

Planning Applications Report

**Planning Committee
11th November 2021**

**Bolton
Council**

Bolton Council has approved a Guide to Good Practice for Members and Officers Involved in the Planning Process. Appendix 1 of the Guide sets down guidance on what should be included in Officer Reports to Committee on planning applications. This Report is written in accordance with that guidance. Copies of the Guide to Good Practice are available at www.bolton.gov.uk

Bolton Council also has a Statement of Community Involvement. As part of this statement, neighbour notification letters will have been sent to all owners and occupiers whose premises adjoin the site of these applications. In residential areas, or in areas where there are dwellings in the vicinity of these sites, letters will also have been sent to all owners and occupiers of residential land or premises, which directly overlook a proposed development. Copies of the Statement of Community Involvement are available at www.bolton.gov.uk

The plans in the report are for location only and are not to scale. The application site will generally be in the centre of the plan edged with a bold line.

The following abbreviations are used within this report: -

CS	The adopted Core Strategy 2011
AP	The adopted Allocations Plan 2014
NPPF	National Planning Policy Framework
NPPG	National Planning Policy Guidance
PCPN	A Bolton Council Planning Control Policy Note
PPG	Department of Communities and Local Government Planning Policy Guidance Note
MPG	Department of Communities and Local Government Minerals Planning Guidance Note
SPG	Bolton Council Supplementary Planning Guidance
SPD	Bolton Council Supplementary Planning Document
PPS	Department of Communities and Local Government Planning Policy Statement
TPO	Tree Preservation Order
EA	Environment Agency
SBI	Site of Biological Importance
SSSI	Site of Special Scientific Interest
GMEU	The Greater Manchester Ecology Unit

The background documents for this Report are the respective planning application documents which can be found at:-

www.bolton.gov.uk/planapps

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Application number 10044/20



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Development Management Section

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Bolton Council

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Date of Meeting: 11/11/2021

Application Reference: 10044/20

Type of Application: Full Planning Application

Registration Date: 08/01/2021

Decision Due By: 08/04/2021

Responsible Officer: Helen Williams

Location: LAND AT GRIZEDALE CLOSE, BOLTON, BL1 5QX

Proposal: DEMOLITION OF BUNGALOWS AND ERECTION OF 43No. DWELLINGS COMPRISING FOUR STOREY BLOCK OF 35No. FLATS AND 8No. HOUSES TOGETHER WITH ASSOCIATED ACCESS, PARKING, LANDSCAPING AND RETAINING WALL ALONG SOUTH WESTERN BOUNDARY

Ward: Smithills

Applicant: Watson Homes

Agent : TADW Architects

Officers Report

Recommendation: Approve subject to conditions

Executive Summary

- Γ The application site comprises Somerville Bungalow and its extensive garden, a triangular parcel of land to the south (same ownership) and 11 Grizedale Close. 11 Grizedale Close is proposed to be demolished to accommodate the proposed vehicular access into the new residential development of 35 apartments and 8 houses (proposed as a 100% affordable scheme).
- Γ The site, although mainly greenfield, is not allocated within the Council's development plan and is within a sustainable urban location. The main part of the site is not publicly accessible, as it is in private residential ownership.
- Γ The LPA has received many objections to the proposal from local residents, as well as an objection from Chris Green MP.
- Γ Whilst the proposed four storey apartment building will be larger in height and scale than neighbouring residential properties, officers do not consider that its presence would be to the detriment of the character and appearance of the area, given neighbouring ground level differences, the building's proposed siting and orientation within the development, the distances away from neighbouring properties and tree screening/softening.
- Γ The applicant has amended their plans a number of times, seeking to retain as many existing trees as possible on site. Whilst there will still be considerable trees loss, these will be mainly lost from the centre of the site, with all but three peripheral trees within the protected woodland being retained along with the majority of 'boundary' trees. The majority of trees within the site are to be retained.
- Γ Despite the loss of trees, the proposed development would achieve a biodiversity net gain, mainly through enhancement measures within the protected woodland and also with

replacement planting and alternative habitat accommodation.

- Γ The Council's Tree Officers recommend that the trees to be retained within the development should be protected by way of a new Tree Preservation Order (TPO).
- Γ Given the distances the proposed houses and apartment building are away from neighbouring properties, as well as differences in ground levels, it is not considered that the proposed development would unduly harm the amenity of neighbouring residents.
- Γ Vehicular access into the development would be via existing residential roads (through a residential estate). The Council's Highways Engineers have raised no objection to the proposed development, concluding that the level of traffic impact on these roads should be accommodated with little additional detriment to road safety or the operational capacity of these roads.
- Γ Following amended plans, the public rights of way that run through the site will not be diverted as part of the proposals.
- Γ As the proposal is for 100% affordable housing, no further planning obligations/contributions are required.
- Γ Officers consider that the proposed development fully complies with the Council's development plan policies and therefore Members are recommended to approve this application subject to the suggested conditions.

