

**Planning Applications Report**

**Planning Committee  
23<sup>rd</sup> June 2022**

**Bolton  
Council**

Bolton Council has approved a Guide to Good Practice for Members and Officers Involved in the Planning Process. Appendix 1 of the Guide sets down guidance on what should be included in Officer Reports to Committee on planning applications. This Report is written in accordance with that guidance. Copies of the Guide to Good Practice are available at [www.bolton.gov.uk](http://www.bolton.gov.uk)

Bolton Council also has a Statement of Community Involvement. As part of this statement, neighbour notification letters will have been sent to all owners and occupiers whose premises adjoin the site of these applications. In residential areas, or in areas where there are dwellings in the vicinity of these sites, letters will also have been sent to all owners and occupiers of residential land or premises, which directly overlook a proposed development. Copies of the Statement of Community Involvement are available at [www.bolton.gov.uk](http://www.bolton.gov.uk)

The plans in the report are for location only and are not to scale. The application site will generally be in the centre of the plan edged with a bold line.

The following abbreviations are used within this report: -

CS	The adopted Core Strategy 2011
AP	The adopted Allocations Plan 2014
NPPF	National Planning Policy Framework
NPPG	National Planning Policy Guidance
PCPN	A Bolton Council Planning Control Policy Note
PPG	Department of Communities and Local Government Planning Policy Guidance Note
MPG	Department of Communities and Local Government Minerals Planning Guidance Note
SPG	Bolton Council Supplementary Planning Guidance
SPD	Bolton Council Supplementary Planning Document
PPS	Department of Communities and Local Government Planning Policy Statement
TPO	Tree Preservation Order
EA	Environment Agency
SBI	Site of Biological Importance
SSSI	Site of Special Scientific Interest
GMEU	The Greater Manchester Ecology Unit

The background documents for this Report are the respective planning application documents which can be found at:-

[www.bolton.gov.uk/planapps](http://www.bolton.gov.uk/planapps)

Date of Meeting: 23 June 2022

Application Reference: 13467/22

**Type Of Application**  
**Registration Date**  
**Decision Due Date**  
**Responsible Officer**  
**Authorizing Officer**

**Full Planning Permission**  
**21 March 2022**  
**9 May 2022**  
**Jodie Turton**

**Location**

**ROYAL BOLTON HOSPITAL MINERVA ROAD  
FARNWORTH BOLTON BL4 0JR**

**Proposal**

ERECTION OF PART FOUR/PART FIVE STOREY  
EDUCATION DEVELOPMENT BUILDING (USE CLASS F1)  
WITH ANCILLARY OFFICE SPACE AND ANCILLARY FOOD  
AND DRINK AREA (USE CLASS E), LANDSCAPING AND  
CAR PARKING PROVISION.

**Ward**

Harper Green

**Applicant:**

**Agent:** Mr J Berggren

## **OFFICERS REPORT**

**Recommendation:** Approve subject to conditions

### Executive Summary

- \* The application proposes the redevelopment of a site within the Royal Bolton Hospital complex to provide Bolton College of Medical Sciences (BCMS). The BCMS project is a collaboration between: Bolton College (the lead partner), Bolton NHS Foundation Trust (Bolton Hospital), the University of Bolton and Bolton Council. The aim of the BCMS project is to provide a state-of-the-art vocational, professional skills and training facility for existing and new hospital staff.
- \* Planning permission was previously approved for the BCMS building at the hospital site (05811/19). The current application is for the same site area and proposes minor amendments to the size, design and layout of the BCMS building, but primarily seeks to progress the development without the multi storey car park (MSCP).
- \* The previously approved multi-storey car park provided 406 parking spaces.
- \* Due to the reorganisation of the hospital site and buildings over the past 2 years (mainly due to a change in how the hospital operates during and following covid) the hospital has freed up land and buildings which has allowed for increased parking to be accommodated around the wider hospital site.
- \* In total, following completion of the BCMS development, 170 dedicated parking spaces for the BCMS development will be accommodated on the application site and the displaced parking spaces will be accommodated within the wider hospital site. In addition, there will be a net increase in parking provision at the hospital of 250 parking spaces. Highway Engineers are satisfied that the net increase of spaces across the hospital site as a whole mitigates for the non-provision of the MSCP and no objections are raised to the proposal on highway grounds (complying with Core Strategy policies S1, P5 and Appendix 3).
- \* The proposed alterations to the building are minor and will not alter the high quality design and appearance of the BCMS building, thus complying with Core Strategy policy CG3.
- \* The proposed amendments will not have any greater impact on residential amenity, drainage, sustainability, land stability, land contamination, crime or ecology than the extant scheme.

- \* The proposed development will result in the loss of several trees from the site and two hedges. These losses will be mitigated for by the provision of mature landscaping to be secured by condition.
- \* There have been no neighbour objections, however Cllr Haworth has raised a number of issues primarily relating to parking on the surrounding residential roads close to the hospital site, a summary of the issues raised are contained in the representations section of the report.
- \* The application has an Officer recommendation of approval, subject to conditions.

### Proposal

1. The application proposes the redevelopment of a site within the Royal Bolton Hospital complex to provide Bolton College of Medical Sciences (BCMS). The BCMS project is a collaboration between: Bolton College (the lead partner), Bolton NHS Foundation Trust (Bolton Hospital), the University of Bolton and Bolton Council. The aim of the BCMS project is to provide a state-of-the-art vocational, professional skills and training facility for existing and new hospital staff.
2. Planning permission was previously approved for the BCMS building at the hospital site (05811/19). The current application is for the same site area and proposes minor amendments to the size, design and layout of the BCMS building, but primarily seeks to progress the development without the multi storey car park.
3. The proposed BCMS building will comprise a part 4-storey, part 5-storey structure of approximately 7,600 sq m internal floorspace. The building will be sited in the northern section of the site and will be dual fronted, with main entrances from Redgate Way to the north and a pedestrianised communal area to the south, which will contain benches, cycle parking, landscaping and an outdoor learning zone.
4. The building will contain a mix of teaching/learning space (with a heavy emphasis on practical learning), a cafe, and associated staff and service spaces.
5. A surface car park of 170 spaces is proposed, which will include 18 electric vehicle charging points and 10 disabled bays. Three secure cycle parking stores are proposed to accommodate 60 cycle parking spaces.

### Site Characteristics

6. The site is located close to the Minerva Road entrance to the hospital site, lying to the south of Redgate Way and flanked by Barnes Drive to the east. To the west are the existing hospital buildings accessed from Kinlay Road. To the immediate south is Barnes Drive and to the south east is an area of existing surface car parking which will be retained. To the north are further hospital buildings and to the north west is the Technology Innovation Centre and Mount St Joseph Secondary School.
7. The site comprises a hard surface car park with a capacity of 346 spaces.
8. Barnes Drive is a tree lined avenue, with trees along both sides of the road, some within the application site. A landscape plan and plans showing trees to be retained and removed have been submitted with the application.
9. Beyond Barnes Drive is a vacant site which is fenced off and overgrown. Beyond this are the residential properties on Kingsland Road.

### Policy

10. Bolton's Core Strategy Policies: P5 Accessibility and Transport; S1 Safe Bolton; CG1 Cleaner and Greener; CG2 Sustainable Construction; CG3 The Built Environment; CG4 Compatible Uses; RA2 Farnworth; SC1 Housing; IPC1 Infrastructure and Planning Contributions.

