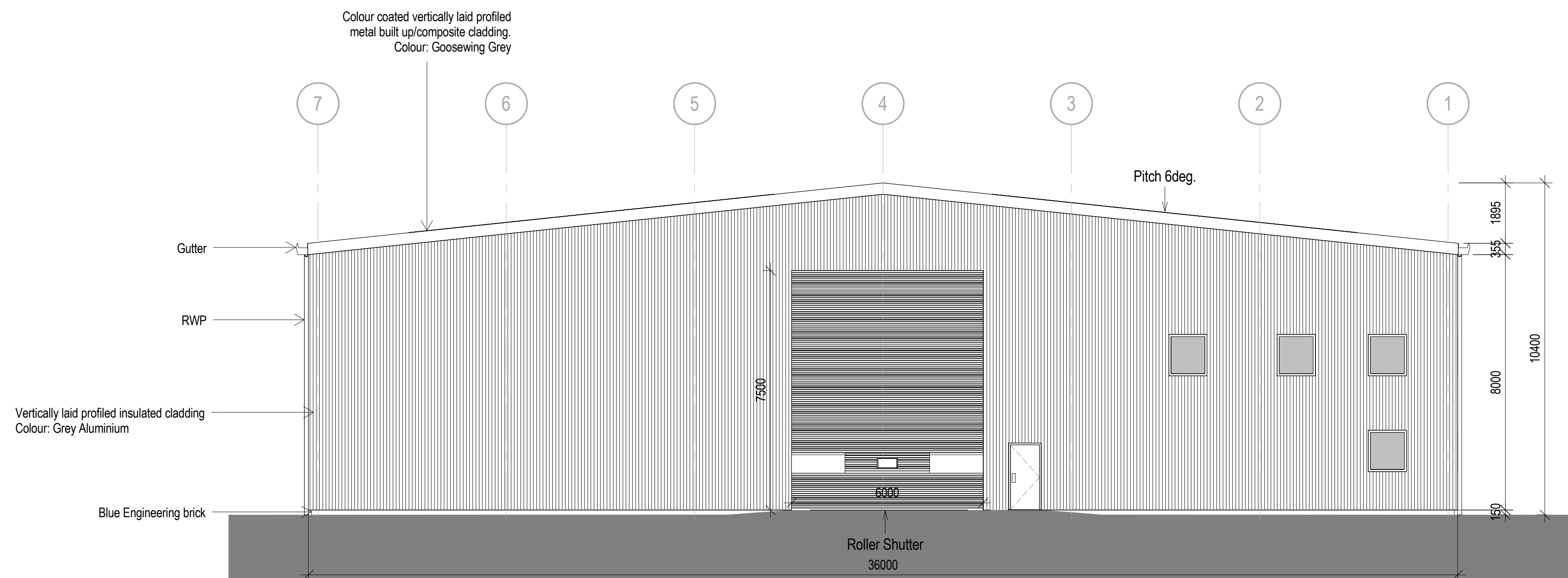
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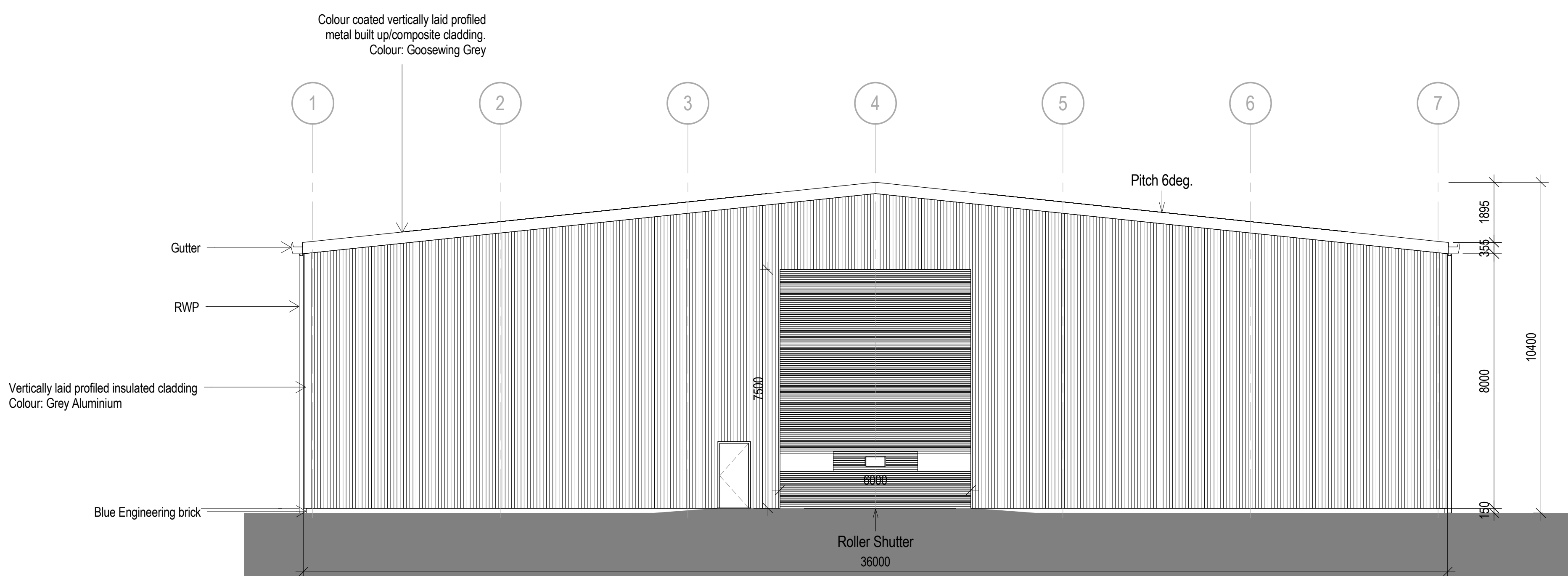
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3 Proposed South Elevation
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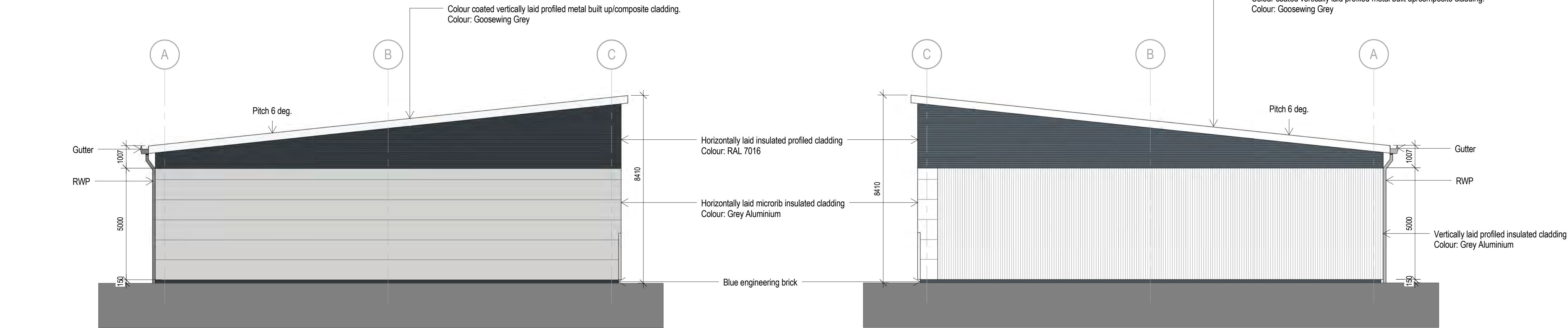


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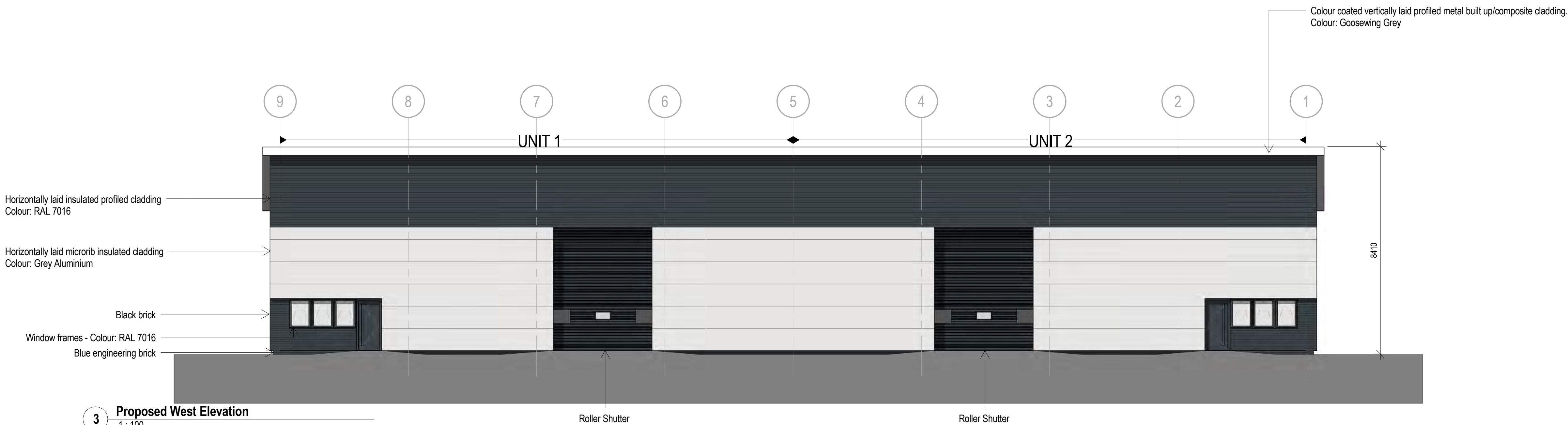
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1	Updated in line with client comments		SPK	29.06.15
Rev	Description	By	Date	
Client	Seddon			
Job	Plodder Lane, Bolton			
Drawing	Waste Transfer Station - GA Elevations			
Revised	P4053_0007		Rev	1
Date	25/05/15	Scale	1 : 100	
	-	Rev	SPK	Approval JW
Project	Feasibility	Tender	Construction	
	Design	Contract	As Built	



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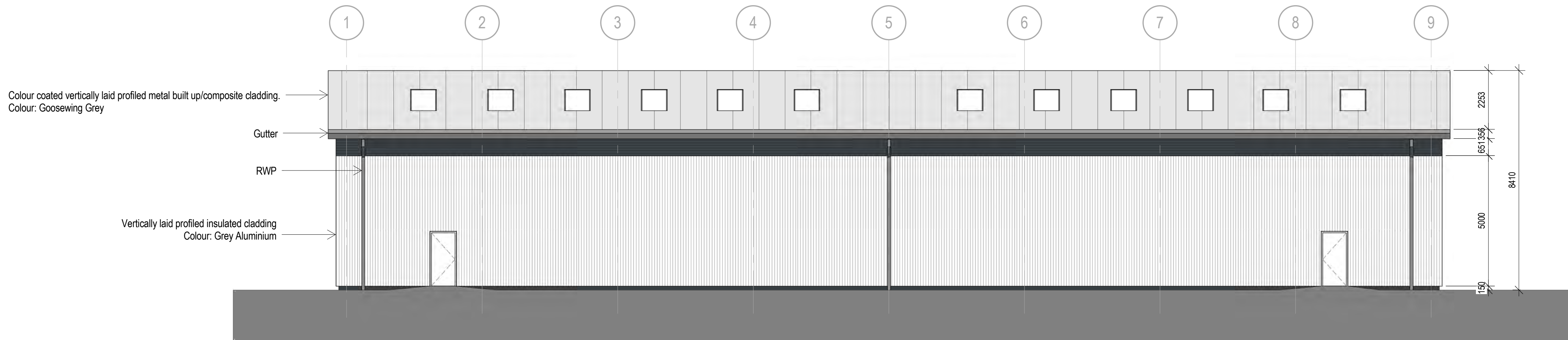
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3 Proposed West Elevation
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Roller Shutter

Roller Shutter



4 Proposed East Elevation
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KEYPLAN

1 Updated in line with client comments SPK 29.06.15

Rev Description By Date

Client Seddon

Job Plodder Lane, Bolton

Drawing Units 1 & 2 - Proposed GA Elevations

P4053_0009 Rev 1

123-PZ-00-DR-A-0009

Date 25/06/15 Scale 1:100 @ A1

Author POZZONI

2D Coord SPK Approved JW

Feasibility Tender Construction

Design Contract As Built

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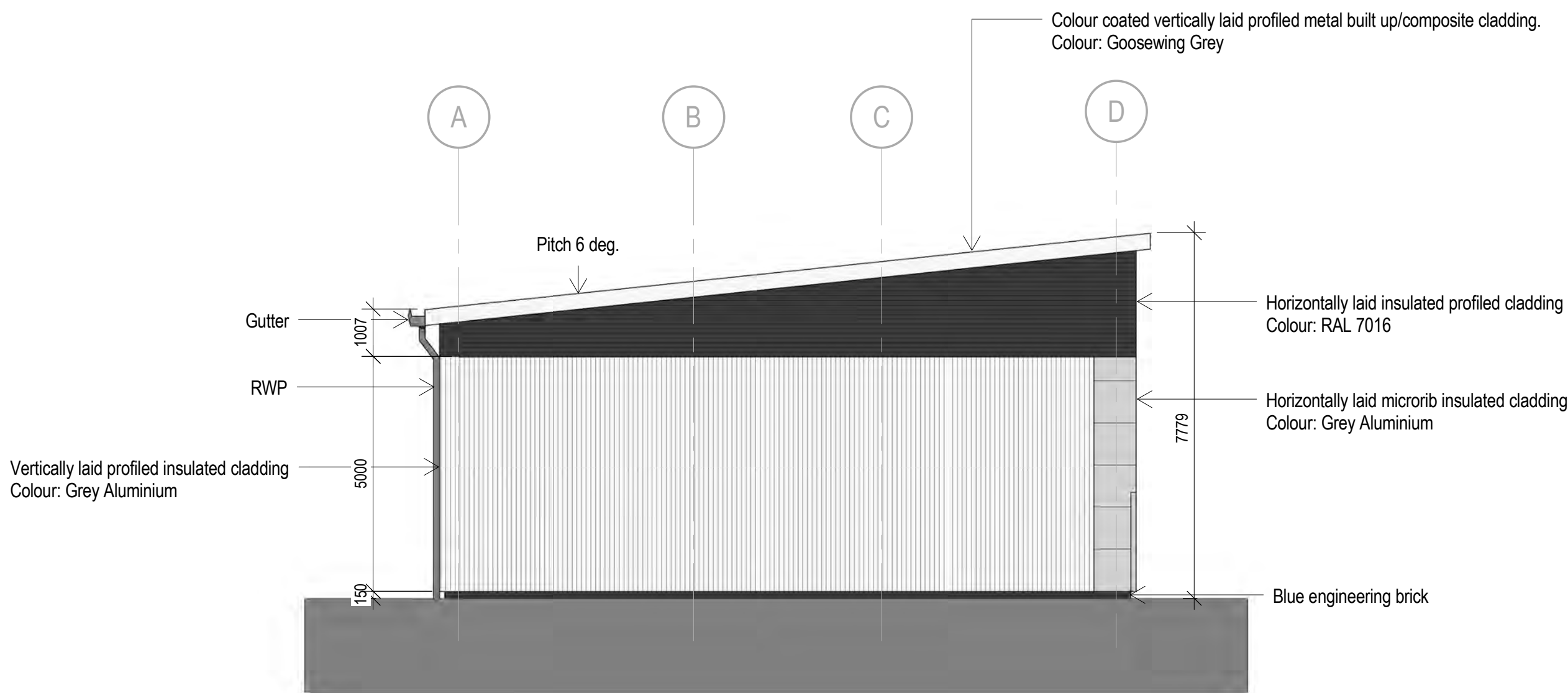
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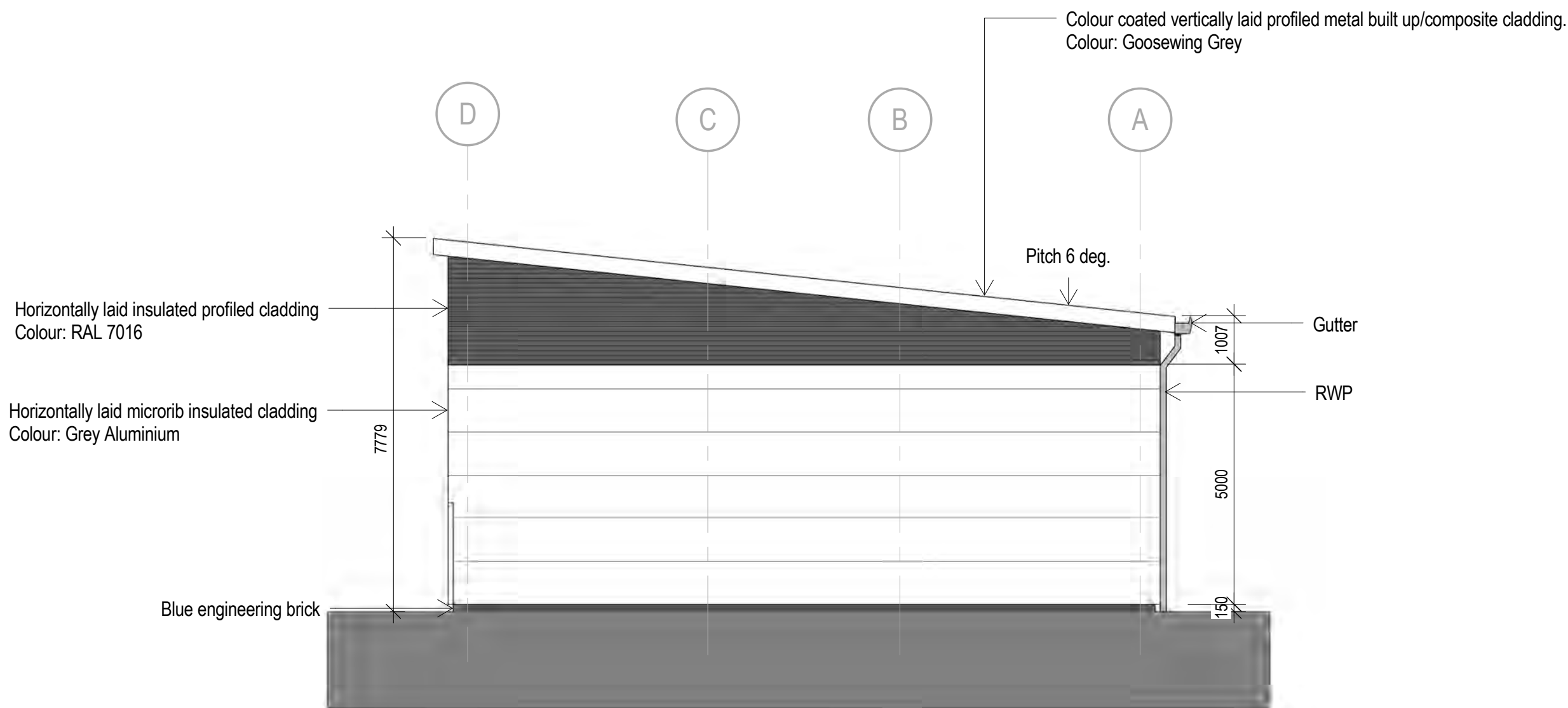
Contractor for details

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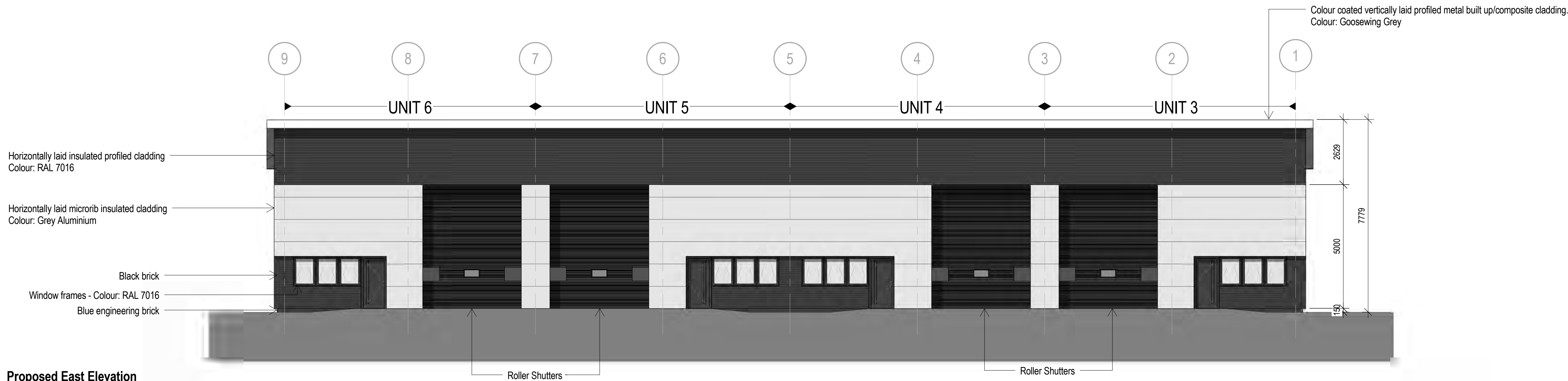
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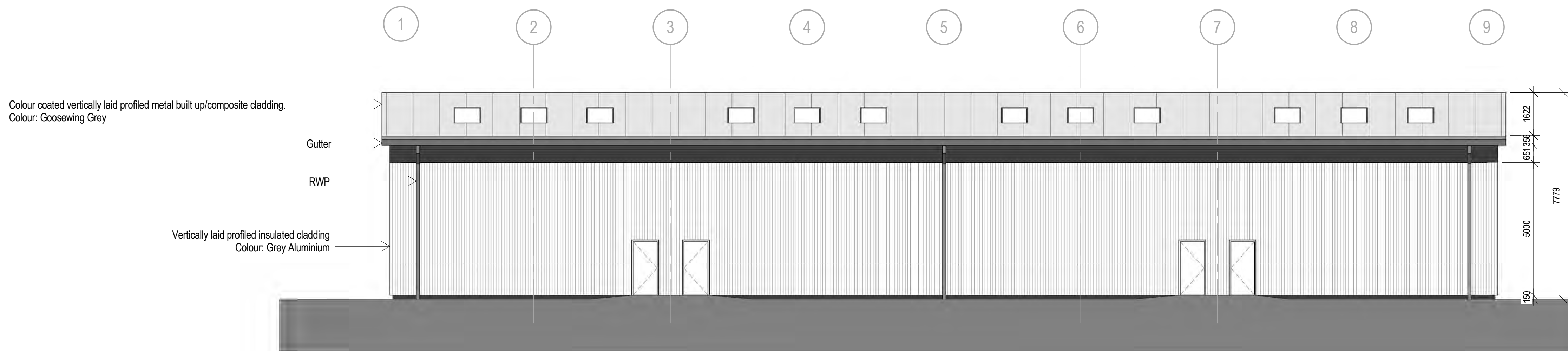
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2 Proposed North Elevation
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3 Proposed East Elevation
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4 Proposed West Elevation
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KEYPLAN

1	Update in line with client comments	SPK	29.06.15
Rev	Description	By	Date
Client	Seddon		
Job	Plodder Lane, Bolton		
Drawing	Units 3 to 6 - Proposed GA Elevations		
Ref	P4053_0011	Rev	1
Issue Ref	123-PZ-00-DR-A-0011		
Date	25/06/15	Scale	1 : 100
By	-	SPK	JW
Feasibility	Tender	Construction	
Design	Contract	As Built	

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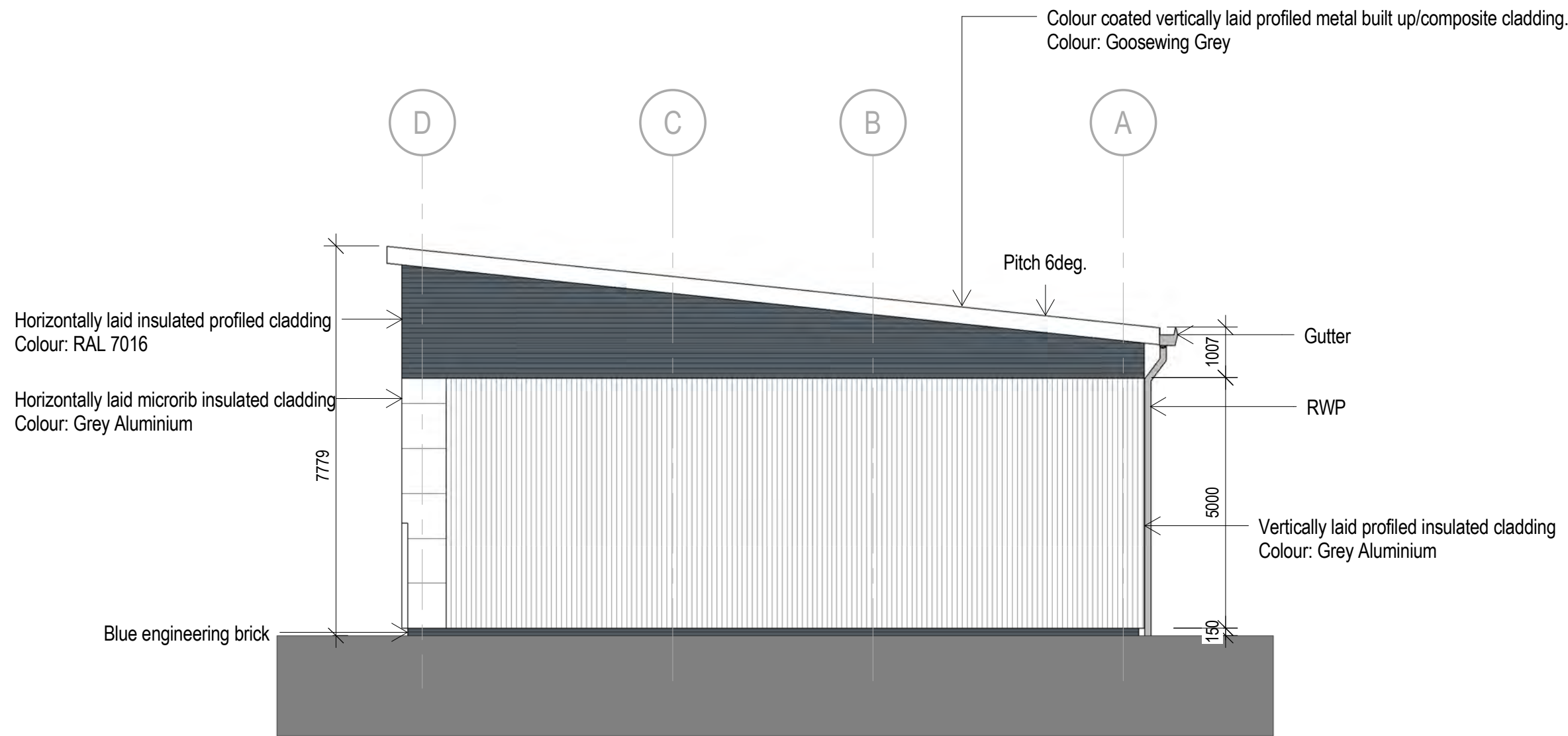
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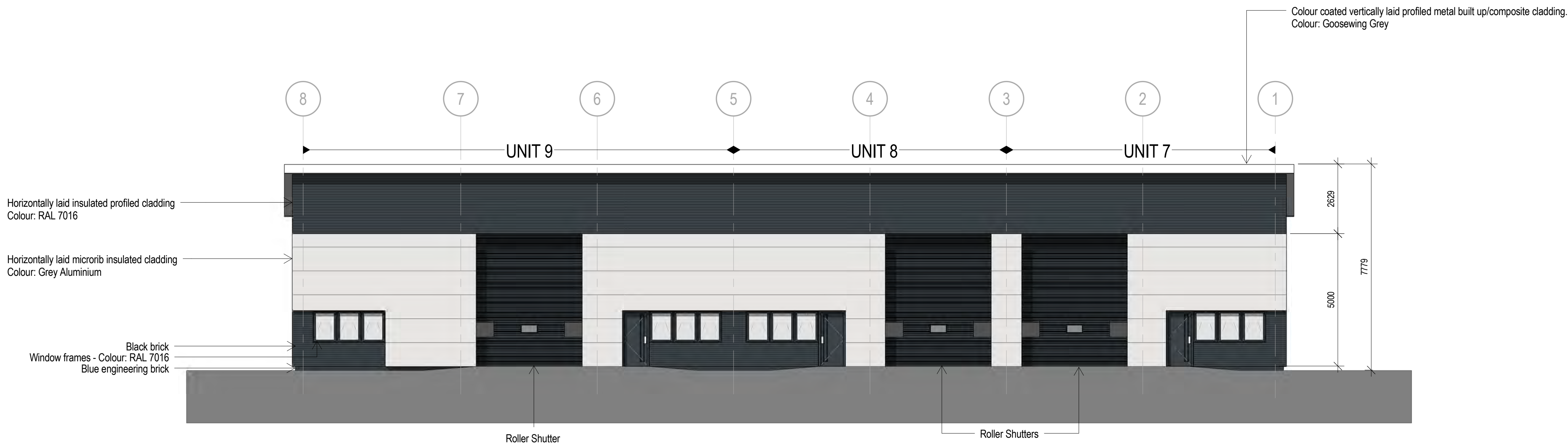
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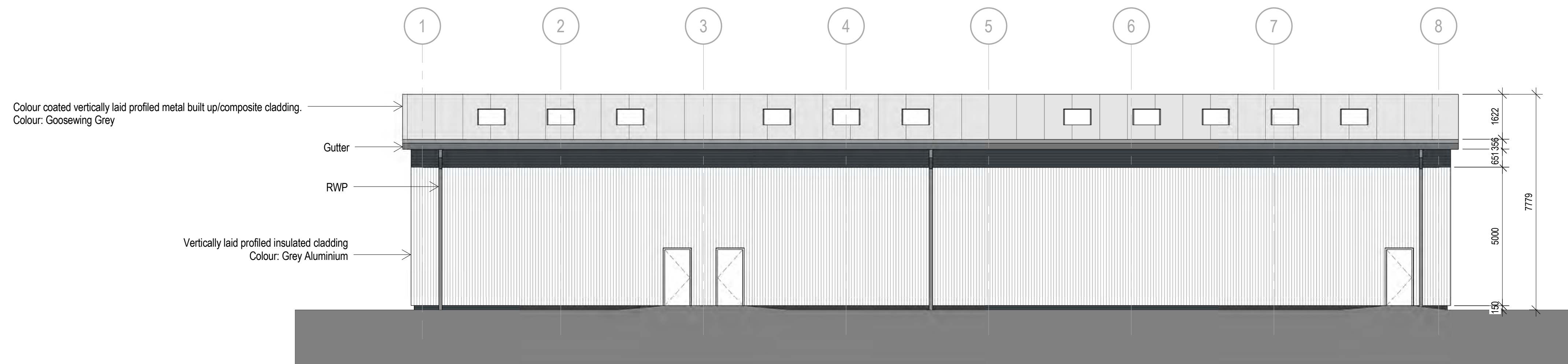
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2 Proposed West Elevation
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3 Proposed North Elevation
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4 Proposed South Elevation
1:100

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KEYPLAN

1 Updated in line with client comments SPK 29.06.15

Rev Description By Date

Client Seddon

Job Plodder Lane, Bolton

Drawing Units 7 to 9 - Proposed GA Elevations

P4053_0013 Rev 1

123-PZ-00-DR-A-0013

Date 26/06/15 Scale 1:100 @ A1

2D Coord SPK Approved JW

Feasibility Tender Construction

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**Application number
94618/15**



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**Bolton
Council**

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Date of Meeting: 22/10/2015

Application Reference: 94618/15

Type of Application: Full Planning Application
Registration Date: 15/07/2015
Decision Due By: 08/09/2015
Responsible Officer: Helen Williams

Location: LAND AT DICKINSON STREET WEST, HORWICH, BOLTON

Proposal: ERECTION OF FIVE DWELLINGS

Ward: Horwich North East

Applicant: AGIT Developments Ltd

Agent : Foxx Ltd.

Officers Report

Recommendation: Approve subject to conditions

Proposal

Permission is sought for the erection of three terraced dwellings and two semi-detached dwellings on a vacant plot of land at the junction of Dickinson Street West with Brady Street. The dwellings would be sited on either side of the existing electricity substation and would front onto Dickinson Street West.

All of the dwellings would be two storeys in height, red brick, comprise two bedrooms and will have rear gardens.

No on-site parking is proposed.

Site Characteristics

The application site is a vacant site comprising mainly hard standing to the rear of the Queens Head public house. An electricity substation (not included within the application site) splits the site into two. There are trees along the boundary with the substation and part of Dickinson Street West on the western part of the site.

The application site is located within Horwich Town Centre Conservation Area and Horwich town centre (local shopping district).

Dickinson Street West (immediately to the north of the site) comprises two storey terraced houses.

To the immediate south of the site is the Queens Head public house, a two storey white rendered building sited at an angle towards the application site.

The rears of the dwellings at 245 and 247 Lee Lane face the eastern half of the application site.

The area to the north and east of the application site is characterised by grid iron streets of small scale and tight knit terraced dwellings.

The majority of houses rely on-street parking. There are no parking restrictions in the area.

Policy

National Planning Policy Framework (NPPF)

Core Strategy Policies: P5 Transport and Accessibility; S1 Safe Bolton; CG1.2 Urban Biodiversity; CG3 The Built Environment; CG4 Compatible Uses; SC1 Housing; OA1 Horwich and Blackrod.

SPD General Design Principles

Horwich Town Centre Conservation Area Appraisal

Analysis

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- Γ principle of residential development
- Γ impact on the character and appearance of Horwich Town Centre Conservation Area
- Γ impact on the amenity of neighbouring residents
- Γ impact on the highway

Principle of Residential Development

Paragraph 49 of the National Planning Policy Framework (NPPF) states that housing applications should be considered in context of the presumption in favour of sustainable development. A core planning principle of the NPPF is to encourage the effective use of land by reusing land that has been previously developed (brownfield land), provided that it is not of high environmental value.

Policy SC1.2 of the Core Strategy states that the Council will ensure at least 80% of housing development will be on previously developed land. Policy OA1.1 states that the Council will continue to promote Horwich town centre as being suitable for a mix of retailing, leisure, employment and housing uses and Policy OA1.4 states that the Council will concentrate sites for new housing in Horwich town centre, at Horwich Loco Works and on other sites within the existing urban area.

The application site is a previously developed site within Horwich town centre, therefore being suitable for housing development and complying with Policies SC1.2, OA1.1 and OA1.4 of the Core Strategy and the principles within the NPPF. It is considered that the site is in a highly sustainable location, being within a town centre and close to shops, local services, schools and transport links.