Proposal

1. Permission is sought for the erection of a total of 43 dwellings following the demolition of Somerville Bungalow and the bungalow at 11 Grizedale Close. The proposed dwellings will comprise a four storey block of 35 apartments and 8 houses (four semi-detached, three mews/terrace and one detached). The development is proposing to provide 100% affordable homes, with funding from Homes England. At the time of writing this report, the applicant was in discussions with Irwell Valley Homes (a Registered Provider/Housing Association) regarding them delivering the dwellings (rather than Bolton at Home, as originally envisaged).
2. The apartment building would consist of 28 one-bedroom apartments and 7 two-bedroom apartments. The proposed houses would comprise 6 three-bedroom and 2 two-bedroom dwellings, and all houses would be two storeys in height.
3. The existing bungalow at 11 Grizedale Close is proposed to be demolished to accommodate the proposed vehicular access into the residential development. The proposed access would continue to provide an access to the neighbouring 13 Grizedale Close.
4. The proposed apartment building would be sited to the west of the proposed internal road (cul-de-sac) and would sit centrally within the application site. A car park comprising 16 spaces is proposed to the north of the building and 19 parking spaces are proposed to the front of the building (one space per apartment). A bin storage compound is proposed to the south of the building, at the end of the turning head. Balconies are proposed for the first, second and third floor apartments, but have been removed from the southern elevation following amended plans. Patio areas are proposed for the ground floor apartments. Communal amenity space is proposed by way of a grassed area to the west/rear of the building.
5. The houses are proposed to be located to the east of the internal road, along the eastern boundary of the application site. The dwellings at plots 1 and 2 would face northwards to address the access road before it curves in a north-south direction. The dwellings at plots 3 to 8 would front onto the proposed apartment building. Two in-curtilage parking spaces are proposed for the 6 three-bedroom houses and one in-curtilage space is proposed for the 2 two-bedroom houses. Rear gardens are proposed for all of the houses and there will be meaningful front gardens for plots 1, 2, 3 and 8.

6. A retaining wall is proposed parallel with the eastern half of the southern site boundary, owing to the changes in ground level.
7. A number of trees are proposed to be removed from the site to accommodate the development. A new planting/landscaping scheme is proposed as mitigation.
8. The proposed plans have been amended a number of times since submission to address comments made by officers and consultees. This has resulted in, amongst other things, the loss of one house and one apartment from the original proposals.

Site Characteristics

9. The application site measures 0.8 hectares and is unallocated within the Council's development plan. The site mainly comprises the residential, 1915 built, Somerville Bungalow and its extensive landscaped garden curtilage. The site also includes a triangular shaped parcel of land to the south (believed to have been bought by the owner of Somerville Bungalow) and the residential bungalow of 11 Grizedale Close and its curtilage.
10. The application site is roughly triangular in shape and constitutes a 'backland' site in that it is located behind and between existing development (dwellings) rather than being adjacent a road.
11. The western portion of the site (which will remain undeveloped) contains protected woodland, protected under Tree Preservation Order Bolton (New Church Road) 1994 (mainly sycamore, beech, horse chestnut, ash, Scot pine and poplar). The protected woodland also neighbours the application site to the north (within the curtilage of 13 Grizedale Close).
12. There are numerous other trees within the site (not afforded statutory protection), mainly along the eastern and northern boundaries of the site and along the southern boundary of Somerville Bungalow's curtilage. The trees that were formerly on the southern triangular portion of the site were felled a couple of years ago/prior to the submission of the application.
13. The application site contains public right of way BOL168, which runs along the eastern boundary of the site, and public right of way BOL165, which runs along the northern boundary, to the north of 11 Grizedale Close. BOL165 becomes BOL167 to the north of 13 Grizedale Close. A further public footpath (BOL166) adjoins the application site to the north, which runs between 15 and 17 Grizedale Close.
14. Along the northern boundary of the application site (within the site) runs a brook. This is culverted under the public right of way BOL165/167.
15. Site levels generally rise in a north to south direction and the garden area to Somerville Bungalow is at a lower ground level than the house and its patio area. The application site is at a lower ground level than the neighbouring houses to the south and west.
16. To the north of the application site is 13 Grizedale Close, a two storey detached dwelling, which is accessed off Grizedale Close via public right of way BOL165/167. The property has a lengthy single storey rear/western element and a single storey garage to its east. Beyond this property is the rears of the two storey detached dwellings at 17 and 19 Grizedale Close (views from these properties to the application site are screened by planting along their rear boundaries).
17. To the east of the application site are the rears of the detached bungalows at 5 to 9 Grizedale Close, the side/rear of the detached bungalow at 3 Grizedale Close and the rears of the two storey detached dwellings at 7 and 9 Great Marld Close.

18. To the south of the application site are the rears of the two storey, red brick and render, terraced houses at 30 to 56 Marld Crescent. These dwellings are elevated from the application site.
19. To the west and north west of the application site are bungalows at 787 to 799b Moss Bank Way, which are sited on higher ground than the application site. Nos. 789 to 799 have lengthy rear gardens.

Policy

20. The Development Plan

Core Strategy Policies: H1.2 Health contributions; A1.4 Education contributions; P5 Accessibility and Transport; S1 Safe Bolton; CG1 Cleaner and Greener; CG2 Sustainable Design and Construction; CG3 The Built Environment; CG4 Compatible Uses; SC1 Housing; OA5 North Bolton; IPC1 Infrastructure and Planning Contributions.

Allocations Plan Policies: P8AP Public Rights of Way.

21. Other material considerations

National Planning Policy Framework (NPPF).

Supplementary Planning Documents (SPDs): General Design Principles; Accessibility, Transport and Road Safety; Affordable Housing; Infrastructure and Planning Contributions.