11. Supplementary Planning Documents:
  - \* General Design Principles
  - \* Accessibility, Transport and Safety
12. National Planning Policy Framework: 6. Building a strong, competitive economy, 9. Promoting sustainable transport, 11. Making efficient use of land, 12. Achieving well-designed places.
13. Planning Practice Guidance
14. Places for Everyone – Greater Manchester Spatial Strategy

### Analysis

15. Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise. Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission. Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission. It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.
16. The main impacts of the proposal are: -
  - \* principle of development
  - \* impact on car parking and the highway
  - \* impact on neighbouring uses and amenity
  - \* impact on the character and appearance of the area
  - \* impact on trees and biodiversity
  - \* impact on sustainable construction
  - \* impact on crime and the fear of crime
  - \* impact on land stability and contamination
  - \* infrastructure and planning contributions

### Principle of Development

17. Policy RA2.4 of the Core Strategy seeks to, ***"Continue to recognise the Royal Bolton Hospital as the principal hospital in the borough, supporting consolidation and opportunities for improvement and employment on its existing site"***. Paragraph 4.7 of the Core Strategy under the Objective 'Achieving Bolton' recognises that, ***"a well-educated and trained workforce will make a major contribution to Bolton's future economic success"***. The proposal complies with the aims of Core Strategy policy RA2.4 and the Objective 'Achieving Bolton'.
18. Policy SC1 of the Core Strategy seeks to identify a range of sites for the provision of housing. Appendix 4 of the Allocations Plan identifies the application site as being allocated for residential development.
19. It is acknowledged that the Council is not able to demonstrate a 5-year supply of deliverable housing sites. This represents a housing shortfall. In this circumstance paragraph 11 of the NPPF indicates that the policies that are most important for determining the application should be regarded as out-of-date and that there is a 'tilted balance' in favour of granting permission for residential schemes. Although it is acknowledged that there is a need for housing development in the borough and significant weight should be afforded to this, in this instance it is proposed that an allocated housing site is developed for an alternative use.
20. The proposed scheme offers a unique opportunity to the hospital, University and the borough to provide a state of the art vocational, professional skills and training facility, which forms

part of the Greater Manchester Estates Strategy produced by Greater Manchester Health and Social Care Partnership. The facility will provide training and experience for existing staff at the hospital as well as new students. The hospital site is the ideal location for the siting of this facility to take advantage of the strong links with the hospital. The development of this site for BCMS will allow a larger existing training site within the hospital complex to be redeveloped and which would be more appropriate for housing. On this basis, it is considered that the site-specific benefits of the proposal outweighs the need for housing and therefore, the tilted balance weighs in favour of developing the site for an alternative use to the allocated housing use.

21. Furthermore, the principle of development has already been established at the site via planning permission 05811/19. Given the permission is extant and there have been no relevant national or local policy changes in the intervening period, the previous permission is a material consideration which should be afforded significant weight in the planning balance.

#### Impact on Car Parking and the Highway

22. Bolton's Core Strategy policy P5 seeks to ensure that developments take the following into account: accessibility by different types of transport, prioritising pedestrians, cyclists, public transport users over other motorised vehicle users; design and accessibility by public transport; servicing arrangements; parking including cycle parking; transport needs of people with disabilities; transport assessment/travel plan.
23. Policy S1.2 seeks to *"promote road safety in the design of new development."*
24. Paragraph 110 of the Framework is focused on the consideration of development proposals in relation to highway impact and ***"it should be ensured that:***
  - a) *Appropriate opportunities to promote sustainable transport modes can be – or have been – taken up, given the type of development and its location;*
  - b) *Safe and suitable access to the site can be achieved for all users; and*
  - c) *Any significant impacts from the development on the transport network (in terms of capacity and congestion), or on highway safety, can be cost effectively mitigated to an acceptable degree."*
25. Paragraph 111 of the Framework goes on to state that, ***"Development should only be prevented or refused on highway grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe."***
26. A Transport Assessment, Travel Plan and Car Park Development Phasing document have been submitted with the application.

#### *Highway Impact*

27. The Transport Assessment (TA) has been submitted with the application in order to justify the **development from a transport/highways perspective. The council's highway engineers have confirmed that, *"The TA appears robust in its assessment in terms of traffic impact and the rationale used in its preparation appears plausible and justifiable."***
28. The site has extant permission for the BCMS building and the highway impact was previously assessed and considered to be acceptable to allow for the proposed development to be permitted. There have been no changes to policy or legislation in the intervening period which would influence the determination of the application. Furthermore, Highway Engineers having reviewed the submitted TA are of the view that although the highways around the hospital site are congested within the peak periods, ***"that this additional traffic can be accommodated***

*with minimal additional detriment to road safety and operational capacity at that location. The residual cumulative impact in terms of traffic could not be considered as severe and would thus be compliant with the requirements indicated in current planning policy guidance (NPPF [para 111])”.*

29. Access to the site via sustainable modes of transport is also recognised and the submitted Travel Plan provides a reasonable set of targets to create a modal shift towards sustainable transport patterns, notably for students of the BCMS facility.
30. Overall, the highway impact is considered to be the same as for the extant permission and no concerns are raised beyond those considered and agreed previously.

#### *Car Parking*

31. One of the principal concerns about the extant scheme was the impact on, and the adequate provision of, car parking spaces.
32. The difference between the current application and the extant scheme is the omission of the previously approved 406 space multi-storey car park (MSCP). The justification for taking the development forward without the provision of the MSCP is that the operation of the hospital has seen significant changes over the last 2 years. Covid-19 has resulted in a significant reduction in the number of visitors to the hospital; many outpatient services are now either delivered off-site or virtually and there has been a resultant reduction in demand for operational facilities, as well as a significant reduction in the number of outpatient visitors. Together this has resulted in a reorganisation of the hospital site and buildings.
33. A consequence of the changes to the operation of the hospital has been the freeing up of land and buildings within the wider hospital site. This has allowed the hospital to revisit land available for parking and although a rationalisation of spaces on within the BCMS site is proposed, across the wider hospital site there will be an increase in parking provision overall.
34. The extant planning permission includes provision for 406 car parking spaces. The current scheme will result in 170 surface spaces being located within the red edge BCMS site, a loss of 236 spaces.
35. However, taking a holistic view of the hospital site and parking provision, a phased parking strategy is proposed which will result in both temporary and permanent parking provision, this will not only accommodate the temporarily displaced parking spaces during the construction process, but will also result in a net gain in the number of parking spaces across the hospital site. In total, following completion of the BCMS development, 170 dedicated parking spaces for the BCMS development will be accommodated on the application site and the displaced parking spaces will be accommodated within the wider hospital site. In addition, there will be a net increase in parking provision at the hospital of 250 parking spaces (to include disabled parking provision and 18 EV charging point spaces). A Car Parking Development and Phasing Strategy has been submitted with the application which details how this will be achieved and it is recommended that this document is conditioned if planning permission is granted.
36. A tandem application is being considered by the LPA for five temporary surface car parks within the hospital site (13288/22), which will provide 281 of the new, temporary parking spaces.
37. It is acknowledged that the primary concern during the consideration of the extant permission centred around the displacement of existing parking spaces and ensuring that these could be accommodated temporarily until the MSCP was up and running. Although the current application seeks to proceed minus the MSCP, it is clear the overall parking situation at the hospital will not be reduced. And sufficient parking will be made available in the short and long

term via the use of temporary and permanent parking arrangements. Highway Engineers are satisfied that the proposals provide a robust parking solution in the short and long term and raise no objections to the removal of the MSCP from the BCMS development.

38. Cllr Haworth has raised a number of issues relating to parking on the surrounding residential roads close to the hospital site. The main concerns are that the BCMS building will put more pressure on parking on the nearby residential streets, especially if people are expected to park in car parks further away from the BCMS building or their usual car park, human nature dictates they will opt for the easiest option. Cllr Haworth would also like to see the NHS Foundation Trust take a long term view of car parking to ensure it is fit for purpose and prevents parking on residential streets, possibly by introducing a parking scheme for local residents.
39. Highway Engineers have responded directly to a number of the points raised. They confirm that the submitted Car Park Management Strategy shows that the hospital site overall will benefit from a greater number of parking spaces even without the provision of the multi-storey car park.
40. A residents parking scheme would require funding and given the hospital will be providing a greater number of parking spaces overall, that parking on residential streets is an existing issue and no contribution was required for the previous approval (and there have been no significant/relevant policy amendments in the intervening period) it would be difficult to justify requiring a funding contribution for a parking scheme at this stage. A condition is however recommended to review traffic restrictions in the local area which would allow additional traffic management measures to be implemented if required.
41. The Agent has also responded to confirm that staff are no longer charged for parking within hospital car parks which encourages staff to park within the hospital site. Over 90% of future BCMS learners, users and staff are employed by the hospital, resulting in a very limited increase in parking requirements. The hospital is planning a wholesale redevelopment of the site, which will include the revision and consolidation of parking provision.
42. The submitted transport work and Travel Plan reiterated the accessibility of BCMS to sustainable transport provision and measures to promote a modal shift. Furthermore, highways note that the nature of the operation within the BCMS will allow for more virtual courses and the college will cater for a vast proportion of staff already employed at the hospital.
43. Overall therefore, on the basis of conditions to ensure the implementation of the Transport Plan, to ensure the replacement of the displaced parking spaces in the short and a greater provision of parking spaces overall in the long term, the proposal is considered to comply with policy in relation to impact on the highway and car parking.