Impact on the Character and Appearance of Horwich Town Centre Conservation Area

Policy CG3 of the Core Strategy states that the Council will conserve and enhance local distinctiveness, ensuring development has regard to the overall built character and landscaping quality of the area, and will require development to be compatible with the surrounding area, in terms of scale, massing, grain, form, architecture, street enclosure, local materials and landscape treatment. Policy CG3.4 states that the Council will conserve and enhance the heritage significance of heritage assets and heritage areas, recognising the importance of sites, areas and buildings of archaeological, historic, cultural and architectural interest and their settings.

Policy OA1 of the Core Strategy refers specifically to developments in Horwich and Blackrod and states that the Council will conserve and enhance the character of the existing landscape and physical environment, especially the conservation areas of Horwich town centre, Horwich Loco Works and Wallsuches, and ensure that development respects street patterns, the grain and the form of predominant architectural styles and where possible makes sympathetic use of locally distinctive materials such as stone.

The application site is located on the edge of Horwich Town Centre Conservation Area (Dickinson Street West is not included within the conservation area). The Council's Conservation Area Appraisal for Horwich Town Centre refers to the area in which the application site is located as forming the approach to Horwich town centre from the west. It is not considered that the application site contributes to the conservation area, as it is not visible from this approach into the town centre and is currently only a vacant piece of land to the rear of the Queens Head public house. It is instead considered, should the application site be developed for housing, then the site would appear more associated with the neighbouring terraced houses to the north of the conservation area (on the streets of Dickinson Street West and Mary Street West) than with the properties along Lee Lane (which the development would have its back to).

The five houses have been designed to replicate the character, appearance and scale of the terraced properties along Dickinson Street West, which they are to face. Like the houses on Dickinson Street West and the surrounding streets, the proposed houses will directly front onto the pavement and will have a rear amenity area. The density of the development will adhere to the established housing density of the area and the siting of the houses will follow the existing grid iron urban grain of the area.

The trees along part of the site boundary with Dickinson Street West (facing numbers 56 to 62) will be lost as a result of the development, however these are not considered to be of amenity value to the area and the Council's Tree Officer has raised no objection to the proposal.

It is therefore considered that the proposed development would be compatible with the character and appearance of the surrounding area, compliant with Policies CG3 and OA1 of the Core Strategy.

Impact on the Amenity of Neighbouring Residents

Policy CG4 of the Core Strategy states that the Council will ensure that new development is compatible with surrounding land uses and occupiers, protecting amenity, privacy, safety and security.

SPD General Design Principles sets out the Council's recommended interface distances between dwellings. Where two storey dwellings are to face each other, the SPD recommends an interface distance of 21 metres. The interface distances to be achieved between the proposed development and the houses opposite on Dickinson Street West will fall significantly short of those recommended within the SPD, at only just over 9 metres. The siting of the proposed houses will therefore take light

away from the existing houses opposite and there will be some overlooking/loss of privacy between the proposed and existing houses.

This proposed relationship however already exists between the houses on the eastern section of Dickinson Street West. The siting of the proposed houses will be following the existing pattern of the street. Given the existing interface distances between the houses on Dickinson Street West it is considered, on balance, that the proposed siting of the new houses would be acceptable, as they would be in keeping with the established urban grain of the area.

The proposed dwellings will be approximately 22 metres away from the rears of 245 and 247 Lee Lane (the elevations containing windows), therefore complying with the guidance contained within SPD General Design Principles.

Impact on the Highway

Policy P5 of the Core Strategy states that the Council will ensure that developments take into account [amongst other things] parking, in accordance with the parking standards set out in Appendix 3. Policy S1.2 states that the Council will promote road safety in the design of new development.

No no-site parking is proposed for the dwellings. Future residents would therefore need to park their vehicles either on Dickinson Street West or on an adjoining road. The existing neighbouring residents on Dickinson Street West also have to rely on-street parking.

The Council's Highways Engineers have commented that the proposal has the potential to lead to additional on-street parking to the detriment of residential amenity, however officers would contend that the application site is located within Horwich town centre and therefore in a highly sustainable location, where the need to car ownership is not as high as in other areas of the borough. Furthermore, the proposed houses are relatively small, comprising only two bedrooms.

It should also be noted that there are no parking restrictions on either Dickinson Street West or the surrounding residential roads.

Engineers have recommended, should the application be approved, that conditions be attached requiring the footway across the Dickinson Street West frontage to be resurfaced and the two street lighting columns at the junction of Dickinson Street West with Brady Street be relocated if necessary.

It is considered, given the sustainable location of the application site and the lack of parking restrictions in the area, that the proposal would not jeopardise highway safety or lead to a loss of parking in the area, compliant with Policies P5 and S1.2 of the Core Strategy.

Conclusion

For the reasons discussed above it is considered that, although the interface distances between the proposed dwellings and the existing dwellings opposite on Dickinson Street West fall significantly below recommended standards and no on-site parking is proposed for the dwellings, the siting, scale and design of the development reflect the existing pattern of housing in the area and the proposal will contribute to the provision of housing within Horwich town centre (on a highly sustainable brownfield site). Members are therefore recommended, on balance, to approve this application.

Representation and Consultation Annex

Representations

Letters:- Two letters of objection have been received from neighbouring residents. These raise the following concerns:

- Γ Dickinson Street West is not wide enough for vehicles to park on opposite sides of the road from each other. It will be difficult for large vehicles such as emergency vehicles and refuse collection wagons;
- Γ The proposal will only add to existing parking problems in the area;
- Γ The close proximity of the proposed houses will block natural daylight/sunlight enjoyed by the properties opposite
- Γ Sewage discharge.

Horwich Town Council:- made no comment on the application at their meeting of 20th August 2015.

Consultations

Advice was sought from the following consultees: Highways Engineers, Pollution Control Officers, Tree Officers and Greater Manchester Police's Architectural Liaison Officers.

Planning History

None.

Recommendation: **Approve subject to conditions**

Recommended Conditions and/or Reasons

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason

Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. Development shall not commence until an investigation and risk assessment has been completed in accordance with a scheme to assess the nature and extent of any contamination on the site. The contents of the scheme shall be approved in writing by the Local Planning Authority. The investigation and risk assessment must be undertaken by competent persons in accordance with Model Procedures for the Management of Land Contamination (CLR 11) and a written report of the findings must be produced. The written report shall be subject to the approval in writing of the Local Planning Authority and shall include:
 - (i) a survey of the extent, scale and nature of contamination;
 - (ii) an assessment of the potential risks to human health, property or the environment;
 - (iii) an appraisal of remedial options and proposal for a preferred option.

Reason

To ensure the development is safe for use.

3. Prior to the commencement of development full details of the highway works at across the Dickinson Street West frontage of the site comprising the resurfing of the footway shall be submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be implemented in full prior to the development being first brought into use and retained thereafter.

Reason

In the interests of highway safety.

4. Prior to the commencement of development full details of the relocation of the two street lighting columns at the junction of Dickinson Street West with Brady Street shall be submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be implemented in full prior to the development being first brought into use and retained thereafter.

Reason

In the interests of highway safety.

5. Prior to the commencement of development the details (samples if required) of the type and colour of materials to be used for the external walls and roof to be agreed with the Local Planning Authority. The approved materials shall be implemented in full thereafter.

Reason

To ensure the development reflects local distinctiveness.

6. Prior to the development being first occupied or brought into use, details (including a brick or masonry specification and colour scheme) of the treatment to all boundaries to the site shall be submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be implemented in full within 21 days of the Local Authorities approval, unless otherwise agreed in writing with the Local Planning Authority and retained thereafter.

Reason

To ensure adequate standards of privacy and amenity are obtained and the development reflects the landscape and townscape character of the area.

7. The development hereby permitted shall be carried out in complete accordance with the following approved plans:

F5592 1; "Proposed Site Plan"; dated June 15
F5592 11A; "Proposed Plans & Elevations"; dated June 15

Reason

For the avoidance of doubt and in the interests of proper planning.

DO NOT SCALE OFF THIS DRAWING



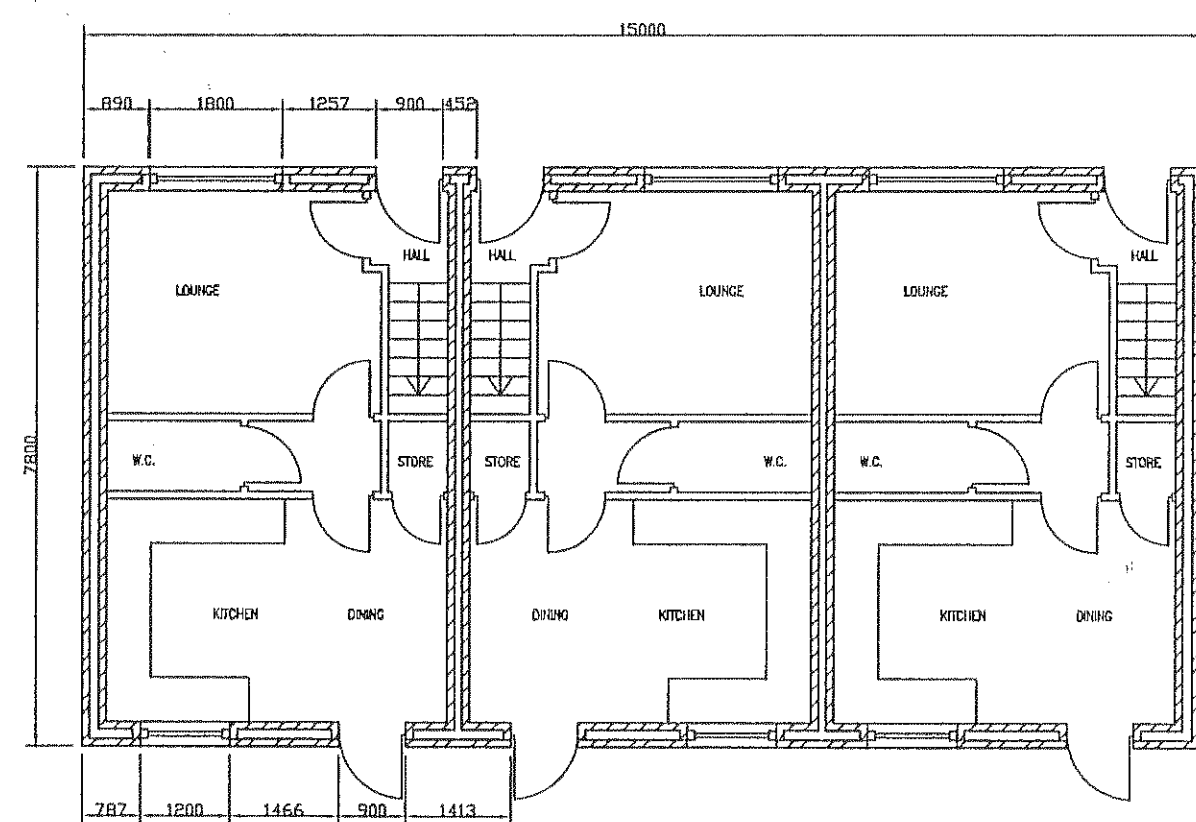
PROPOSED HOUSING DEVELOPMENT AT DICKINSON ST WEST, HORWICH.

Revisions:
Client: BOLTON COUNCIL
DEVELOPMENT & REGENERATION
RECEIVED
15 JUL 2015
Drawing Title: PROPOSED SITE PLAN.

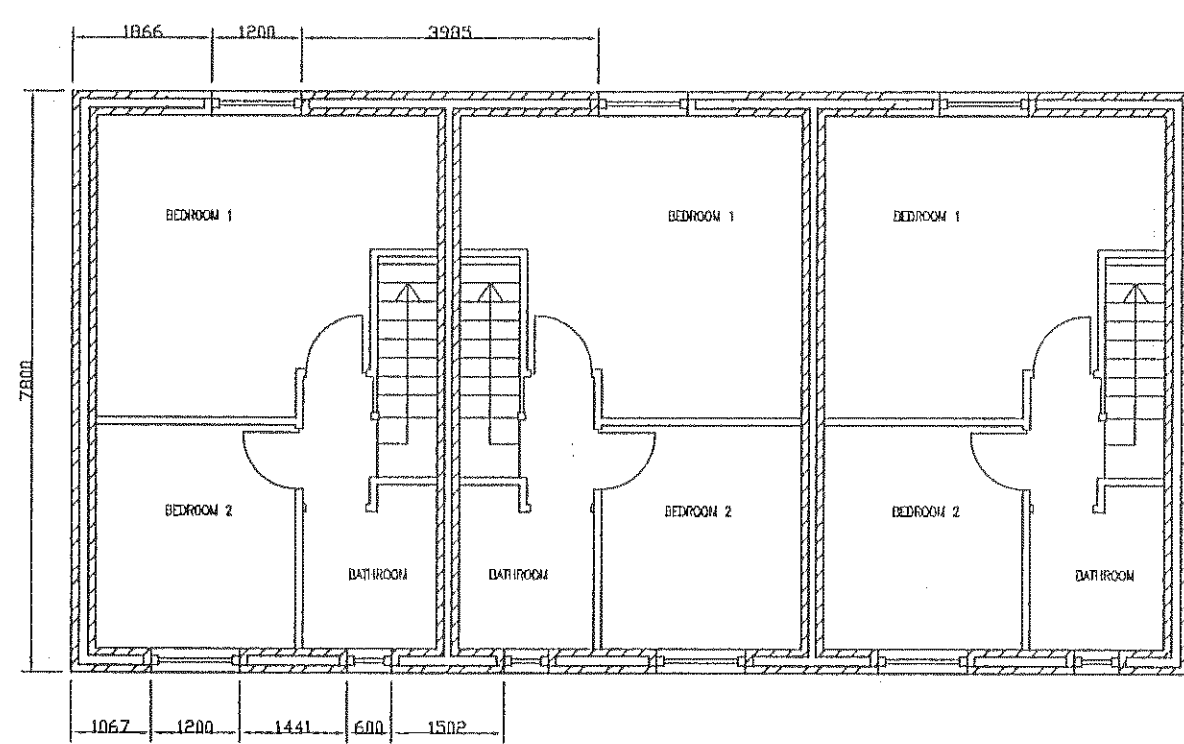
Foxx Limited.
Consulting Civil & Structural Engineers,
66-70 Park Hill Street, Bolton, BL1 4AR.
Tel: (01204) 362300
Fax: (01204) 399200
Email: info@foxx.co.uk

SCALE: 1:100 DATE: JUNE 15
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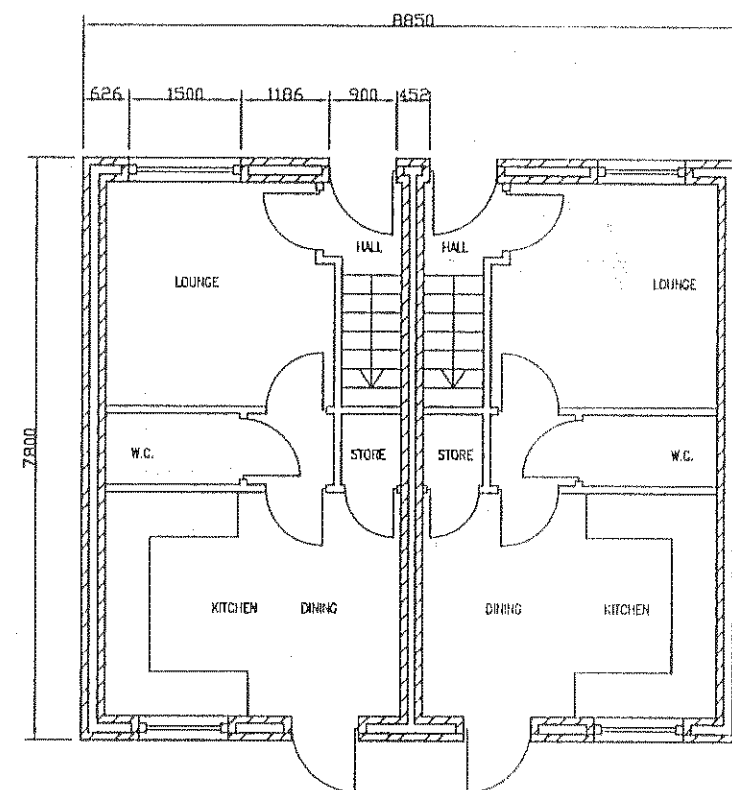
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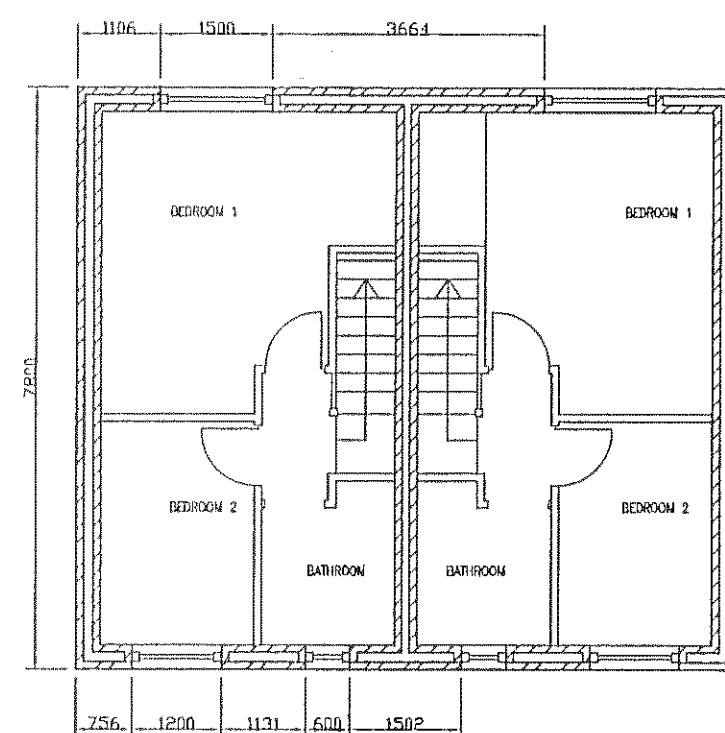
GROUND FLOOR PLAN. (PLOTS 1-3)



FIRST FLOOR PLAN. (PLOTS 1-3)



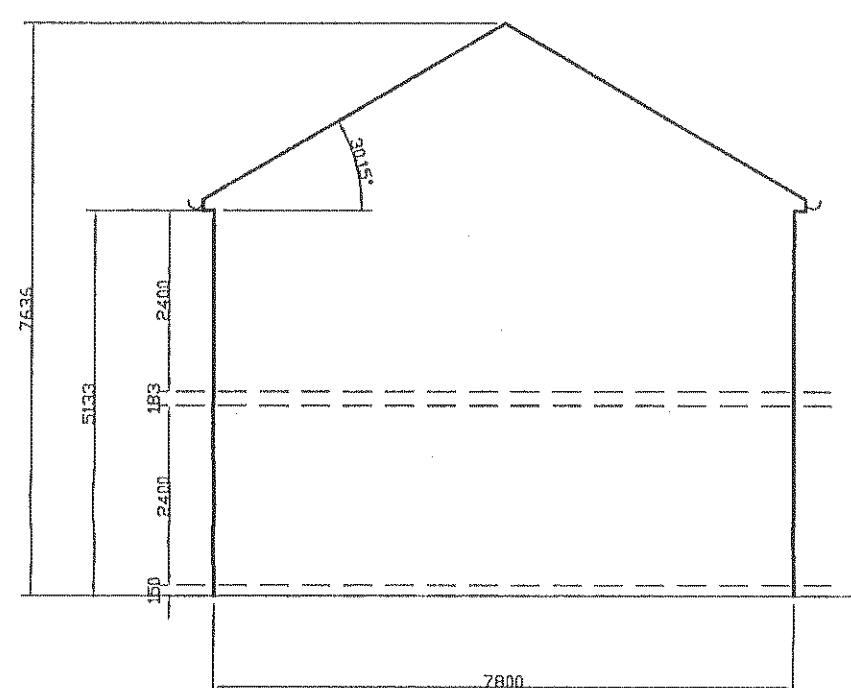
GROUND FLOOR PLAN. (PLOTS 4 & 5)



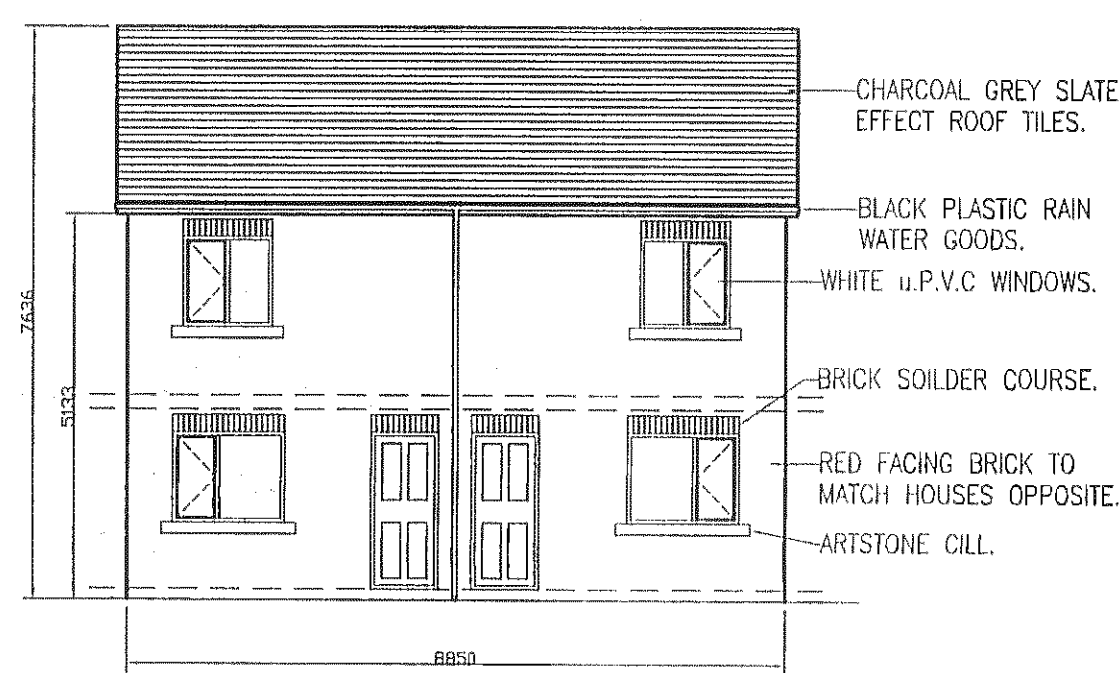
FIRST FLOOR PLAN. (PLOTS 4 & 5)



FRONT ELEVATION. (PLOTS 1-3)



TYPICAL SIDE ELEVATION.



FRONT ELEVATION. (PLOTS 4 & 5)



REAR ELEVATION. (PLOTS 1-3)



REAR ELEVATION. (PLOTS 4 & 5)

Revisions.

Client.

Drawing Title.

PROPOSED PLANS & ELEVATIONS.

Foxx Limited.

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66-70 Park Hill Street, Bolton, BL1 4AR.
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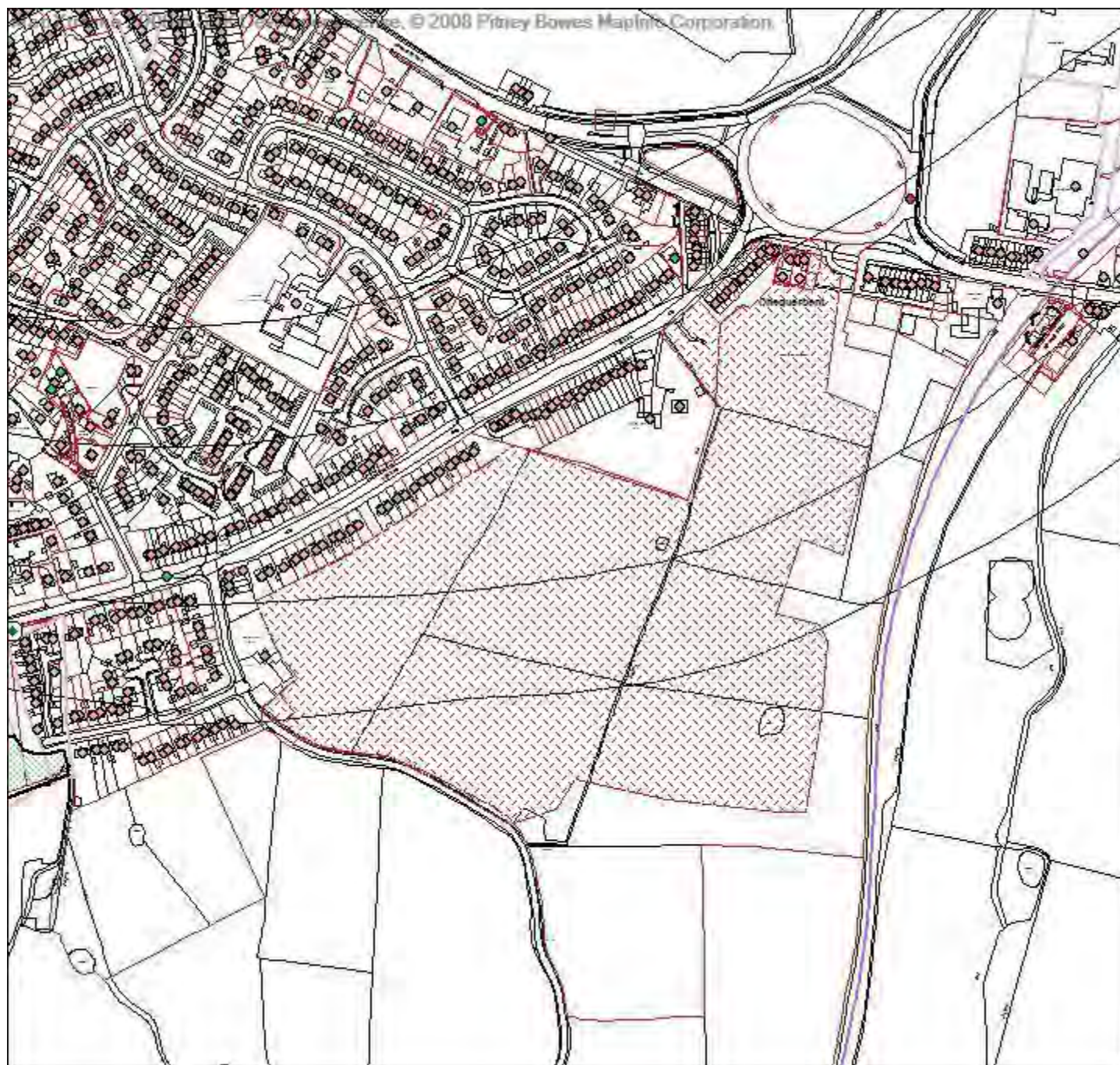
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DRAWN: R.M. CHECKED: N.T

CONTRACT No: DRAWING No:

F5592 11A

Application number
94696/15



Development & Regeneration Dept
Development Management Section

Town Hall, Bolton, Lancashire, BL1 1RU
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Bolton
Council

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Date of Meeting: 22/10/2015

Application Reference: 94696/15

Type of Application: Full Planning Application
Registration Date: 30/07/2015
Decision Due By: 28/10/2015
Responsible Officer: Alex Allen

Location: LAND NORTH OF PLATT LANE, EAST OF PARK ROAD & SOUTH OF CHEQUERBENT ROUNDABOUT, WESTHOUGHTON, BOLTON

Proposal: ERECTION OF 300 NO. DWELLINGS INCLUDING PUBLIC OPEN SPACES, LANDSCAPING & PLAY AREAS, TOGETHER WITH CREATION OF NEW INTERNAL ACCESS ROAD WHICH CONNECTS INTO THE EXISTING ROAD NETWORK AT CHEQUERBENT ROUNDABOUT IN THE NORTH AND PLATT LANE IN THE SOUTH

Ward: Westhoughton North

Applicant: Persimmon Homes, Harcourt Developments, Peel Investments (Intermediate) Ltd

Agent : Mosaic Town Planning

Officers Report

Recommendation: Refuse

Proposal

The applicant proposes the erection of 300 dwellings in a number of development parcels including culs de sac and avenues along main thoroughfares. The dwellings are a mix of 2 and 2.5 storey detached, semi detached, mews and bungalow properties ranging in size from 1 to 5 bedroom units. The applicant proposes that a total of 105 dwelling are affordable. Integral to the scheme as a whole is the provision of 6.05 hectares of open space (37% of the gross site area). The proposal would create a new formal play area with associated car parking within the northern part of the site. Informal open spaces are situated in three locations on the eastern portion of the site with landscaping, planting, footpaths and a pond.

Two areas of open space feature a play area (Neighbourhood Equipped Area of Play [NEAP] with informal kick about area and a Locally Equipped Area of Play [LEAP]). A pond with associated landscaping, footpaths and planting is located in the centre of the site. The applicant has sought to retain traditional field boundary planting wherever possible to respect the local landscape.

Vehicular access to the site would be from two new roads, the first would be from Platt Lane to the south and the second by the creation of a new/enhanced spur off the Chequerbent roundabout.

Site Characteristics

The site is located on the eastern edge of the settlement of Westhoughton. The site is approximately 15.275 hectares in area and is predominantly comprised of pastoral agricultural land

with regular field patterns with two ponds present within the site. The northern part of the site is non-agricultural open land. A public right of way runs through the middle of the site in a north south axis and then a north west to south east axis to Park Road. The public right of way links Park Road with Platt Lane.

Properties on A58 Park Road bound the site to the north west, the southern boundary of the site is defined partially by Platt Lane and field boundaries to agricultural fields. To the east the site boundary is located between 50-90 metres from the existing tree belt with the northern part of the site reaching up to the properties fronting the Chequerbent roundabout.

The A58 links junction 5 of the M61 motorway with the A6.

Policy

National Planning Policy Framework (NPPF)
Planning Practice Guidance

Greater Manchester Joint Minerals Plan

Core Strategy Policies: H1.2 Health Contributions; A1.4 Education Contributions; P4 Minerals; P5 Accessibility and Transport; S1 Safe Bolton; CG1 Cleaner and Greener Bolton; CG2 Sustainable Design and Construction; CG3 The Built Environment; CG4 Compatible Uses; SC1 Housing; OA1 Horwich and Blackrod; IPC1 Infrastructure and Planning Contributions.

Allocations Plan Policies: P8AP - Public Rights of Way and CG6AP Other Protected Open Land.

SPD Accessibility, Transport and Safety; SPD Affordable Housing; SPD General Design Principles PCPN1 Health and Well Being; PCPN8 The Provision for Children's Play Within New Residential Developments; PCPN22 Provision of Public Art; PCPN30 Education.

Bolton's Authority Monitoring Report 2013/2014

A Landscape Appraisal of Bolton (2001).

Analysis

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- Γ impact on Other Protected Open Land
- Γ other material considerations: Bolton's supply of housing
- Γ impact on Mineral Safeguarding Areas

- Γ impact on the highway
- Γ impact on public rights of way
- Γ impact on biodiversity
- Γ impact of loss of agricultural land
- Γ impact on flooding
- Γ impact on local infrastructure
- Γ impact on the character and appearance of the area
- Γ impact on the overall design of the proposed development
- Γ impact on the amenity of neighbouring residents

Impact on Other Protected Open Land

The application site is allocated within Bolton's Allocation Plan as 'Other Protected Open Land'.

Policy OA3 of the Core Strategy states that the Council will ensure Protected Open Land around Westhoughton remains undeveloped.

Allocations Plan Policy CG6AP states that the Council will permit development proposals within the defined areas of Protected Open Land shown on the Proposals Map provided that they fall within one or more of the following categories:

1. The development represents limited infilling within an established housing or industrial area, is in scale with it and would not adversely affect its character or surroundings; or
2. It forms part of, and is required for, the maintenance of an existing source of employment; or
3. The development requires a location outside the urban area, but is inappropriate within the Green Belt, and providing it maintains the character and appearance of the countryside; or
4. The development would be appropriate within the Green Belt.

The reasoned justification for the policy within the Allocations Plan states that the Core Strategy's overall approach is to concentrate development in the existing urban area and to constrain most forms of development on Protected Open Land. It explains that the urban area is defined as that which is not Protected Open Land or Green Belt.

The Allocations Plan continues at paragraph 5.12 that it is a general principle of national policy that the character and appearance of Protected Open Land should be protected from inappropriate development. This also supports the Council's efforts to achieve urban regeneration. It is explained that some of this land could be appropriate for development in the future, but not during the plan period up to 2026 and not without a further review of the Core Strategy.

The applicant is proposing to erect 300 dwellings on the application site. It is not considered that the proposed residential development of this site would represent limited infilling within an established housing area, would form part of (or is required) for the maintenance of existing employment, nor would require a location outside the urban area. The residential development of a greenfield site would also not be appropriate development within the Green Belt, also failing to meet category 4 of Allocations Plan Policy CG6AP.

It is therefore considered that the proposed residential development of this 'Other Protected Open Land' site would be contrary to Core Strategy Policy OA1.6 and Allocations Plan Policy CG6AP.

Other Material Considerations: Bolton's Supply of Housing

Strategic Objective 15 of the Core Strategy is, "To focus new housing in the existing urban area, especially in Bolton town centre, council-owned housing areas and in mixed-use developments on existing older industrial sites".

Policy SC1 of the Core Strategy states that the Council will identify a range of housing sites for additional provision of 694 dwellings per annum between 2008 and 2026, with at least 80% of housing development to be on previously developed land. Policy SC1.2 states that the 20% of housing on greenfield land will be provided through the Transforming Estates programme.

Bolton's Allocations Plan (2013) states that since 2008 a total of 1,754 net new dwellings have been completed leaving the Allocations Plan to make provision for 10,738 new dwellings for the period 2012-2026. Specific sites over 0.4 hectares in size have been identified for housing on the Proposals Map sufficient to accommodate 5,912 new dwellings. This is in addition to the (up to) 1,700 dwellings allocated through the Core Strategy at Horwich Loco Works. With the exception of Westbrook (the former Manchester Road college site) the Proposals Map does not identify any other specific sites within Bolton town centre, however it is anticipated that around 1,831 dwellings can be delivered within Bolton town centre on sites of 0.4 hectares and above. Housing development will also continue to come forward on sites less than 0.4 hectares that are not identified within the Proposals Map. Evidence from the Strategic Housing Land Availability Assessment (SHLAA) suggests that a small site allowance of 90 dwellings is appropriate which would yield 1,260 units over the years from 2012 to 2026. In addition Bolton has a legacy of windfall sites becoming available through the redevelopment of former built uses. It is anticipated that this will continue in line with the Core Strategy's approach to regeneration. It should also be noted that the Allocations Plan also does not identify areas for comprehensive mixed use development (housing is a key component in driving the regeneration of these areas and will provide additional housing opportunities).