Bolton's Authority Monitoring Report (AMR) (February 2020)
Housing Delivery Test Action Plan (August 2020)

Places for Everyone – consultation closed 3rd October 2021

DCLG Technical House Standards – Nationally Described Space Standard – March 2015

Analysis

22. Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise. Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission. Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission. It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

23. The main impacts of the proposal are:-

- Γ principle of residential development on the application site
- Γ impact on the character and appearance of the area
- Γ impact on trees and biodiversity
- Γ impact on the amenity of neighbouring and future residents
- Γ impact on the highway
- Γ impact on public rights of way
- Γ impact on drainage and flood risk
- Γ impact on local infrastructure

Principal of Residential Development on the Application Site

24. Policy OA5 of the Core Strategy specifically relates to developments in North Bolton, where the application site is located. Policy OA5.1 states that the Council will concentrate sites for new housing within the existing urban area. The application site is located within an urban area, and within an established residential area.
25. Core Strategy Policy SC1 concerns new housing development and seeks to direct at least 80% of housing development on previously developed land, however this policy has been deemed to be out-of-date by the Planning Inspectorate as it is inconsistent with the NPPF (the Council is unable to demonstrate a 5 year housing land supply).
26. Paragraph 60 of the National Planning Policy Framework (NPPF) states, to support the Government's objective of significantly boosting the supply of homes, it is important that a sufficient amount and variety of land can come forward where it is needed, that the needs of groups with specific housing requirements are addressed and that land with permission is developed without unnecessary delay.
27. The majority of the application site is considered to be greenfield/not previously development: that being the large residential garden to Somerville Bungalow (including the woodland) and the triangular undeveloped portion of the site to the south. The areas where Somerville Bungalow (and its immediate curtilage) and 11 Grizedale Close are located are considered to be brownfield/previously developed.
28. The Council has not set out any policies to restrict the development of greenfield sites/residential gardens (para. 71 of the NPPF) and a presumption must be taken in favour of sustainable development (para. 11 of the NPPF).
29. It is considered that the proposed residential development of the application site, a site within the existing urban area surrounding by compatible neighbouring residential uses and in an accessible location close to local schools and amenities, would constitute sustainable development.
30. The application site comprises of private property, therefore there is no loss of public open/green space.
31. It is therefore considered that the principle of residential development on the application site would comply with Policy OA5.1 of the Core Strategy and the guidance contained within the NPPF.

Impact on the Character and Appearance of the Area

32. Policy CG3 of the Core Strategy states that the Council will [amongst other things] conserve and enhance local distinctiveness, ensuring development has regard to the overall built character and landscape quality of the area, and will require development to be compatible with the surrounding area, in terms of scale, massing, grain, form, architecture, street enclosure, local materials and landscape treatment. Policy OA5 relates specifically to developments in North Bolton and states [amongst other things] that the Council will conserve and enhance the character of the existing physical environment and require special attention to be given to the massing and materials used in new development.
33. Section 12 of the NPPF seeks to achieve well-designed places. It states that the creation of high quality, beautiful and sustainable buildings and places is fundamental to what the planning and development process should achieve. Good design is a key aspect of sustainable development, creates better places in which to live and work and helps makes development acceptable to

communities.

34. Paragraph 131 of the NPPF states that trees make an important contribution to the character and quality of urban environments. Planning decisions should ensure that new streets are tree-lined, that opportunities are taken to incorporate trees elsewhere in developments, that appropriate measures are in place to secure the long-term maintenance of newly planted trees, and that existing trees are retained wherever possible.
35. The proposed development would constitute 'backland' development in that the application site does not adjoin and is not visible from a road. The proposed development will instead be visible from the public rights of way that run through the site to the north and east and from the rear of neighbouring houses and their rear gardens.
36. Many objections have been received from neighbouring residents with regards to the siting, height, scale and appearance of the proposed four storey apartment building, particularly in comparison with the neighbouring bungalows to the east and west of the site. Officers note that there are no other examples of buildings constructed of more than two storeys within the neighbouring area, however that in itself does not render the proposal unacceptable. The apartment building would be sited within the centre of the application site at a finished floor level (FFL) of 171.25. Following initial concerns from officers regarding the scale of the building, the applicant has reduced the height of the proposed roof and has removed the southern most apartment at third floor level (reducing the number of apartments proposed from 36 to 35). With regards to the proposed apartment building's relationship with the bungalows to the east on Grizedale Close, the proposed building will be approximately 52 metres away from the rear of no.7, approximately 47 metres away from the rear of no. 5, and approximately 59.5 metres away from the side/rear of no. 3, with the line of proposed two storey houses in between the bungalows and the apartment building, meaning there would not be a harsh contrast between the differing heights of the buildings. With regards to the bungalows to the west along Moss Bank Way, these are sited at a higher ground level than the proposed apartment building (approximately 3.5 metres higher) and are between 47 and 71 metres away. The apartment building will also be partially screened from view by the retained woodland area (the apartment building would be viewed through the trunks of the trees).
37. The neighbouring two storey houses to the south on Marld Crescent are elevated above the FFL of the proposed apartment building by approximately 5 metres. Only the top part of the fourth storey of the apartment building would therefore exceed the eaves of these neighbouring houses. This relationship and contrast in height and massing (with the building being sited side onto Marld Crescent) is therefore considered acceptable.
38. There will be a contrast in height between the apartment building and 13 Grizedale Close to the north, which is augmented by no. 13 being at a lower ground level than the proposed apartment building, however there will be a distance of 32.2 metres between the two buildings and the apartment building will only be sited side on to no. 13.
39. The layout of the development has been constrained by the protected woodland area to the west and the need for an adoptable standard internal road with turning head. It is however considered that the proposed linear form of development is not out of character with the area. The proposed orientation of the houses at plots 1 and 2 provide active frontages from entering the cul-de-sac.
40. The proposed apartment building will inevitably be bulky owing to its scale, but it is considered that the proposed central, recessed, glazed element helps to break up the overall massing of the

building, as do the balconies and the contrasting brickwork at third floor level (fourth storey). It is acknowledged that the building is otherwise simplistic in its design. The proposed dwellings are also quite modest in design, but the proposed gabled elements and front canopies add some interest.