#### Impact on Neighbouring Uses and Amenity

44. Policy CG4.1 of the Core Strategy seeks to ensure that, ***"new development is compatible with surrounding land uses and occupiers, protecting amenity, privacy, safety and security"***. Policy CG4.2 states that, ***"development should not generate unacceptable nuisance, odours, fumes, noise or light pollution"***.
45. The proposed BCMS building remain in the same use as the extant permission, with minor alterations to the height (decreased by 300mm), siting (moved 1.5m to the east) and design (removal of solar shading to southern elevation; parapet to theatre roof dropped). The proposed alterations are minor and will have no greater, or indeed different impact on either neighbouring residential amenity, the hospital or noise impact to the extant scheme. To summarise the potential impacts as detailed in the previous report to committee:

#### *Residential Amenity*

46. The application site is located to the west and over 80 metres away from the rear of residential properties on Kingsland Road. Given the distance of the building to these residential dwellings and the belt of trees that forms the western perimeter to the hospital site, the proposed building will not impact upon the residential amenity in terms of amenity, privacy, safety or security of these nearby dwellings.
47. The siting and scale of the proposed building will not prejudice any future development of the adjacent Minerva Road site.
48. Pollution Control have recommended a condition to control the brightness of any external lighting, to ensure that this remains at or below 5 LUX when measured from the closest residential properties, which will prevent any unacceptable light nuisance.

#### *Impact on the Hospital*

49. The location of the BCMS within the Bolton Hospital site forms an integral part of the future development of the hospital site. Rather than resulting in any detrimental impact on the hospital site therefore, the proposed development is considered to bring benefits to the hospital and the future of the hospital site.

#### *Noise*

50. A noise report has been submitted with the application. The report covers issues of noise generated from traffic and plant machinery. Noise from traffic is not considered to result in any measurable change in noise level on the road network. New items of noise generating plant are likely to be installed as part of the proposed development and on the basis of appropriately low noise plant being installed there will not be any unacceptable impact in terms of noise. Pollution control officers have assessed the submitted information and concur with the findings of the report. A condition is recommended to ensure that noise from all sources associated with the development does not exceed the agreed level and the development accords with policy in this respect.
51. The proposed BCMS development will not have any detrimental impact on the residential amenity of existing neighbours or on the consideration of any future residential schemes at the adjacent site and will be a positive addition to the hospital site, the proposal thereby **complies with Bolton's Core Strategy policy CG4.**

#### Impact on the Character and Appearance of the Area

52. Policy CG3 of the Core Strategy states that the Council will conserve and enhance local distinctiveness, ensuring development has regard to the overall built character and landscape quality of the area, and will require development to be compatible with the surrounding area in terms of scale, massing, grain, form, architecture, street enclosure, local materials and landscape treatment. Policy RA2.14 and RA2.15 seek to encourage excellent quality design and to conserve and enhance the distinctive character of the existing physical environment. This policy aim is supported by the guidance contained in Supplementary Planning Document **"General Design Principles"**.
53. Section 12 of the NPPF is focused on the achievement of well-designed places. The Government attaches great importance to the design of the built environment. Paragraph 124 asserts that, "Good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities."
54. The design, siting and scale of the proposed BCMS building is the largely the same as the extant permission, a small number of minor amendments are proposed:

- \* Building height decreased by 300mm
- \* Solar shading removed from south elevation as the thermal model and overheating report did not require it.
- \* Storey high parapet to theatre roof (level1) dropped to 1100mm from roof deck.
- \* Building moved 1.5m east to allow for existing deep drainage run and high reach fire tender access.
- \* Site expanded to encompass retained area to south-east. Surface parking added including electric charging points.
- \* External substation added to west boundary. More external social space proposed.

55. The proposed alterations are considered minor and will not impact upon the overall design concept of the building. The amended scheme retains the key characteristics in that it has been well designed to be sympathetic to the surrounding character whilst striking a balance with a high quality and distinctive design in its own right. Strong fenestration detailing, focusing on vertical lines breaks up the bulk of the building. The stepped back layout of the eastern elevation will serve to break up the appearance of bulk from the main approach into the hospital site where vehicles and pedestrians will view the building from a lower level given the topography of the approach and site.

56. Overall, it is considered that the design is well conceived and will present a high-quality architectural addition to the hospital site and complies with Core Strategy policy CG3.

#### Impact on Trees and Biodiversity

57. Core Strategy policy CG1.2 seeks to, *"safeguard and enhance biodiversity in the borough by protecting sites of urban biodiversity including trees, woodland and hedgerows from adverse development, and improving the quality and interconnectivity of wildlife corridors and habitats."*

58. Paragraph 174 of the Framework states that, ***"Planning policies and decisions should contribute to and enhance the natural and local environment by:***

- d) minimising impacts on and providing net gains for biodiversity, including establishing coherent ecological networks that are more resilient to current and future pressures".***

59. Paragraph 180 of the Framework states that when making decisions local authorities should encourage developers to [amongst other things] ***"improve biodiversity in and around developments ..., especially where this can secure measurable net gains for biodiversity..."***

60. A Vegetation Removal and Retention Plan, Arboricultural Report, Cotoneaster Management Plan, Ecological Assessment and Landscape Plan have been submitted with the application. The proposed development is to be sited on a largely hard surfaced area of the hospital site, which currently accommodates a surface car park. There are however pockets of landscaping and trees within the site and around the edge of the site.

#### *Trees and Landscaping*

61. The proposed development will result in the loss of several trees from the site, these include four category C trees and three category B trees. In addition two hedges will be lost.

62. **The Council's** Tree and Woodland Officer has stressed that replacement tree planting should aim to enhance the area as well as compensate for losses and that trees that will reach a large final size should be incorporated into the landscape design and appropriate space afforded them to allow for crown growth without repeated pruning. A detailed landscape plan is therefore recommended to be secured via condition. A condition for an Arboricultural Method

Statement is also recommended to ensure root damage to protected trees is avoided through use of geosystem.

63. Whilst no detailed landscape plan has been provided with the application, the proposed site plan does show areas for landscaping which should be sufficient to deliver sufficient landscaping to soften the development and to include replacement tree planting. A condition is therefore recommended to secure a detailed landscape plan, and to specify tree planting as part of this.

#### *Biodiversity*

64. The submitted Ecological Assessment has not identified the presence of any protected species on the site. However, it is acknowledged that there are ponds within the vicinity which have **the potential to be used by Great Crested Newts. The Council's Greenspace Officer is content** that he proposed reasonable avoidance measures are adequate. The Assessment also identifies the opportunity for enhancement and development of habitat with bat and bird boxes and landscape planting proposals. A condition is recommended for details of the location and number of bat and bird boxes, together with other biodiversity enhancements.
65. **The invasive species 'Cotoneaster' has been identified on the site. Due to its potential to cause** damage to the natural environment, a treatment methodology has been submitted and the works will be secured via condition to prevent future damage to the development and spread of the weed during the clearance and construction process.
66. Overall, the proposed landscape and biodiversity measures are considered to comply with policy.

#### Impact on Sustainable Construction

67. Core Strategy policy CG2.2 seeks to ensure that non-residential buildings of 500 m<sup>2</sup> or larger meet the following criteria:
- \* Achieve the "very good" BREEAM rating.
  - \* Incorporate appropriate decentralised, renewable or low carbon energy sources to reduce the CO<sub>2</sub> emissions of predicted regulated and unregulated energy use by at least 10%.
  - \* Demonstrate the sustainable management of surface water run-off from developments.
68. A Sustainability and Energy Statement has been submitted with the application which demonstrates how the development proposal is going to meet (1) and (2) requirements detailed above. The strategy for the building is to, *"aim to design the most energy efficient building and services that meet the needs of the occupants, are responsive to the external climate, and have a low carbon footprint."*
69. The proposed building is to comply with all the following sustainability targets:
- a) Building Regulations Part L2A compliance (statutory requirement).
  - b) **BREEAM "very good" certification (client requirement). Under BREEAM 2018, eligibility** for this rating is conditional on achieving four Ene 01 credits.
  - c) 10% of predicted carbon emissions offset through renewable or decentralised energy sources (local authority requirement).
70. To achieve the second criteria listed, the development will source heating from the hospital's existing district heating network. Hot water will also be sourced from this which will provide a significant carbon emissions improvement over conventional gas-fired system.
71. In addition, the thermal performance of the building fabric will be designed to provide a comfortable internal environment throughout the year, and to exceed the requirements of criterion 3 Part L2A of the Building Regs with the intention of reducing gains further to lower

the building's cooling load. Lighting has also been considered as part of the sustainable construction of the building, maximising natural daylight, utilising automatic lighting control systems, and internal monitoring of energy usage.