As discussed above, it is considered that the proposed residential development of the application site is contrary to Policy OA3.6 of Bolton's Core Strategy and Allocations Plan Policy CG6AP, as the site is allocated as 'Other Protected Open Land' and as the proposal would constitute inappropriate development on that land.

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan (that is the Core Strategy and the Allocations Plan) *unless material considerations indicate otherwise*. The applicant is arguing in this case that there are material considerations that indicate that the residential development of application site would be appropriate and planning permission should be forthcoming; that is, they are contending that the Council has a shortage of deliverable housing land.

Paragraph 49 of the NPPF states that housing applications should be considered in the context of the presumption in favour of sustainable development. Relevant policies for the supply of housing should not be considered up-to-date if the local planning authority cannot demonstrate a five-year supply of deliverable housing sites.

Updated 2014/15 monitoring of the housing land supply position in Bolton demonstrates that there is a five-year supply of deliverable housing sites in the borough. This takes into account both the Allocations Plan Inspector's comments and national policy regarding the meeting of backlog (where completions have been below Core Strategy requirements) within the five-year period (2015-2019/20) rather than over the remaining life of the plan, the need for a 5% buffer (as required within paragraph 47 of the NPPF) and also the more realistic clearance allowance of 40 dwellings per annum. These factors taken together increase the underlying five-year requirement above that in previous reports to 5,288 dwellings (2015-2019/20). The projected supply for 2015-2019/20 is 5,417 dwellings, which equates to a 5.1 year supply. The Council's policies for the supply of housing are therefore considered to remain up-to-date as a five-year supply of deliverable housing sites can be demonstrated.

The Council's policies for the supply of housing are therefore considered to remain up-to-date as a

five-year supply of deliverable housing sites can be demonstrated.

It is therefore subsequently considered that the benefits of providing more houses on this previously undeveloped site outside the existing urban area would not outweigh the harm that would be caused to the Council's objective of focusing new housing within the existing urban area, contrary to Strategic Objective 15 and Policy OA1.6 of the Core Strategy and Allocation Plan Policy CG6AP.

Impact on Mineral Safeguarding Areas

The application site forms part of a Mineral Safeguarding Area for sandstone, surface coal, brick and clay.

Policy 8 of the Greater Manchester Joint Minerals Plan states that all non-mineral development proposals within the Mineral Safeguarding Area should extract any viable mineral resources present in advance of construction. Proposals for non-mineral development within the Mineral Safeguarding Areas that do not allow for the prior extraction of minerals will only be permitted where:

1. The need for the development outweighs the need to extract the mineral; or;
2. It can be clearly demonstrated that it is not environmentally acceptable or economically viable to extract the mineral prior to non-mineral development taking place; or
3. It can be clearly demonstrated that the mineral is either not present or of no economic value or too deep to extract in relation to the proposed development; or;
4. The development is limited or temporary and would not prevent minerals extraction taking place in the future.

Policy P4.3 of the Core Strategy states that the Council will safeguard known resources of minerals, and existing and planned infrastructure that supports mineral exploitation including facilities for manufacturing and the handling, processing and distribution of substitute recycled and secondary aggregate materials.

The applicant has submitted a desk based study of the site's potential for mineral extraction. They state that due to the proximity of residential properties this limits the safe working area with a required standoff distance of at least 200 metres. Officers from the Minerals Waste Planning Unit have concluded that the site is too small to be economically viable.

Therefore, it is considered that the proposed development would comply with Policy 8 of the GM Joint Minerals Plan.

Impact on the Highway

Policy P5 of the Core Strategy states that the Council will ensure that developments take into account [amongst other things] accessibility by different types of transport, servicing arrangements, parking (in accordance with the parking standards set out in Appendix 3), and the requirement for a Transport Assessment and Travel Plan with major trip generating developments. Policy S1.2 states that the Council will promote road safety in the design of new development.

The applicant proposes two vehicular access points to the residential scheme, the first a new access would be created off Platt Lane in the southern part. The second access point would involve the creation of an enhanced spur onto/off the existing Chequerbent roundabout in close proximity to two existing major access/egress points.

The applicant has submitted a Transport Assessment (TA) in order to substantiate development from a transport/highways perspective. This document along with the additional transport note that has been submitted has been reviewed by the Highways Authority and independently reviewed by Transport for Greater Manchester (TfGM). Acting on this the following comments/observations have

been made in relation to transport highways issues associated with the site.

The detailed comments are as follows:

-) There is a concern that the modelling that forms part of the TA does not adequately demonstrate the congestion and queuing issues associated with the highway network surrounding the site within the base-line traffic scenario. There is an issue with vehicles queuing within the circulating carriageway of Chequerbent Roundabout within the AM/PM peak periods of traffic flow. This has not been demonstrated within the Arcady analysis of the roundabout in terms of exit restriction parameters that could demonstrate the transfer of queuing to the upstream entries of the junction. This would create a more robust traffic model of the current highway situation associated with the Chequerbent Roundabout and the surrounding network.
-) There are concerns that the proposed access onto Chequerbent Roundabout will pose road safety implications to users of the public highway at that location. Although there is an existing access point onto the highway network at that location it only serves a limited number of business premises and residential dwellings. The applicant is proposing to upgrade the access to accommodate the level of vehicle movement associated with the new residential development and also re-routed traffic flows from Platt Lane. Although the applicant appears to have demonstrated that the highway improvements proposed for the existing access appear to comply with the DMRB design standards for the 'Geometric Design of Roundabouts' the Highways Authority has additional concerns. The existing access will be upgraded to a 5th arm onto the roundabout and will create an unusually tight layout at that location considering the level of observed queues blocking back from Park Road past the arm of Manchester Road East entry. The Highways Authority has concerns that the close proximity of the existing entries on the roundabout, inadequate weaving for egressing vehicles through the circulating carriageway between the Park Road arm and sub-standard visibility for vehicles across the roundabout will exacerbate the potential for road safety implications to occur associated with the increase in traffic volumes. The applicant has submitted a Type 1 Road Safety Audit in order to demonstrate potential highway issues. Scrutiny of this document raised concerns about weaving lengths and compliance with design standards, the applicant has been asked to analyse the weaving lengths but has not done so.
-) The junction at Platt Lane/Park Road suffers from capacity issues during the peak periods of traffic flow. The applicant is proposing a highway link between Platt Lane and Chequerbent Roundabout. Owing to congestion issues at this location traffic will inevitably re-route through the development as an alternative route. There is a contradiction within the TA that appears to show that the applicant is reliant on traffic re-routing on Platt Lane in order to improve capacity at the junction and reduce traffic entering Chequerbent from the Park Road arm. However, the applicant is also proposing to make this route onerous to traffic flow by the alignment of the carriageway, the introduction of traffic-calming and traffic management restrictions in the form of a weight limit. This appears to be a contradiction in terms of the rationale behind the traffic re-allocation on the highway network surrounding the site. The Highways Authority has concerns relating to potential road safety implications associated with the traffic volumes passing through the site on what would actually be classed as a residential road.
-) Analysis of the accident data over the last 5 year period (30/06/10-30/06/15) for both the Chequerbent Roundabout and the Park Lane/Platt Road revealed an high accident record over that time period both involving multiple KSI's (Killed and Seriously Injured). A blackspot analysis of both junctions revealed that Chequerbent is the 9th worst accident black-spot junction in the Bolton conurbation and that the Park Road/Platt Lane junction is ranked 25th in the borough. The

increase in traffic volumes associated with the development and the re-allocation of traffic surrounding the site has the potential to exacerbate these road safety issues at both junctions.

-) Highways also have concerns that this development may impede the ability to deliver a Westhoughton bypass at some point in the future. Whilst not currently allocated in any Council document the development site is located in the area where there has been previous consideration for a Westhoughton Bypass. It is also acknowledged that significant infrastructure improvements are required in Westhoughton to accommodate existing traffic issues and enable any future development. The applicant has left a strip of land to the east of the site as a corridor for a future bypass. In the absence of a design for a potential future bypass it is unclear at this point whether there is sufficient land reserved for a bypass, especially to the south of Chequerbent Roundabout.

The Council's Highways Engineers have raised objections to both the principle of providing a new major access point onto the Chequerbent roundabout and also the detrimental impact that the proposal would have on the existing highway network in terms of the operation capacity of the highway network and the potential impact on road safety. The current proposal is considered to be fundamentally flawed and contrary to Core Strategy policies P5 and S1.2 of the Core Strategy.

Impact on Public Rights of Way

Allocations Plan policy P8AP states that the Council will permit development proposals affecting public rights of way provided that the integrity of the right of way is retained.

The existing right of way runs from Park Road to the north through the proposed development site to Platt Lane. The main line of the footpath will be retained with a minimum width of 3 metres. The surface treatment of the new footpath would be improved to enable it to withstand the increased use which would result if the proposed development is approved. A small diversion would be necessary in the location where it currently meets Park Road. The actual line of this section is overgrown and as a result of the proposed development it is logical to rationalize the line of the footpath. This would also provide the benefits of reducing pedestrian potential conflict with the cars from the adjoining houses by providing enhanced visibility.

It is considered that the proposed development would maintain the integrity of the existing footpath network thus complying with Allocations plan policy P8AP.

Impact on Biodiversity

Policy CG1.1 of the Core Strategy states that the Council will safeguard and enhance the rural areas of the borough from development that would adversely affect its biodiversity including trees, woodland and hedgerows, geodiversity, landscape character, recreational or agricultural value.

The applicant has submitted an Environmental Statement, which includes an assessment of the Ecology Survey and Assessment, and a Tree Survey Report with the application.

Both the Greater Manchester Ecology Unit (GMEU) and the Council's Wildlife Liaison Office has confirmed that the applicant has provided a comprehensive and thorough ecological assessment of the site with not significant ecological constraints in terms of any high value ecological features. GMEU officers do state that due to the scale of the proposed development the area is significant and represents a negative impact in a local context.

It is concluded that the proposal would retain the majority of local features of ecological value e.g. existing hedgerows, trees and ponds together with significant levels of on-site mitigation.

In terms of the impact on protected species GMEU officers conclude the following:

Great Crested Newts - no evidence of GCNs;

Bats -at the time of the survey there were a small number of Pipistrelle bats using the existing hedgerows. They conclude that the proposal would potentially disrupt the continuity of existing hedgerows and some concerns over disruption of bats by lighting from the proposed residential development. It is considered that through the provision of a sensitively designed lighting scheme that the impact of lighting would be minimised/mitigated against;

Reptiles - rare occurrence within Greater Manchester - no further assessment required;

Badgers - no evidence of badgers found within the site. It is recommended that precautionary measures are taken prior to earthworks within the site a survey of the site including land within 30 metres of the development boundaries will be submitted to the LPA with findings and any mitigation required;

Barn owls - no discussion of the foraging potential of barn owls was discussed. However, as the majority of hedgerows are to be retained the impact of the proposal is considered to be neutral;

Nesting birds - significant areas of potential bird nesting habitat will be lost through the loss of hedgerows and scrub. No short term mitigation is proposed. In the longer term replacement hedgerow planting with additional trees and gardens will more than compensate for the loss of habitat. It is recommended that no clearance of trees, hedgerows or shrubs shall occur within the bird breeding season;

Invasive species - Japanese Knotweed, Himalayan Balsam, rhododendron and Cotoneaster are present on site. It is recommended that a method statement be provided by the applicant prior to earthwork's being undertaken for the eradication of these invasive species.

Without mitigation or compensation the development would result in a net loss of biodiversity contrary to guidance contained within NPPF. Significant tree planting and hedge planting as proposed would more than mitigate for the losses. The mitigation also involves the creation of an additional pond and the enhancement of the two retained ponds. Mitigation for the loss of over 10 hectares of grassland is through the creation of around 1.5 hectares of species rich grassland.

Officers from GMEU and the Council's Greenspace Management team remain satisfied that the proposal provides sufficient mitigation to offset the loss of the site from an ecological perspective. This conclusion is on the basis of the deliverability of a high quality product which is maintained. This would be controlled through the provision and implementation of an acceptable management regime for the site.

The Council's Tree Officers (TWO) have confirmed that the trees to be removed as part of the proposal are not excessive and would be replaced by new tree planting. The TWO has recommended that a number of trees be retained which would assist in providing some initial screening to adjoining properties and also the replacement of a number of trees which were considered to be unsuitable. The applicant has amended the proposed plans to comply with the TWO's requirements.

It is therefore considered, subject to the retention of the pond and the hedgerows and the proposed mitigation as outlined above that the proposed development would safeguard the biodiversity of the site, compliant with Policy CG1.1 of the Core Strategy.

Impact of Loss of Agricultural Land

Paragraph 112 of the NPPF states that local planning authorities should take into account the economic and other benefits of the best and most versatile agricultural land. Where significant development of agricultural land is demonstrated to be necessary, local planning authorities should seek to use areas of poorer quality land in preference to that of a higher quality.

An extract from Natural England's Agricultural Land Classification Map North West Region submitted by the applicant shows that around 92% of the application site is of Grade 3a agricultural land, which is of a moderate quality, with 6.4% of the site being of poor quality agricultural land with the remainder of the land not being agricultural use.

It is therefore considered that the site does not fall within the category of the best and most versatile agricultural land.

Impact on Flooding

Policy CG1.5 of the Core Strategy states that the Council will reduce the risk of flooding in Bolton and other areas downstream by minimising water run-off from new development and ensuring a sequential approach is followed, concentrating new development in areas of lowest flood risk. Policy CG2.2c states that all proposals for five or more residential units should demonstrate the sustainable management of surface water run-off from developments. On greenfield sites the rate of run-off should be no worse than the original conditions before development.

The Council's Drainage and Flood Risk team have no objections to the proposed development subject to the provision of a sustainable urban drainage system within the site which seeks to ensure that surface water run-off from the site is no greater than under the existing site conditions. They also confirm that there is a culvert which runs through the site which would need diverting.

The Environment Agency has also confirmed that they have no objections to the proposed development subject to satisfactory remediation of the site as necessary.

It is considered that subject to the provision of appropriate conditions the proposal would comply with both Core Strategy policy CG1 and CG2.

Impact on Local Infrastructure

Policy IPC1 of the Core Strategy states that the Council will seek to ensure that developers make reasonable provision or contribution towards the cost of appropriate physical, social and green infrastructure required by the proposed development and/or to mitigate the impact of that development. In doing so, the Council will ensure that a scheme is made acceptable in planning terms and achieves the objectives of sustainable development. In determining planning applications the Council will apply a threshold of 15 dwellings in assessing whether planning contributions will be required from proposed housing developments.

The proposal for 300 dwellings in this location generates the following Section 106 requirements:

- Γ Affordable housing: 105 affordable units on site, with 79 (75%) being for social/affordable rent at 60% discount off open market value and 26 (25%) being for intermediate housing at 30% discount off open market value.
- Γ Education: £371,333.98 to be used at the local primary schools in Westhoughton to assist with pupil place provision.
- Γ Public open space: on site provision which would be maintained by the developer.
- Γ Health: £67,200
- Γ Public art: 1% of total development costs - the applicant has commented that they would wish to make this provision on site.

The applicant has stated within their submissions that they are committed to providing s.106 contributions as long as they comply with Core Strategy policy IPC1 and also the Community Infrastructure Levy regulations.

It is considered that the above requirements would meet both the requirements of Core Strategy IPC1 and CIL regulations and if Members are minded to approved the proposal that the Council enters into a s.106 agreement with the applicant / developer to ensure the on / off contributions are provided.

Impact on the Character and Appearance of the Area

Policy CG3 of the Core Strategy states that the Council will conserve and enhance local distinctiveness, ensuring development has regard to the overall built character and landscape quality of the area, will require development to be compatible with the surrounding area in terms of scale, massing, grain, form, architecture, street enclosure, local materials and landscape treatment, and will maintain and respect the landscape character of the surrounding countryside and its distinctiveness.

Policy OA3.7 refers specifically to developments in Westhoughton and states that the Council will conserve and enhance the character of the existing landscape and physical environment whilst Policy OA3.8 states that the Council will ensure that regard is had to the character of farm complexes, folds, vernacular cottages and the wider open landscape.

The application site falls within the Agricultural Coal Measures Landscape Character type which is characterised by a landform which is gently rolling, characterised by poor grade agricultural farmland/ fields separated by hedgerows. The applicant maintains that the character of the site/area would experience temporary to medium term moderate to slight adverse impacts as landscape elements are removed. Mitigation in the form of the proposed planting and landscape treatments will ensure would assist in strengthening the landscape character of the area.

The comments of the Council's Landscape Architects will be reported by way of the Supplementary Information List/Note.

Impact of the Overall Design of the Proposed Development

Core Strategy policy CG3 states that the Council will expect proposals to display innovative, sustainable designs that contribute to good urban design whilst conserving and enhancing local distinctiveness.

Notwithstanding the potential loss of open land per se, it is considered that the proposed residential development has been well designed to provide a high quality residential environment which from a landscape perspective has sought to retain key features of the existing landscape setting e.g. the existing hedgerows and provide distinct development parcels which are bisected by retained / improved areas of open space, amenity areas and play facilities.

It is noted that the Council's Landscape architects consider that the proposal is well designed. Officers from Greater Manchester Police / Design for Security comment that the layout has been positively designed to ensure houses front public space. They require clarity over the proposed boundary treatment, strong definition of public and private space and to ensure all car parking is overlooked by the properties to which they relate.

The applicant has provided clearer plans which identify that properties do overlook the parking which they own and makes provision for strong definition of public/private space. Where rear parking areas are used the side access is gated to promote security.

It is considered that the proposed development complies with Core Strategy policy CG3.

Impact on the Amenity of Neighbouring Residents

Policy CG4 of the Core Strategy states that the Council will ensure that new development is compatible with surrounding land uses and occupiers, protecting amenity, privacy, safety and security. SPD General Design Principles sets out the Council's minimum interface distance requirements between new dwellings and neighbouring dwellings.

The proposed site layout illustrates that the Council's recommended minimum interface distance of 21 metres between fronts of dwellings can be achieved both within the development and between the proposed and neighbouring dwellings. Whilst it is noted that a number of local residents have raised concerns regarding disruption which may be caused during the construction process it is considered that these issues would be dealt with by other control regimes e.g. environmental health.

The proposal complies with policy.

Conclusion

For the reasons discussed above it is considered that the proposed residential development of the site would represent inappropriate development of 'Other Protected Open Land' and the benefits associated with developing the site for housing would not outweigh the harm that would be caused to the Council's strategic objective of focusing new housing development within the existing urban area. It is also considered that the proposed development by virtue of the proposed vehicular access points would be detrimental to highway safety and the existing highway network does not have the capacity to accommodate further development without further improvements to the existing highway network. The applicant has provided insufficient information to enable the LPA to assess whether the proposal would prejudice a proposed Westhoughton bypass if required. The proposed development would also increase traffic flow at already congested junctions to the detriment of highway safety and the free flow of traffic.

Members are therefore recommended to refuse this application for the reasons listed above.

Representation and Consultation Annex

Representations

Letters:- a total of 149 representations (2 comment letters and 147 objections). As a result of the Drop In Session which took place on 10/09/15 a total of 25 comment sheets were completed, 20 of which were fully complete i.e. name and address filled in. All comments were objections. The following issues were raised:

- \ Highways – existing roads are heavily congested - the road network cannot cope with further development / vehicles, Platt Lane is heavily congested and the Chequerbent roundabout is dangerous.
- \ Westhoughton doesn't require any further housing;
- \ Construction noise and disturbance if approved;
- \ Loss of wildlife and countryside/greenspace;
- \ Need for social housing not private housing;
- \ Inadequate infrastructure in Westhoughton e.g. schools, doctors etc;
- \ Impact on drainage / water table;
- \ The site is not allocated for housing;
- \ Will devalue properties;
- \ Part of a larger plan for 1600 dwellings - if approved the precedent will be set;
- \ Light and noise pollution;
- \ Loss of Green Belt land,
- \ Result in urban sprawl;
- \ Should use existing vacant/empty homes;
- \ Poor condition of local roads will be made worse;
- \ There is a need for a bypass of Westhoughton to improve the quality of life;
- \ Over provision of car parking within the proposal;
- \ Contravenes right to respect family and private life as set out in the Human Rights Act;
- \ Requirement for an archaeological survey;
- \ Impact on farmers who currently rent the land;
- \ Platt Lane is a bottleneck;
- \ Impact on existing drainage into the site;
- \ Footpath at junction with Park Road needs amending to enhance safety;
- \ Will result in a loss of privacy;
- \ Will compromise the construction of the Westhoughton by pass.

Petitions:- one petition has been received with a total of 1643 names contained within it stating that the proposed development is unnecessary and will have a considerable impact on the already congested roads of Westhoughton.

Town Council:- raise objection to the proposal.

Elected Members:- Councillor Graham has requested that the application be the subject to an advanced site visit.

Consultations

Advice was sought from the following consultees: Westhoughton Town Council, the Ramblers Association, Open Spaces Society, Peak and Northern Footpath Society, Coal Authority, Trees and Woodland Officer, Highways Engineers, Economic Strategy, Pollution Control, Landscape Architects, Greenspace Management, Strategic Development Unit, Housing Strategy, Primary Care Trust, Design for Security (Greater Manchester Police), United Utilities, Environment Agency, Greater Manchester Ecology Unit and Greater Manchester Archaeological Advisory Service.

Planning History

Planning application for the development of the land for residential purposes was withdrawn in June 1984 (Ref: 11942/79).

An outline planning application for the erection mixed use development was withdrawn in June 1999. Outline planning permission was refused in 1999 for business use (B1, B2 and B8) of the land. The proposal was dismissed on appeal in November 1999.

Recommendation: Refuse

Recommended Conditions and/or Reasons

1. The proposed residential development of the site would represent inappropriate development of 'Other Protected Open Land' in that it would not fall within any of the categories listed within Bolton's Allocations Plan Policy CG6AP. The benefits associated with developing the site for housing would not outweigh the harm that would be caused to the Council's strategic objective of focusing new housing in the existing urban area, contrary to Strategic Objective 15 and Policy OA3.6 of Bolton's Core Strategy and Bolton's Allocations Plan Policy CG6AP.
2. The proposed enhanced vehicular access point onto Chequerbent roundabout would by virtue of an increased traffic volume at this location and the proposed layout and design of the proposal have a detrimental impact on the operational capacity of the highway network exacerbating existing congestion and likely to be detrimental to highway safety contrary to Core Strategy policy P5 and S1.
3. The proposed residential spine road through the development is likely to result in increased traffic using the proposed new residential road through the site to the detriment of road safety contrary to Core Strategy policy P5 and S1.
4. The application would represent piecemeal development of site which form part of a larger area of safeguarded land that has potential to be considered for housing in the longer term through Local Plan review. The applicant has submitted insufficient information to ensure that the proposed development would not preclude the development of land to the south contrary to Core Strategy policy P5, S1, SC1 and CG6AP by insufficient land being given over to a future link road.

GENERAL NOTE
DO NOT SCALE FROM THIS DRAWING

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St Thomas
C of E Primary School

NORTH
SCALE BAR 1:1000
0 10 20 30m

- KEY
- Red line boundary
 - Existing trees to be retained
 - Existing hedge to be retained
 - Public Right of Way
 - Proposed tree planting
 - Proposed hedge planting
 - Proposed native woodland mix
 - Proposed ornamental shrub and perennial mix
 - Proposed marginal mix
 - Proposed wildflower grassland
 - Proposed amenity grass
 - Proposed block paved surface
 - Proposed 1.2m high estate railings
 - Proposed 1.2m high gate
 - Proposed bollards

Rev	Description	Date	By	Chk'd
D	UPDATED RED LINE	28/07/15	RK	SR
C	UPDATED RED LINE	26/06/15	RK	SR
B	COLOURED VERSION	20/05/15	CO	SR
A	GENERAL AMENDMENTS	15/05/15	BK	SR
/	DRAFT FOR COMMENT	12/05/15	CO	SR

purpose
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client
PERSIMMON HOMES

project
LEE HALL, WESTHOUGHTON

drawing title
LANDSCAPE MASTERPLAN

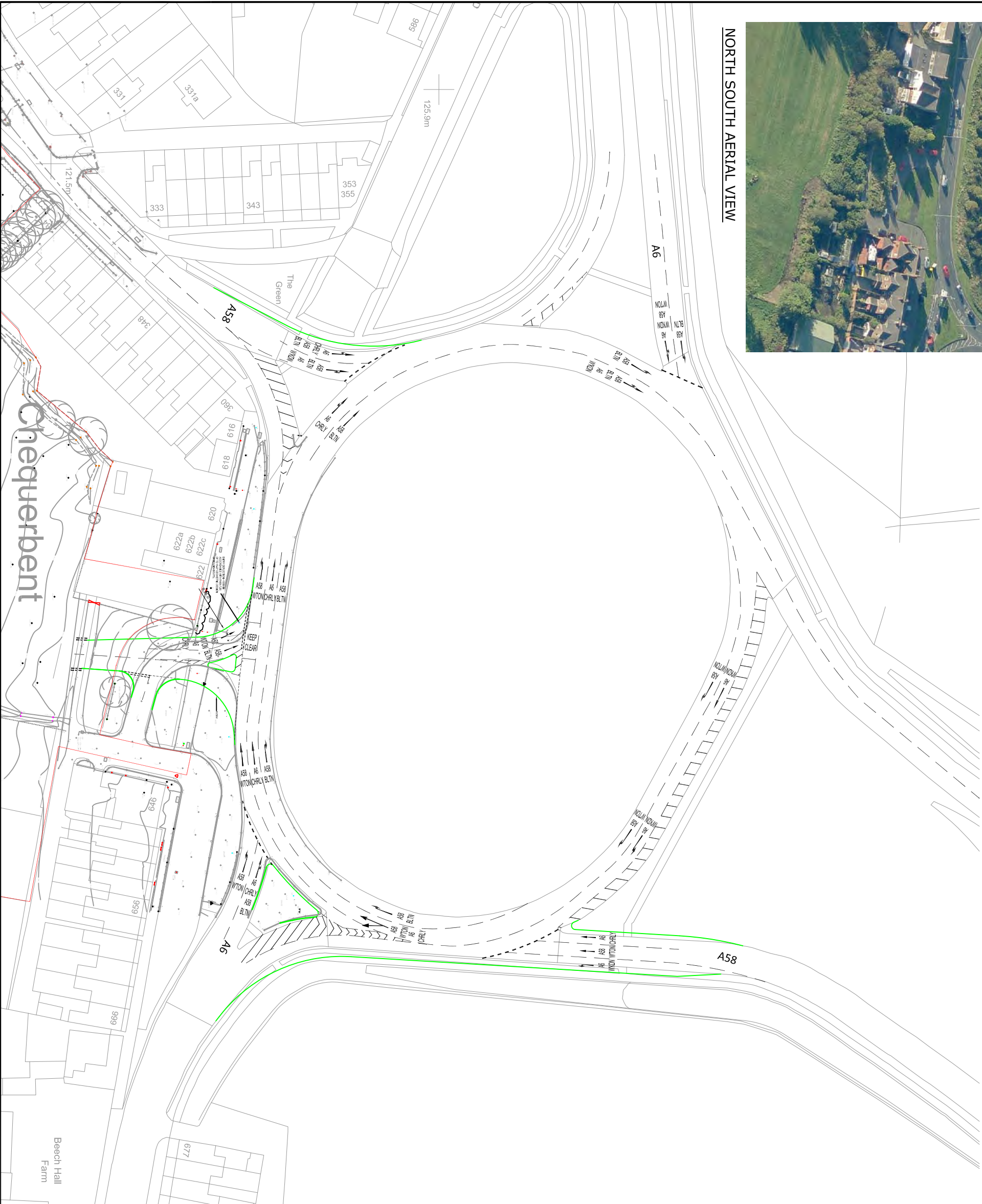
scale @ A1 1:100
date 15/05/2015
dm CO chkd SR

1244/100
D

Original paper size (A1) 841x 594mm



NORTH SOUTH AERIAL VIEW



NOTES

— LAND BOUNDARY

REV.	DETAILS	DRAWN	CHECKED	DATE
A	LAYOUT TRANSFERRED ONTO TOPO MAPPING	JC	TSB	JUL 15
1				

CLIENT:

PERSIMMON HOMES

PROJECT:

LEE HALL, WESTHOUGHTON

DRAWING TITLE:

PROPOSED SITE ACCESS OFF
CHEQUERBENT ROUNDABOUT

SCALES:

1:1000 @ A3

DRAWN:	MC	CHECKED:	MC	DATE:	NOV 14
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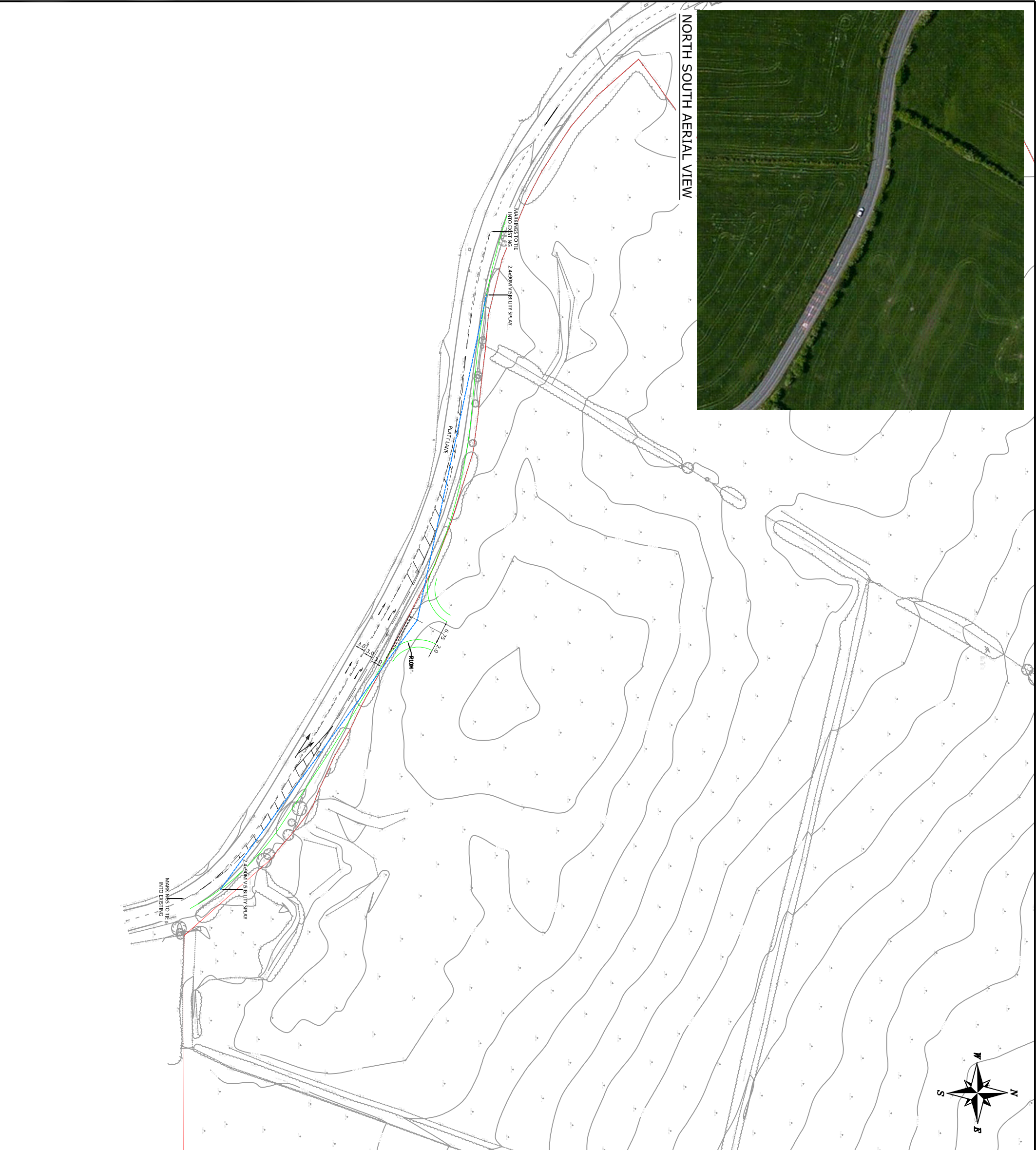


DRAWING NUMBER:

0624-F06

REVISION:

A



NOTES

— LAND BOUNDARY

REV.	DETAILS	DRAWN	CHECKED	DATE
B	ACCESS LOCATION AMENDED TO SUIT ARCHITECT'S LAYOUT	JC	TSB	APR 15
A	GHOST ISLAND RIGHT TURN LANE ADDED	JC	TSB	JAN 15

CLIENT: PERSIMMON
HOMES/HARCOURT DEVELOPMENTS

PROJECT: LEE HALL, WESTHOUGHTON

DRAWING TITLE: PROPOSED GHOST ISLAND
RIGHT TURN JUNCTION, PLATT
LANE, WESTHOUGHTON

SCALES: 1:1000 @ A3

DRAWN: JC	CHECKED: TSB	DATE: APR 14
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croft
Transport Solutions

DRAWING NUMBER: 0624-F01

REVISION: B



Key plan



Elevation A: House types along the primary road



Elevation B: House types in the south west of the site fronting on to public open space

Application number 94721/15



Development & Regeneration Dept
Development Management Section

Town Hall, Bolton, Lancashire, BL1 1RU
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Bolton Council

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Date of Meeting: 22/10/2015

Application Reference: 94721/15

Type of Application: Full Planning Application

Registration Date: 04/08/2015

Decision Due By: 28/09/2015

**Responsible
Officer: Paul Bridge**

**Location: BOLTON OPEN GOLF CLUB, LONGSIGHT LANE, BOLTON, BL2
4JY**

**Proposal: THE ERECTION OF 7 DWELLINGS FOLLOWING DEMOLITION
OF THE EXISTING CLUB HOUSE AND DRIVING RANGE
CANOPY.**

Ward: Bradshaw

Applicant: .

Agent : Sedgwick Associates

Officers Report

Recommendation: Refuse

Background to Proposal

Members will recall that planning application 91968/14 was refused by Planning Committee on the 28th August 2014 for the following reason:-

"The proposed development, by virtue of its use, siting and scale, represents inappropriate development within the Green Belt and would harm the purposes and openness of the Green Belt, contrary to section 9 of the National Planning Policy Framework and saved Unitary Development Plan Policies G1 and G2".

The applicant appealed the decision to the Planning Inspectorate (Ref: APP/N4205/W/15/3003728) who subsequently dismissed the appeal in a decision dated 4th June 2015. The Inspector concluded that *"...on balance the harm by reason of inappropriateness, and any other harm, is not clearly outweighed by other considerations. Thus the very special circumstances needed to justify the development do not exist"*.