41. Of the 57 trees and six groups of trees recorded on the application site, 20 trees, a group of 8 Leyland Cypress, and part of two groups of mixed trees are proposed to be felled to accommodate the proposed development. Three of the trees to be lost are within the protected woodland to the west of the site (on the periphery of this woodland). An additional three trees on site are proposed to be felled owing to their poor condition. The majority of the trees to be lost are within the centre of the application, where the apartment building, internal road and southern portion of houses are proposed.
42. The applicant has amended their proposals a number of times to address the comments and concerns from the Council's Tree Officers. Existing trees have been retained where possible.
43. The Council's Tree Officers have not raised an objection to the proposed development but advise that the loss of the three category B Sycamore trees at the north of the site to accommodate the access road and repair work to the culvert (brook) would be to the detriment of the amenity of the area. The applicant has sought to mitigate for this loss with the proposed planting of three Silver Birch along the access road along with a line of native hedge planting and wildflower areas.
44. As the majority of trees within the site are to be retained, including the four that have been categorised as category A/B and the majority of trees along the northern and eastern boundaries (which would help screen/soften the development from neighbouring views), and as the applicant is proposing considerable new planting as mitigation, Officers do not consider that the proposed development would result in the unacceptable loss of trees from the site. The impact of the tree loss and the applicant's proposed landscaping scheme is considered in more detail in the trees and biodiversity section below.
45. The Council's Tree Officers are advising, should the application be approved, that the trees to be retained within the development be protected by virtue of a new Tree Preservation Order (TPO).
46. It is not considered that the demolition of 11 Grizedale Close and the construction of the access road in its place would harm the character and appearance of the street scene, given there are many culs-de-sac in the area.
47. For the reasons discussed above, it is not considered that the proposed development would unduly harm the character and appearance of the area, and therefore the development is considered to be compliant with Policies CG3 and OA5 of the Core Strategy.

Impact on Trees and Biodiversity

48. Policy CG1.2 of the Core Strategy states that the Council will safeguard and enhance biodiversity in the borough by protecting sites of urban biodiversity including trees, woodland and hedgerows from adverse development, and improving the quality and interconnectivity of wildlife corridors and habitats.
49. Paragraph 174d) of the NPPF states that planning decisions should contribute to and enhance the natural and local environment by minimising impacts on and providing net gains for biodiversity, including by establishing coherent ecological networks that are more resilient to current and future pressures.

Trees

50. As reported above, there needs to be considerable tree felling within the site to accommodate the proposed development, however the majority of the trees on the site will be retained, including all but three of the trees already protected by the woodland TPO, and the four unprotected trees on site that have been categorised within the submitted Arboricultural Report as category A/B trees (the four best quality trees).
51. During the application process, the applicant amended their plans to move the dwellings at plots 1 and 2 further westward, to allow for the retention of two further trees (T7 and T8). The proposed retaining wall to the south of the site has also been reduced in length to reduce any impact on the trees to the west.
52. As identified above in the last section, the Council's Tree Officers consider that the loss of the three category B trees (T10, T11 and T12) at the north of the site (which need to be felled to accommodate the access road and to allow repair works to the culverted brook) would be to the detriment of the area. The applicant has however sought to mitigate for this loss with the proposed planting of three Silver Birch along the access road along with a line of native hedge planting and wildflower areas.
53. The applicant has confirmed that Group G1 (the group of trees along the eastern boundary, behind 5 to 9 Grizedale Close, and which the Tree Officers consider provides an important screen to those neighbours) will only be cut back, with shrubs removed, and the eight Cypress trees along the boundary will be retained. The Tree Officer considers this to be acceptable.
54. The car parking area to the north of the proposed apartment building is proposed to be constructed using a geosystem, where the bays and access are within the root protection areas of the neighbouring trees that are to be retained to the north. The Tree Officers consider that this is acceptable within the car park area, however they advise that the main road and pavement would still have some impact on the root protection zones of two of the trees (T14 and T15). The proposed geosystem construction for the car park, along with the proposed geosystem for one of the parking spaces to plot 1 (adjacent T9) would be secured via a condition.
55. A Woodland Management Plan has been submitted during the application process. This plan relates to the protected (TPO) woodland within the western section of the application site and sets out the management requirements for this area, as well as outlining future management steps required. The woodland management will, along with the other open space within the development, be funded and managed via a management company. Planting aftercare and site management are to be undertaken by this company for a period of at least 10 years. This management of the protected woodland is to be secured via a condition.
56. Conditions are also suggested to ensure that the development is constructed in accordance with the measures detailed within the Arboricultural Report and that protective fencing (in the approved locations) is erected prior to commencement of any development.
57. As reported above, the Council's Tree Officers are advising, should the application be approved, that the trees to be retained within the development be protected by virtue of a new Tree Preservation Order (TPO).

Bats

58. The surveys accompanying the planning application found two bat roosts to be present on the

site, which were classified as being day roosts of a common species. Greater Manchester Ecology Unit (GMEU) has commented that the proposed indicative mitigation provided for the loss of these roosts (four bat boxes) is acceptable. A condition ensuring the provision of the bat boxes is therefore suggested.

59. GMEU has also advised that the demolition of the buildings would require a license from Natural England (pursuant to Regulation 55 of the Conservation of Habitats and Species Regulations 2018), which would need to be an informative note on any planning consent, and has recommended a planning condition for a lighting design strategy for biodiversity (to not affect any new roost sites or the foraging and commuting habitat of the bats).