72. The application is supported by a Flood Risk Assessment and Surface Water Drainage Design. The submitted information has been assessed by Floodrisk and they are satisfied that the surface water drainage rates meet the requirements of policy CG2.2 for a brownfield site. A condition is recommended which will require further information to be submitted for approval. It is also noted that if the site is designed adequately it is possible that the development could join any future SUDS that may be developed as part of the wider master planning of the hospital site.
73. Overall, the proposal is considered to meet the requirements of points (a), (b) and (c) of Core Strategy policy CG2.2.

#### Impact on Crime and the Fear of Crime

74. Policy S1 of the Core Strategy seeks to, *"ensure that the design of new development will take into account the need to reduce crime and the fear of crime"*.
75. A Crime Impact Statement has been submitted with the application, providing an assessment of the site and a security risk analysis of the area and recommendations for crime prevention through design. Natural light and natural surveillance are central to the design and layout of the site both internally and externally.
76. Throughout the site it is recommended that lighting is designed so that it provides good illumination and does not 'pool' to provide lit and dark areas, but a consistent level of lighting and that soft landscaping is designed to be low adjacent to pathways to increase natural surveillance and infiltration of light. Secure by Design standards are also recommended.
77. Overall, the recommendations of the Crime Impact Statement are considered to be of good practice and the submitted Statement will be included in the list of approved plans to ensure that the development is carried out in accordance with these recommendations.

#### Impact on Land Stability and Contamination

78. Policy CG4.3 of the Core Strategy seeks to ensure, *"development proposals on land that is (or suspected to be) affected by contamination or ground instability must include an assessment of the extent of the issues and any possible risks. Development will only be permitted where the land is, or is made, suitable for the proposed use."*
79. The site falls within the defined Development High Risk Area for coal mining. The application is therefore supported by a Coal Mining Legacy and Risk Assessment. The documents have been assessed by The Coal Authority and further information has been submitted by Tier consult Ltd which addresses TCA concerns about areas of shallow mining risk in the proposed car park area of the site. TCA are satisfied that the proposed works to address risk of unacceptable differential settlement through the use of a bi-axial geogrid into the sub-base of the car park construction will suitably address the potential issue. A condition is therefore recommended to secure these works in order to mitigate against any residual coal mining legacy related ground movement.
80. Pollution Control have assessed the submitted Phase 2 Report, Remediation Strategy and gas monitoring information and have recommended a condition to ensure that the works are carried out in full accordance with the submitted information. It is noted that Pollution Control also recommended a condition for asbestos surveys, however given there are no buildings to be demolished and asbestos found at development sites is covered by alternative legislation Officers do not consider a condition to be necessary in this instance.

81. On the basis of the above, the proposal is complies with Core Strategy policy CG4.3 in relation to ground contamination and land stability.

#### Infrastructure and Planning Contributions

82. Policy IPC1 confirms **that the council will expect, "public art from all development above one hectare in area or from the construction of buildings containing the threshold of 2500 sq m of floor space."**
83. A condition is therefore recommended requiring on site public art, which will assist in the successful assimilation of the development within the hospital site and wider area.

#### Conclusion

84. The proposed development of Bolton College of Medical Science at the hospital site is considered to be a positive and complementary development, providing a state of the art educational and vocational facility that will not only benefit the hospital and University, but also the wider borough in terms of education, training and employment.
85. The principal difference between this application and the extant permission is the removal of the multi storey car park (MSCP). The submitted parking strategy shows that the displaced surface parking spaces from the application site, as well as parking spaces which exceed the MSCP provision, will be accommodated within the wider hospital site. Highway Engineers are satisfied that the parking provision for the BCMS and hospital as a whole will be served adequately and this will be secured via condition.
86. The main issue considered for the extant permission (05811/19) was the potential exacerbation of the existing highway congestion and parking pressures. Despite this, the Transport Assessment proved that the BCMS will not put undue additional pressure on the highway network and measures encouraged via the Transport Plan to encourage the use of sustainable forms of transport as well as sensitive timetabling and online/off site access to educational resources will further ensure that additional highway pressures at peak times are minimised.
87. The BCMS building has been well designed and although large in format will sit comfortably within the hospital site. The application proposes minor amendments to the extant scheme, which will not impact detrimentally on the high quality design of the building, are acceptable and comply with policy CG3.
88. Due to the siting of the proposed building it will not result in impact on the amenity of the local residents or the users of the hospital and the proposal complies with the requirements of policy CG4 and SPD "General Design Principles".
89. Issues of drainage, coal mining legacy, ecology, trees, contaminated land, sustainable construction, crime and designing out crime have all been satisfactorily addressed, in some instances requiring conditions where additional information is required.
90. Overall, the proposed development is considered to present a positive development proposal which complies with policy. Members are recommended to approve the application subject to the suggested conditions.

## Representation and Consultation Annex

### Representations

Letters: - none received.

Petitions: - none received.

Elected Members: - a letter of general comment has been received from Cllr Haworth, making the following comments (where the issue raised is not covered in the main body of the report an Officer comment is detailed in italics):

- \* Many residents welcome the skills college but are concerned about parking on nearby residential roads, notably parking on: Broadway and the Andrews Estate, Greenland Road and also across Bradford Road, Glenmore Avenue, Broad Oak Road, Brian Road, and Waters Edge.
- \* A couple of the proposed temporary car parks are close to the BCMS but three others are further away on foot – could anything be done to allocate current regular car park users to one of the temporary car parks? If it is all left to chance there is a risk more people will choose to park on residential roads.
- \* The NHS Foundation Trust is undergoing change and is making long term plans – parking, visitors and proximity to residential streets needs new long term fit for purpose solutions. Hospitals in other areas which are close to residential streets (for example Chrisities in South Manchester) have parking schemes for local residents via a partnership between the hospital and Manchester City Council – this should be done at Bolton Hospital.
- \* Litter – residents are concerned about increased litter from people using the skills college. Residents would like additional litter bins on Minerva Road and Redgate Way – *Officer comment: Officers have contacted Neighbourhood Services to ask about the possibility for additional bins in the vicinity. They have replied to say that there is no funding for new bins and even if funding was provided (this was put forward by Officers as a possible solution) there is no capacity for additional bins to be emptied or staff to empty these.*

### Consultations

Advice was sought from the following consultees:

Bolton Council: Pollution Control, Floodrisk, Highway Engineers, Greenspace, Landscape, Tree and Woodland Officers, Education.

External Consultees: Coal Authority, Greater Manchester Police.

### Planning History

13288/22 – application for the formation of 5 temporary car parks

05811/19 - Planning permission was approved by Planning Committee in June 2019 for the erection of a part 4/part 5 storey education and development building, incorporating ancillary office space and ancillary food and drink area; multi-storey car park, landscaping and boundary treatment.

Other than this, the hospital has a rich history of planning applications for the wider hospital site, however none relate directly to the current development proposal. The most recent applications dealt with are: 03791/18, 96327/16 and 91240/13.

## **Recommendation: Approve subject to conditions**

### **Recommended Conditions and/or Reasons**

Subject to the following conditions, which have been imposed for the reasons stated:-

#### **1 COMMENCEMENT**

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason

Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

#### **2 TREE PROTECTION METHOD STATEMENT**

Prior to the commencement of development of any works on site within, or close to, the root protection zones of trees: T19, T20 and G4, the developer shall submit a method statement detailing how the following elements of the site will be constructed without causing harm or damage to the trees found on the site to the Local Planning Authority. The specified areas are:

- Works in the root protection zones of trees T19, T20 and G4 - details of a geosystem to be used in these areas to prevent root damage taking place.
- Full details of the services and routes and drainage, which should be located outside of the RPZs of the trees to be retained.

No development or site clearance within areas of the root protection zones of trees: T19, T20 and G4 shall take place until the Local Planning Authority has agreed the measures in writing, and these measures shall then be implemented fully in accordance with the approved details.

Reason

To ensure the safe development of the site and favourable retention of trees and in order to comply with Bolton's Core Strategy policies CG1 and CG3.

Reason for Pre-Commencement Condition:

Tree protection measures must be implemented prior to the commencement of works within the root protection zones to protect the health and appearance of the tree(s) during the construction phase.

#### **3 TREE PROTECTION MEASURES**

No demolition, development or stripping of soil shall be started until:

1. The trees and hedgerows within or overhanging the site which are to be retained/ subject of a Tree Preservation Order (TPO) have been surrounded by fences of a type and siting as detailed in the submitted Arboricultural Impact Assessment (section 5.2 Construction Phase) prior to such works commencing.
2. The approved fencing shall remain in the agreed location (in accordance with BS 5837:2012) until the development is completed or unless otherwise agreed in writing with the Local Planning Authority and there shall be no work, including the storage of materials, or placing of site cabins, within the fenced area(s).