This current proposal seeks to address those reasons for refusal by amending the proposal as follows:-

-) The quantum of development has been reduced by one dwelling and the proposed development has been made more compact.
-) The siting of the proposed houses has been moved away from both Longsight Lane and the existing clubhouse allowing the proportion of new amenity space to be increased from 36% of the site to 48%.

-) The design approach of the development has been amended from a scheme with the appearance of suburban development to one that reflects a 'fold'.

Proposal

Planning permission is sought for the demolition of the existing golf club house and driving range canopy and the erection of 7 dwellings. The proposed site layout of the dwellings has an 'L' shaped configuration which will overlook a shared courtyard area. The proposed schedule of accommodation is as follows:-

unit 1 - 4 beds - 1500 sq.ft.
unit 2 - 4 beds - 1500 sq.ft.
unit 3 - 4 beds - 1500 sq.ft.
unit 4 - 4 beds - 1500 sq.ft.
unit 5 - 4 beds - 1850 sq.ft.
unit 6 - 3 beds - 1250 sq.ft.
unit 7 - 4 beds - 1750 sq.ft.

The proposed character of the scheme exhibits a detached double fronted 'farmhouse' of traditional appearance and finish in close proximity to a 1 and a half storey house and a compact range of 2 storey 'mews' houses. Each dwelling will have its own enclosed rear garden and the boundary treatments would consist of a mixture of hedgerows, post and rail fences and a stone wall.

Nineteen car parking spaces will be provided within the site by a mixture of integral garages and spaces within the site, which would be accessed from Longsight Lane by a private drive.

Site Characteristics

The application site is at the end of the motor-able section of Longsight Lane, approaching 100 metres south of the nearest existing dwellings. Much of the 0.26 hectare site is a hard-surfaced car park. There is a 2 storey former extended golf clubhouse at the southern edge of the site, now boarded up and in a state of disrepair. A driving range canopy occupies much of the western side of the site, though this has been reduced very largely to a skeletal timber framework without cladding.

The site is on the edge of the Green Belt that takes in open land along the valley of Bradshaw Brook, which enters the urban area from open countryside to the north and is also allocated within a Wooded/Rural Valley Landscape Character Area.

A public footpath runs to the south of the club house (Turton Bradshaw South 043).

Policy

National Planning Policy Framework (NPPF)

Core Strategy (CS) Policies: P5 Accessibility and Transport; S1 Safe Bolton; CG1.1 Biodiversity in Rural Areas; CG1.5 Reduce Risk of Flooding; CG2 Sustainable Design and Construction; CG3 The Built Environment; CG4 Compatible Uses; SC1 Housing; OA5 North Bolton.

Allocations Plan Policies: CG7AP Green Belt

SPD Accessibility, Transport and Safety; SPD General Design Principles

Analysis

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be

determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the National Planning Policy, Development Plan and then take account of other material considerations.

It is considered that the main issue in the determination of the application is:-

Γ impact on the Green Belt

- (i) whether the proposal would be inappropriate development for the purposes of the National Planning Policy Framework and development plan policy;
- (ii) whether any other harm would arise with respect to the Green Belt; and
- (iii) if there is inappropriate development, whether the harm by reason of inappropriateness, and any other harm, is clearly outweighed by other considerations, so as to amount to the very special circumstances needed to justify the development.

Impact on the Green Belt

Whether there would be inappropriate development

National Green Belt policy is contained within section 9 of the National Planning Policy Framework (NPPF). It states that the fundamental aim of Green Belt policy is to prevent urban sprawl by keeping land permanently open; the essential characteristics of Green Belts are their openness and their permanence.

Paragraph 87 confirms that inappropriate development is, by definition, harmful to the Green Belt and should not be approved except in very special circumstances. Paragraph 88 goes on to state that very special circumstances will not exist unless the potential harm to the Green Belt be reason of inappropriateness, and any other harm, is clearly outweighed by other considerations.

The NPPF advises that local planning authorities should ensure that substantial weight is given to any harm to the Green Belt. Paragraph 89 of the NPPF states that local planning authorities should regard the construction of new buildings as inappropriate in Green Belt. However, exceptions to this are [amongst other things] limited infilling or the partial or complete redevelopment of previously developed sites (brownfield land), whether redundant or in continuing use, which would not have a greater impact on the openness of the Green Belt and the purpose of including land within it than the existing development. It is considered that paragraph 89 of the NPPF is relevant to the application proposal, as the proposed erection of seven new dwellings would constitute the redevelopment of a previously developed site within the Green Belt.

Policy CG7AP in the Allocations Plan generally reflects the wording of the NPPF in this respect.

The Local Planning Authority, the applicant and the Planning Inspector are/were all in agreement that the application site is considered to be a previously developed site within the Green Belt that is no longer in use. The redevelopment of the site is considered acceptable in principle, which was recognised by the Local Planning Authority on granting planning permission to convert and extend the former clubhouse to create 6 dwellings (19 December 2011, Ref APP/N4205/A/11/2154668).

The Inspector, in his decision to dismiss the previous appeal, defined "openness" to primarily mean the absence of buildings. The existing footprint of the club house and the driving range canopy has a gross floor area of approximately 396sqm. This current proposal would be sited in a different location than the existing clubhouse and would have a footprint of approximately 1007sqm (based on the schedule of accommodation figures provided by the applicant, which results in an increase of approximately 611sqm).

This current proposal (as with the previously refused scheme) would take up more of the site than the existing built development. The proposal would be in the form of a two storey detached double fronted 'farmhouse', a 1 and a half storey house and a range of 2 storey 'mews' houses compared to the existing golf clubhouse, which is only partly two storey. The proposal would undoubtedly involve a greater quantum of built development than presently exists on the site and openness would be therefore be significantly reduced. The Inspector in paragraph 9 of his decision acknowledged that much of existing site is covered by a car park, and is still 'open' in appearance. Whilst the current proposal has increased the amount of amenity space of the total site area from 36% to 48%, it would still appear less open than it does at present. He also stated that the determinative consideration in terms of Green Belt openness would be the amount of built development. This development would significantly increase the amount of built development within the site and correspondingly reduce the openness, contrary to one of five the purposes which green belt land serves, namely to assist in safeguarding the countryside from encroachment.

In conclusion, the current proposal would have a greater impact on the openness of the Green Belt and on one purpose of including land within it than the existing development. The proposal would be inappropriate development for the purposes of the National Planning Policy Framework and Policy CG7AP in the Allocations Plan.

Any other harm to the Green Belt

The proposal would remove the derelict club house and driving range which are visually unattractive and currently detract from the appearance of the site and wider surrounding area. It is acknowledged that the redevelopment of the site would offer significant improvements in terms of the appearance of the site and views southwards along Longsight Lane. However, the fact remains that proposal (an inappropriate development in the Green Belt) would replace a "not inappropriate" facility for outdoor recreation.

The site is surrounded by open Green Belt on three sides and by protected open land to the east. Whilst the applicant has moved the siting of the proposed dwellings away from both Longsight Lane and the existing clubhouse, they would still be well removed from the existing built up area to the north and would still be deemed to intrude on the open landscape especially when viewed from the more distant higher ground to the east.

In conclusion, whilst the proposal would improve the current appearance of the Green Belt, it would still detract from its character as an area of open land in close proximity to the wider built up area.

Whether there are any special circumstances

Paragraph 88 of the NPPF says that substantial weight is to be given to any harm to the Green Belt. It further makes clear that inappropriate development, which the proposal is considered to be, should not be approved except in very special circumstances. "Very special circumstances" will not exist unless the potential harm to the Green Belt by reason of inappropriateness, and any other harm, is clearly outweighed by other considerations. The Planning Inspector in his decision stated that "...such considerations typically involve some public benefit arising from a proposal, not simply what is argued to be a low level of harm".

The applicant in support of the proposal has stated that the development would contribute towards the shortfall of housing currently being built on previously developed land, would fulfil the role of sustainable development and would improve the appearance of a visually unattractive site. Whilst this is acknowledged, the applicant has not provided any evidence why other sustainable housing schemes could not be brought forward within the borough or why the release of Green Belt land is required to meet the adopted target for providing housing on previously developed land (as stated by the Inspector in paragraph 18 of his decision).

Therefore whilst there would be some benefits of the development, in respect of an improved landscape, visual amenity and biodiversity, these are still not considered substantial enough when compared to the weight of harm to the openness and character of the Green Belt and harm by reason of inappropriateness.

Other Matters

The Council's Highways Engineers have recommended that the proposed site layout be amended to show a 4.8 metre carriageway width on Longsight Lane and that a swept path analysis be submitted to demonstrate that a large refuse vehicle would be able to enter, turn and leave the site in forward gear. Engineers do not raise an objection to the proposed development subject to the submission of acceptable amended plans and request that a number of conditions be attached to any approval.

Public Rights of Way TBS043 and TBS041 run through the application site. The Council's Public Rights of Way Officer has stated that the proposed layout plan records the route of TBS043 along the wrong alignment and therefore it must be retained along its existing legal line. The Officer advises that the footpaths must not be damaged, altered or obstructed either during or as a result of the development.

Greater Manchester Ecology Unit have confirmed that the existing buildings on site are unlikely to be significant roosting sites, but recommend that further surveys for bats be conducted prior to any demolition works.

Conclusion

It is concluded that the harm on the Green Belt by the proposed development by reason of its inappropriateness is not still not clearly outweighed by other considerations. The very special circumstances needed to justify the development still do not exist. It is therefore considered that the proposal is contrary to section 9 of the National Planning Policy Framework and Bolton's Allocations Plan Policy CG7AP. Members are therefore recommended to refuse this application.

Representation and Consultation Annex

Representations

Letters:- 20 letters of objection from 17 different households have been received from local residents.

These letters raise the following concerns:

- Γ The site is within Green Belt;
- Γ Residential development is contrary to Green Belt policy;
- Γ There is enough available brownfield housing land in Bolton without this site;
- Γ There is no need for additional housing in the vicinity of Longsight Lane;
- Γ Shortage of primary school places;
- Γ Increased traffic and traffic congestion
- Γ The land should only be enhanced for recreational purposes;
- Γ Bats have been seen in the vicinity;
- Γ Visual impact;
- Γ The Planning Inspectorate has recently dismissed an appeal for housing on this site.
- Γ This current proposal is not materially different than what was previously refused.
- Γ Covenants restrict the use of the site to golfing purposes.

Elected Members:- A letter of objection has been received from Councillor Hall. The objection is in relation to the site being located within the Green Belt and as no special circumstances exist.

Consultations

Advice was sought from the following consultees: Highway Engineers, Economic Strategy, Strategic Development Unit, Pollution Control Officers, Public Rights of Way Officer, Landscape Officers, Greater Manchester Ecology Unit, Ramblers Association, Open Spaces Society and Peak and Northern Footpaths Society.

Planning History

A planning application which proposed the conversion of the clubhouse to six dwellings, together with extensions, a new access road, provision of 12 car parking spaces, boundary fencing and diversion of a public footpath, was refused by Bolton Council in May 2011 (Ref: 85729/11).

An appeal against the refusal of application 85729/11 was allowed by the Planning Inspectorate (Ref: APP/N4205/A/11/2154668).

An outline planning application for the erection of 8 dwellings with 16 car parking spaces together with conversion and extension of the clubhouse into 5 dwellings with 10 car parking spaces, and the construction of a new golf club course reception with 55 car parking spaces was refused by Bolton Council in July 2010 (Ref: 84326/10) .

An outline application for the demolition of the club house followed by the erection of 16 dwellings and 6 apartments together with 26 additional car parking spaces was withdrawn in March 2010 (Ref: 83274/09).

The use of agricultural land and farm buildings as a golf course with club house, new driving range, maintenance building and car parking was approved by Bolton Council in June 1993 (43117/93).

An application 91968/14 for the erection of 8 dwellings following demolition of the clubhouse was refused by the Council on 3 September 2014 and the subsequent appeal was dismissed on 4 June

2014.

Recommendation: Refuse

Recommended Conditions and/or Reasons

1. The proposed development, by virtue of its use, siting and scale, represents inappropriate development within the Green Belt and would harm the purposes and openness of the Green Belt, contrary to section 9 of the National Planning Policy Framework and Bolton's Allocations Plan Policy CG7AP.

general notes:
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site view from entrance



site view from public footpath



new site access

Rev. Date By Description

Client:

K RUTTLE

mck associates limited
architecture | building surveying | urban design

burnaby villa ■ 48 walling street road ■ fulwood ■ preston ■ pr2 8bp
tel: 01772 774510 fax: 01772 774511 email: mck@mckassociates.co.uk

Project:
PROPOSED DEVELOPMENT
HARWOOD GOLF CLUB

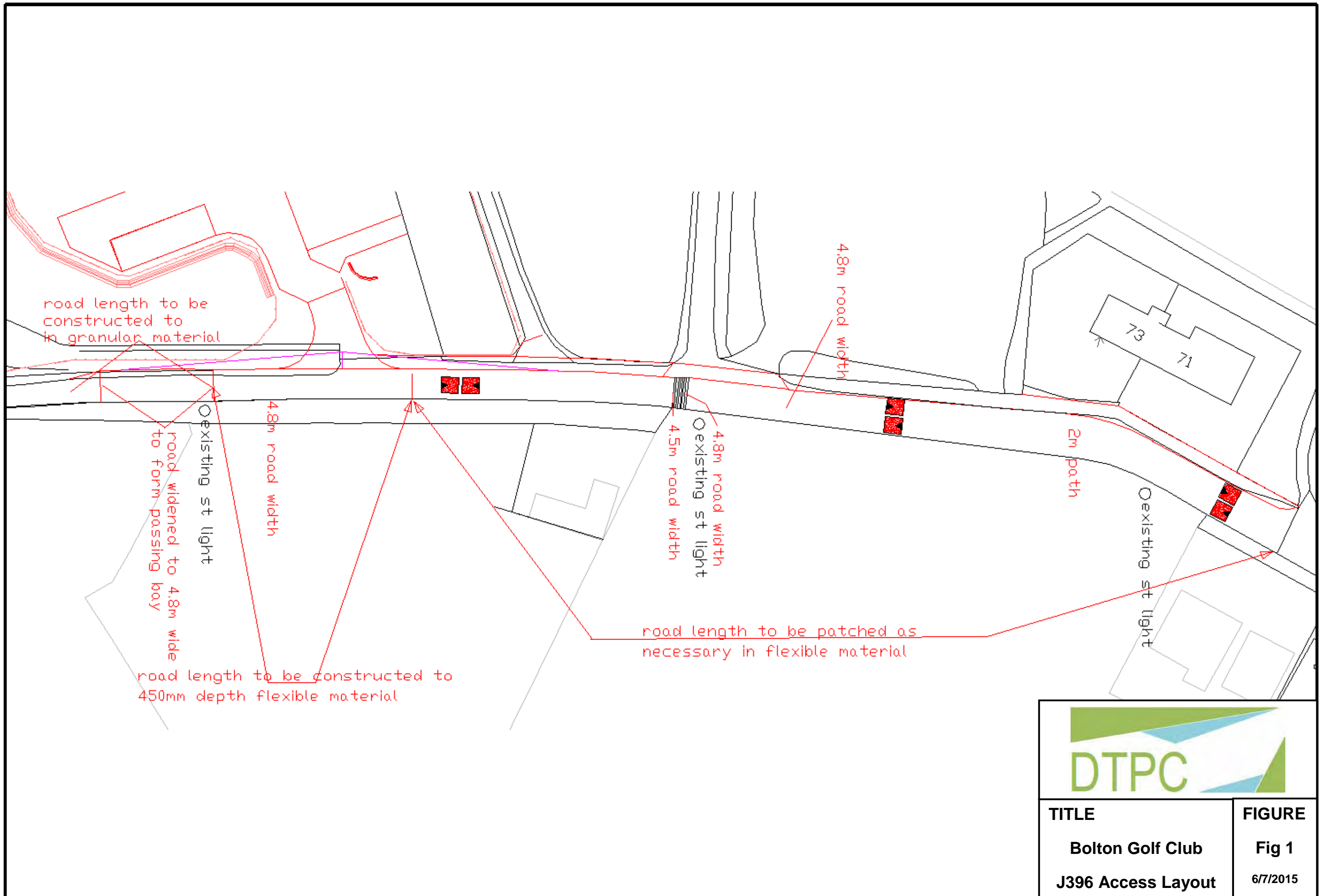
Drawing Title:

3D VIEWS OF THE SITE

Drawn: ps	Checked:	Scale: NTS	Date: JUNE 2015
Job No: 14-034	Drawing No: 1025	Rev:	

A1

PLANNING



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Schedule of Accommodation

unit 1 - 4 beds - 1500 sq.ft.
unit 2 - 4 beds - 1500 sq.ft.
unit 3 - 4 beds - 1500 sq.ft.
unit 4 - 4 beds - 1500 sq.ft.
unit 5 - 4 beds - 1850 sq.ft.
unit 6 - 3 beds - 1250 sq.ft.
unit 7 - 4 beds - 1750 sq.ft.

SITE AREA = 5800 sq.m.
(1.43 acres approx.)

Rev. Date By Description

Client:

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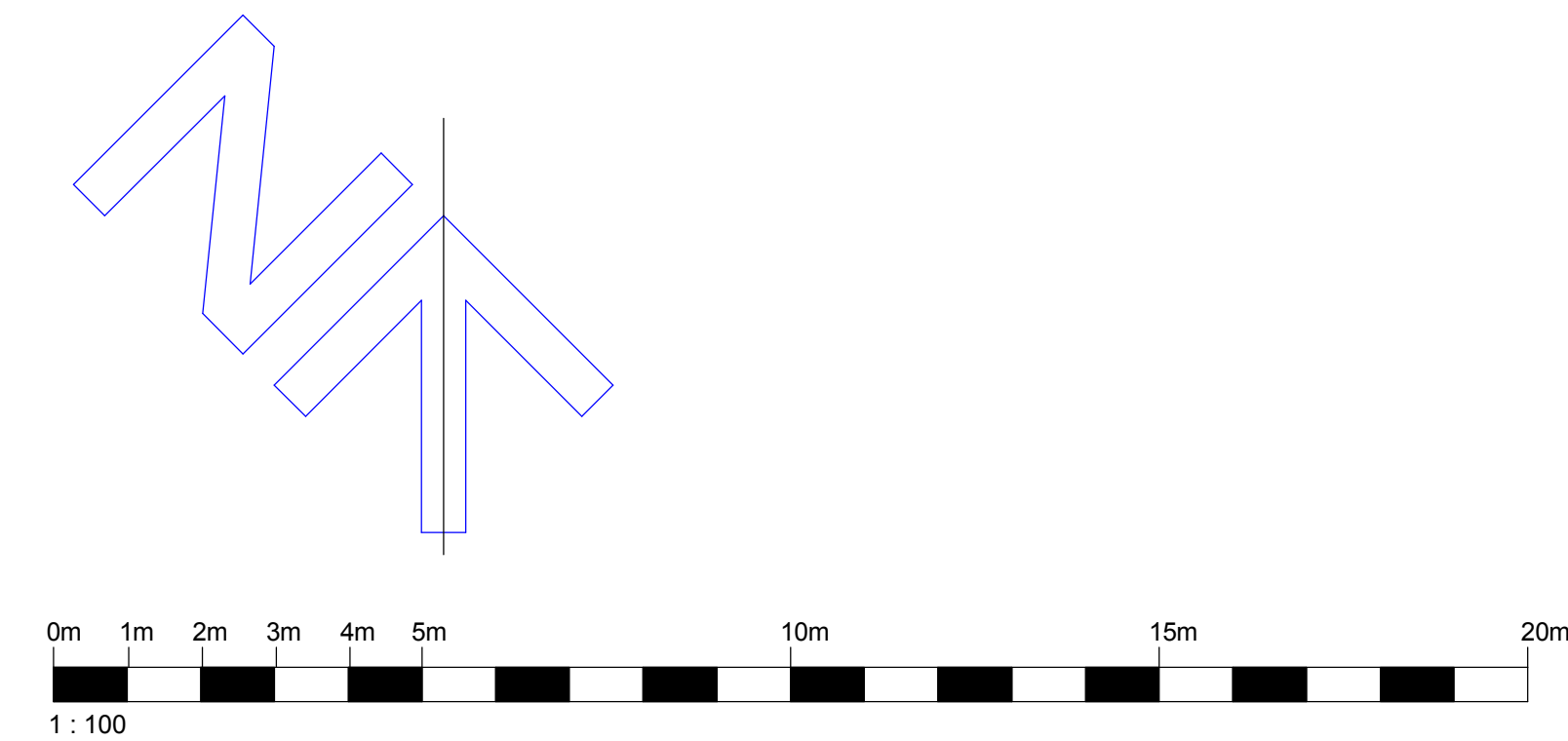
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Project:
PROPOSED DEVELOPMENT
HARWOOD GOLF CLUB

Drawing Title:

FIRST FLOOR LAYOUT

Drawn: ps	Checked:	Scale: 1:100 (A1)	Date: JUNE 2015
Job No: 14-034	Drawing No: 1021	Rev:	





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Rev.	Date	By	Description
Client:			

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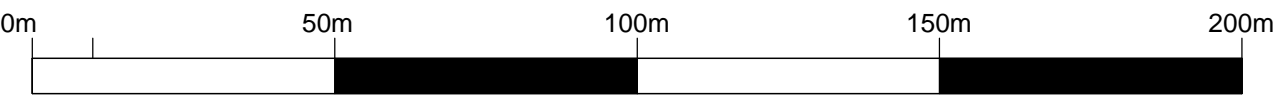
Project:
PROPOSED DEVELOPMENT
HARWOOD GOLF CLUB

Drawing Title:
GROUND FLOOR LAYOUT

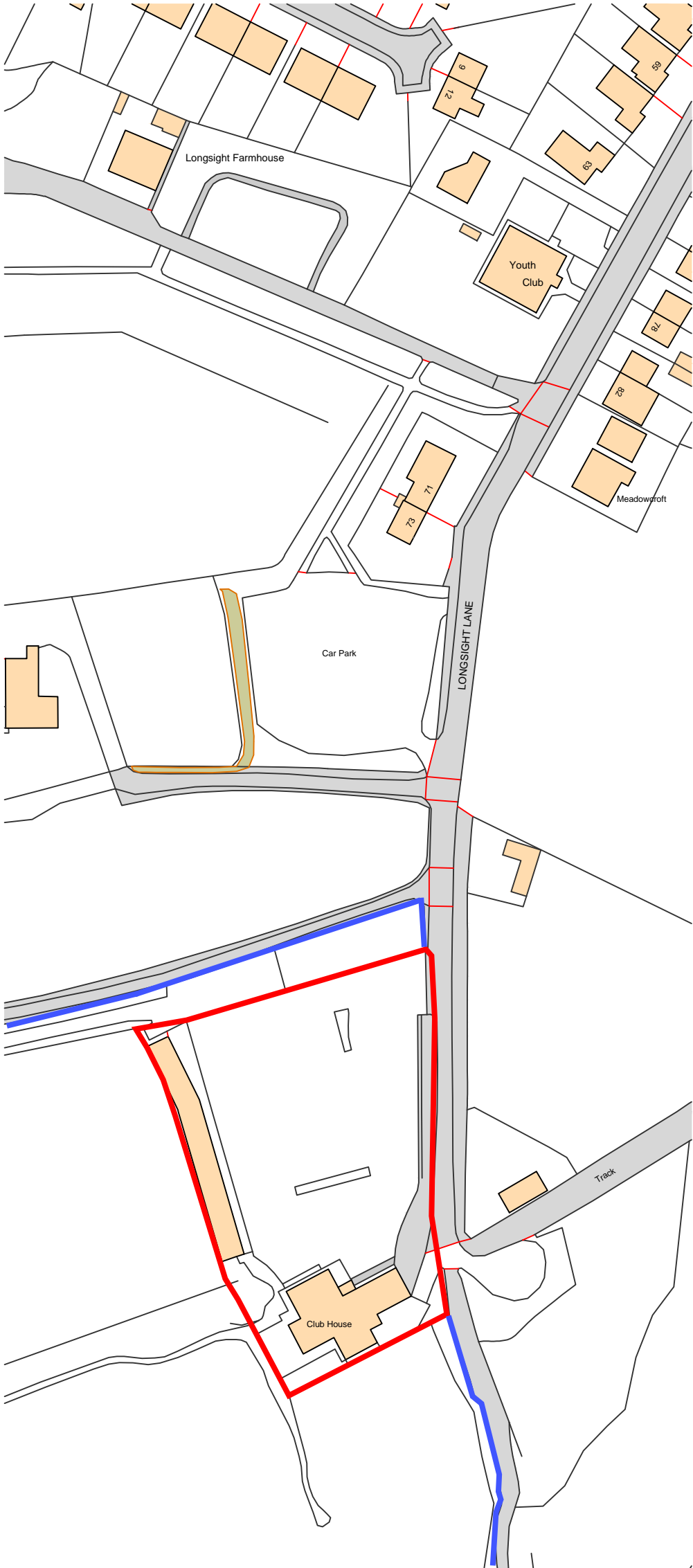
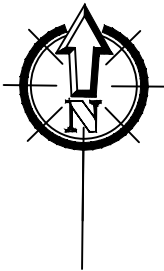
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Job No: 14-034	Drawing No: 1020	Rev:	

A1

PLANNING



1 : 1250



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Rev.	Date	By	Description
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Client:

Sedgwick Associates
on behalf of Mr K Ruttle



associates limited
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Project:

Longsight Lane,
Harwood (Golf Club)

Drawing Title:

Site Location Plan 1:1250

Drawn: KP	Checked: AK	Scale: 1:1250 @A3	Date: April 14
Job No: 14 - 034	Drawing No: SLP 01	Rev:	

A3

PLANNING

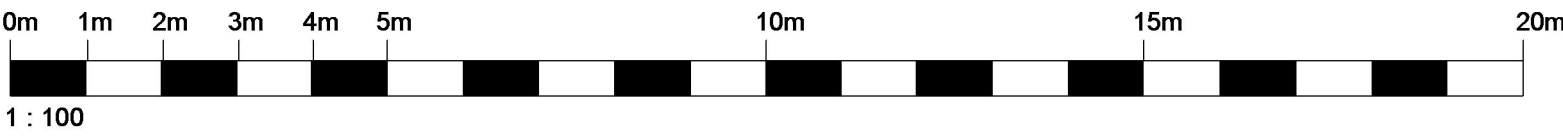
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elevation from rear of the site (north)



elevation from side access road (east)



Rev.	Date	By	Description
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Client:

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Project:

PROPOSED DEVELOPMENT
HARWOOD GOLF CLUB

Drawing Title:

PROPOSED ELEVATIONS
SHEET 2 OF 2

Drawn: ps	Checked:	Scale: 1:100 (A1)	Date: JUNE 2015
Job No: 14-034	Drawing No: 1023	Rev:	

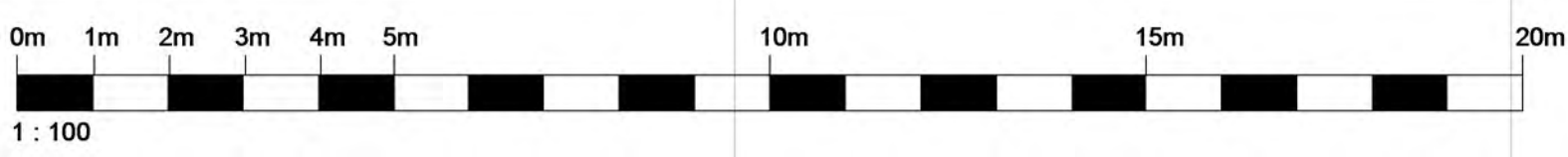
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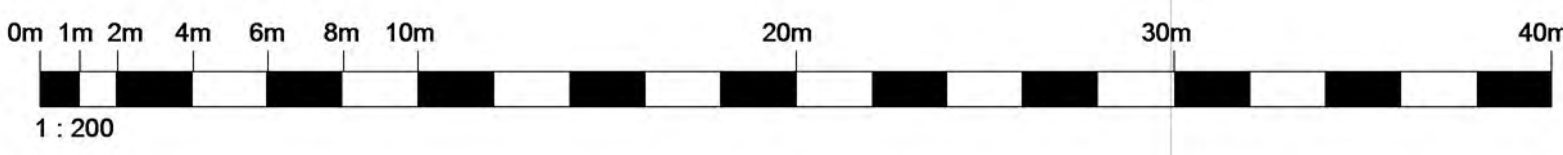
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elevation from long sight lane (south)



cross section through new courtyard looking north



cross section through new site entrance looking east

long sight lane



elevation from the new open space (west)

Rev. Date. By Description

Client:

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tel: 01772 774510 fax: 01772 774511 email mck@mckassociates.co.uk

Project:
PROPOSED DEVELOPMENT
HARWOOD GOLF CLUB

Drawing Title:

PROPOSED ELEVATIONS
SHEET 1 OF 2

Drawn: ps	Checked:	Scale: 1:100; 1:200 (A1)	Date: JUNE 2015
Job No: 14-034	Drawing No: 1022	Rev:	

A1

PLANNING



regrade and restore public right of way through site

existing clubhouse and driving range demolished and ground made good as required remaining field areas planted with indigenous tree and grass/wildflower mix to LA requirements. native hedgerows planted as indicated.

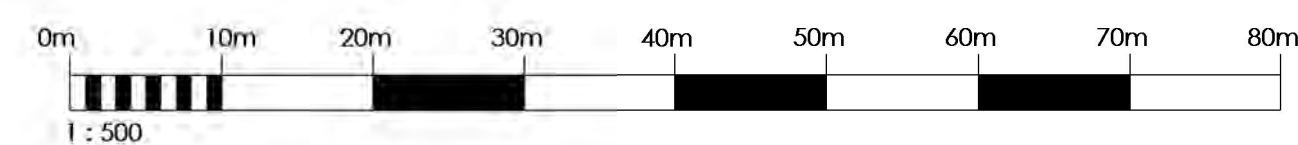
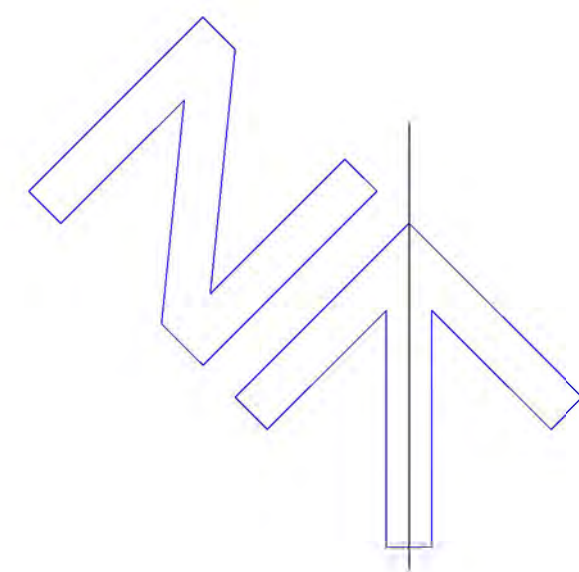
rear access to gardens
line of existing driving range demolished

new hedgerow
post and rail fences

existing hawthorn retained
existing road access made good and new turning head formed to LA req'ts - see Traffic Consultant details

4.5m road

new footway



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unit 7 - 4 beds - 1750 sq.ft.

SITE AREA = 5800 sq.m.
(1.43 acres approx.)

A 06-07-2015 PS Road Layout adjusted to Engineer's details

Rev. Date By Description

Client:

K RUTTLE

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tel: 01772 774510 fax: 01772 774511 email mck@mckassociates.co.uk

Project:
PROPOSED DEVELOPMENT
HARWOOD GOLF CLUB

Drawing Title:

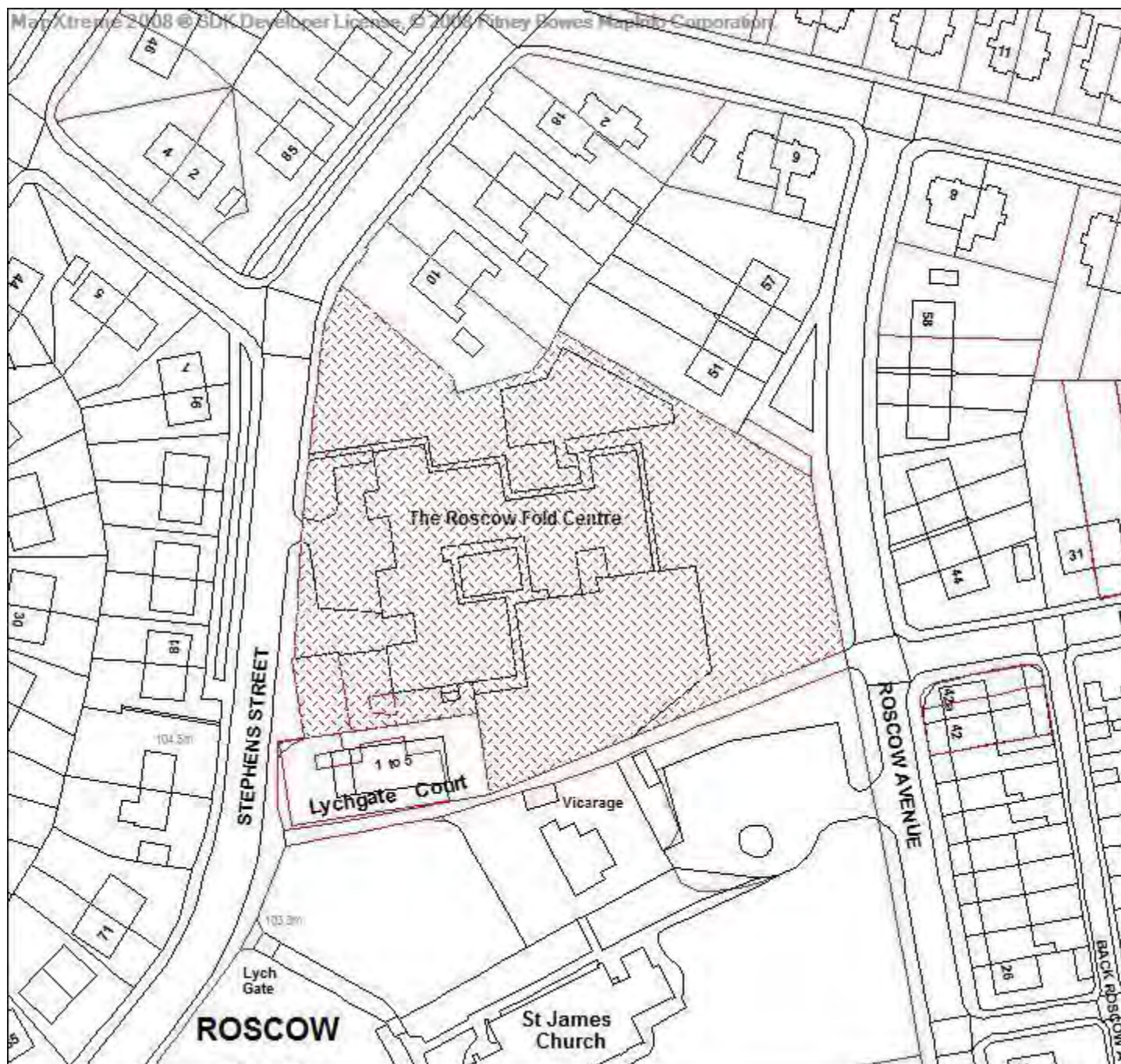
PROPOSED SITE LAYOUT

Drawn: PS	Checked:	Scale: 1:500 (A1)	Date: JUNE 2015
Job No: 14-034	Drawing No: 1010	Rev: A	

A1

PLANNING

Application number 94724/15



Development & Regeneration Dept
Development Management Section

Town Hall, Bolton, Lancashire, BL1 1RU
Telephone (01204) 333 333

Bolton Council

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Date of Meeting: 22/10/2015

Application Reference: 94724/15

Type of Application: Full Planning Application

Registration Date: 04/08/2015

Decision Due By: 28/09/2015

**Responsible
Officer: Paul Bridge**

**Location: ROSCOW FOLD PRIMARY SCHOOL, STEPHENS STREET,
BOLTON, BL2 5DX**

**Proposal: ERECTION OF A SINGLE STOREY EXTENSION TO FORM
ADDITIONAL CLASS ROOM SPACE AND ERECTION OF A
SHELTER TO NORTH ELEVATION**

Ward: Breightmet

Applicant: Bolton M.B.C.