Water vole

60. GMEU initially commented that the water vole survey undertaken by the applicant was outside the water vole survey season. The applicant's ecology consultant however responded directly to GMEU to confirm that the brook within the application site is fast-flowing and highly isolated from any suitable water vole habitat with the surrounding landscape by way of extensive residential development. The applicant's consultant also raised other factors, such as the absence of in-channel vegetation and lack of erosion to the embankments. The site is therefore not suited to the requirements and preferences of this species. GMEU offered no further comments following this response.

Protection of biodiversity during the construction phase

61. The submitted ecology report suggests a number of measures to protect biodiversity on the application site during the construction period, such as avoidance of the bird breeding season for tree felling and protection measures for hedgehogs. GMEU therefore recommend that these measures are secure through the use of a construction environmental management plan for biodiversity condition (CEMP).

Invasive species

62. A number of invasive species (Montbrecia, Rhododendron, Japanese Rose, Wall Cotoneaster, Variegated Yellow Archangel and Three-cornered Leek) have been found on the site, therefore a pre-commencement condition for their removal is suggested.

Biodiversity net gain

63. The Wildlife Trust for Lancashire, Manchester & North Merseyside objected to the proposal back in February, stating that they considered the proposal to be an, "...overdevelopment resulting in the loss of broadleaved woodland habitat and the species dependent on it." This objection was however received prior to the submission of amended plans, the submission of the biodiversity metric and the submission of the woodland management plan. The Wildlife Trust were re-consulted in September, but no further comments have been received from them.
64. Both local and national planning policy concerning biodiversity requires there to be enhancement and net gains for biodiversity. The Environment Bill and Places for Everyone will eventually require a minimum biodiversity net gain of 10% for proposed development, but until that time there is no set percentage requirement, only a requirement for enhancement.
65. The applicant's submitted biodiversity metric indicates that there will be a net gain of 25% in habitat units (GMEU advise this is largely achieved through the proposed enhancement of the retained protected woodland), no change in river habitat units, and a net loss of 22% of hedgerow units. Using simple maths, this would equate to a 3% biodiversity net gain on site.
66. Following the submission of the biodiversity metric, the applicant has significantly increased the

amount of proposed hedgerow planting within their scheme (within their amended landscaping scheme) by introducing a long line of hedgerow to the rear of the apartment building and along the western section of the southern boundary to the site. The biodiversity metric has not been updated to reflect this: it is not considered that this would be necessary given the applicant has demonstrated that there would be an overall biodiversity net gain as a result of the development.

67. GMEU has advised, as the biodiversity net gain within the development is reliant on the enhancement measures proposed within the woodland, that the woodland management plan is secured by a planning condition, and that monitoring reports, to prove that the woodland is being managed in accordance with the approved plan, are required on an annual basis: this condition is within the list of suggested conditions to Members.
68. For the reasons discussed above it is considered, subject to the suggested conditions, that the proposed development will safeguard biodiversity both during the construction period and during the life of the development and will provide a biodiversity net gain. It is therefore considered that the proposed development is compliant with Policy CG1.2 of Bolton's Core Strategy.

Impact on the Amenity of Neighbouring and Future Residents

69. Policy CG4 of the Core Strategy states that the Council will ensure that new development is compatible with surrounding land uses and occupiers, protecting amenity, privacy, safety and security, and does not generate unacceptable nuisance, odours, fumes, noise or light pollution. SPD General Design Principles sets out the Council's recommended interface distances between dwellings. This states that the recommended interface distance between single or two storey dwellings is 21 metres (where elevations contain main room windows) and 3 metres should be added for each additional floor or equivalent level difference. Where a single or two storey dwelling with an elevation containing a main room window faces a two storey blank elevation (no main windows) the recommended interface distance is 13.5 metres, and again 3 metres should be added for each additional floor or equivalent level difference.

Neighbouring residents

70. Bungalows on Grizedale Close neighbour the application site to the east. The side of the two storey dwelling proposed at plot 1 would be 16.7 metres away from the rear of 7 Grizedale Close. The two houses are on a similar ground level (as shown in proposed site section E-E) and therefore this interface distance comfortably exceeds the recommended interface distance contained within the SPD. 5 Grizedale Close would not directly overlook the rear elevation of the dwelling at plot 3 and would be approximately 19.5 metres away (rear elevation to rear elevation). This exceeds the recommended interface distance in the SPD of 17 metres, where there is an oblique view. The rear of 3 Grizedale Close would be 26.3 metres away from the rear of the dwellings proposed at plots 3 to 7, which far exceeds the recommended interface distance within the SPD.
71. As reported previously, the proposed apartment building will be approximately 52 metres away from the rear of 7 Grizedale Close, approximately 47 metres away from the rear of no. 5, and approximately 59.5 metres away from the side/rear of no. 3.
72. The two storey dwelling at 9 Great Marld Close, also to the east, would be approximately 20 metres away from the proposed dwelling at plot 8, and at an oblique angle. This distance therefore exceeds the recommended 17 metres within the SPD.
73. The neighbouring two storey houses to the south on Marld Crescent are elevated above the FFL of the proposed apartment building by approximately 5 metres. Only the top part of the fourth

storey of the apartment building would therefore exceed the eaves of these neighbouring houses. The nearest houses to the southern side of the apartment building are 36 (22.4 metres away) and 38 Marld Crescent (22.9 metres away). Owing to the substantial difference in ground levels (the FFL for the apartment building being almost two storeys lower) there is only about half a storey in difference in overall 'height' between the apartment building and the houses on Marld Crescent. The proposed interface distances are therefore considered to be acceptable, especially as there will be no main windows within the apartments directly facing Marld Crescent. In addition, no balconies are proposed on the southern elevation of the apartment building.