Reason

To protect the health and appearance of the tree(s) and in order to comply with Bolton's Core Strategy policies CG1 and CG3.

Reason for Pre-Commencement Condition:

Tree protection measures must be implemented prior to the commencement of works to protect the health and appearance of the tree(s) during the construction phase.

#### 4 ECOLOGICAL MEASURES

Prior to the commencement of any above ground construction works, a scheme for the provision of biodiversity enhancement measures, including bat and bird boxes shall be submitted to, and approved in writing by, the local planning authority. The approved scheme shall be implemented in full prior to the first use/occupation of the building and retained thereafter.

Reason

To safeguard and enhance biodiversity in the borough and in order to comply with Bolton's Core Strategy policy CG1.1.

#### 5 TRAFFIC MANAGEMENT MEASURES

Prior to the commencement of any above ground construction works, all developer obligations shall be fulfilled to enable the Local Highway Authority to carry out a review of Traffic Regulation Orders in the locality where necessary and to enable the Local Highway Authority to consult, advertise, promote and implement Traffic Regulation Orders to help restrict overspill parking congestion and reduce residential amenity issues.

Reason

In the interests of highway safety and in order to comply with Bolton's Core Strategy policies S1, P5 and Supplementary Planning Document "Accessibility, Transport and Road Safety".

#### 6 SURFACE WATER DRAINAGE

Prior to the commencement of any above ground construction works, full details of the surface water drainage works, including a timetable for their implementation shall be submitted to and approved in writing with the Local Planning Authority. Before these details are submitted an assessment shall be carried out of the potential for disposing of surface water by means of a sustainable drainage system in accordance with the principles set out in the National Planning Policy Framework, and the results of the assessment provided to the Local Planning Authority. Where a sustainable drainage system is to be provided, the submitted details shall:

- 1) Provide information about the design storm period and intensity, the method employed to delay and control the surface water discharged from the site and the measures taken to prevent pollution of the receiving groundwater and/or surface waters.
- 2) Include a timetable for its implementation, and
- 3) Provide a management and maintenance plan for the lifetime of the development which shall include the arrangements for adoption by any public authority or statutory undertaker and any other arrangements to secure the operation of the scheme throughout its lifetime.

The development shall be carried out in full accordance with the approved drainage scheme (including timescales for implementation) and be retained as approved thereafter.

Reason

To ensure a satisfactory method of surface water disposal to reduce the risk of flooding elsewhere and to ensure pollution prevention in accordance with policies CG1.5 and CG2.2 of Bolton's Core Strategy and seeks to provide betterment in terms of water quality and surface water discharge rates and meets requirements set out in the following documents:

- NPPF
- Water Framework Directive and the NW River Basin Management Plan
- The national Planning Policy Guidance and the Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015)

Reason for pre-commencement condition

The solution for surface water disposal must be understood prior to works commencing on site as it could affect how underground works are planned and carried out.

## 7 ERADICATION OF COTONEASTER

The development hereby approved shall be carried out in full accordance with the submitted Cotoneaster Survey and Management Plan by Japanese Knotweed Solutions Ltd, dated 21.01.22. Should there be a delay of more than one year between the date of this decision and the implementation of the Cotoneaster removal programme then a new site survey and, if necessary, further remedial measures shall be submitted for the further approval of the Local Planning Authority. The scheme shall be carried out as approved and retained thereafter.

Reason

To ensure the safe development of the site and eradication of an invasive species and to comply with policy CG1 of Bolton's Core Strategy.

## 8 SITE INVESTIGATION ACCEPTED

i) The development hereby approved shall be carried out in full accordance with the following approved Site Investigation: BCMS, Bolton Hospital, dated 3rd June 2019 (ref: 069748-cur-00-xx-RP-GE-003 revision 5 by Curtins and Additional Ground Gas Risk Assessment, dated 12th January 2022, (Ref: TE1277GSVL1.1) by Tier Environmental Ltd.

ii) The scheme of remediation as specified in A REMEDIATION STRATEGY FOR THE BOLTON COLLEGE OF MEDICAL SCIENCES, BOLTON - TO SUPPORT NEW PLANNING APPLICATION, dated 30th March 2020, (Ref: TE1277RS1.2) by Tier Environmental Ltd, is approved. The scheme of remediation shall be implemented in accordance with the approved scheme.

iii) During the period of construction, should any unforeseen contamination be found on site that has not been previously identified, no further works shall be undertaken in the affected area. Prior to further works being carried out in the affected area, the contamination shall be reported to the Local Planning Authority within a maximum of 5 days from the discovery, a further contaminated land assessment shall be carried out, appropriate remediation identified and agreed in writing by the Local Planning Authority. The development shall be undertaken in accordance with the agreed remediation measures.

iv) Prior to first use/occupation of the development hereby approved, a Verification Report shall be submitted to, and approved in writing by, the Local Planning Authority. The Verification Report shall validate that no unforeseen contamination was found and that all remedial works undertaken on site were completed in accordance with those agreed by the Local Planning Authority.

The discharge of this planning condition will be given in writing by the Local Planning

Authority on completion of the development and once all information specified within this condition and other requested information have been provided to the satisfaction of the Local Planning Authority and occupation/use of the development shall not commence until this time, unless otherwise agreed by the Local Planning Authority.

#### General Note to Applicant

Please note: The responsibility to properly address contaminated land issues, including safe development and secure occupancy, irrespective of any involvement by this Authority, lies with the owner/developer of the site.

#### Reason

To safeguard the amenity of the future occupants of the development and to comply with Core Strategy policy CG4.

### 9 COAL MINING LEGACY

The development hereby approved shall be carried out in full accordance with the measures set out in the letter from Tier Consult, ref: TE1277-TE-00-XX-CO-GE-002-V01, dated 03 May 2022. The prescribed measures include the incorporation of a bi-axial geogrid into the sub-base of the car park construction. The measures shall be carried out prior to the car park being first brought into use and retained thereafter, unless otherwise agreed in writing by the LPA.

#### Reason

To safeguard the development and future users from land stability issues and in order to comply with Bolton's Core Strategy policy CG4.3.

### 10 LANDSCAPE PLAN

Trees and shrubs shall be planted on the site in accordance with a landscape scheme to be submitted and approved in writing by the Local Planning Authority prior to the development being first brought into use. The approved scheme shall include provision for replacement and enhancement tree planting, to include some larger canopy species (such as Dutch Elm disease-resistant variety of Elm, Lime, Oak, or London Plane) and shall be implemented in full and carried out within 6 months of the occupation of any of the buildings or the completion of the development, whichever is the sooner, or in accordance with phasing details included as part of the scheme and subsequently approved by the Local Planning Authority. Any trees and shrubs that die or are removed within five years of planting shall be replaced in the next available planting season with others of similar size and species.

#### Reason

To reflect and soften the setting of the development within the landscape and in order to comply with Core Strategy policies CG1 and CG3.

### 11 CAR AND CYCLE PARKING

Before the approved/permitted development is first brought into use and in accordance with approved plan ref: Landscape General Arrangement, drawing no. BCMS-RFM-ZO-SL-DR-L00002, Rev P05, dated 29.03.22, the following shall be provided:

- No less than 170 car parking spaces with minimum dimensions of 2.4 metres by 4.8 metres shall be marked out and provided within the curtilage of the site;
- A minimum of 18 EV charging points shall be marked out and provided;
- A minimum of 10 disabled parking bays shall be marked out and provided; and
- Three cycle stores providing secure cycle parking for up to 60 bicycles.

The spaces (as approved) shall be made available for the parking of cars/cycles at all times (the premises are in use).

Reason

To ensure that adequate provision is made for vehicles to be left clear of the highway and in order to comply with Bolton's Core Strategy policies S1, P5 and Appendix 3.

12 TRACKING ANALYSIS - SERVICE VEHICLES

Prior to the development hereby approved/permitted being first occupied or brought into use tracking analysis for service vehicle accessibility from Redgate Way/Kinlay Road and the ability to enter/leave the site in forward gear shall be submitted to and approved in writing by the local planning authority. The approved scheme shall be implemented in full before the development is first brought into use and retained thereafter and not to be used for any purpose except the turning/loading/unloading of vehicles.

Reason

In the interests of highway safety and in order to comply with Bolton's Core Strategy policies S1, P5 and Supplementary Planning Document 'Accessibility, Transport and Road Safety'.

13 VEHICLE AREAS

Prior to the development hereby approved being first occupied or brought into use, a scheme detailing how parts of the site to be used by vehicles are to be laid out, constructed, surfaced, drained and lit shall be submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be implemented in full and thereafter made available for the use of vehicles at all times the development is in use.