Agent : Good & Tillotson

Officers Report

Recommendation: Approve subject to conditions

Proposal

Planning permission is sought for the erection of a single storey extension and shelter.

The proposed extension would be sited on the northern elevation and would create extra space for the classroom. It would be 16 metres in width and approximately 3.9 metres in height. The proposed external materials will be facing brickwork for the external walls, single ply membrane roof covering, white UPVC double glazed windows in a similar style to those on the main school building and the doors would be powdercoated blue.

The proposed shelter be sited adjacent to the new building and would provide a covered area to the northern entrance. The shelter would be approximately 4.4 metres in height and would be approximately 13 metres in width. The canopy would be steel framed with a powder coated finish. The structure will be open sided, with a flat roof.

The proposal would reduce the number of parking spaces within the site from 25 to 23.

Site Characteristics

The application site forms part of a complex of buildings which make up Roscow Fold Primary School. The site is irregular in shape. Both vehicular and pedestrian access to the school would remain as existing. External areas of the school are accessed via stepped and ramped approaches with all areas accessible.

Policy

National Planning Policy Framework (NPPF)

Core Strategy policies:

CG3 The Built Environment, CG4 Compatible Uses, S1 Safe Bolton, P5 Accessibility, SC2 Cultural and Community Facilities.

Analysis

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- Γ principle of the development
- Γ impact on surrounding residential and visual amenity
- Γ impact on highways

Principle of the Development

The site is unallocated within the Allocations Plan and is occupied by Roscow Fold Primary School. The proposed development would not alter the principal use of the site. The proposed extension would seek to improve the facilities and provide essential facilities required by the school.

The proposed development would be sited on an existing area of land beyond the northern elevation. The proposed development constitutes the improvement of an existing school and seeks to provide improved facilities for the school and wider community. The principle of the proposed development is considered to be acceptable and is in accordance with policy SC2 of the Core Strategy and aims and objectives of the NPPF.

Impact on Surrounding Residential and Visual amenity.

Policy CG3 of the Core Strategy states that the Council will conserve and enhance local distinctiveness and will require development to be compatible with the surrounding area. Policy CG4 states that the Council will ensure that new development is compatible with surrounding land uses and occupiers, protecting amenity, privacy, safety and security.

The proposed development (extension) would provide two additional classrooms and a shelter. The development would be set in from the site boundaries and would be approximately 13 metres from the surrounding nearest residential dwellings on Stephens Street. It would provide a functional need for the school and it is considered that the appearance of the building and shelter is acceptable in this location and would not have a significant detrimental impact on the amenity neighbouring residents or the visual amenity of the area.

Impact on Highways

Policy P5 of the Core Strategy states that the Council will [amongst other things] take into account

servicing arrangements and parking. Policy S1.2 states that the Council will promote road safety in the design of new development.

The proposed development seeks an enlarged space for a classroom and group room to provide improved facilities for the School. The existing pedestrian and vehicular access and parking arrangements serving the site will remain unchanged. In view of the potential increase pupils numbers the Council's Highway Engineers have requested that the applicant funds a parking review and additional Traffic Regulation Orders to prevent overspill parking on the bend in St Stephens Street or where it would obstruct visibility for vehicles emerging from the school egress road.

Subject to the conditions, the development presents no unacceptable highway issues.

Conclusion

The proposal overall will make a valuable contribution to the continuing planned improvement to the Borough's shared community facilities and will provide a much need extended facility for the school. It is considered that the proposed development is in accordance with Local Regional and National planning policy and the application is recommended for approval.

Representation and Consultation Annex

Representations

Letters:- one comment has been received from a neighbouring resident questioning whether there will be additional parking, as they consider that there is not enough on-site parking already.

Consultations – Highways, Drainage and Asset Management

Planning History

None relevant.

Recommendation: Approve subject to conditions

Recommended Conditions and/or Reasons

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason

Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. Prior to the commencement of development hereby permitted, all developer obligations shall be fulfilled to enable the Local Highway Authority to carry out a review of Traffic Regulation Orders in the locality and to enable the Local Highway Authority to consult, advertise, promote and implement Traffic Regulation Orders to mitigate the likely impact of the development.

Reason

In the interests of highway safety.

3. Prior to the development hereby approved being first occupied or brought into use, all Traffic Regulation Orders required by the Local Highway Authority to mitigate the impact of the development shall be implemented in full.

Reason:

In the interests of highway safety.

4. The development hereby permitted shall be carried out in complete accordance with the following approved plans:

1560-07 - Location Plan, dated July 2015
1560-08 - Plan as Existing, dated July 2015
1560-09 - Elevations as Existing, dated July 2015
1560-10 - Site Layout Plan as Existing, dated July 2015
1560-11 - Plan as Proposed, dated July 2015
1560-12 - Elevations as Proposed, dated July 2015
1560-13 - Site Layout Plan as Proposed, dated July 2015.

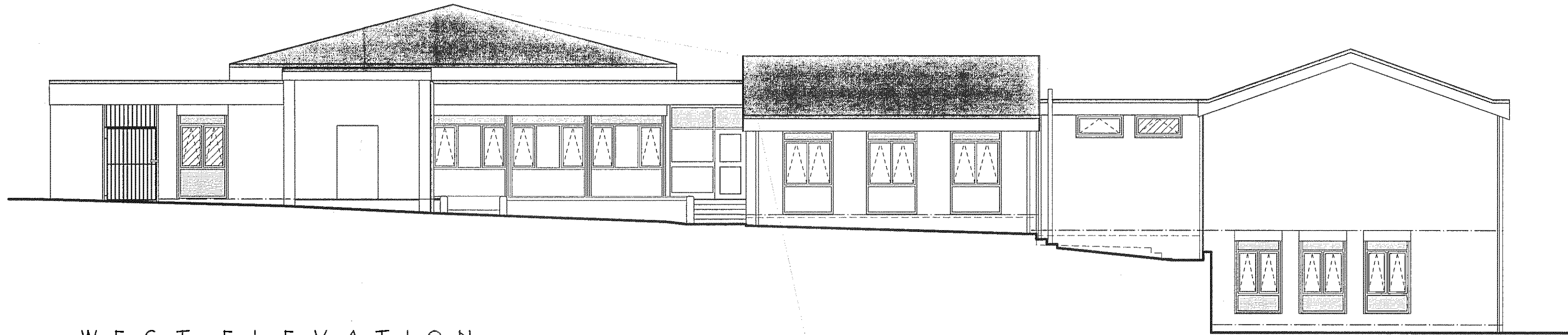
Reason

For the avoidance of doubt and in the interests of proper planning.

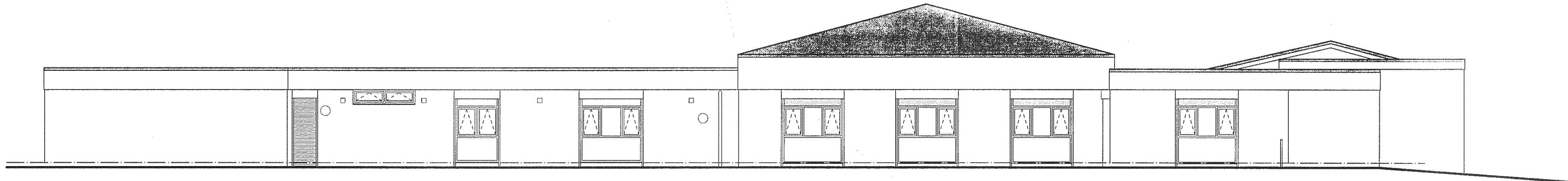
5. The external surfaces of the extension and shelter hereby permitted shall be of a similar colour, texture and size of those of the existing building, and shall be retained thereafter.

Reason

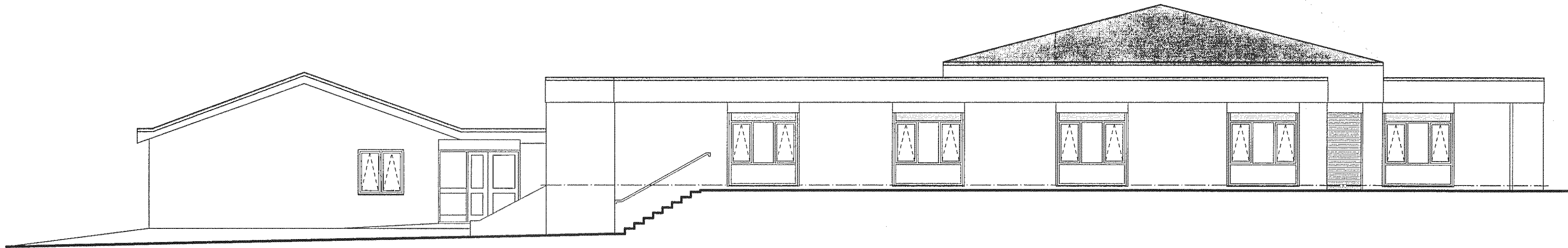
To ensure the development visually reflects the existing building.



WEST ELEVATION



NORTH ELEVATION

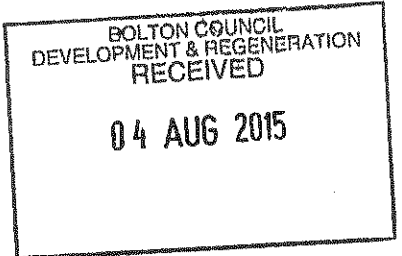


EAST ELEVATION



SOUTH ELEVATION

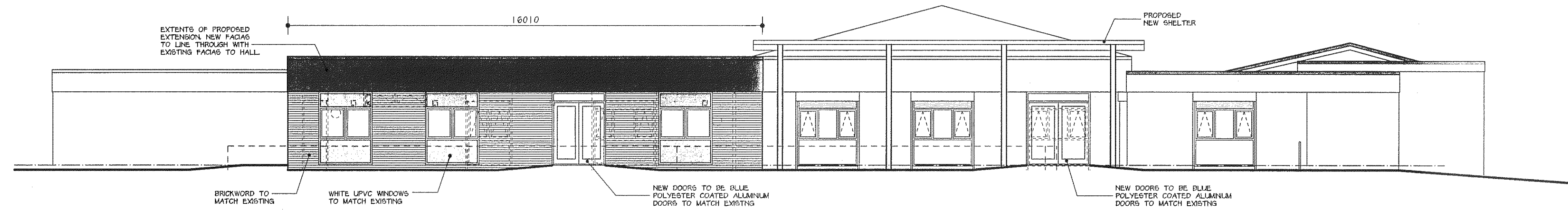
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 ALL LEVELS TO BE CHECKED ON SITE.
 All discrepancies between information shown on the drawings and the information in the specification to be referred to the architect prior to proceeding.
 All component sizes and references to be checked prior to ordering of materials.
 Positions and designation (i.e. combined, surface water or foul) of existing drain runs on site are to be checked / confirmed by the building contractor prior to commencement of any drainage works. Any existing drains, to which additional connections are to be made, are also to be checked as necessary to ensure that they are suitable to accommodate additional capacity and the type of effluent being connected.
 All relevant boundary positions to be checked prior to proceeding.



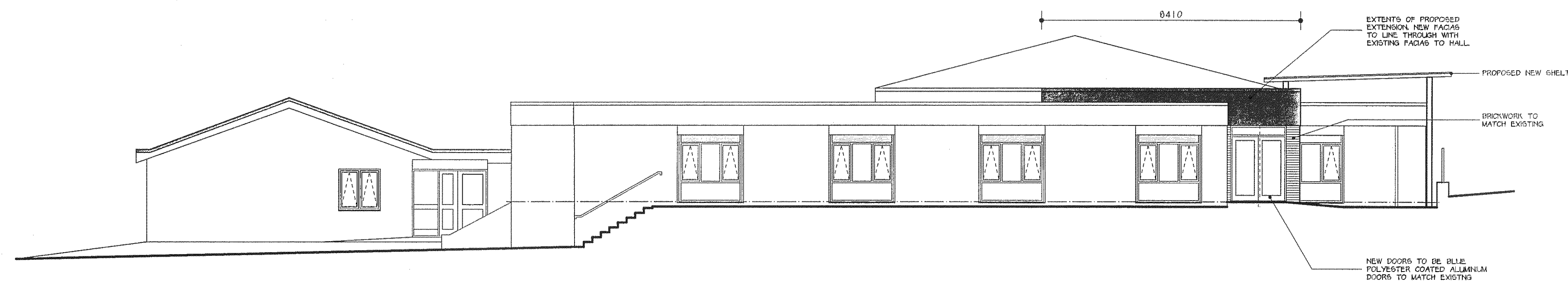
Rev.	Revision	Int.	Date
Good & Tilotson Chartered Architects 2 The Studios, 318 Chorley Old Road, Bolton, BL1 4JU Tel : 01204 497700 Fax : 01204 497776 E-mail : info@goodandtilotson.co.uk Web : www.goodandtilotson.co.uk			
Client : Bolton MBC			
Project : Proposed Alterations to Pupil Referral Unit at The Former Roscow Fold Primary School Stephens Street Bolton BL2 5DW			RIBA Chartered Practice
Drawing : ELEVATIONS AS EXISTING EXISTING DRAWING			Date : July 2015 Drawn : MDJ / MJL Scale : 1 : 100 @ A1
Project No. : 1560		Drawing Number : 09	
		Drawing Revision :	



WEST ELEVATION



NORTH ELEVATION



EAST ELEVATION



SOUTH ELEVATION

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Drawing : ELEVATIONS AS PROPOSED		Date : July 2015 Drawn : MDJ / MJL Scale : 1 : 100 @ A1
Project No. : 1560	Drawing Number : 12	Drawing Revision :

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Client : Bolton MBC			
Project : Proposed Extension to Pupil Referral Unit at the former Roscow Fold Primary School		RIBA Chartered Practice	
Drawing : PLAN AS EXISTING		Date : July 2015 Drawn : PJS / MJL / MDJ Scale : 1 : 100 @ A1	
Project No. : 1560		Drawing Number : 08 Drawing Revision :	

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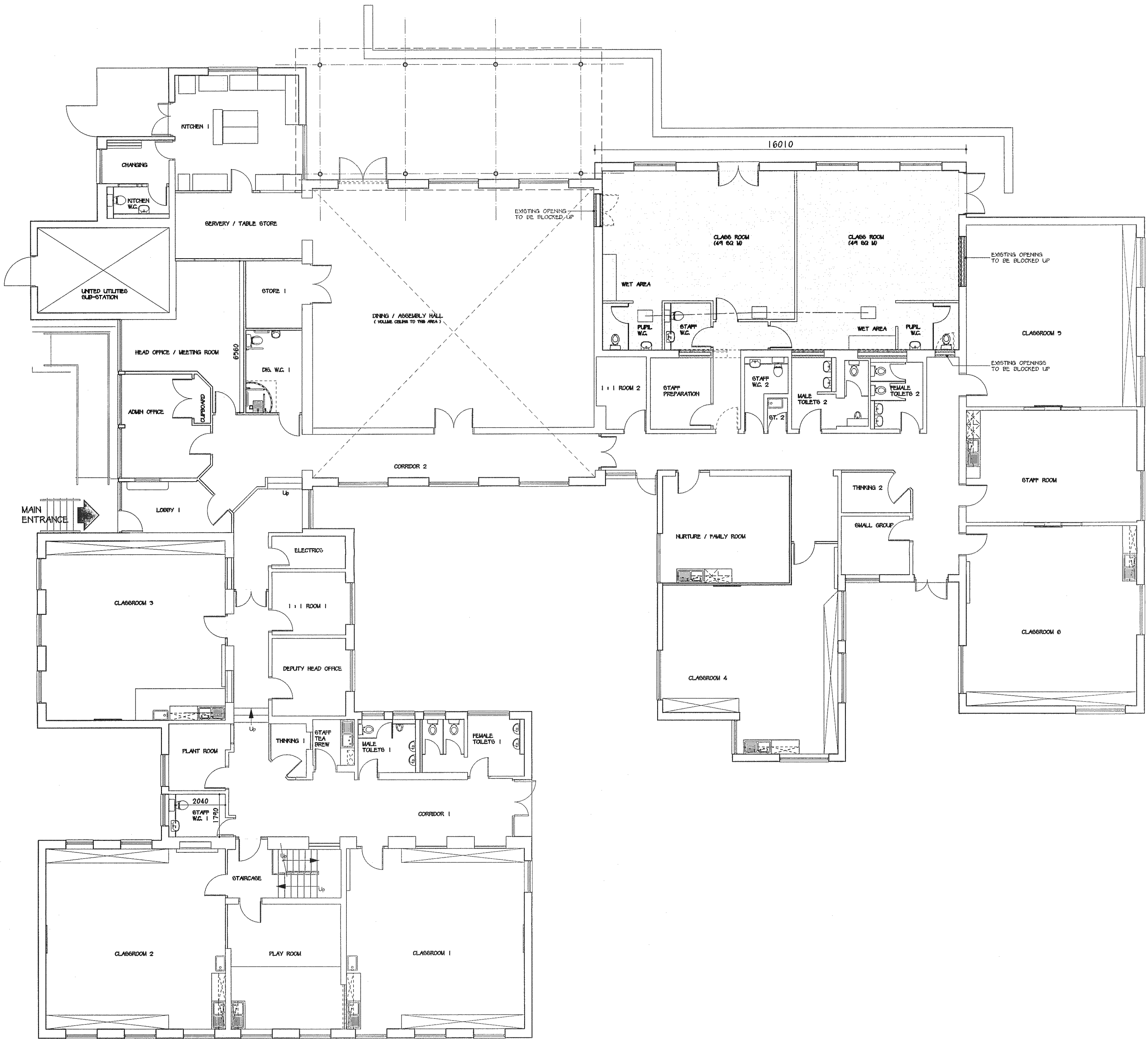
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Client : Bolton MBC			
Project : Proposed Extension to Pupil Referral Unit at the former Roscow Fold Primary School		RIBA Chartered Practice	
Drawing : PLAN AS PROPOSED		Date : July 2015 Drawn : PJS / MJL / MDJ Scale : 1 : 100 @ A1	
Project No. : 1560	Drawing Number : 11	Drawing Revision :	



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g&t

Client / Project :
 Bolton MBC
 Proposed Alterations to
 Pupil Referral Unit at the
 Former Roscow Fold Primary School
 Stephens Street
 Bolton BL2 5DW

179

Drawing :
 LOCATION PLAN

Date : July 2015	Project No. : 1560
Drawn :	Drawing Number : 07
Scale : 1 : 1250 @ A4	Drawing Revision :

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STANDARD REFERENCE & ABBREVIATIONS

- | | |
|------------------------------------|--------------------------------|
| (AR) Assumed Route | IC Inspection Cover |
| AV Air Valve | IL Invert Level |
| BW Barbed Wire | IO Joint Outlet |
| BS Basins Basin | LB Lateral Box |
| BH Berchide | LP Lamp Post |
| BL Bed Level | MS Manhole Girth |
| BS Bus Stop | MR Marker |
| BT Box-British Telecom Box | MP Mile Post or Stone |
| BU Brick Wall | NP Street Name Plate |
| Bar Barrier | OT Overhead |
| BS Barbed | OP Open Paving |
| CB Concrete Boarded | OSM Ordnance Survey Bench Mark |
| CC Corrugated Iron | P/C Post & Chain |
| CP Chain Pipe | P/R Post & Rail |
| CB Control Box | P/W Post & Wire |
| CCTV Closed Circuit TV | PAV Paving |
| CD Cable Duct | PB Piller Box |
| CH Chain Link | PC Pedestrian Crossing |
| CL Cover Level | PP Petrol Pump |
| CM Gate Marker | PT Post or Piller |
| CL Column | RE Railing Eye |
| CONC Concrete | RS Road Sign |
| CP Catch Pit | RTW Retaining Wall |
| CTV Cable Television | RWP Rain Water Pipe |
| CW Concrete Block Wall | SA Sapling |
| Ch Chimney | SL Sump Level |
| CL Clutter | SP Sign Post |
| DC Drainage Channel | SV Stop Valve (specified) |
| DP Down Pipe | SVP Sump Valve Pipe |
| DR Drain | SW Stone Wall |
| ECP Electricity Cable Pit | SL Soffit Level |
| EC Electricity Inspection Cover | TB Traffic Island |
| EJ Expansion Joint | TBM Temporary Bench Mark |
| EJB Electricity Junction Box | TGB Telephone Call Box |
| EL SUB 6TH Electricity Sub Station | TFR Taken From Records |
| EL Eaves Level | TE Anchor Point (Main/Pole) |
| ELD Electricity Box | TL Traffic Light |
| EP Electricity Pole | TLC Traffic Light Control Box |
| ER Earth Road | TM Ticker Machine |
| ET Electricity Transmission Line | TMC Terrace |
| F/B Flower Bed | TP Telecommunication Pole |
| FL Finished Floor Level | TRP Trial Pit |
| PH Fire Hydrant | U/C Under Construction |
| FL Floor Level | U/G Underground |
| FP Flagpole | UL Liable to Lift (MHUC etc) |
| GL Girth (of tree) | VDP Vehicle Detector Pad |
| GP Gully | VP Vent Pipe |
| GS Gully Stop | WW Wire Mesh |
| GSV Gas Stop Valve | WL Water Level |
| HS Housing | WM Water Meter |
| HV High Voltage | WO Wash Out |
| HP High | WST Water Stop Tap (domestic) |
| IR Iron Railing | WSV Water Stop Valve (main) |

SURVEY STATIONS		
Name	Eastings	Northings
S01	374034.28	409656.23
S02	374121.05	409656.23
S03	374131.64	409678.82
S04	374136.34	409639.29
S05	374102.80	409639.29
S06	374083.91	409639.29
S07	374076.14	409639.29
S08	374037.02	409639.29
S09	374040.44	409621.10
S010	374066.23	409645.66

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Web : www.goodandtilotson.co.uk

Client : **Bolton MBC Corporate Property Services**

Project : **Proposed Alterations to Pupil Referral Unit at the former Roscow Fold Primary School**

Drawing : **SITE LAYOUT PLAN AS EXISTING**

Project No. : **1560**

RIBA
Chartered Practice

Date : **July 2015**
Drawn : **PJS / MJL**
Scale : **1 : 200 @ A1**
Drawing Revision :

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STANDARD REFERENCE & ABBREVIATIONS

- | | |
|------------------------------------|---------------------------------|
| (AK) Assumed Road | IC Inspection Cover |
| AV Air Valve | IL Inverted Level |
| BW Barbed Wire | KO Kerb Outlet |
| BB British Beacon | LB Lateral Box |
| BL Borehole | LP Lamp Post |
| BL Bed Level | MS Manhole |
| BS Bus Stop | MR Marker |
| BT British Telecom Box | MP Mile Post or Stone |
| BT British Telecom IC | NS Street Name Plate |
| BR Brick Wall | OP Open Paving |
| BS Barrier | OSBM Ordnance Survey Bench Mark |
| BS Board | P/C Foot & Chain |
| CB Concrete Boarded | P/P Foot & Rail |
| CC Corrugated Iron | P/W Foot & Wire |
| CC Control Box | PAV Paving |
| CCV Closed Circuit TV | PB Pillar Box |
| CD Cable Duct | PC Pedestrian Crossing |
| CH Chain Link | PF Foot & Pillar |
| CL Cover Level | RE Ridding Eye |
| CM Cable Marker | RS Road Sign |
| COL Column | RTW Retaining Wall |
| CONC Concrete | RWP Rain Water Pipe |
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| EB Electricity Junction Box | TFR Taken From Records |
| EL GUB GTE Electricity Sub Station | TL Anchor Point (Mast/Pole) |
| EL Level | TL Traffic Light |
| ELB Electricity Box | TLB Traffic Light Control Box |
| EP Electricity Pole | TM Traffic Machine |
| ER Earth Rod | TP Telecommunications Pole |
| ET Electricity Transmission Line | TPIT Trial Pit |
| F/B Flower Bed | UC Under Construction |
| FF Finished Floor Level | UG Underground |
| FI Fire Hydrant | UT Unable to Lift (NHAC etc) |
| FL Floor Level | VDP Vehicle Detector Pad |
| FP Flagpole | VP Vent Pipe |
| GA Girder (of tree) | WM Wire Mesh |
| GB Gully | WL Water Level |
| GP Guide Post | WM Water Meter |
| GS Gas Stop Valve | WO Wash Out |
| HB Housing | WST Water Stop Tap (domestic) |
| HV High Voltage | WV Water Stop Valve (main) |
| IR Iron Railing | |

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Name	Existing	Height
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S02	374121.05	409632.23
S03	374131.64	409632.60
S04	374135.34	409632.29
S05	374102.80	409632.06
S06	374083.91	409632.26
S07	374076.14	409632.44
S08	374037.02	409632.24
S09	374040.44	409632.10
S010	374036.23	409632.65

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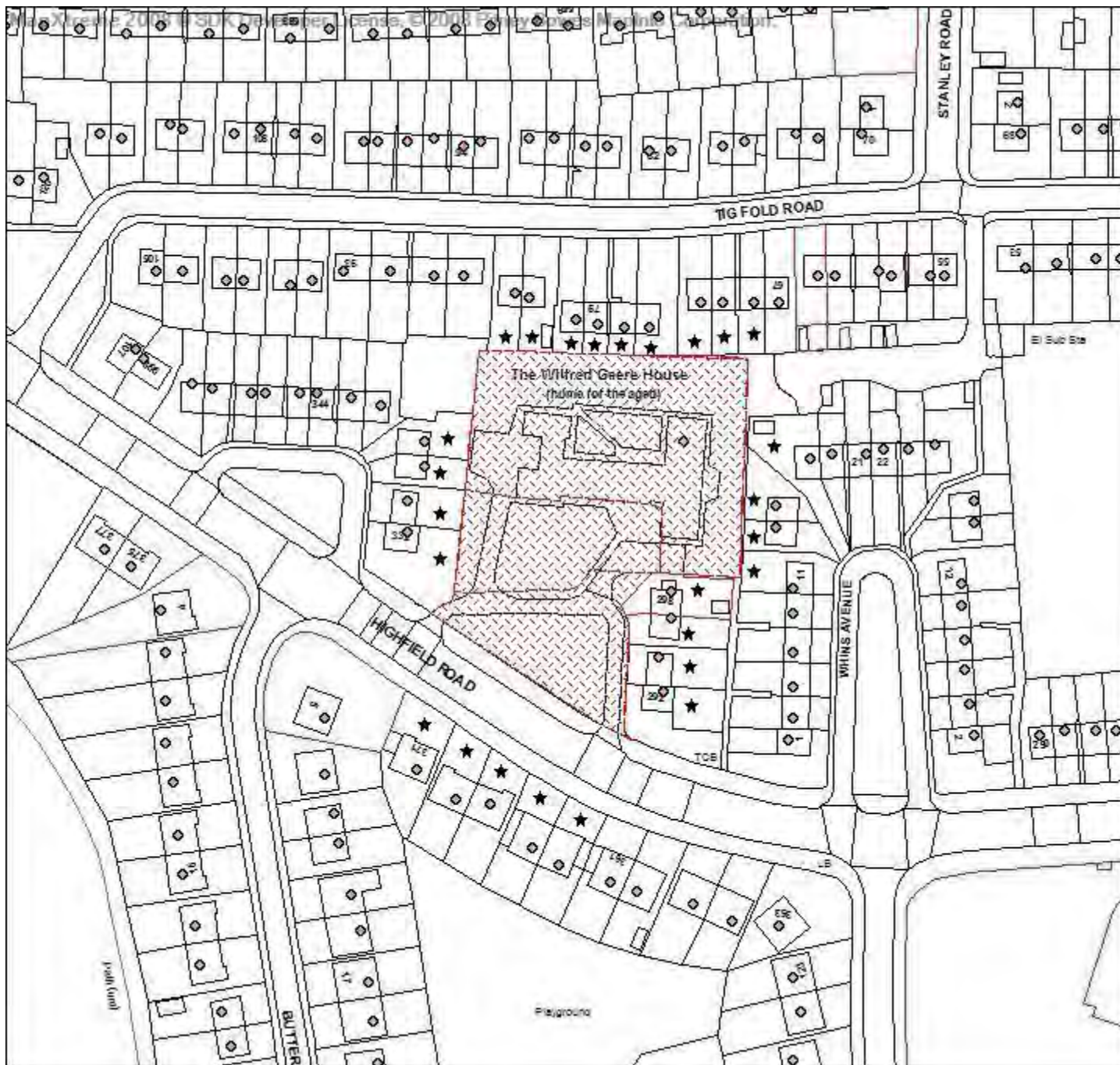
Project : Proposed Alterations to Pupil Referral Unit at the former Roscow Fold Primary School

RIBA
Chartered Practice

Drawing : SITE LAYOUT PLAN AS PROPOSED
Date : 15 July 2015
Drawn : PJS / MJL
Scale : 1 : 200 @ A1
Drawing Revision :

Project No. : **1560**
Drawing Number : **13**

Application number 94730/15



Development & Regeneration Dept
Development Management Section

Town Hall, Bolton, Lancashire, BL1 1RU
Telephone (01204) 333 333

Bolton Council

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Date of Meeting: 22/10/2015

Application Reference: 94730/15

Type of Application: Full Planning Application

Registration Date: 05/08/2015

Decision Due By: 29/09/2015

**Responsible
Officer: Paul Bridge**

**Location: WILFRED GEERE HOUSE, HIGHFIELD ROAD, FARNWORTH,
BOLTON, BL4 0PG**

**Proposal: ERECTION OF SINGLE STOREY EXTENSIONS TO FRONT, SIDE
AND REAR TO PROVIDE NEW ENTRANCE, DAY ROOMS, STAFF
AND STORAGE FACILITIES TOGETHER WITH ADDITIONAL
PARKING AND LANDSCAPING.**

Ward: Harper Green

Applicant: Bolton M B Council

Agent : Good & Tillotson

Officers Report

Recommendation: Approve subject to conditions

Proposal

Planning permission is sought for the erection of single storey extensions to front, side and rear of the building to provide new entrance, day rooms, staff and storage facilities together with additional parking and landscaping.

The single storey rear extension would be approximately 23.8 metres in width, 4.7 metres in length with an overall height of 3.40 metres. It would be finished in coloured 'Trespa' panels for the walls and 'Decra' cladding for the roof. Internally, it would accommodate a day lounge for patients, meeting facilities for staff and for general storage. This extension will allow for the existing lounges to be converted for additional bedrooms.

The single storey side extension would be located on the eastern elevation. It would be 5.34 metres in width, 5.32 metres in length and 3.37 metres in height with a dual pitched roof. It would be finished with UPVC upper frames to the side elevations and solid masonry for the rear elevation. Internally, it would provide a day lounge.

The single storey front extension (lobby) will be approximately 4.37 metres in width and 1.65 metres in length with a maximum height of 3 metres. It would be finished in with a proprietary aluminium framing system with full glazing for the walls and roof.

The parking area to the front of the building will be extended for use by visitors. This will increase the total number of parking spaces from 11 to 19.

Vehicular and pedestrian access would remain unaffected by the proposals.

Site Characteristics

Wilfred Geere House is currently used as an intermediate care facility which focuses on care for patients with dementia. The application site is located on the northern side of Highfield Road (off Plodder Lane). Vehicular access is via a cul-de-sac/link road running off Highfield Road. The building is a part two storey/part single storey brick building which is irregular in layout. The existing parking provision is currently sited to the front of the building, beyond which is a small area of landscaping.

Residential dwellings bound the site to the north (set on a substantially higher level), east and west.

Policy

National Planning Policy Framework 2012

Bolton Core Strategy: S1 Safety, SC2 Community Facilities, P5 Transport and Accessibility, RA2 Farnworth, CG3 The Built Environment and CG4 Compatible Uses.

Supplementary Planning Document: Accessibility, Transport and Safety.

Analysis

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- Γ principle of the development
- Γ impact on the character and appearance of the area
- Γ impact on residential amenity
- Γ impact on the highway
- Γ impact on trees

Principle of the Development

The site is unallocated within the Allocations Plan and is occupied by Wilfred Geere House. The proposed development would not alter the principal use of the site. The proposals would seek to improve the facilities and provide essential facilities required by the home.

The proposed development would be sited on existing area of land to the front, rear and side and would result in an increase in parking provision within the site from 11 to 19 spaces. The proposed development constitutes the improvement of an existing care home and seeks to provide improved facilities for the occupants. The principle of the proposed development is considered to be acceptable and is in accordance with policy SC2 of the Core Strategy and aims and objectives of the NPPF.

Impact on the Character and Appearance of the Area

Policy CG3 of the Core Strategy states that the Council expects development proposals to contribute to good urban design, respect and enhance local distinctiveness, ensuring development has regard to the overall built character and landscape quality of the area.

The existing building is a part two storey/part single storey brick built building which, whilst functional, is unremarkable in its design. The proposed extensions would be sited to the front, rear and eastern side elevation. Given their locations only the small front extension would be visible from within the street scene. The proposals are single storey and therefore appear subordinate to the existing building. Both the rear and front elements have a modern design, which will add a contemporary feel and contrast to the existing building and also provide a focal point to the front of the building. The single storey side extension is akin to a conservatory, albeit with a solid brick rear elevation.