74. The bungalows to the west of the application on Moss Bank Way are sited at a higher ground level than the proposed apartment building (approximately 3.5 metres higher) and are between 47 and 71 metres away. The apartment building would also be partially screened from view by the retained woodland area (the apartment building would be viewed through the trunks of the trees). These interface distances far exceed what is expected within the General Design SPD.
75. There would be a distance of 32.2 metres between the northern side of the apartment building and 13 Grizedale Close. No main room windows or balconies are proposed on the side elevation of the apartment building and therefore there would be no undue overlooking. The distance also exceeds the 26 metre recommended interface distance (17 plus 6 metres for two additional storeys and an additional 3 metres for the level difference).
76. By the nature of the proposal, there will inevitably be some disturbance to neighbouring residents during the construction period. The Council's Pollution Control Officers are therefore recommending that the applicant submits a construction management plan, detailing how construction disturbance is to be kept at a minimum, for the LPA's approval prior to commencement of development.
77. For these reasons, it is not considered that the proposed development would unduly harm the living conditions and privacy of neighbouring residents, compliant with Policy CG4 of the Core Strategy.

Future residents

78. The proposed one bedroom apartment measure 45 sq. metres and the proposed two bedroom apartments measure 57.5 sq. metres. These fall short of the nationally described space standard (DCLG 2015) by 5 sq. metres for the one bedroom apartments and 3.5 sq. metres for the two bedroom apartments. Whilst the nationally described space standards are a material consideration, only some planning weight can be afforded to these as they do not form part of the Council's development plan policies. In this instance each apartment would also have either a balcony or patio area measuring approximately 4 sq. metres (which is not included in the calculated floor space) and a sizeable communal, lawned, amenity area of over 300 sq. metres, and therefore officers do not consider that the amenity of the future residents of the apartment development would be unduly harmed.
79. Furthermore, the applicant contends that Home England will be funding the development, and that anything over 85% of the national space standard for apartments are acceptable for their funding requirements.
80. The Council's Pollution Control Officers have recommended sound insulation and noise reduction measures for the apartments, but this is considered to be a Building Regulations matter rather than Planning.
81. The sizes of the two storey houses (2 bed 73 sq. metres and 3 bed 85 sq. metres) exceed the

nationally described space standards. Each house would also have its own rear garden.

82. For the reasons discussed above, it is not considered that the proposed development would unduly harm the amenity of either neighbouring or future residents, compliant with Policy CG4 of Bolton's Core Strategy.

Impact on the Highway

83. Policy P5 of the Core Strategy states that the Council will ensure that developments take into account [amongst other things] accessibility by different types of transport, servicing arrangements and parking, in accordance with the parking standards sets out in appendix 3 of the Core Strategy. Policy S1.2 states that the Council will promote road safety in the design of new development.
84. Paragraph 111 of the NPPF states that development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe.
85. The existing bungalow at 11 Grizedale Close is proposed to be demolished as part of the development, to accommodate the proposed vehicular access into the residential development. The proposed access would continue to provide an access to the neighbouring 13 Grizedale Close. To access the development, vehicles would need to travel via New Church Road, then Bleasdale Road, then Grizedale Close.
86. A Transport Statement has been submitted with the application. The Council's Highways Engineers have reviewed this statement and comment that it gives a robust assessment of the potential traffic impact on the surrounding highway network and appears to demonstrate that the site is reasonably accessible to sustainable modes of transportation.
87. The Transport Statement considered the impact of the originally proposed 45 dwellings: 43 dwellings are now proposed following amendments to the scheme. Taking into consideration a development of 45 dwellings, the development would generate a potential of 27 two-way vehicle trips within the morning and afternoon peak periods (14 in and 13 out, or 1 additional two-way trip every 2 minutes). The Council's Highways Engineers state that, although the development site will be served from existing residential roads, based on the submitted transport information, the level of traffic impact on those roads should be accommodated with little additional detriment to road safety or the operational capacity of the surrounding highway network. The Engineers therefore conclude that the residual traffic impact associated with the proposed development could not be considered "severe", as is the test of paragraph 111 of the NPPF.
88. Engineers advise that the proposed access would be compliant with the Council's adoptable standards and that the visibility requirements are in line with national guidance and sufficient to support the level of development proposed. It has also been confirmed that the internal road layout appears to be to adoptable standards.
89. Engineers have commented that it appears that the proposed access would have minimal impact on the adjacent property's (9 Grizedale Close's) driveway.
90. Car parking for 35 vehicles (one space per unit) is proposed for the apartments. Two in-curtilage parking spaces are proposed for the 6 three-bedroom houses and one in-curtilage space is proposed for the 2 two-bedroom houses. These meet the maximum parking standards set out in Appendix 3 of the Core Strategy (the standards are maximum not minimum standards). Engineers have recognised that the development site is in an accessible location and have raised

no objection to the level of parking proposed. Engineers recommend that the applicant fund the promotion of a traffic regulation order for the promotion of additional waiting restrictions at the access junctions from Grizedale Close and around the turning provision in the site: this is suggested by condition.

91. Highways Engineers comment that the conditioning of a Construction Management Plan (as discussed previously within this report) would help to manage construction traffic, as this would include a traffic management plan.
92. It is therefore considered, subject to the conditions recommended by the Council's Highways Engineers, that the proposed development would not jeopardise highway safety or have a harmful impact on the capacity of the local highway network, compliant with Policies P5 and S1.2 of Bolton's Core Strategy.