Reason

To encourage drivers to make use of the parking and circulation area(s) provided and in order to comply with Core Strategy policies S1 and P5 and Supplementary Planning Document 'Accessibility, Transport and Road Safety'.

14 CAR PARKING PROVISION

The development hereby approved shall be carried out in full accordance, including the prescribed timescales, with the submitted Development Phasing Programme dated 14.03.22.

Within 5 years from the date of approval a scheme for the permanent replacement of the displaced parking spaces, including a timetable for implementation, shall be submitted to and approved by the local planning authority and once approved the scheme shall be implemented in full.

Reason

In order to ensure appropriate levels of car parking at the hospital site and in order to comply with Bolton's Core Strategy policy P5 and Appendix 3.

15 ACCESS

Prior to the development hereby approved/permitted being first occupied or brought into use the means of vehicular access to the site from the site access/Barnes Drive/Kinlay Road shall be constructed as shown on the approved site plan: Landscape General Arrangement,

drawing no. BCMS-RFM-ZO-SL-DR-L00002, Rev P05, dated 29.03.22. The access shall be retained as approved thereafter.

Reason

In the interests of highway safety and in order to comply with Bolton's Core Strategy policies S1, P5 and Supplementary Planning Document 'Accessibility, Transport and Road Safety'.

16 TRAVEL PLAN

Before the development hereby approved is first brought into use, a detailed Travel Plan incorporating measures to promote and maximise the use of sustainable transport and a means of monitoring travel methods and cycle provision on the site shall be submitted to and approved in writing by the LPA. The plan should include a mechanism for the regular monitoring and updating of the Travel Plan. The development shall be implemented in accordance with the approved details prior to the first occupation of approved development and retained thereafter.

Reason

To encourage the use of sustainable means of transport and in order to comply with Bolton's Core Strategy policy P5 and S1.

17 FIXED PLANT AND EQUIPMENT NOISE RATING

The rating level (L<sub>A</sub>r,Tr), as determined by BS4142:2014+A1:2019 Methods for Rating and Assessing Industrial and Commercial sound, from all sources associated with the development, when operating simultaneously or individually, shall not exceed the background sound levels (L<sub>A</sub>90) that are specified in table 4.2 of the report by BWB ref:MCP2167, when measured 4m from the boundary of any noise sensitive receptors.

Reason

To minimise the impact of noise on the general and residential amenity from the increase and alteration of commercial and/or industrial uses in the area and in order to comply with Core Strategy policy CG4.

18 MATERIALS, DOORS AND WINDOWS

Notwithstanding any description of materials/details in the application, no works shall be undertaken to construct the external shell/facing materials of the building, or doors/windows installed, unless and until full details of external materials, doors and windows have been submitted to and approved in writing by the Local Planning Authority. Such details shall include the manufacturer, type, colour and texture. The development shall be carried out in full accordance with the approved details and retained thereafter.

Reason

To ensure the development reflects local distinctiveness and to comply with policy CG3 of Bolton's Core Strategy.

19 LIGHTING

Before the development hereby approved is first brought into use a scheme shall be submitted to and approved in writing with the Local Planning Authority for external lighting. The lighting shall be designed to an illumination value of no greater than 5 lux at the nearest residential property. The beam angle of any lights directed towards any potential observer should be kept below 70 degrees. Spill shields should also be fitted (if necessary). The

approved scheme shall be implemented in full before the development is first brought into use and retained thereafter.

Reason

To safeguard the character and appearance of the locality and to prevent light pollution and in order to comply with Bolton's Core Strategy policies CG3 and CG4.

20 BREEAM ACCREDITATION

Within 6 months of the college building hereby approved being first occupied/brought into use a final certificate certifying that BREEAM (or any such equivalent national measure of sustainable building which replaces that scheme) rating "very good" has been achieved for this development shall be submitted to and approved in writing by the local planning authority.

Reason

In the interests of tackling climate change and in order to comply with Bolton's Core Strategy policy CG2.2.

21 PUBLIC ART

The building hereby approved shall not be occupied/brought into use until details of public art to be provided within the development site have been submitted to and approved by the local planning authority. The public art shall be installed in accordance with the approved details, in a timeframe agreed with the local planning authority and shall be retained thereafter.

Reason

To conserve and enhance local distinctiveness and in order to comply with Bolton's Core Strategy policy IPC1 and Supplementary Planning Document 'Infrastructure and Planning Contributions'.

22 SOIL TESTING METHODOLOGY

The approved soil testing methodology as detailed in the submitted remediation strategy by Tier Environmental Ltd Tier ref: TE1277RS1.2, dated 30th March 2020 shall be implemented in full during the importation of soil or soil forming material.

Prior to the development being first brought into use or occupied a verification report including soil descriptions, laboratory certificates and photographs shall submitted to and approved in writing by the Local Planning Authority.

Reason

To ensure the site is safe for use and in order to comply with Core Strategy policy CG4.

23 CRIME AND SUSTAINABILITY ASSESSMENTS

The development hereby approved shall be carried out in full accordance with the following approved reports:

- The recommendations of the submitted Crime Impact Assessment by Dobson Owens dated 18th January 2019.
- Sustainability & Energy Statement by Couchperrywilkes (ref: 180586)

Reason

To ensure the security and safety of future users of the college, and to ensure the development achieves appropriate sustainability levels and in order to comply with Bolton's Core Strategy policies S1 and CG2.

## 24 CONSTRUCTION MANAGEMENT PLAN

The development hereby approved shall be carried out in full accordance with the Construction Phase Plan, Version 3, Ref: RMS-PL-021, Rev F3, dated 27.05.22.

### Reason

To ensure that adequate consideration is given to the need to minimise the impact on the road network and reduce pollution, in accordance with Policies P5 and CG4 of Bolton's Core Strategy.

## 25 APPROVED DRAWINGS

The development hereby permitted shall be carried out in complete accordance with the following approved plans:

### Site:

- Landscape General Arrangement; drawing no. BCMS-RFM-ZO-SL-DR-L00002, Rev P05; dated 29.03.22
- Site Location Plan; drawing no. BCMS-AAR-ZZ-ZZ-DR-A-90001, Rev P3; dated 11.03.22

### Elevations/Internal Layout:

- Proposed Elevations; drawing no. BCMS-AAR-ZZ-ZZ-DR-A-00010, Rev P5; dated 11.03.22
- Typical Bay Study - North Elevation; drawing no. BCMS-AAR-ZZ-XX-DR-A-21001, Rev P3; dated 11.03.22
- Typical Bay Study - South Elevation; drawing no. BCMS-AAR-ZZ-XX-DR-A-21002, Rev P3; dated 11.03.22
- GA Plan - Ground Floor; drawing no: BCMS-AAR-ZZ-00-DR-A-00001, Rev P11; dated 11.03.22
- GA Plan - First Floor; drawing no: BCMS-AAR-ZZ-01-DR-A-00002, Rev P11; dated 11.03.22
- GA Plan - Second Floor; drawing no: BCMS-AAR-ZZ-02-DR-A-00003, Rev P11; dated 11.03.22
- GA Plan - Third Floor; drawing no: BCMS-AAR-ZZ-03-DR-A-00004, Rev P11; dated 11.03.22
- GA Plan - Fourth Floor; drawing no: BCMS-AAR-ZZ-04-DR-A-00005, Rev P11; dated 11.03.22
- GA Plan - Fifth Floor; drawing no: BCMS-AAR-ZZ-05-DR-A-00006, Rev P8; dated 11.03.22

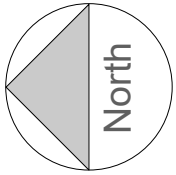
### Other:

- Proposed Cycle Shelters
- Vegetation Removal and Retention Plan; drawing no. BCMS-RFM-ZO-SL-DR-L-00001, Rev P03; dated 29.03.22
- 220508 - BCMS Drainage Adjacent To Retained Trees\_B; Drawing No.: BCMS\_SK001; received 17 May 2022

### Reason

For the avoidance of doubt and in the interests of proper planning.





Copyright of this drawing is vested in the Architect and it must not be copied or reproduced without consent. Only figured dimensions are to be taken from this drawing. All contractors must visit the site and be responsible for taking and checking all dimensions relative to their work. Notify the Architect immediately of any variation between drawings and site conditions.

**DO NOT SCALE OFF THIS DRAWING - IF IN DOUBT ASK.**

Where drawings are based on survey information received, surveys are available on request.

All drawings to be read in conjunction with all relevant Structural and M&E Engineers drawings and specifications.

Where dimensions are given on a drawing they must always be checked against site dimensions. Dimensions on drawings are indicative only. Site checks must be used for fabrication information.