A 20 metre by 4.8 metre section of the existing grass verge to the front of the site would be lost in order to accommodate the additional 8 parking spaces within the site. However, the loss of this area of landscaping is considered to be acceptable given that a large proportion will still remain and the two existing trees would be unaffected. The benefit of providing the additional parking spaces and improved access to the site is considered to outweigh any harm to the character and appearance of the area, which is negligible in any case.

It is considered that the proposed single storey extensions would constitute limited extensions and would not result in disproportionate additions over and above the size of the original building. It is considered therefore that the proposals would not have an unacceptable impact on the appearance or character of the existing building, or the wider surrounding area, and thus complies with policy CG3 of the Core Strategy.

Impact on Residential Amenity

Policy CG4 aims to ensure that new development is compatible with surrounding land uses and occupiers and protects amenity.

Single Storey Front Extension

Given the size and siting of the proposal and its intended use as an entrance lobby it is not considered that this element of the proposal would result in an unacceptable impact on the residential amenity of neighbouring occupiers in terms of loss of light, privacy or being overbearing.

Single Storey Side Extension

The proposed single storey side element would be sited on the eastern elevation. The rear elevation of the proposal would be approximately 4.2 metres from the rear common boundary with No.15 Whins Avenue. There are no windows in the rear elevation of this element of the proposal and the 2 metre high boundary treatment provides adequate screening. In addition, a distance of approximately 17 metres would be maintained to the side/rear common boundary with No. 298 Highfield Road and approximately 27 metres to the rear boundaries of the residential dwellings along Tig Fold Road, which are also set on a substantially higher level than the application site. It is not considered that this element of the proposal would result in an unacceptable impact on the residential amenity of neighbouring occupiers in terms of loss of light, privacy or being overbearing.

Single Storey Rear Extension

The proposed single storey rear element would maintain a distance of approximately 19 metres to the rear common boundaries of the dwellings along Tig Fold Lane, which are also set on a substantially higher level than the application site. Given this, it is not considered that this element of the proposal would result in an unacceptable impact on the residential amenity of neighbouring

occupiers in terms of loss of light, privacy or being overbearing.

Noise and Disturbance

The proposed extensions will allow for the existing lounges to be converted for additional bedrooms which would increase the capacity of the home from 29 bedrooms to 32 bedrooms. It is considered that this increase would not unduly intensify the use of the site to such a degree to unacceptably impact on the residential amenity of the nearest residential dwellings.

In order to accommodate the additional 8 car parking spaces, a 20 metre by 4.8 metre section of the existing grass verge to the front of the site and beyond the existing car park area would be lost. This area would be approximately 16.5 metres to the nearest residential dwellings to east (along Highfield Road) and therefore the potential impact on the residents of Highfield Road needs to be assessed.

The provision of additional car parking will result in additional comings and goings in area of the site where there is currently little or no activity. Due to the use of the premises as a care home, it is open 24 hours a day, seven days a week. The additional parking area is primarily for use of visitors to the home and therefore it is highly likely that this additional car park area would only be used during peak times of the day, with little or no activity in the late evening. This would be due to capacity being available within the existing car park area should visitors arrive late into the evening. Given the additional parking area is not that large and would only accommodate a maximum of 8 additional vehicles, the proposed use is not likely to result in significant comings and goings at unsociable hours and therefore it is unlikely to cause any unacceptable disturbance for the nearest residents of Highfield Road, which are sited approximately 16.5 metres from the proposed car park area.

The proposal complies with policy CG4 of the Core Strategy.

Impact on the Highway

Policy P5 of the Core Strategy states that the Council will ensure that developments take into account accessibility, servicing arrangements and parking. Policy S1.2 states that the Council will promote road safety in new developments.

The additional parking spaces would serve the existing building and would alleviate any parking pressures that currently exist. Consequently, it is not considered that the proposal would have any adverse impact upon highway safety or parking provision. Therefore, it is considered that the proposal would not cause any significant harm to highway safety, and is in accordance with the NPPF and policy P5 of the Core Strategy. Bolton Council's Highway Engineers have been consulted and have raised no objections to the proposal.

The proposal complies with policy S1 of the Bolton Core Strategy.

Impact on Trees

Core Strategy policy CG1 seeks to protect urban biodiversity including trees.

There are two mature trees sited adjacent to the proposed car parking area, which are shown as being retained on the submitted plans. These trees are not protected by way of Tree Preservation Orders. The submitted plans indicate that the car parking provision would fall within the 'Tree Root Protection Zone' of the two trees. As these trees are not protected an informative would be attached to advise the applicant that care should be taken to minimise excavations in this area by following the existing contours as closely as possible. As such the proposal is considered to be in accordance with policy CG1 of the Core Strategy.

Conclusion

By virtue of the reasons outlined above, the application is considered to be acceptable and in accordance with both local and national planning policy and guidance. The application therefore recommended for approval.

Representation and Consultation Annex

Representations

Letters: - Three letters of objection have been received in response to the planning application publicity and who have raised the following objections:-

- Γ The proposed car parking spaces would be an eyesore
 - Γ The proposed car park would have an impact upon highway safety
- The above issues have been addressed in the appraisal.*
- Γ The land is used by children to play on, well away from the main road. *Response: - Whilst it is acknowledged that local children may use the existing grass verge as a play area, it remains that this land is in private ownership and is not a designated play area or park.*
 - Γ The additional car parking spaces could be located elsewhere within the site without the loss of the grass verge. *Response: - The applicant is under no obligation to alter the proposal. The current application has to be assessed on its own planning merits.*

Consultations

Advice was sought from the following consultees: Highways, Coal Authority, National Care Standards Commission and Adult and Community Services.

Planning History

62780/02 – Erection of Single Storey Extension to Form Boiler Room. Approved 14.11.2002

91072/13 – Formation of Car Park, Ambulance Bay and Access Road. Approved. 20.11.2013

Recommendation: **Approve subject to conditions**

Recommended Conditions and/or Reasons

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason

Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. Prior to the development hereby approved being first occupied or brought into use, a scheme detailing how parts of the site to be used by vehicles are to be laid out, constructed, surfaced, drained and lit shall be submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be implemented in full and thereafter made available for the use of vehicles at all times the development is in use.

Reason

To encourage drivers to make use of the parking and circulation area(s) provided.

3. The development hereby permitted shall be carried out in complete accordance with the following

approved plans:

**Location Plan - Received 05 Aug 2015,
1554/01 - Site Plan/Layout as Existing, dated July 2015,
1554/02 - Site Plan/Layout as Proposed, dated July 2015,
1554/03 - Plans as Existing (Survey Drawing), dated July 2015,
1554/04 - Elevations as Existing (Survey Drawing), dated July 2015,
1554/05 - Plans as Proposed (Planning Drawing), dated July 2015,
1554/06 - Elevations as Proposed (Planning Drawing), dated July 2015**

Reason

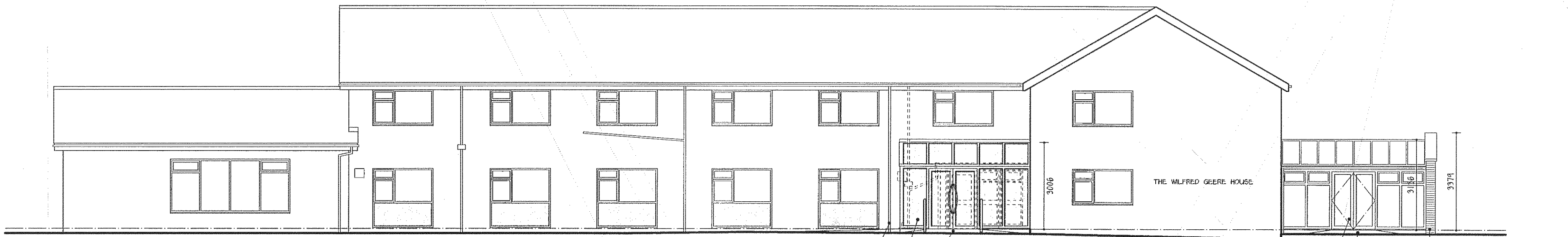
For the avoidance of doubt and in the interests of proper planning.

4. The development hereby permitted shall be constructed entirely of the materials details of which are shown on plan refs:

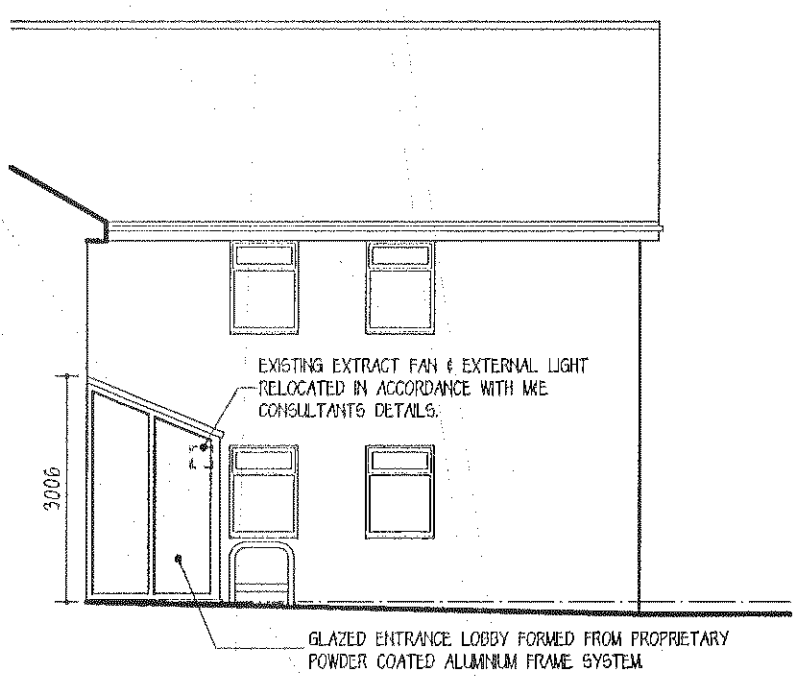
**1554/05 - Plans as Proposed (Planning Drawing), dated July 2015,
1554/06 - Elevations as Proposed (Planning Drawing), dated July 2015**

Reason

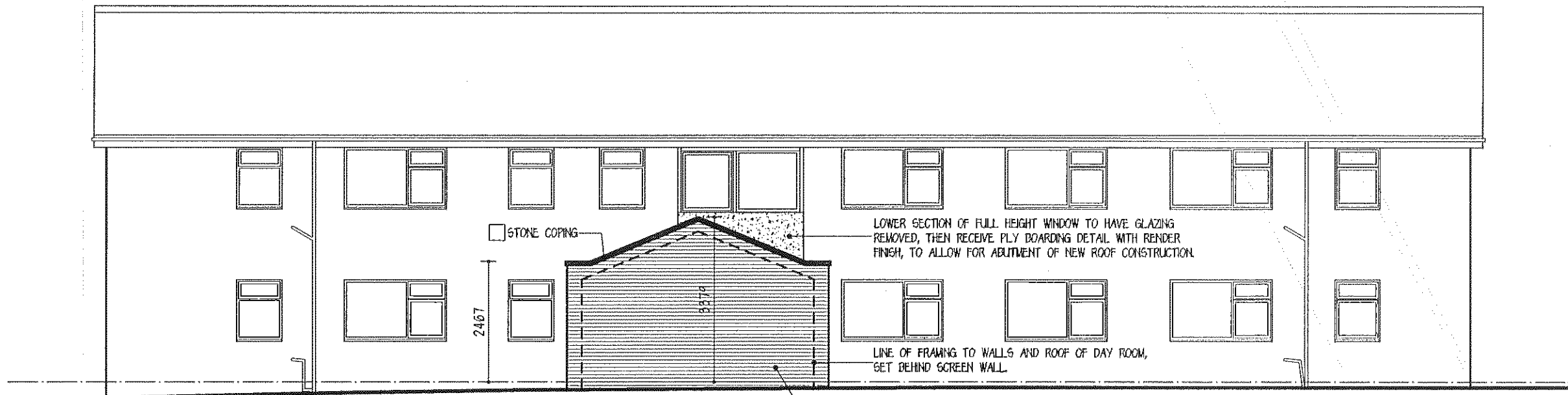
For the avoidance of doubt as to what is permitted.



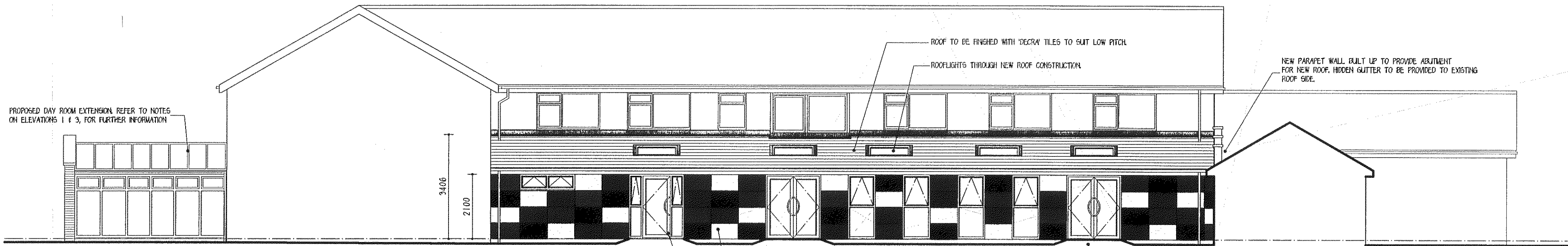
Proposed Front Elevation (1)



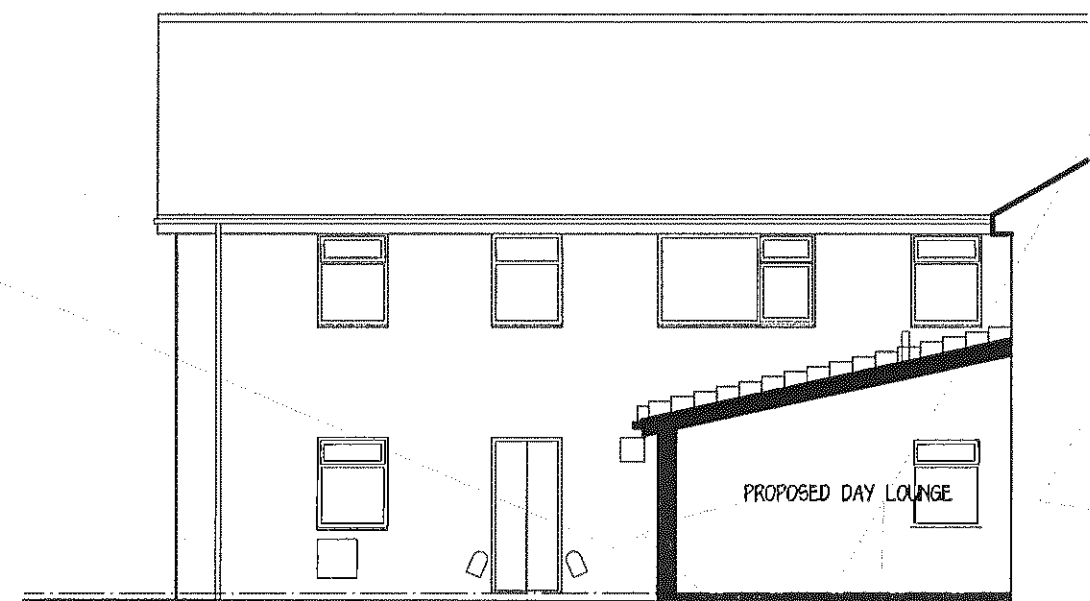
Proposed Front Side Elevation (2)



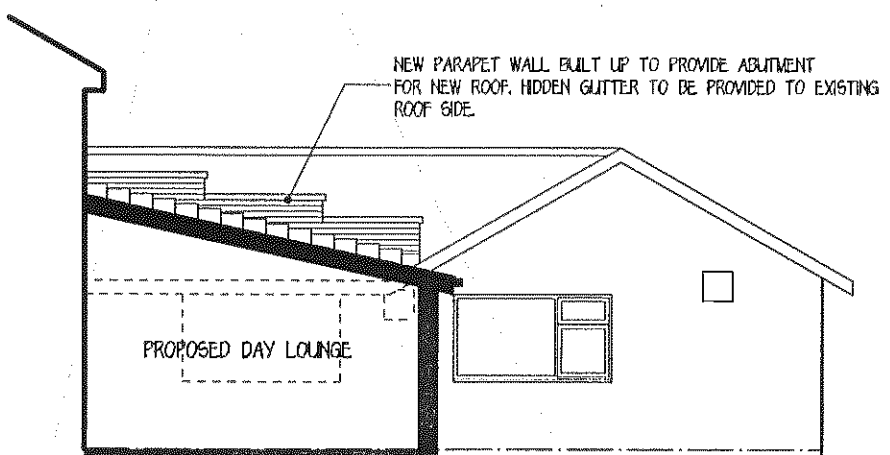
Proposed Side Elevation (3)



Proposed Rear Elevation (4)

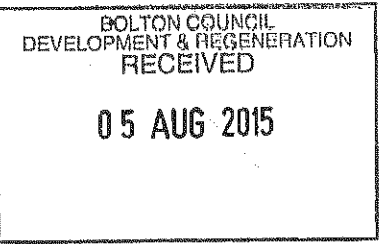
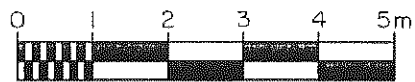


Proposed Section and Side Elevation 1 (5)

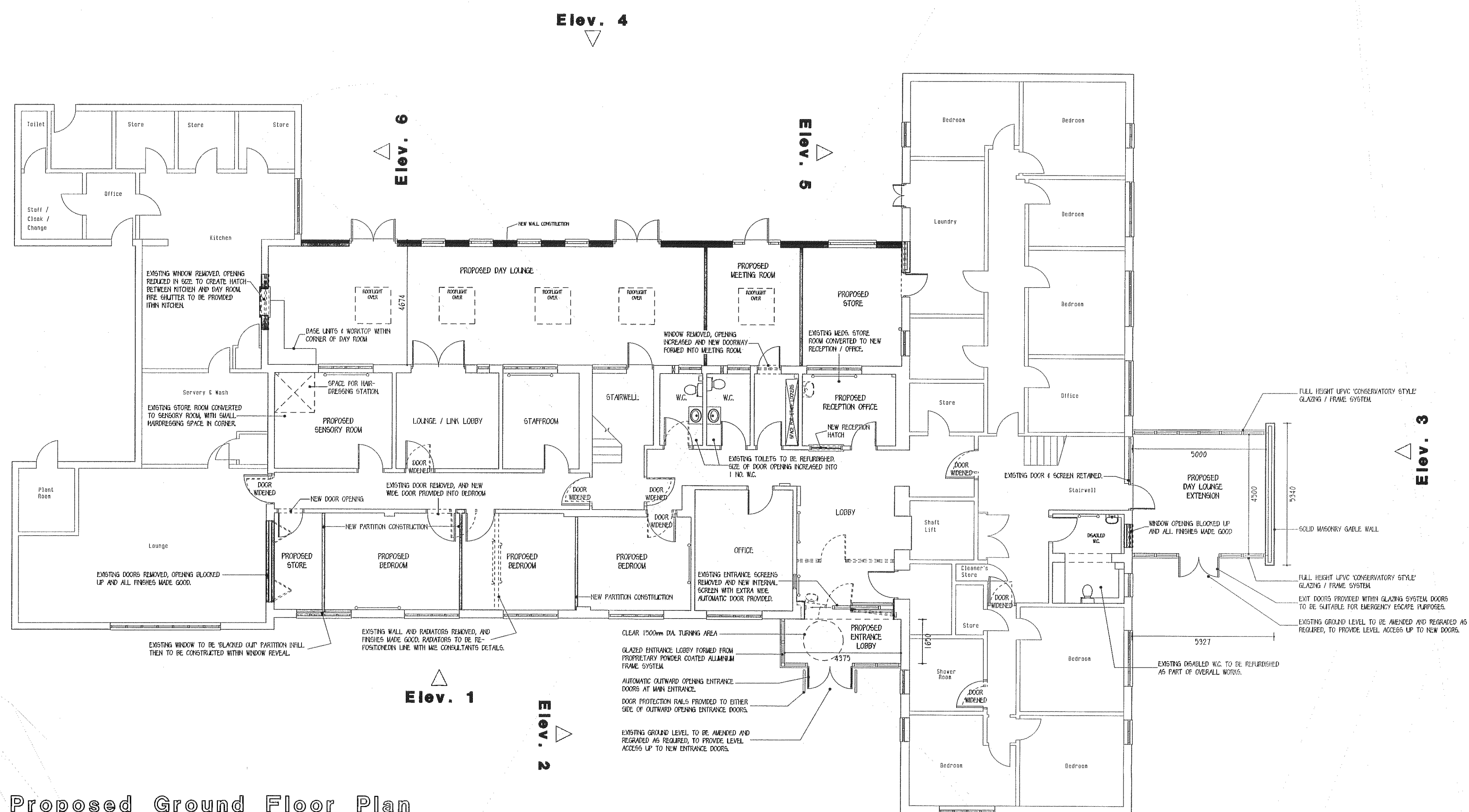


Proposed Section and Side Elevation 2 (6)

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All component sizes and references to be checked prior to ordering of materials.
Positions and designation (i.e. combined, surface water or foul) of existing drain runs on site are to be checked / confirmed by the building contractor prior to commencement of any drainage works. Any existing drains, to which additional connections are to be made, are also to be checked as necessary to ensure that they are suitable to accommodate additional capacity and the type of effluent being connected.
All relevant boundary positions to be checked prior to proceeding.



Rev.	Revision	Int.	Date
 Good & Tillotson Chartered Architects 2 The Studios, 318 Chorley Old Road, Bolton, BL1 4JU Tel : 01204 497700 Fax : 01204 497776 E-mail : info@goodandtiltotton.co.uk Web : www.goodandtiltotton.co.uk			
Client : Bolton MBC			
Project : Proposed Extensions, Alterations & External Works at Wilfred Geere House Highfield Road, Farnworth, Bolton, BL4 0PG			RIBA  Chartered Practice
Drawing : Elevations as Proposed			Date : July 2015
PLANNING DRAWING			Drawn : R.P.W.
Project No. : 1554			Scale : 1 : 100 @ A1
Drawing Number : 06			Drawing Revision :



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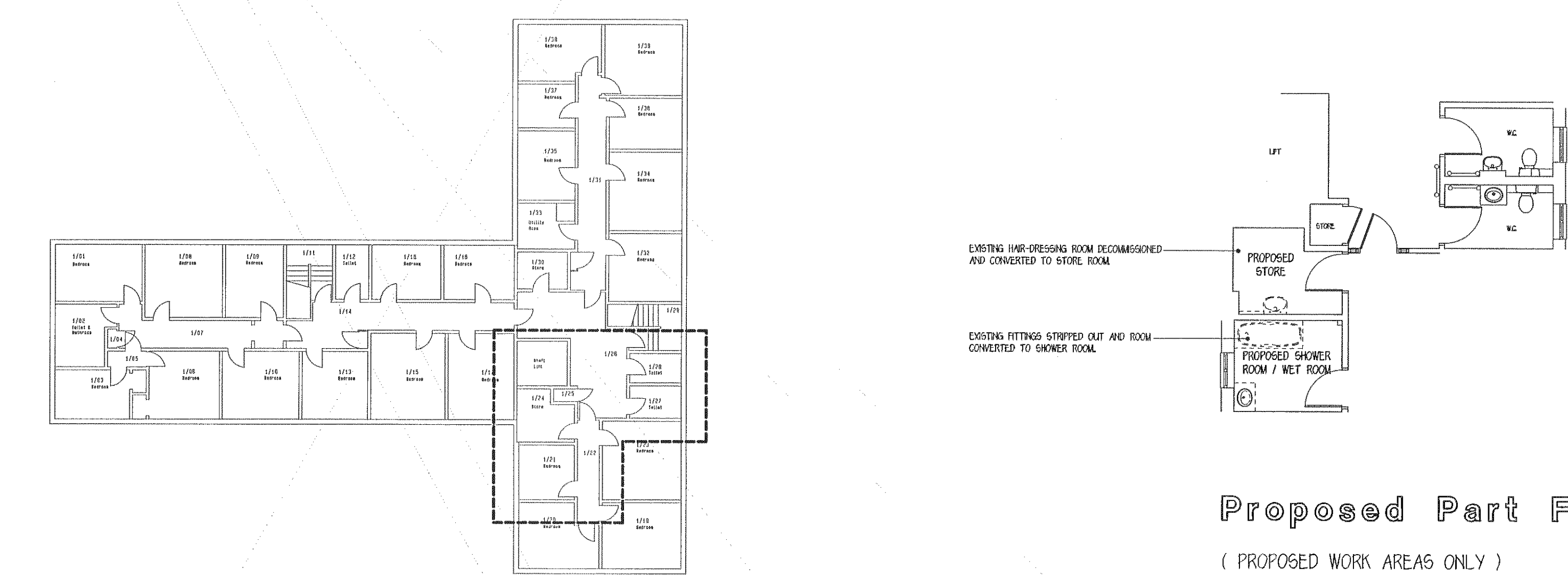
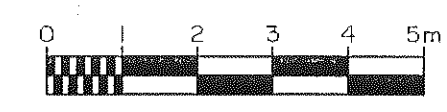
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NOTES :

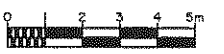
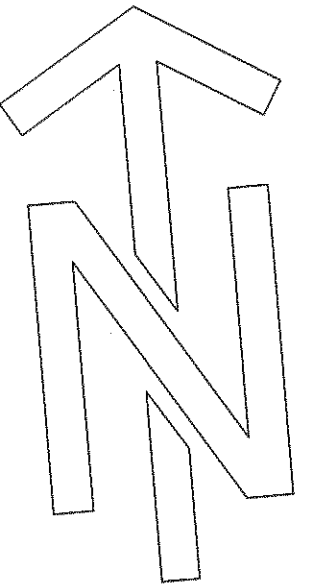
BUILDING NOT FULLY SURVEYED BY G & T. AREA'S SHOWN IN FINE PEN NOT SURVEYED AND LAYOUT IS BASED ON RECORD DRAWINGS.



First Floor Key Plan

BOLTON COUNCIL DEVELOPMENT & REGENERATION RECEIVED 05 AUG 2015			
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Client : Bolton MBC		RIBA Chartered Practice	
Project : Proposed Extensions, Alterations & External Works at Wilfred Geere House Highfield Road, Farnworth, Bolton, BL4 0PG		Date : July 2015 Drawn : R.P.W. Scale : 1 : 100 @ A1	
Drawing : Plans as Proposed PLANNING DRAWING		Drawing Revision :	
Project No. : 1554		Drawing Number : 05	

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All relevant boundary positions to be checked prior to proceeding.



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Rev. Revision Inl. Date



Good & Tillotson Chartered Architects

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Client : **Bolton MBC**

Project :
Prosed Extensions, Alterations &
External Works at Wilfred Geere House
Highfield Road, Farnworth,
Bolton, BL4 0PG

Date :
July 2015

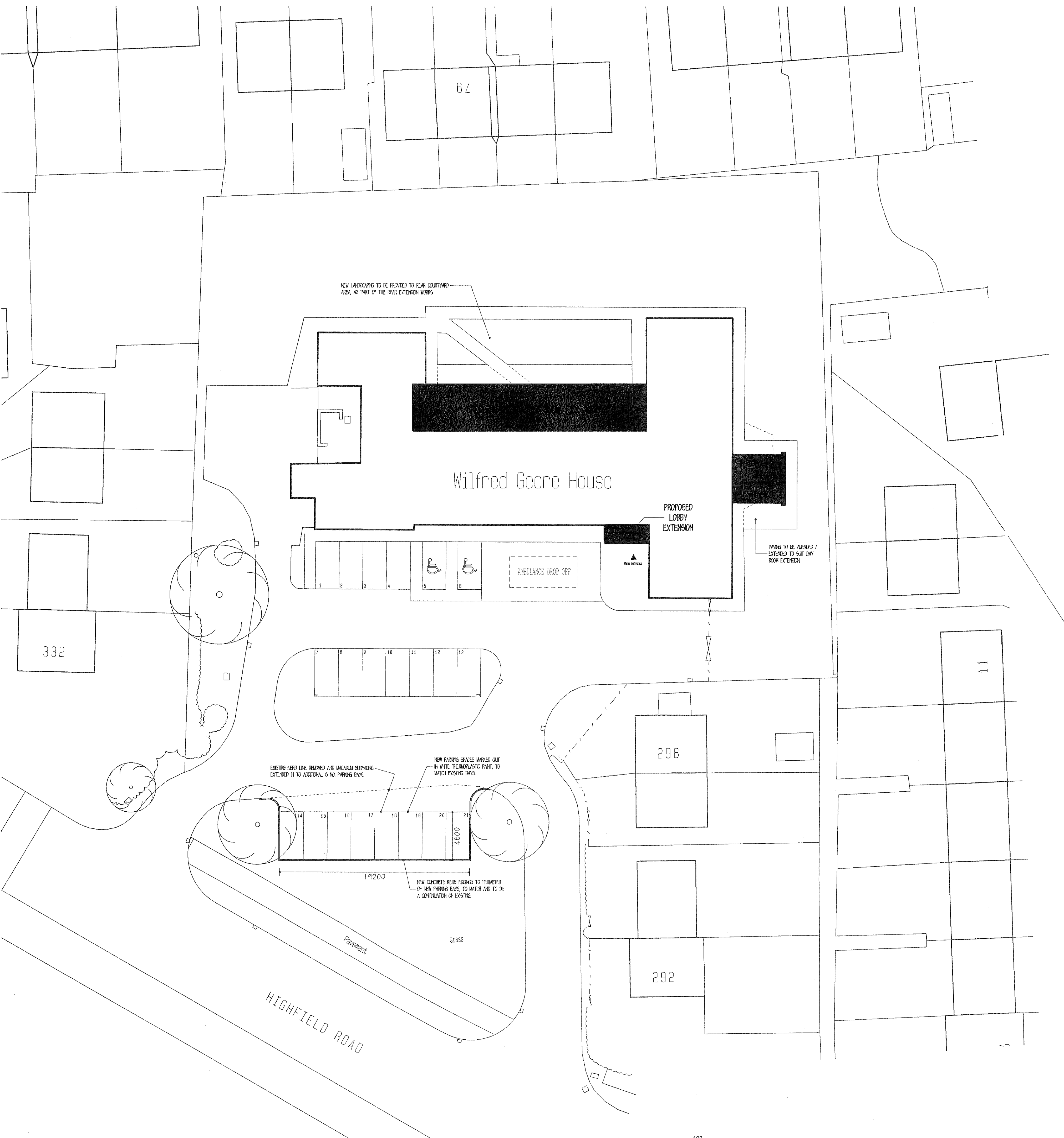
Drawn :
R.P.W.

Scale :
1 : 200 @ A1

Drawing Revision :

Project No. :
1554

Drawing Number :
02



Application number 94848/15



Development & Regeneration Dept
Development Management Section

Town Hall, Bolton, Lancashire, BL1 1RU
Telephone (01204) 333 333

Bolton Council

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Date of Meeting: 22/10/2015

Application Reference: 94848/15

Type of Application:

Registration Date: 21/08/2015

Decision Due By: 15/10/2015

Responsible Officer: Alex Allen

Location: 2 THE GRANGE, WESTHOUGHTON, BOLTON, BL5 2DQ

Proposal: OUTLINE APPLICATION FOR THE ERECTION OF ONE DWELLING (ALL MATTERS RESERVED)

Ward: Westhoughton South

Applicant: Mr James

Agent : Steven Abbott Associates LLP

Officers Report

Recommendation: Approve subject to conditions

Proposal

The applicant proposes the erection of one dwellinghouse within the rear garden of No. 2 The Grange, a large detached dwelling set in spacious grounds. The applicant seeks to establish the principle of residential development with the application being in outline form with all matters reserved for future consideration.

Site Characteristics

The application site forms part of the rear garden of an existing large detached dwelling (No. 2 The Grange) which is itself set in a large rear garden. The application site is characterised by a mown lawn area bounded by an established hedge to the rear and side boundaries of the site.

Policy

National Planning Policy Framework (NPPF)

Core Strategy policies: P5 Accessibility, S1 Safe, CG3 The Built Environment, CG4 Compatible Uses, OA3 Westhoughton

SPD General Design Principles and SPD Accessibility, Transport and Safety.

Analysis

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- * principle of residential development;
- * impact on the amenity of neighbouring residents;
- * impact on the highway;

Principle of residential development

Paragraph 49 of the National Planning Policy Framework (NPPF) states that housing applications should be considered in the context of the presumption in favour of sustainable development. Policy OA3 of the Core Strategy states that the Council will concentrate sites for new housing in Westhoughton town centre and on other sites within the existing urban area.

The application site is located within the established urban area located within the Dobb Brow area of Westhoughton.

The application site is a greenfield site in that it is the side garden to No. 2 The Grange. Paragraph 53 of the NPPF states that local planning authorities should consider the case for setting out policies to resist inappropriate development of residential gardens, for example where development would cause harm to the local area. The Council does not have a local policy relating specifically to the development of gardens, however Core Strategy Policy CG3 seeks to conserve local distinctiveness and requires development to be compatible with the surrounding area.

It is considered that the proposal would represent infill development (being at the rear / side of an existing dwelling within an established urban area) and a dwelling sited here would be viewed as a continuation of the established urban form of the area, which would not be to the detriment of local distinctiveness or the character and appearance of the area. Whilst the proposed dwelling shown on the indicative layout plan is large, detailed assessment regarding the siting, scale and massing would be provided at the reserved matters stage.

Officers consider that the rear garden of No. 2 The Grange is substantial to accommodate one dwelling and therefore does not represent overdevelopment.

The proposal complies with policy.

Impact on the Amenity of Neighbouring Residents

Policy CG4 of the Core Strategy states that the Council will ensure that new development is compatible with surrounding land uses and occupiers, protecting amenity, privacy, safety and security. Section 3 (1) of SPD General Design Principles sets out recommended interface distances between dwellings.

The proposed new dwelling would be sited within the curtilage of an existing property. The application site is relatively large and therefore there is scope to locate any new dwelling away from the existing dwelling to ensure compliance with the Council's interface guidelines. An existing garage and stable are located adjacent to the application site which are within the curtilage of the existing dwelling. The applicant has confirmed that the manure storage area for the existing stables has

been sited away from the application site.

It is therefore considered that the proposed development would not unduly harm the amenity of neighbouring residents and would comply with Policy CG4 of the Core Strategy.

Impact on the Highway

Policy P5 of the Core Strategy states that the Council will ensure that developments take into account [amongst other things] accessibility, servicing arrangements and parking. Policy S1.2 states that the Council will promote road safety in the design of new development.

The indicative plans submitted with the application show a proposed vehicular access off the existing driveway to No. 2 The Grange with garage provision and the parking for new vehicles.

The Council's Highways Engineers have commented that they have no objections to the proposal. Detailed consideration of this matter would be provided at the reserved matters stage.