Impact on Public Rights of Way

93. Policy P8AP of the Allocations Plan states that the Council will permit development proposals affecting public rights of way provided that the integrity of the right of way is retained.
94. The application site contains public right of way BOL168, which runs along the eastern boundary of the site, and public right of way BOL165, which runs along the northern boundary, to the north of 11 Grizedale Close. BOL165 becomes BOL167 to the north of 13 Grizedale Close. A further public footpath (BOL166) adjoins the application site to the north, which runs between 15 and 17 Grizedale Close.
95. When the application was first submitted it was proposed to divert footpath BOL168 so that it ran along the eastern pavement of the internal road within the development, then join the existing route at the south eastern corner of the application site. To accommodate the proposed changes in levels, steps were being proposed by the side of the southern retaining wall. The rear gardens to the proposed dwellings would have extended up to the rear gardens of the properties on Grizedale Road. The Council's Public Rights of Way Officer and Bolton Ramblers both raised concerns regarding this proposed diversion, mainly concerning the potential conflict between users of the footpath with vehicles and the potential accessibility problem with the introduction of steps.
96. Taking these concerns on board, as well as objections from local residents, the applicant amended their plans to retain footpath BOL168 as existing (at its current width). This resulted in shorter rear gardens for the proposed houses.
97. A bollard is proposed at where BOL168 meets to the proposed access road, to prevent vehicular access. Tactile paving will be fitted at the northern entrance of the footpath, but the remaining of the path will be untouched.
98. The route of BOL165, along the northern boundary of the development, is retained.
99. It is therefore considered that the proposed development, as amended, retains the integrity of the public rights of way through the application site, compliant with Policy P8AP of the Allocations Plan.

Impact on Drainage and Flood Risk

100. Policy CG1.5 of the Core Strategy states that the Council will reduce the risk of flooding in Bolton and other areas downstream by minimising water run-off from new development and ensuring a sequential approach is followed, concentrating new development in areas of lowest

flood risk. Policy CG2.2 states that the Council will ensure that all proposals for 5 or more residential units demonstrate the sustainable management of surface water run-off from developments. On brownfield sites the rate of run-off should be 50% less than conditions before the development. On greenfield sites the rate of run-off should be no worse than the original conditions before development.

101. The application site is located within Flood Zone 1. Flood Zone 1 is an area of low risk flooding, with a risk of flooding lower than a 1 in 1000 year event.
102. A Flood Risk Assessment and a proposed drainage strategy has been submitted with the application. The applicant is proposing that the main drainage, which will comprise a pipe down the internal road connecting to an existing pipe to the north, will be adopted by United Utilities. United Utilities have raised no objection to the proposal. A private cellular storage tank within the car park to the north of the apartment building would contain any flooding event.
103. The Council's Drainage Officers have raised no objection to the proposed surface water drainage scheme but have requested the imposition of the LPA's standard surface water drainage condition, to allow for further details to be submitted and agreed, as well as the proposed management and maintenance plan for the proposed private cellular storage tank.
104. It is therefore considered, subject to the suggested condition, that the proposed development would comply with Policies CG1.5 and CG2.2 of the Core Strategy.

Impact on Local Infrastructure

105. Policy IPC1 of the Core Strategy states that the Council will seek to ensure that developers make reasonable provision or contribution towards the cost of appropriate physical, social and green infrastructure required by the proposed development and/or to mitigate the impact of that development. In doing so, the Council will ensure that a scheme is made acceptable in planning terms and achieves the objectives of sustainable development. In determining planning applications the Council will apply a threshold of 15 dwellings in assessing whether planning contributions will be required from proposed housing developments.
106. As the applicant is proposing that the development would deliver 100% affordable housing through a Registered Social Landlord (RSL) there is no policy requirement for the applicant to contribute to any other infrastructure obligations. However, at the time of writing this report, the applicant was still in talks with a local RSL about delivering the site. As there is currently no written confirmation that the RSL will take on the units, officers are therefore suggesting a condition be attached (suggested condition 2) to ensure that a scheme for the provision of the affordable housing is approved prior to any commencement of development. The development therefore would not be able to commence unless this condition is met.

Other Matters

Land contamination

107. The submitted Preliminary Risk Assessment, Geo-Environmental Investigation, Risk Assessment and Remediation Strategy have been approved by the Council's Pollution Control Officers.

Coal mining legacy

108. A coal mining report was submitted with the application. The Coal Authority have raised no objection to the proposal but recommend the imposition of a standards intrusive site survey condition, which would need to be discharged prior to commencement of development.

Sustainability

109. A sustainability statement has been submitted with the application. It is stated that the development will be built to at least the minimum Part L Building Regulations 2014, which will be equivalent to Level 4 of the Code for Sustainable Homes. It is anticipated that over 10% of materials used will be recycled and the development will have an air source heat pump cylinder and mechanical ventilation with heat recovery (MVHR) provided to each apartment.
110. A condition requiring details of electronic vehicle charging points within the development is suggested.

Tilted balance

111. It is acknowledged that the Council is not able to demonstrate a 5-year supply of deliverable housing sites. This represents a housing shortfall. In this circumstance paragraph 11 of the NPPF indicates that the policies that are most important for determining the application should be regarded as out-of-date and that there is a 'tilted balance' in favour of granting permission. It is found in this case that there is no harm that would significantly and demonstrably outweigh the tilted balance in favour of granting permission.