**H&S HAZARDS:**

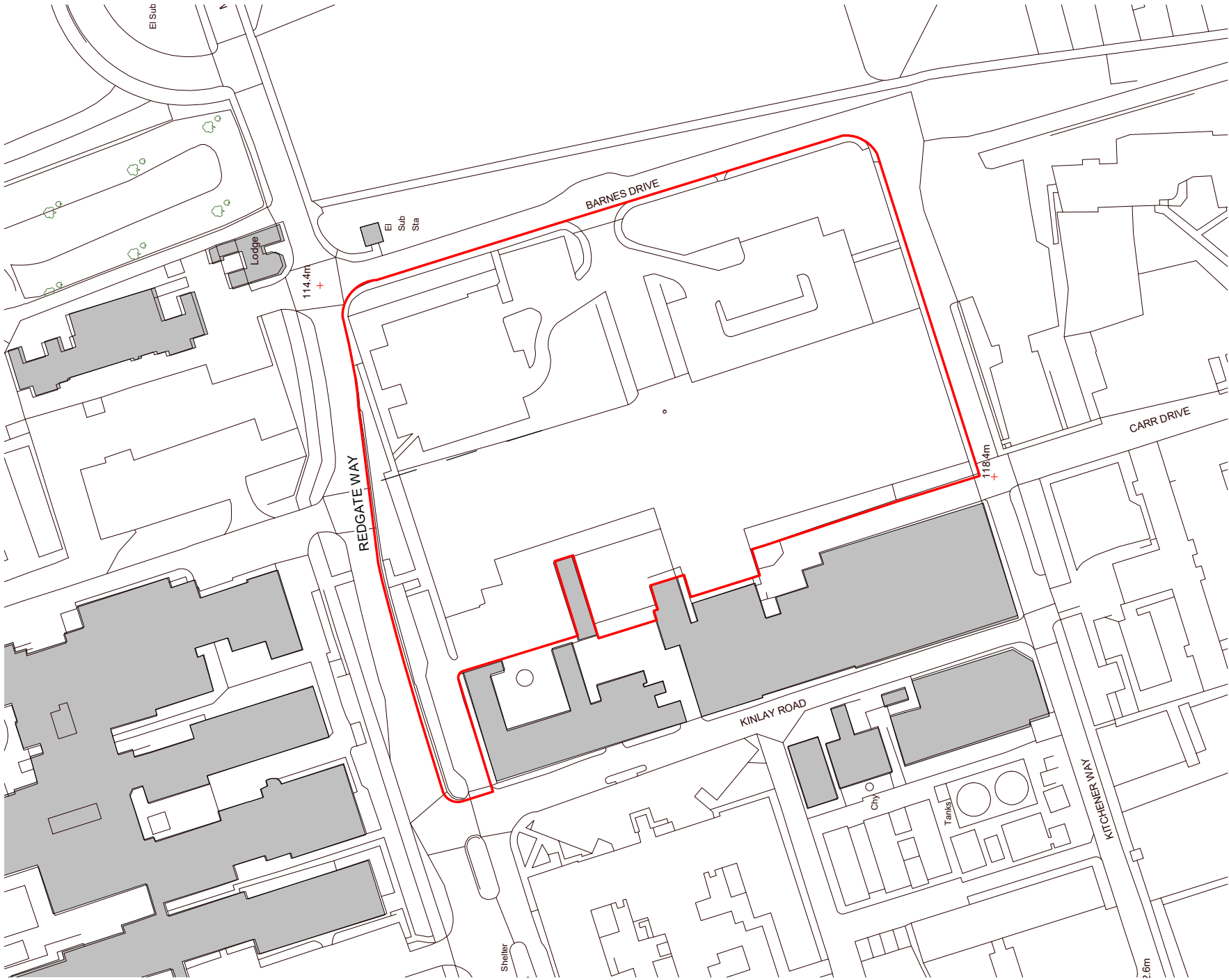
Significant hazards relative to the architectural design shown on this drawing have been assessed and highlighted with reasonable skill and care using a warning triangle system.

This drawing is to be read in conjunction with all other hazards/risks which have been identified and recorded within the construction phase H&S plan.

It is assumed that all works on this drawing will be carried out by a competent contractor working, where appropriate, to an approved method statement.

Significant hazards are defined as: -

- Those not likely to be obvious to a competent contractor or other
- **Geographical Notes:**
- Those of an unusual nature.



P3	Planning Issue	RBAW	11.03.2022
P2	Stage 3 VE Drawings	RBAW	10.02.2022
P1	First Issue	MM	21.01.19
Rev:	Details:	By/Chk	Date:

# ASSOCIATED ARCHITECTS

www.associated-architects.co.uk  
Associated Architects LLP  
100, 101 & 102, The Quadrant, 100 Victoria Road, Birmingham, B1 1SE  
Associated Architects, 1 Severn Street Place, Birmingham, B1 1SE  
OC3046503

Client:	BOLTON COLLEGE		
Project:	Bolton College of Medical Sciences		
Drawing Title:	Site Location Plan		
Stage:	Stage 3	Status:	S4
Drawn By:	MM	Date:	January 2019
Scale:	1 : 1250		
AA Job Number:	3580	Client Project Number:	BCMS
Sheet Size:	A3		
Revision	BCMS-AAR-ZZ-ZZ-DR-A-90001 P3		

00-Site Location Plan

1  
1 : 1250



Key

Planning application boundary

Site works boundary

Existing buildings

Proposed buildings

Proposed concrete paving to pedestrian areas Q25.120A

Proposed macadam surfaces to highway and secondary pedestrian routes

Self-binding gravel surface Q25.310

Concrete tactile blister paving Q25.320A

Concrete tactile corduroy paving to top and bottom of steps Q25.320B

Proposed seating Refer to drawing: BCMS-RFM-Z0-SL-DR-L-08003

Steps to external areas L37.315A and L37.310

Proposed lawn Q30.400A Refer to drawing BCMS-RFM-Z0-SL-DR-L-00003

Proposed herbaceous planting Refer to drawing BCMS-RFM-Z0-SL-DR-L-00003

Proposed hedging Refer to drawing BCMS-RFM-Z0-SL-DR-L-00003

Proposed tree in soft landscape Refer to drawings BCMS-RFM-Z0-SL-DR-L-00003, 08002