It is considered that the proposed development would not jeopardise highway safety, compliant with Policies P5 and S1.2 of the Core Strategy.

Other Matters

As the application is in outline form all other matters would be considered at the reserved matters stage should Members be minded to grant approval for the current application.

Officers from Design for Security have commented that the application site would not benefit from natural surveillance. The site is located at the rear of No. 2, the sole access to the property would be from the existing driveway at No. 2. Therefore, the occupants of No. 2 would be able to survey who enters and leaves the application site. Furthermore, the existing road network in Dobb Brow serves a significant number of existing dwellings who look over the proposed access road. It is considered that by providing an additional dwelling in this location would increase the footfall and vehicular movement which would to the benefit of natural surveillance.

Conclusion

For the reasons discussed above it is considered that the proposal would represent a sustainable form of development with the potential to be compatible with the character and appearance of the area without compromising highway safety or the living conditions of the occupants of the adjoining property.

Details matters of design, siting and scale would be considered at the reserved matters stage. The proposal complies with policy and Members are therefore recommended to approve this outline application.

Representation and Consultation Annex

Representations

Letters/petitions:- none received.

Town Council:- Westhoughton Town Council raised objection as the proposal represents overdevelopment of the site.

Elected Members:- no comments received.

Consultations

Advice was sought from the following consultees: Westhoughton Town Council, the Council's Highways Engineers and Environmental Health Officers and Design for Security Officers from Greater Manchester Police.

Planning History

Planning permission was granted in September 1996 (Ref: 49051/96) for the erection of 6 detached houses and garages, construction of access road, footpaths after demolition of the farm.

Recommendation: **Approve subject to conditions**

Recommended Conditions and/or Reasons

1. Application for the approval of 'Reserved Matters' must be made not later than the expiration of three years beginning with the date of this permission and the development must be begun not later than whichever is the later of the following dates:
 - i) The expiration of five years from the date of this permission, or
 - ii) The expiration of two years from the final approval of the Reserved Matters, or, in the case of approval on different dates, the final approval of the last such matter to be approved.

Reason

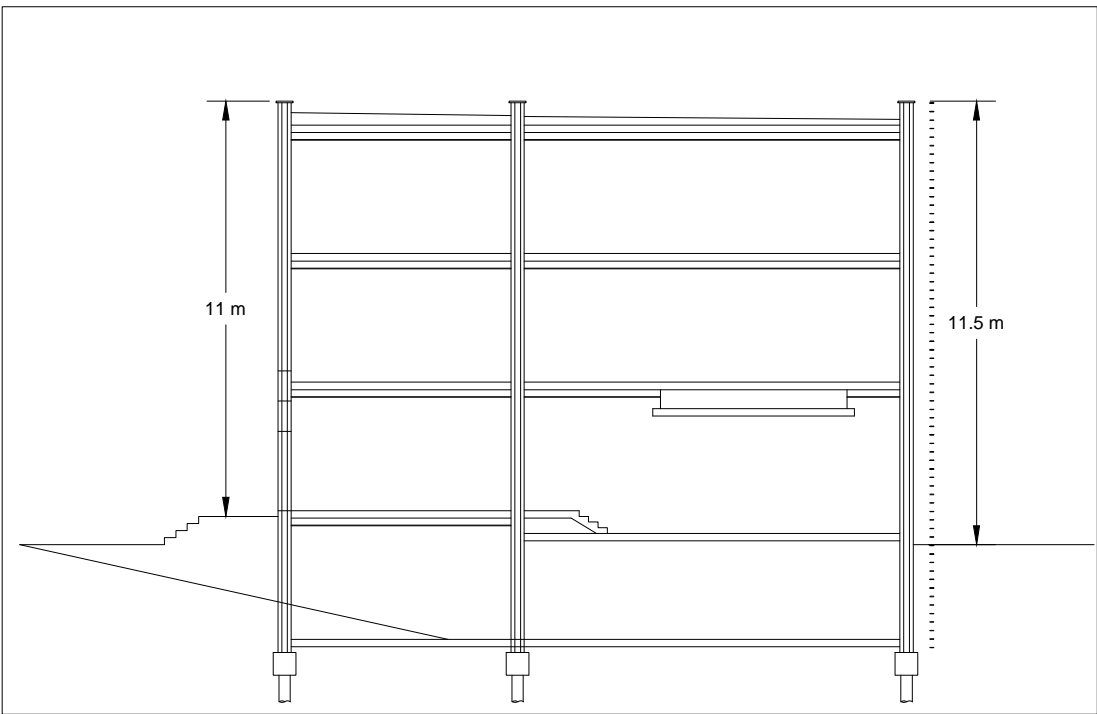
Required to be imposed pursuant to Section 92 of the Town and Country Planning Act 1990.

2. Details of the access, appearance, landscaping, layout, and scale, (hereinafter called "the reserved matters") shall be submitted to and approved in writing by the Local Planning Authority before any development begins and the development shall be carried out as approved.

Reason

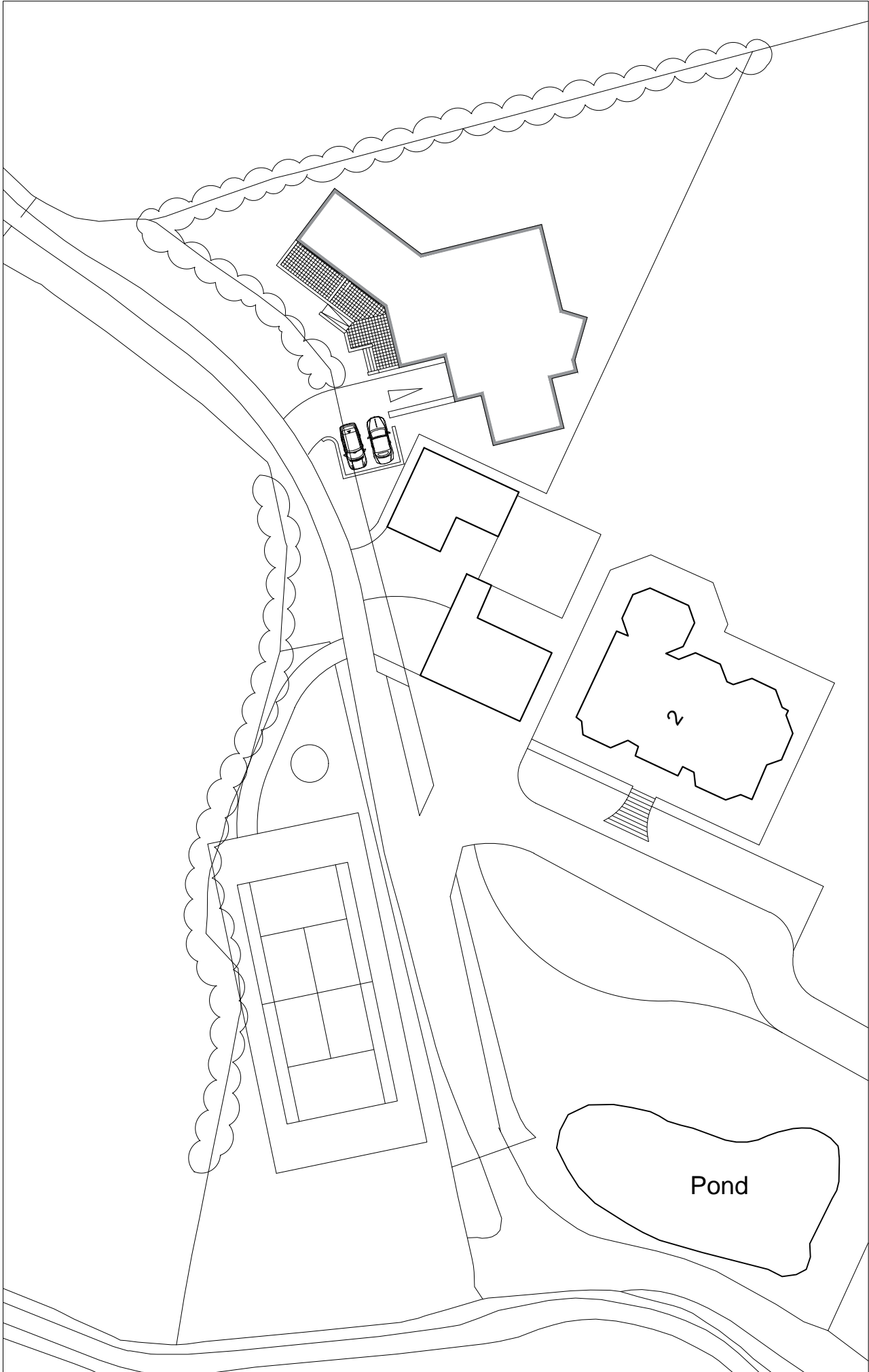
To application is for outline planning permission and these matters were reserved by the applicant for subsequent approval.

Dobb Brow, Westhoughton.

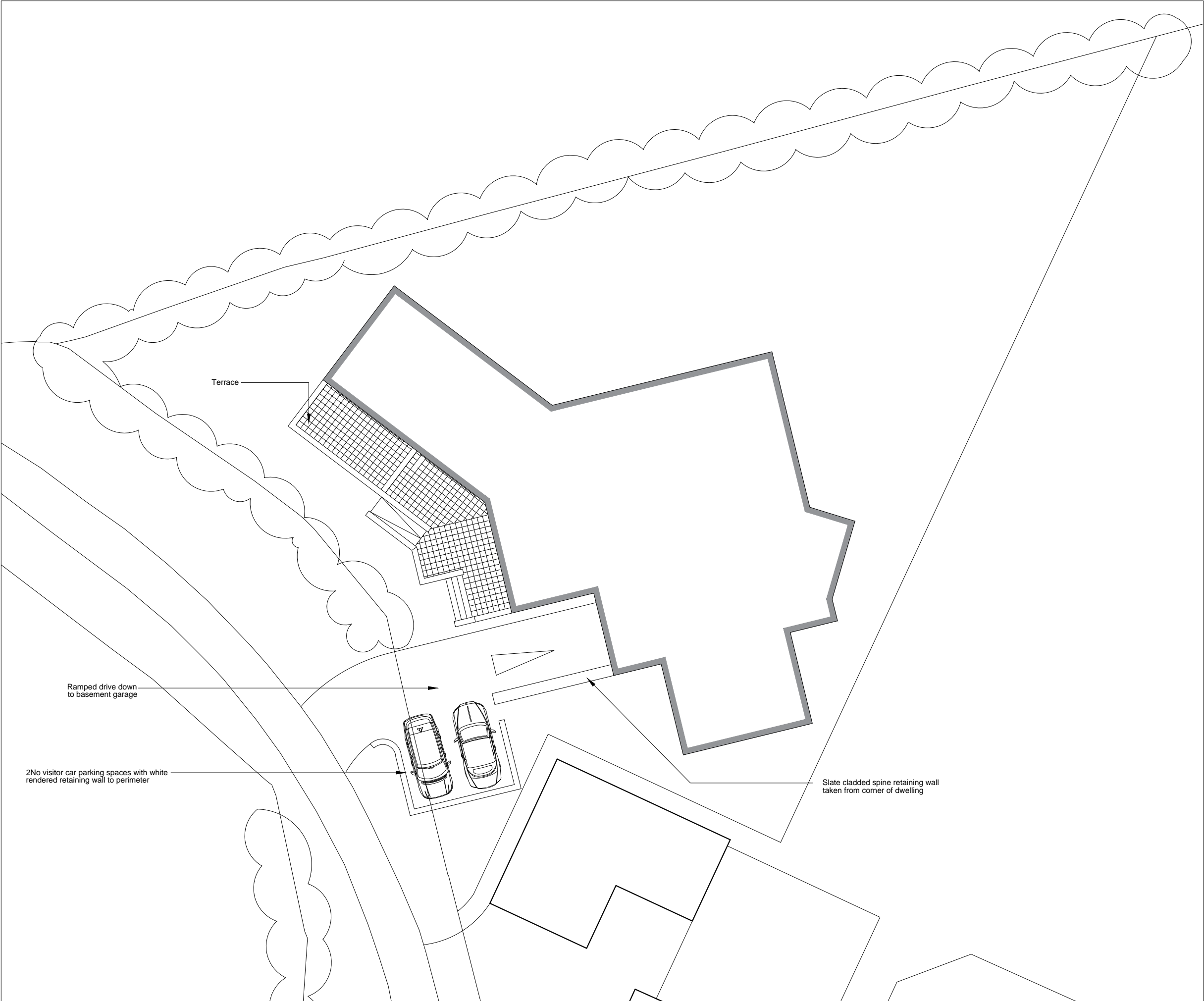


Illustrative Section
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Illustrative Location Plan
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Illustrative Site Layout
Scale 1:200 @ A2

Rev	Date	Revision	Initial
+ Client			
MICHAEL JAMES			
+ Drawing Title			
DESIGN PROPOSALS Illustrative Only			
+ Project			
Dobb Brow, Westhoughton. Westhoughton.			
Job No	Drg No	Drawn	Rev
14007	01	J. Scott	
Scale	Date	Stage	DRAFT
As ind.@A2	24.11.14		
+ MPSL Planning & Design Ltd 14 West Point Enterprise Park, Clarence Avenue, Trafford Park, Manchester. M17 1GS			
			
* Tel: 0161 772 1999 Email: admin@mpslgroup.co.uk www.mpslgroup.com			

Application number 94882/15



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Date of Meeting: 22/10/2015

Application Reference: 94882/15

Type of Application: Full Planning Application

Registration Date: 26/08/2015

Decision Due By: 20/10/2015

Responsible Officer: Jodie Turton

Location: 20 DEVONSHIRE ROAD, BOLTON, BL1 4PG

Proposal: VARIATION OF CONDITION 2 AND 3 ON PLANNING APPLICATION 90074/13 (WORDING TO SAY:THE PREMISES SHALL BE USED AS AN EDUCATION/COMMUNITY CENTRE AND A PLACE OF WORSHIP, AND FOR NO OTHER PURPOSE (IN CLASS D1 OF THE SCHEDULE TO THE TOWN AND COUNTRY PLANNING USE CLASSES ORDER 1987, OR IN ANY PROVISION EQUIVALENT TO THAT CLASS IN ANY STATUTORY INSTRUMENT REVOKING AND RE-ENACTING THAT ORDER).

Ward: Smithills

Applicant: Azhar Academy

Agent : Urban Solutions

Officers Report

Recommendation: Delegate the decision to the Director

Background

Planning permission was approved by the Planning Committee in July 2013 (90074/13) for the variation of conditions 2 and 3 on planning permission 82947/09 to allow for the ancillary use of the education centre as a prayer hall and to extend the hours of operation of the premises. This applied to two rooms only, located on the first floor of the building. The conditions state:

2. *The use of the rooms, within the premises, as a prayer hall shall only be from the time of first daylight until two hours after dusk all year round and this area shall be restricted to the two rooms on the first floor of the building as shown on the approved plans (First Floor Plan; received 02 July 2013).*
3. *The premises shall be used as an education/community centre with ancillary prayer rooms only and for no other purpose (including a mosque and any other purposes in Class D1 of the Schedule to the Town and Country Planning Use Classes Order 1987, or in any provision equivalent to that Class in any statutory instrument revoking and re-enacting that Order).*

Proposal

The current proposal is a result of an enforcement investigation into the use of the premises as a mosque/prayer hall over and above the use of the two rooms approved previously.

The application proposes the unrestricted use of the premises as a place of worship. The application seeks to amend the conditions as follows:

2. *The premises shall be in use/open as a place of worship only, from the time of first daylight until two hours after dusk all year round.*
3. *The premises shall be used as an education/community centre and a place of worship, and for no other purpose (including any other purposes in Class D1 of the Schedule to the Town and Country Planning Use Classes Order 1987, or in any provision equivalent to that Class in any statutory instrument revoking and re-enacting that Order).*

Site Characteristics

The site is located between two existing schools, Devonshire Road Primary School and Thomasson Memorial School. The site was formally a detached property within a large plot of land. Trees located along the main road frontage of Devonshire Road are protected by a Tree Preservation Order. The surrounding area is predominately residential.

Policy

National Planning Policy Framework: 8. Promoting Healthy Communities.

Planning Practice Guidance

Core Strategy Policies: CG3 The Built Environment, CG4 Compatible Uses, SO2 Education, SO9 & S1 Crime and Safety, P5 Accessibility and Road Safety, SC2 Community Provision.

SPD Accessibility, Transport and Safety

Analysis

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- * impact on neighbouring residential amenity
- * impact on parking and the highway
- * impact on community provision

Impact on Neighbouring Residential Amenity

Core Strategy policy CG4 seeks to ensure that new development is compatible with surrounding land uses and occupiers, protecting amenity and privacy. Furthermore development should not generate unacceptable nuisance or noise.

Members are aware that the application is a result of enforcement action due to the prayer hall function of the premises. Residents raised concerns that the prayer hall use, permitted in two rooms on the first floor of the premises, had extended over and above the permitted area, which has resulted in an increased number of worshippers at the centre and a consequent detrimental impact on residential amenity due to traffic congestion, parking issues, noise and disturbance. The purpose of this application is for Members to balance whether the proposed increase in floor area of the place of worship and the resultant increase in the number of users would have a greater detrimental impact on residential amenity and parking congestion than the limited use at present.

The previous applications for the prayer hall use were approved on the basis of there being between 2 and 20 people using the centre out of the permitted education centre hours. Although within the education centre hours, in April 2015 the Enforcement Officer counted 137 people entering the centre for Friday prayers and 36 cars were parked in the vicinity. It was however confirmed by the centre that a special event had taken place on this day which resulted in the higher numbers of worshippers and it has also been confirmed that it is normal for the prayer hall to have between 45 and 55 worshippers for Friday prayers.

Devonshire Road is a largely residential street within a predominantly residential area. The eastern side of Devonshire Road is however dominated by educational establishments, with two schools and the Islamic education centre (20 Devonshire Road). It is quite evident from the number of objections received that there is an issue with community cohesion and integration around the place of worship function of the centre. A large proportion of the 66 objections received to date are from residents on the surrounding streets (for example Devonshire Road, Lonsdale Road, Kirkby Road, Somerset Road). The primary concerns are detailed in the representations section below, however there is a clear theme throughout of parking problems, particularly inconsiderate parking, a lack of consideration for residents, especially by centre users late at night where car doors are slammed, music is played and groups hang around chatting loudly after prayers. These issues are clearly from the place of worship function and not the education centre function due to the issues being primarily at Friday prayers and late at night.

There is no change proposed to the hours of operation, these would remain the same as for the existing prayer hall function. The issue in terms of impact on residential amenity would be the increased volume of people using the centre at unsociable hours (at the height of Summer dawn is at 03:53 and dusk is at 22:34, the prayer hall has permission to open at dawn and until 2 hours after dusk) and the resultant noise and disturbance from the increased number of users.

Impact on the Highway

Core Strategy policy S1 seeks to promote road safety in the design of new development. Core Strategy policy P5, in conjunction with Appendix 3 and SPD 'Accessibility, Transport and Safety', seeks to ensure that adequate parking provision is made for the development.

The education/community centre has an existing, dedicated car park with 13 spaces. There is also the added provision of an overspill car park at Thomasson Memorial Primary School. There are certain times of the day where the site and surrounding uses, one being Devonshire Road Primary School, would generate a high level of vehicular movements to and from the area, however these times are limited and would generally only last for short periods i.e. drop off and pick up times at the school.

Appendix 3 of the Core Strategy makes a requirement for places of worship to have 1 parking space per 5 sq metres of public space. If the public space it taken to be the area of the prayer hall then the current area of prayer hall would have a requirement of 13 car parking spaces, which is met by

the existing car park. The applicant has been asked to provide clarification of the precise floor area to be used for the additional prayer halls proposed.

Despite this, it is clear that the current parking provision is not sufficient to serve the existing demands of the centre, and indeed any increase in the number of users of the centre would further exacerbate this problem. The overspill car park at Thomasson Memorial Primary School is not used and indeed one of the most pressing times for parking pressure is during Friday prayers, which is during the school day and therefore the school car park would not be available for use. A parking management plan was previously recommended by Officers, however this was not considered necessary at the time. If Members are minded to approve the application then a parking management plan is essential, in addition to the centre exploring the opportunity for additional dedicated parking for the centre.

Impact on Community Provision

Core Strategy Policy SC2 states that the council and its partners will ensure that local cultural activities and community facilities are located in the neighbourhoods that they serve. The Council encourages community cohesion, taking into account the needs of different groups in the borough, including different ethnic groups and faith groups. In doing so, it must make sure that all these different groups have access to cultural and community facilities.

The site is an existing educational/community facility and serves the locality and immediate area. The place of worship function of the centre is considered to be a cultural activity that is compatible with the existing use of the site. Indeed the educational, community and prayer hall uses all fall within Use Class D1 as defined by the Town and Country Planning (Use Classes Order) 1987 (as amended), and are therefore considered to have similar impacts in terms of traffic generation, noise, number of visitors, etc. Planning permission would not therefore usually be required for the creation of, or change of use to, a prayer hall/place of worship within an educational/community facility such as this, however in this case, it has been prevented without further consent by condition 3 on the original planning permission (82947/13).

There have been a number of objections to the application from nearby residents. They have raised concerns that it is generally people from outside of the area that are using the prayer rooms and that it is not therefore a local, community facility as covered by Core Strategy policy SC2. Furthermore, they raise the fact that a new mosque has recently been built within walking distance of the application site, and therefore question the need for additional prayer rooms.

In contrast, there have been 57 letters of support and a large proportion of these are from members of the local community who live on surrounding streets. The letters stress the importance of the prayer hall as a local community facility, the convenience of this meaning that they can walk there rather than having to drive somewhere further away and that the facility is good for community cohesion.

The previous application was approved on the basis that the prayer hall function would be ancillary to the existing community/educational operation and would be unlikely to generate any new visitors. However, to allow the community centre to be used as a place of worship, not ancillary to the education/community function would undoubtedly result in additional visitors to the centre, as has already been evidenced with just the two rooms permitted at present.

The site is considered to be in an accessible location, and is close to, and accessible to the community it serves, however it has become and will further become if permission is granted, a place of worship for people outside of the local area. Policy SC2 recognises the benefits of religious, educational and community development. The application is considered to deliver community

benefits, however more needs to be done by the centre in terms of community cohesion and integration to address the issues and the disharmony with the local residents.

Conclusion

Members need to consider whether it is in the best interests of the local community to approve the application and take this as an opportunity to resolve some of the fundamental issues that have emerged from the use of the building as an educational facility and more pertinently a place of worship. If Members are minded to take this route then it is recommended that the application is delegated to the director in order to negotiate a traffic management plan, investigate opportunities for additional car parking, and to develop a community liaison strategy to be devised, implemented and monitored. The aim of this would be to alleviate the parking pressure on the surrounding streets, to work with the mosque and local residents to overcome some of the points of friction, namely noise (especially at night/early morning), inconsiderate parking and to develop stronger community relations.

Representation and Consultation Annex

Representations

Letters:-

Objection

66 letters of objection have been received. A large number are from local residents on Devonshire Road and the neighbouring streets. Some objections have been received from residents across Bolton objecting to the principle of another mosque in the Borough. The objectors raise the following issues:

-) The proposal will result in more people using the facility and more traffic.
-) The traffic congestion on this road with two schools and Bolton School nearby is too much.
-) The excess traffic and disturbance will be too much from an almost 3,500 sq ft mosque. This is unacceptable for the safety and security of the two neighbouring schools and for the congested road network.
-) This is not a community facility but one for a section of the population only.
-) The planning statement makes false claim that worshippers would have to travel 2-3 miles to another mosque when there are other mosques very close by (e.g. Tudor Avenue).
-) Concerns about on-street parking and congestion.
-) The area has a strong local community – the proposed mosque could change this.
-) The applicants have a breach of planning breaches at the site and they are trying to get a mosque by stealth.
-) Impact on highway safety – particularly in relation to the neighbouring schools.
-) The planning statement makes false claims – there are problems with parking and traffic.
-) To increase the capacity for those praying would make the current situation worse.
-) Anti-social behaviour from users of the centre when asked to move vehicles/be quieter in the middle of the night.
-) The centre has failed to address the issues with the local community.
-) There is no integration or positive dialogue with the local community.
-) The gateway to Devonshire Court is often used as a temporary car park, making it impossible for residents to enter/leave.
-) The hours of operation are unreasonable in a residential area.
-) Too many mosques in Bolton and several within a 1 mile radius of this site.
-) Dangerous driving and parking.
-) High levels of noise from people using the facility – car doors slamming, radios blaring, hanging around chatting in the middle of the night.
-) Back street used for parking inappropriately.
-) One Friday 137 people were counted leaving the premises after prayers.
-) The number of cars coming and going late in the evening has steadily increased over the last year.

Residents Association:

-) The original aspiration of the building was for educational attainment, we the residents do not believe this is happening.
-) The Resident's Association maintains a dialogue with the management team who admit it is a struggle to enforce members to comply with the current conditions.
-) On-going problems of noise – notably from car horns and car doors slamming.
-) The current volume of traffic is a danger to local residents and children using the centre.
-) The vast majority of people using the centre do not come from the local community and thus the high car usage.
-) To vary the conditions as proposed would have an even greater impact on the local community.
-) There has been no evidence that the use of the building has promoted citizenship, reduced

anti-social behaviour, and supported physical, social, emotional and spiritual education development of young people.

- \ The management team do a good job trying to ensure that attendees to the centre park responsibly, however there is not enough parking so this is an impossible job.
- \ Attendees to the Friday prayer are increasing causing more traffic problems.
- \ People park inconsiderately, blocking residents driveways.

Support

57 letters of support have been received from members of the local community on Devonshire Road and the neighbouring streets as well as further afield, making the following comments:

- \ Very happy with the facility, well run and well managed.
- \ An excellent local facility for my family.
- \ Allows local people to fulfil their religious obligations with ease.
- \ A good education facility for local children.
- \ The community centre helps with Muslim integration in the community.
- \ This local facility helps to avoid people having to use their cars.
- \ Good for community cohesion.
- \ The application would improve facilities and accessibility to members of the public.
- \ The centre provides little inconvenience to local people due to it having off road parking.

Two letters of general comment have also been received.

Elected Members:- Councillor Shaw has raised objection to the proposal.

Consultations

Advice was sought from the following consultees: Highway Engineers, Pollution Control Officers, Tree and Woodland Officers, Counter Terrorism Unit.

Planning History

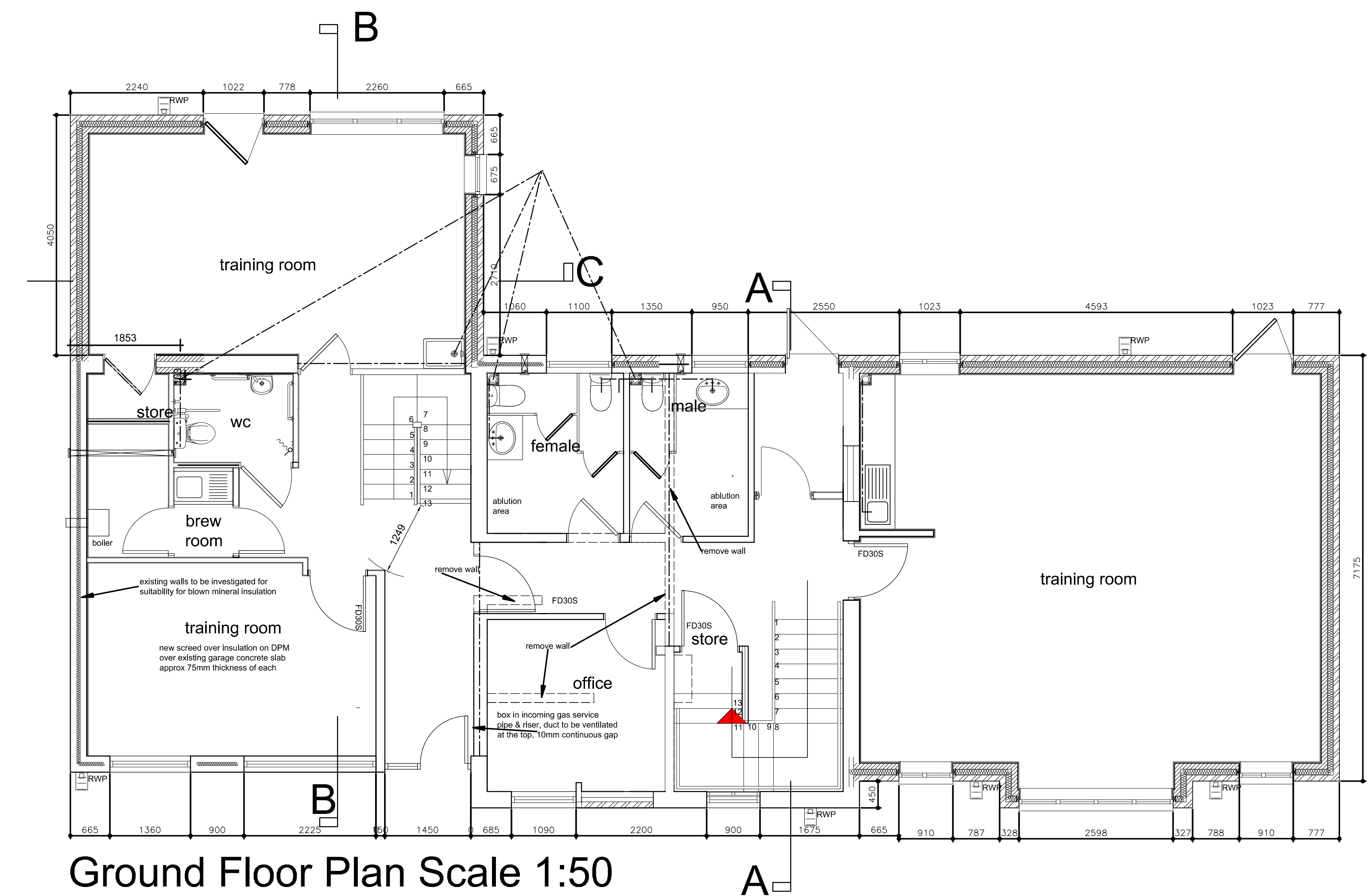
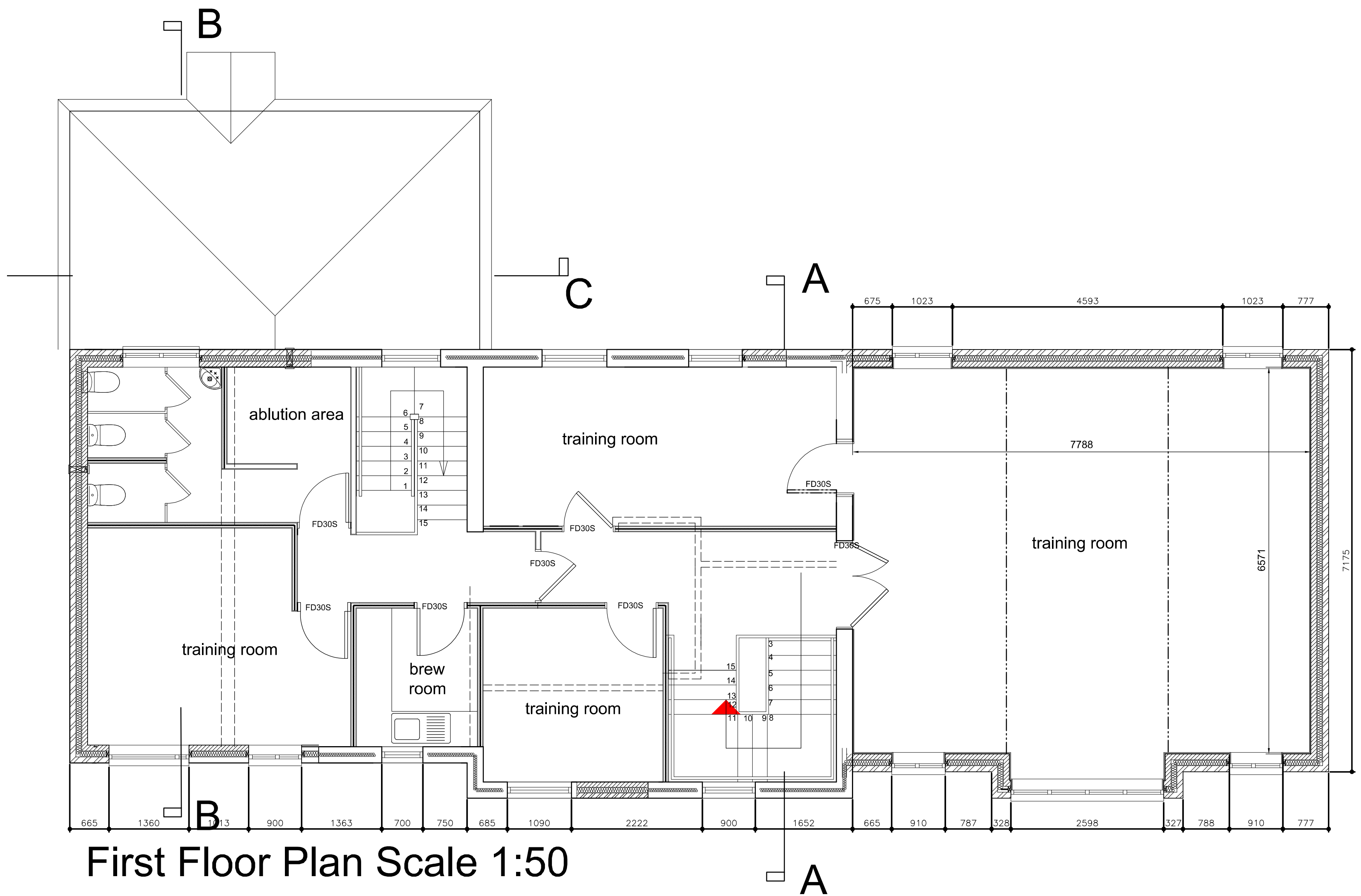
Planning permission was approved by Committee in July 2013 for the use of two rooms on the first floor as prayer rooms and the extension of opening hours associated with this (90074/13).

Temporary planning permission was approved by Committee for a period of 1 year in July 2012 for the use of two rooms as prayer rooms and the extension of opening hours associated with this use (88119/12).

Planning permission was approved by Committee in December 2009 for the change of use from residential to education centre (Class D1) including erection of first floor extension at side, two storey side extension and single storey rear extension, changes to elevations, provision of 13 additional parking spaces together with access to over spill car park, formation of new access to Devonshire Road and drop off facility (82947/09).

Recommendation: **Delegate the decision to the Director**

Recommended Conditions and/or Reasons



nicol thomas

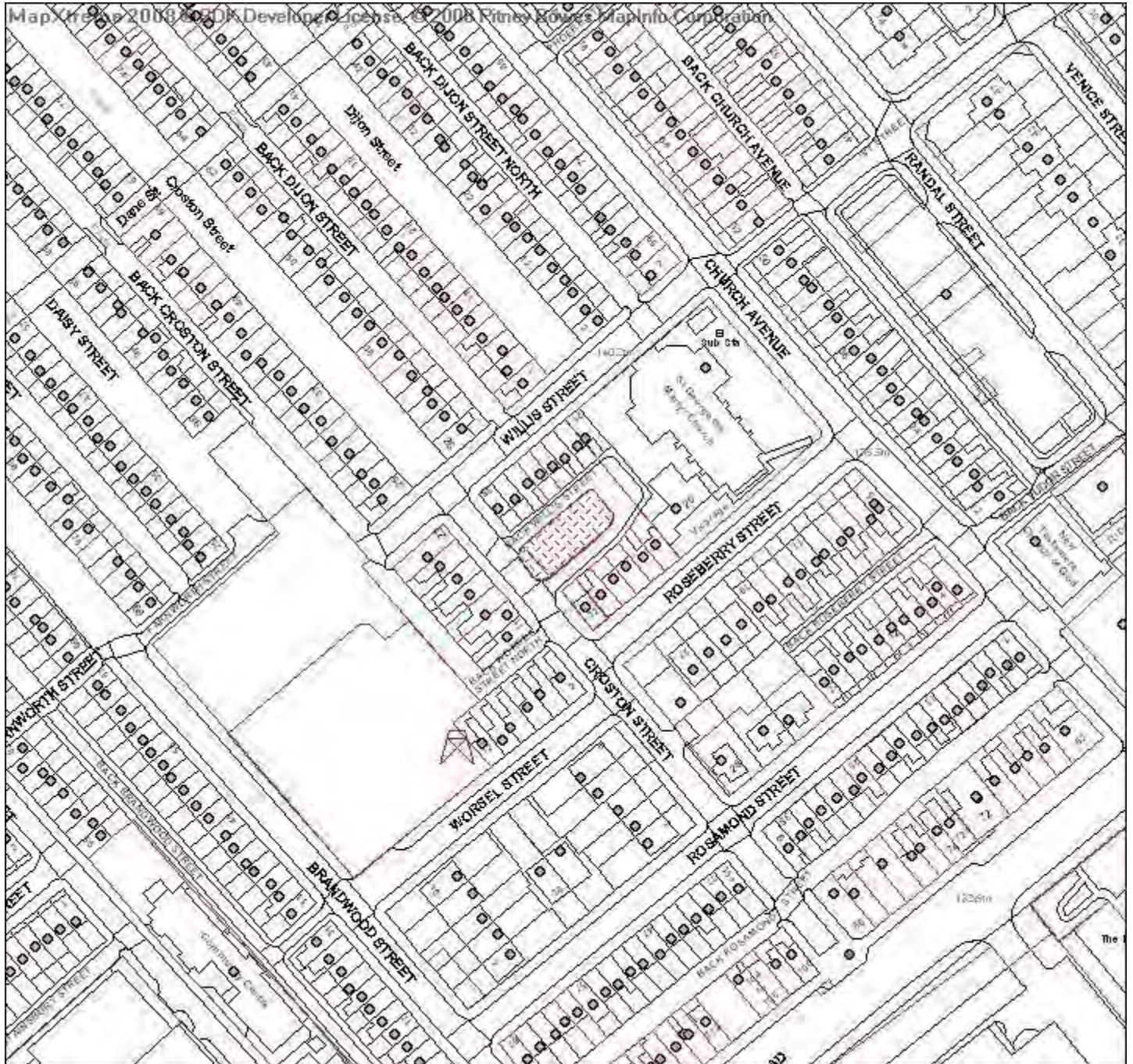
Revision	Date	Details
A	01-12-2010	corridors made 1200mm min width, additional disabled wc added to GF, second staircase added
B	01-03-2011	revisions to clients requirements
C	08-03-2011	Ablution area indicated to GF toilets

nicol thomas
architects project managers construction cost consultants CDM co-ordinators
Registered in England and Wales, Reg No. 2140639
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Client:	Azhar Academy Bolton			
Job:	Proposed education center Devonshire Rd			
Drawing title:	Proposed plans			
Drawing Number: (Job number)	M3188	(C)	102	Revision: c
Scale:	1:50 @A1			
Date:	AUG 2010			
Drawn by/ checked by:	mcs			

Application number 94947/15



Development & Regeneration Dept
Development Management Section

Town Hall, Bolton, Lancashire, BL1 1RU
Telephone (01204) 333 333

Bolton Council

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Date of Meeting: 22/10/2015

Application Reference: 94947/15

Type of Application: Full Planning Application
Registration Date: 10/09/2015
Decision Due By: 04/11/2015
Responsible Officer: Jeanette Isherwood

Location: LAND AT CROSTON STREET, BOLTON

Proposal: ERECTION OF ONE DWELLING INCLUDING THE INSERTION OF THREE OBSCURE GLAZED WINDOWS TO THE GROUND FLOOR (AMENDMENT TO PREVIOUSLY APPROVED APPLICATION)

Ward: Rumworth

Applicant: Mr Dhoraji
Agent : RA Design & Project Management

Officers Report

Recommendation: Approve subject to conditions

Proposal

A proposal for the erection of one detached dwelling was approved at Planning Committee on the 12th March 2015 under reference 92959/14.

A further application to extend the height of the property was refused at Planning Committee on the 27th August 2015 under reference 94454/15.

This application now proposes an amendment to the previously approved scheme (92959/14) allowing for the insertion of 3 windows to the north west elevation to provide light to the dining room.

The windows will be of a narrow but long design and are detailed to be of level 5 obscurity.

Site Characteristics

Construction of the property has commenced on the site.

The site is a rectangular parcel of land on Croston Street. The immediate vicinity is one of high density Victorian terraced properties with small rear yards. The land originally contained two rows of small trees which have since been removed.

To the rear of the site is a large church and associated church house. The gables of the proposed property are directly overlooked by the rears of two rows of terraced properties, all with principal windows in their rear elevations. The majority of these properties have single storey rear extensions with one having a rear dormer in the roof space.

To the front, the site overlooks the front of properties on Croston Street. There is a distinct difference in ground levels on Croston Street with the road sloping downwards towards St Helens Road.

The site is bounded on both sides by the small cobbled access street serving the rears of the properties on Willis and Roseberry Street.

Policy

National Planning Policy Framework 2012

Core Strategy: CG3 Built Environment, CG4 Compatible Uses and RA1 Inner Bolton.

SPD General Design Guidance

Analysis

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- * impact on the character and appearance of the area
- * impact on residential amenity

Impact on the Character and Appearance of the Area

Policy CG3 of the Bolton Core Strategy states that the Council expects development proposals to contribute to good urban design, respect and enhance local distinctiveness, ensuring development has regard to the overall built character and landscape quality of the area.

Policy RA1 aims to respect and strengthen the traditional grid iron pattern and the street-scaping of existing housing where it is compatible with good urban design.

The windows are positioned centrally in the side elevation facing the rear walls of the properties on Willis Street. The addition of the windows is considered to be a minor amendment in terms of the visual appearance of the building and is not thought to impact detrimentally on the character and appearance of the area.

This minor proposal is considered to be compliant with Core Strategy policies CG3 and RA1.

Impact on Residential Amenity

Policy CG4 aims to ensure that new development is compatible with surrounding land uses and occupiers, protecting amenity, whilst SPD 'General Design Guidance' gives guidance on sufficient interface distances between properties.

It is acknowledged that the immediate vicinity is one of high density housing and there is an existing shortfall in interface distances. The windows will be sited approximately 9 metres from the rears of the properties on Willis Street which is considered to be insufficient when principal windows directly face each other. However there are several site conditions evident that alleviate these issues. These are:-

-) There is a distinct slope to the ground level with properties on Willis Street being sited on a higher level.
-) The windows are sited at ground level height and are somewhat screened from the properties directly facing by high boundary walls.
-) The windows will have a high level of obscurity and are therefore to be considered to have as much impact as a blank gable.

A condition will be added to the approval to ensure the correct degree of obscurity is retained and the window will remain non-opening.

The proposal is therefore considered to be acceptable and compliant with Core Strategy policy CG4 and SPD General Design Guidance.

Conclusion

For the reasons discussed above the application is considered to be acceptable and complies with Core Strategy Policies CG3, CG4 and SPD General Design Guidance.

Members are advised to approve the application.

Representation and Consultation Annex

Representations

Three letters of objection have been received from neighbouring residents, with the concerns being that the windows will lead to a loss of privacy. The other main objection is to the building of the approved dwelling itself.

Three letters of support have been received.

Consultations

Advice was sought from the following consultees: Highways, Environmental Health, GMP Design for Security.

Planning History

A proposal for the erection of one detached dwelling was approved at Planning Committee on the 12th March 2015 under reference 92959/14.

A further application to extend the height of the property was refused at Planning Committee on the 27th August 2015 under reference 94454/15.

Recommendation: Approve subject to conditions

Recommended Conditions and/or Reasons

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason

Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. Before the first occupation of the building hereby permitted the ground floor windows facing Willis Street shall be non-opening and fitted with obscure glazing whose obscuration level is 5 on a scale of 1-5 (where 1 is clear and 5 is completely obscure) and shall be permanently retained in that condition thereafter.

Reason

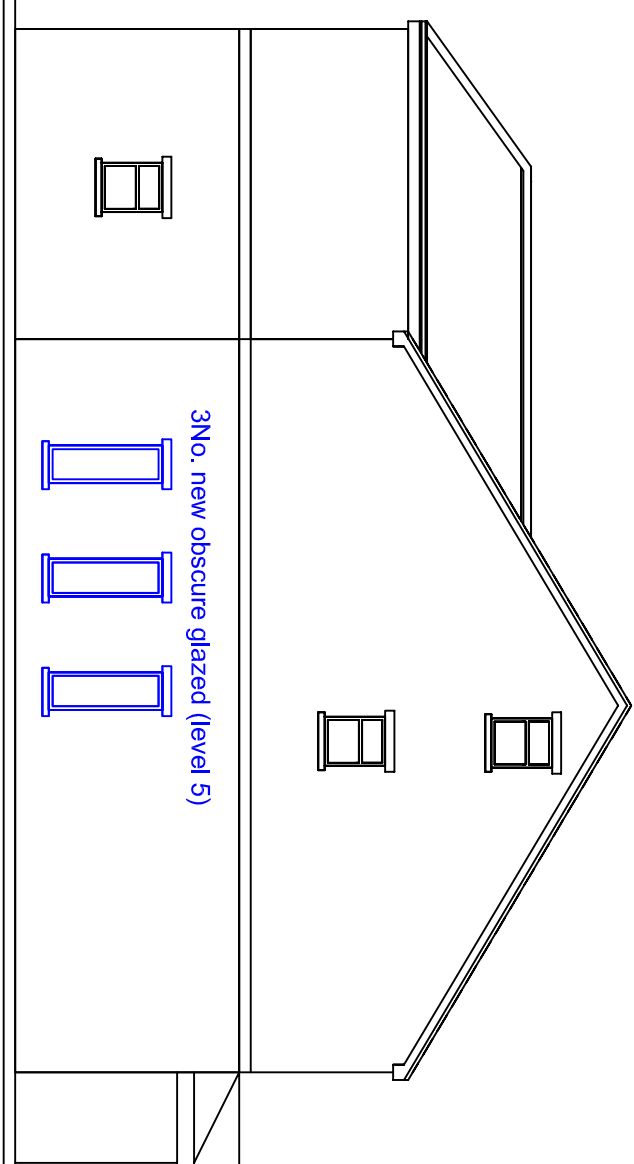
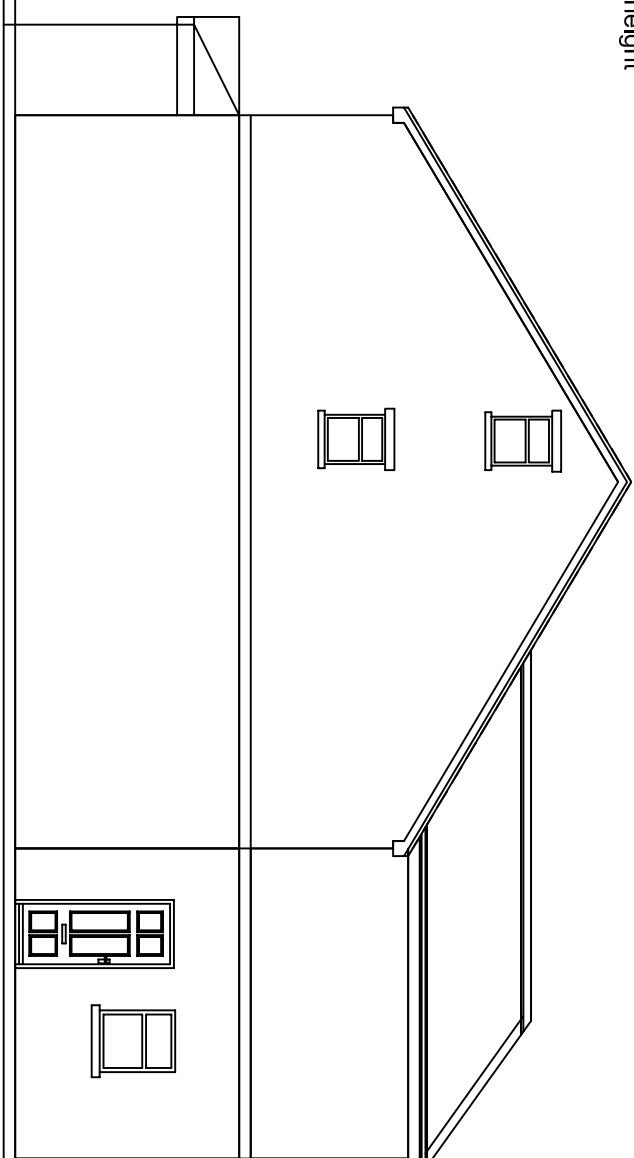
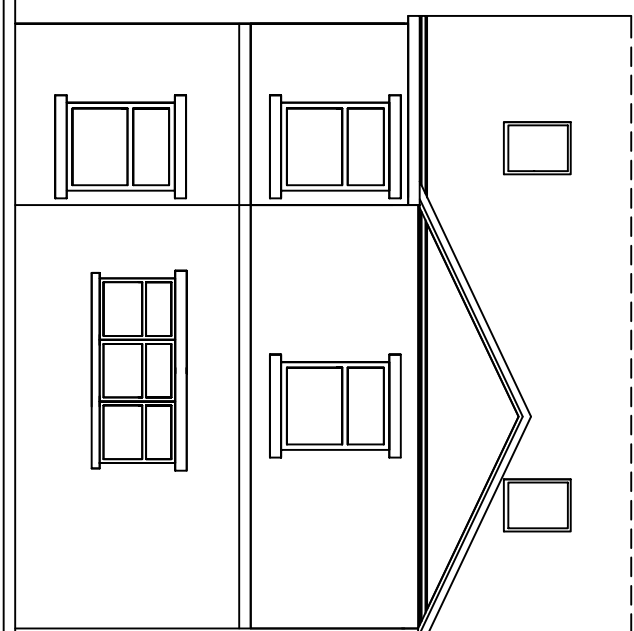
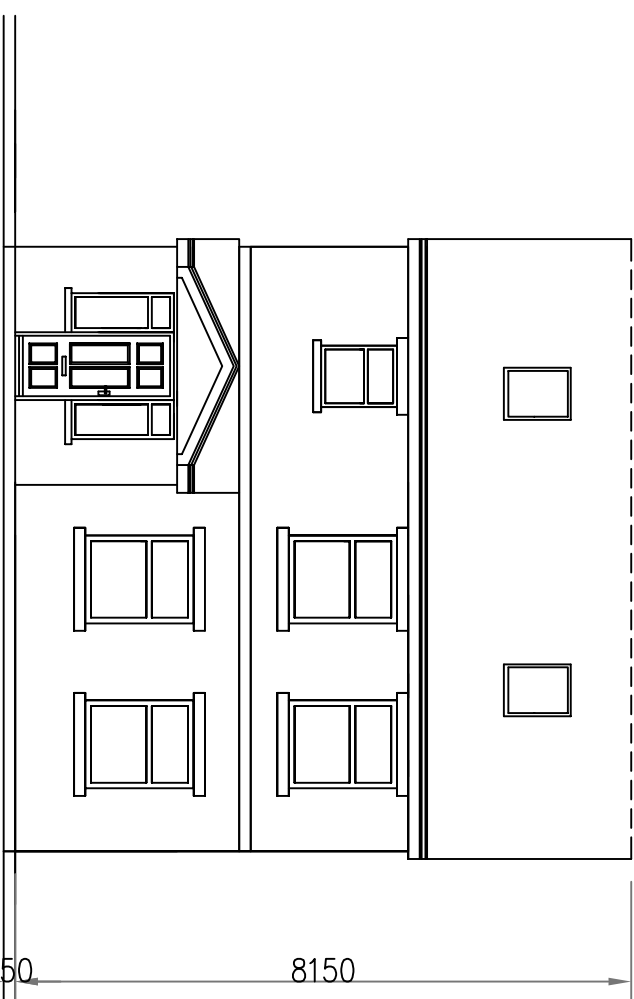
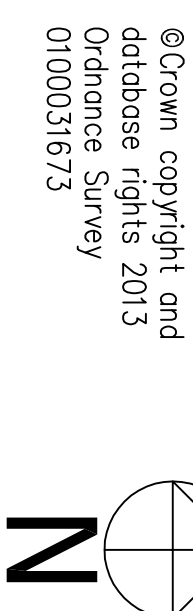
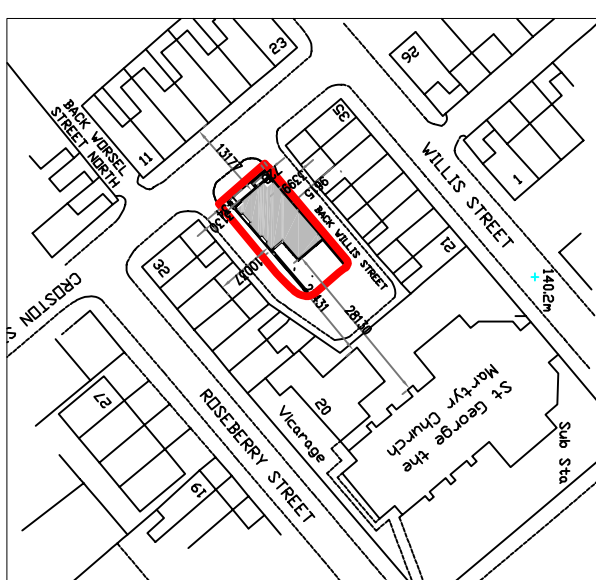
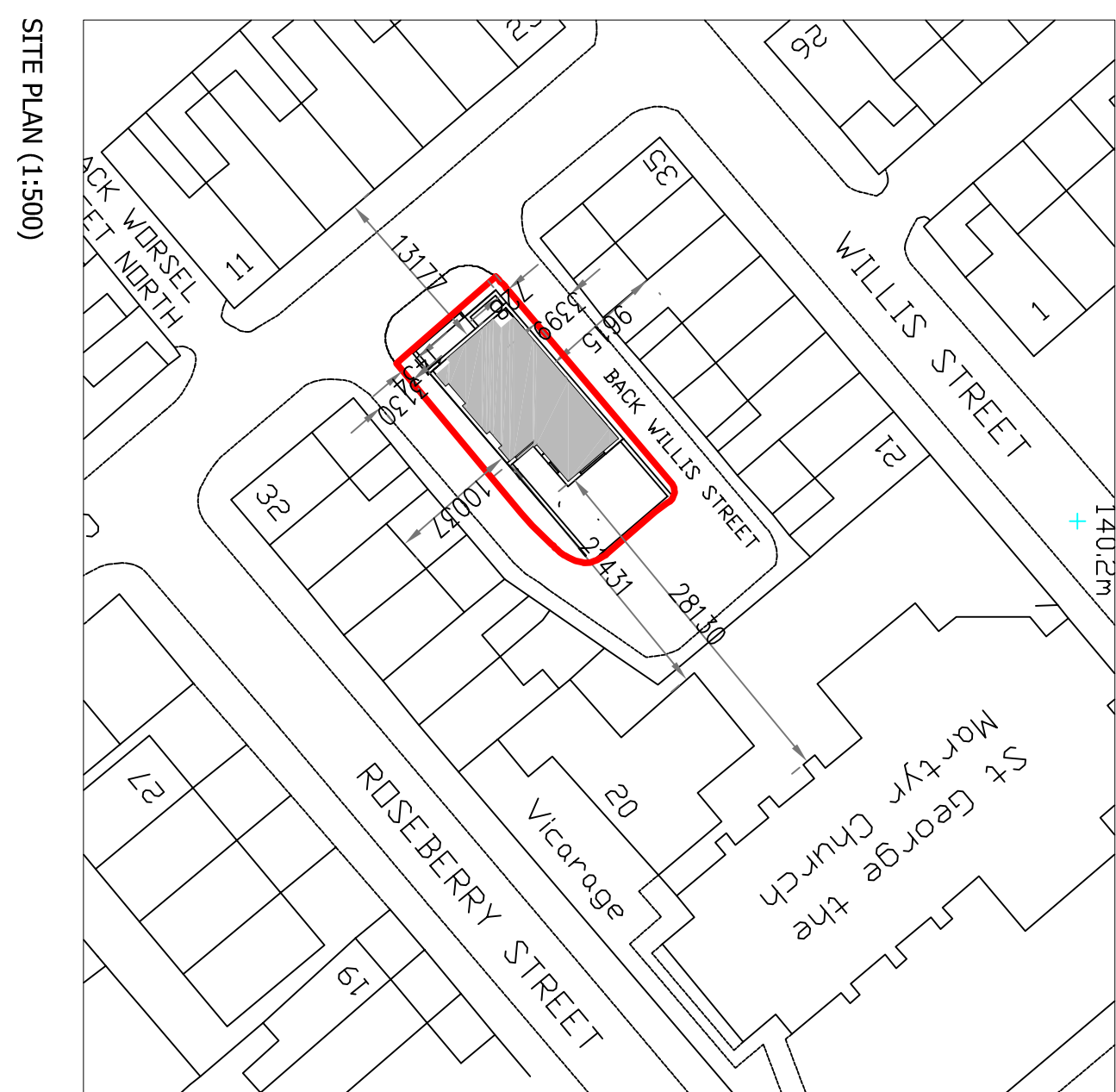
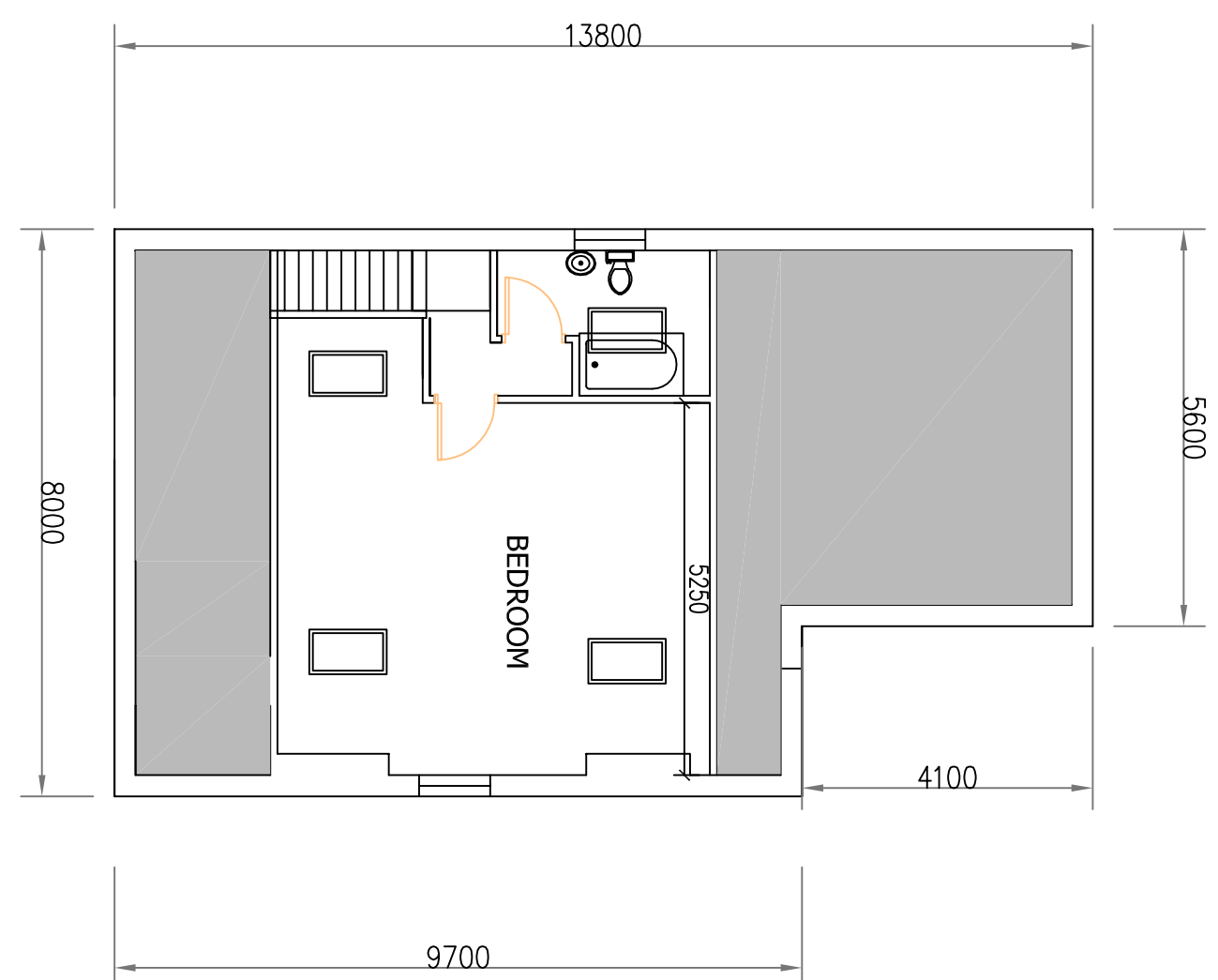
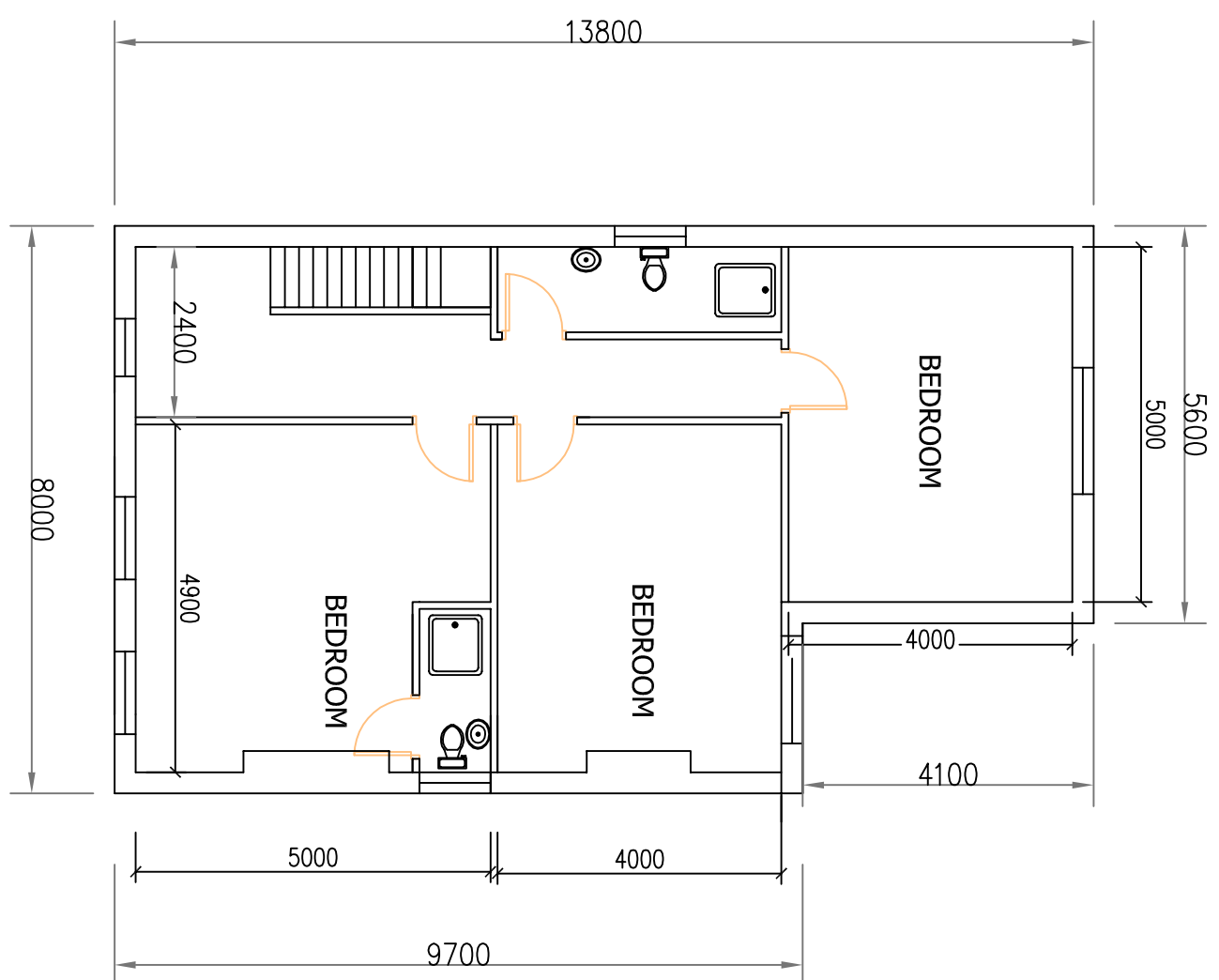
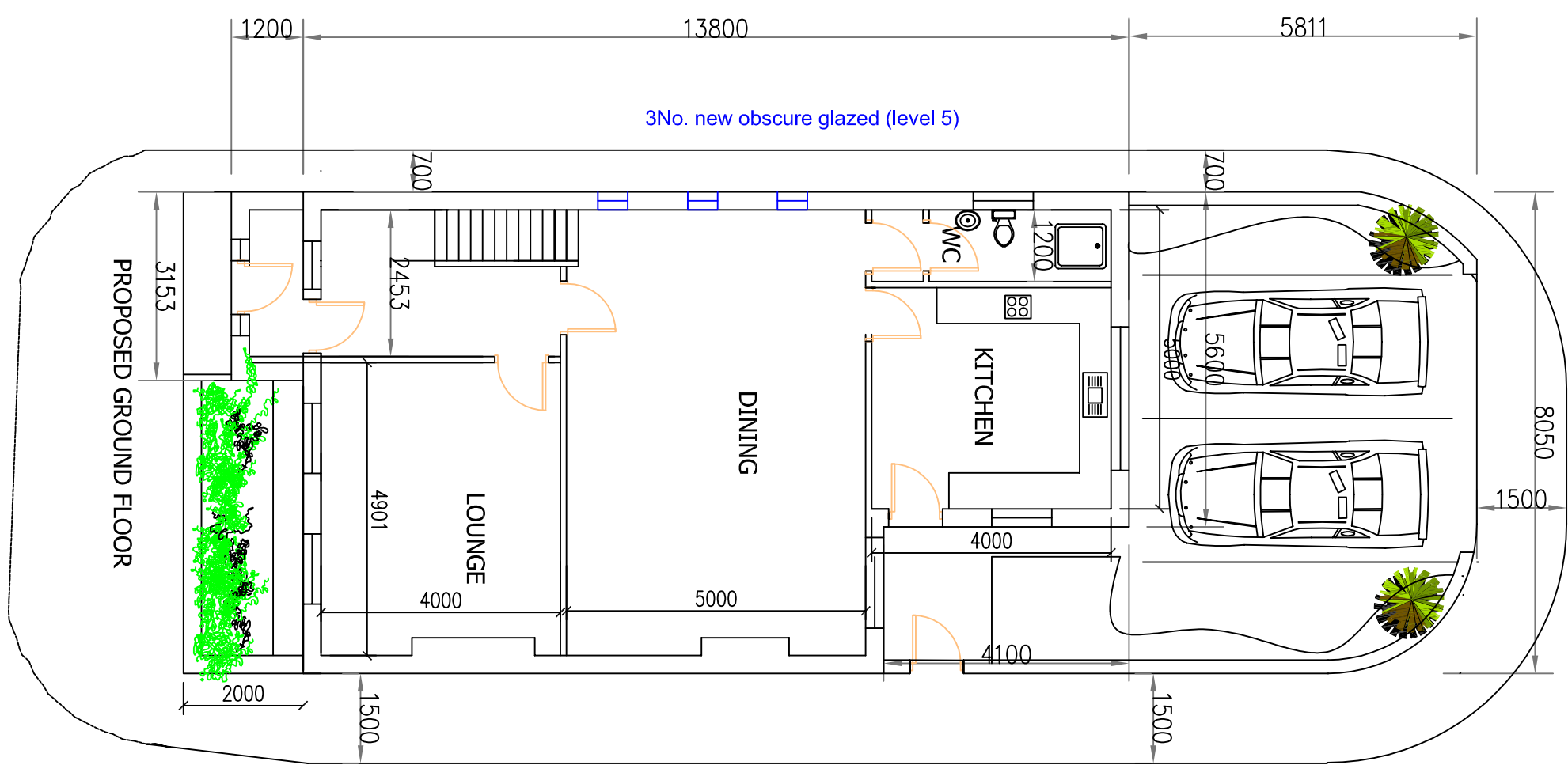
To protect the privacy, outlook and living conditions of neighbouring occupiers.

3. The development hereby permitted shall be carried out in complete accordance with the following approved plans:

Proposed Plans - RAD/1056/14/1/Rev D - Dated June 2014

Reason

For the avoidance of doubt and in the interests of proper planning.



Date	Rev.	Initials
4/12/14	A-B.	RA
14/6/15	C.	RA
9/9/15	D.	RA
	Pywerimby/Ilwings amended	
	Ridge increased and site windows added	
	Ridge increased and site windows added	

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Figured dimensions take precedence over scaled measurements from the drawing. All dimensions to be checked by Contractor and discrepancies clarified with the Client prior to commencement.

The whole of the works to comply with current Building Regulations. No work to commence without Building Regulations approval.

Notes

RAD

Project	Title
LAND AT CROSTON STREET (BACK WILLIS STREET) BOLTON BL3 4BB	PROPOSED NEW DWELLING HOUSE

Client	MR. M. DHORAJI
Drawn	RA
Date	JUNE 2014
Scale	1:100
Drawing Number	R