Existing tree to be retained

Proposed kerb Refer to drawing BCMS-RFM-Z0-SL-DR-L-00005

Existing wall to be retained

Proposed retaining wall F10.110

Lightpost single sided To M+E engineer's specification

Lightpost double sided To M+E engineer's specification

Uplighter to M&E Engineers specification

Litter bin Q50.240A

Fixed bollards Q50.190A

Picnic bench and table Q50.230A

Proposed handrail to stepped and ramped access L37.150

Proposed levels

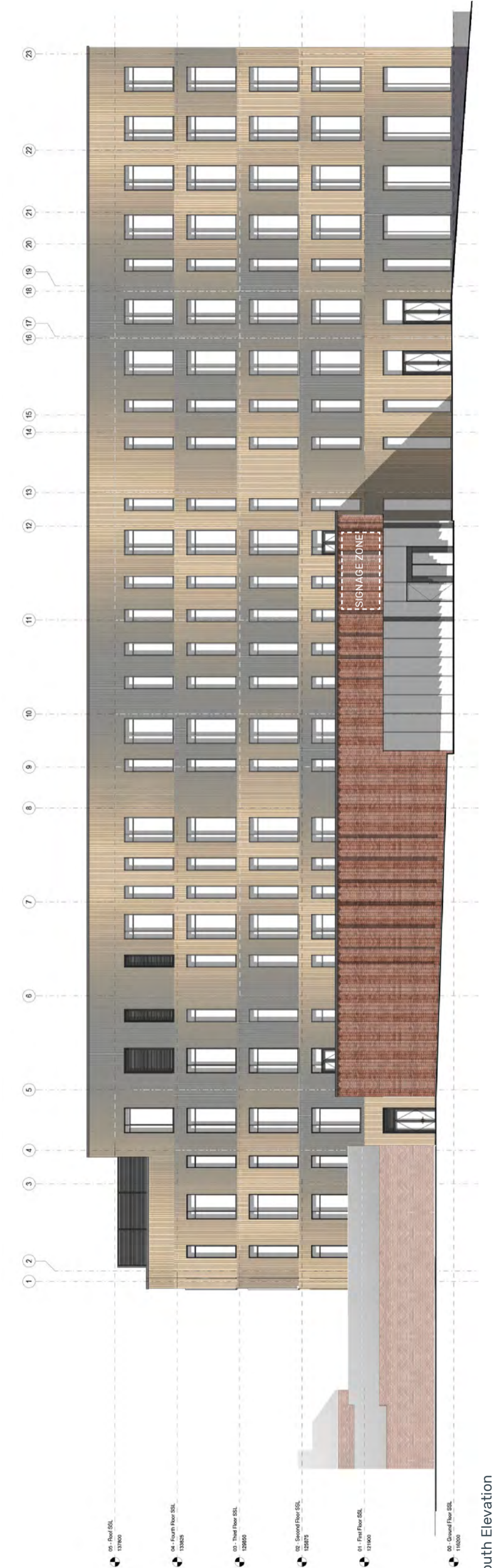
Existing levels

# Elevations

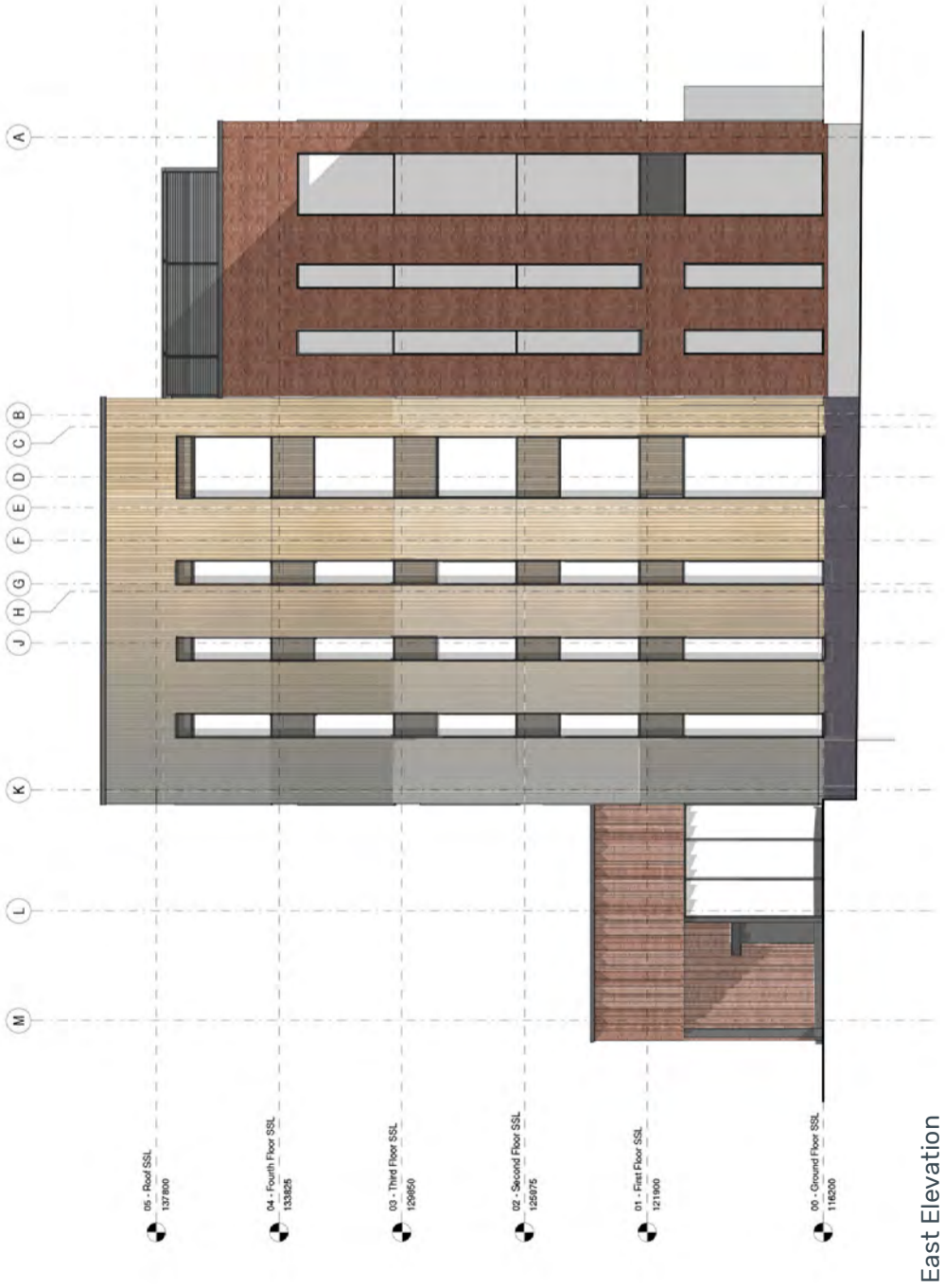


North Elevation

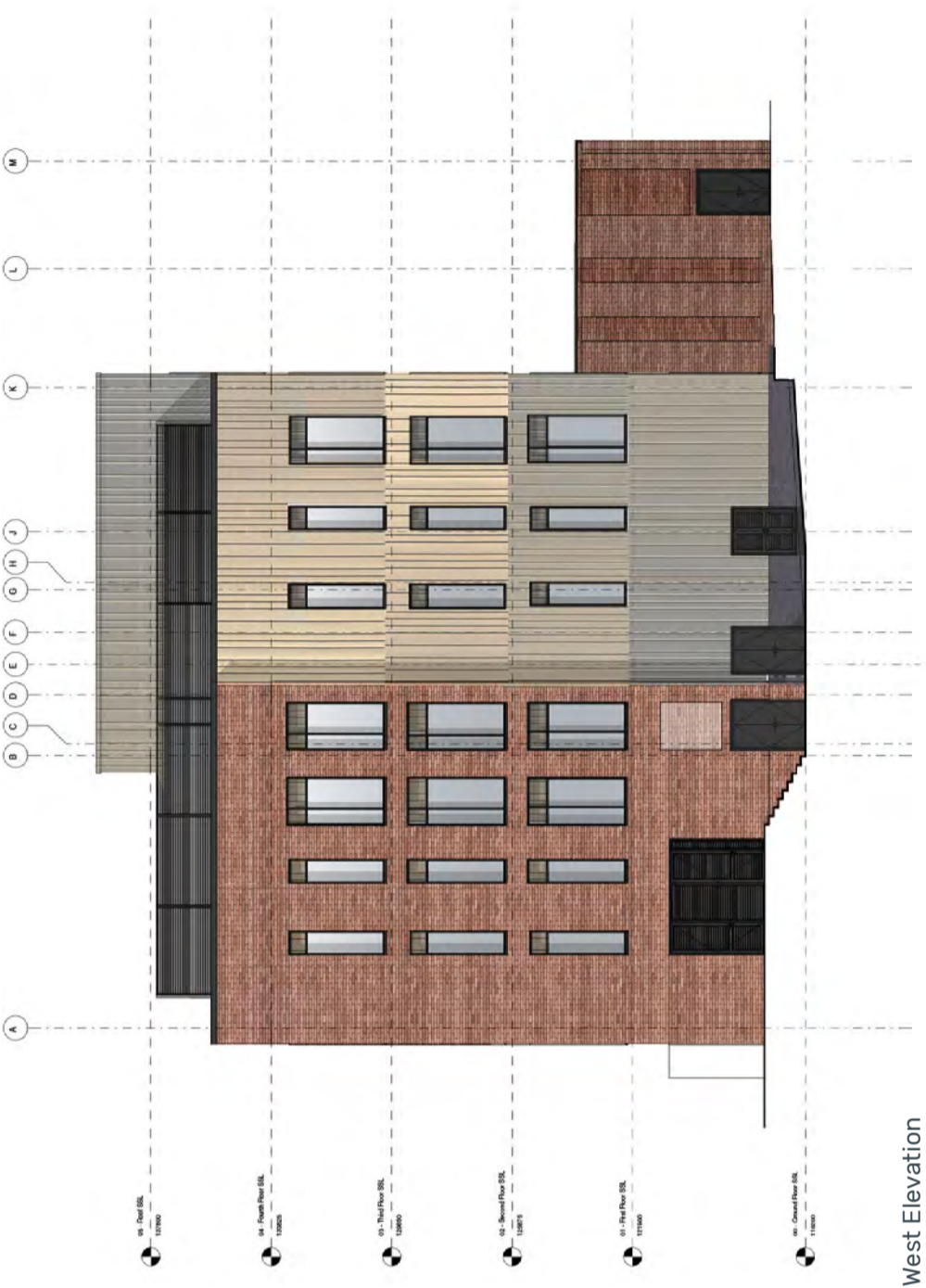
# Elevations



# Elevations



East Elevation



West Elevation

# Artist Impressions