Conclusion

112. For the reasons discussed above, it is considered that the proposed development of this unallocated site within an urban area of the borough would not be detrimental to the character and appearance of the area, would safeguard and enhance the biodiversity of the site, would not unduly harm the amenity of neighbouring and future residents, would not jeopardise highway safety or harm the capacity of the local highway network, would not affect the integrity of the public rights of way through the site, and would not increase the risk of flooding. It is therefore considered that the proposed development is compliant with the Council's development plan policies and that there are no other material considerations that would indicate the application should be refused.
113. Members are therefore recommended to approve this application, subject to the suggested conditions.

Representation and Consultation Annex

Representations

Letters:- 81 objections have received, which raise the following concerns:

- Γ The development is out of character with the area. Effect on the character and appearance of the area;
- Γ The apartment building will look completely out of place. It is more fitting for an inner city area. It will be an eyesore. It will be overbearing;
- Γ The proposed apartment building is incompatible with the neighbouring bungalows;
- Γ There are no other apartments within Smithills;
- Γ Rental properties are out of character (*Officer comment: Tenure is not considered to be a determining factor in whether or not the development is compatible to the character and appearance of the area*);
- Γ The proposed density of the development is unsuitable. The proposal would represent overdevelopment of the site;
- Γ Somerville Bungalow should be protected, it is over 100 years old (*Officer comment: The bungalow is not listed, not within a conservation area and not on a local list, and therefore is not afforded any protection. Officers are aware of potential structural concerns, owing to damage to the dwelling from neighbouring trees*);
- Γ Loss of privacy to neighbouring residents, especially from the proposed apartment building;
- Γ Neighbouring houses and gardens will be overlooked. Loss of trees from the site will further reduce privacy;
- Γ Loss of green space. The site is a valued space;
- Γ The site could be better used/used by the community (*Officer comment: The Council cannot consider alternative uses for the site, only the application/s submitted to them*);
- Γ The proposal is "garden grabbing";
- Γ Loss of trees and the impact of this on the amenity of the area;
- Γ The landowner has already felled a number of trees before submitting the planning application (*Officer comment: This is noted. The trees were not protected*);
- Γ There are discrepancies between plans with regards to which trees are to be retained (*Officer comment: The plans have now been amended/corrected*);
- Γ Impact on/loss of wildlife and habitats (birds, bats, amphibians and mammals). The site is a wildlife haven;
- Γ Full bat surveys should be carried out (*Officer comment: A further survey has been undertaken, as recommended by GMEU*);
- Γ There is insufficient biodiversity off-setting proposed to compensate for loss;
- Γ There is little space within the development for new landscaping;
- Γ Increase in traffic on Bleasdale Road and Grizedale Close;
- Γ Substandard access. The surrounding roads and junctions are not wide enough to take additional traffic, emergency vehicles and refuse vehicles;
- Γ Insufficient parking proposed on-site, which will lead to an increase in on-street parking on the surrounding roads;
- Γ Highway and pedestrian safety concern from the increase in traffic and parked cars;
- Γ Increase in pollution from additional traffic and effect on health;
- Γ Increase in noise from additional traffic. The surrounding roads are currently quiet;
- Γ Increase in damage to local roads. Local roads are already damaged/pot-holed (*Officer comment: The roads leading to the application site are adopted and therefore the responsibility of the Local Highway Authority*);
- Γ Noise and disturbance from the proposed residential development (*Officer comment: the use of the site for residential purposes is considered to be compatible with the surrounding residential*

uses);

- Γ Light pollution (*Officer comment: An external lighting condition has been recommended, to ensure there is no light pollution*);
- Γ An Environmental Impact Assessment should be carried out (*Officer comment: The development does not exceed the threshold within the EIA Regulations 2017 – 150 dwellings or over 5ha – to require a screening opinion to be undertaken*);
- Γ Object to the proposed diversion of the public right of way and the proposed introduction of steps (*Officer comment: As explained within this report, the footpath is no longer proposed to be diverted and there is no longer a requirement for steps*);
- Γ The bridleway cannot be closed. The bridleway may be used as a rat run to Moss Bank Way (*Officer comment: BOL165/167 is not a bridleway: it will remain as existing*);
- Γ Increase in crime (*Officer comment: Greater Manchester Police have raised no objection to the proposal*);
- Γ Insufficient sewers and drainage in the area for the proposed size of development. The site is liable to flooding;
- Γ Pressure on local services, such as schools and doctors;
- Γ Disruption and potential damage during the construction phase;
- Γ The size of the apartments fall below the national space standards;
- Γ The apartments are not wheelchair friendly (*Officer comment: A lift is proposed within the building. As the apartments would be managed by a registered social landlord, it is expected that the apartments will meet the tenant's needs*);
- Γ There is Japanese knotweed on the site;
- Γ The Coal Authority's recommendations have not been addressed (*Officer comment: the Coal Authority has raised no objection subject to a standard condition requiring a scheme of intrusive site investigations: this condition is suggested to Members*);
- Γ Monetary gain for the landowner. Concern that the Council sold some of the site to the landowner for a small amount of money (*Officer comment: This is not a material planning consideration*);
- Γ Concerns about the 'types' of residents that would move in. Concerns that young people will move into an area where the majority of residents are of pensionable age (*Officer comment: This is only a perceived harm and therefore this concern can only be given limited planning weight in the determination of this application*);
- Γ The apartment building will reduce local house prices (*Officer comment: This is a private rather than public concern and therefore is not a material consideration*);
- Γ Loss of views from neighbouring properties (*Officer comment: This is a private rather than public concern and therefore is not a material consideration*);
- Γ There are other sites in the borough more suitable for this development (*Officer comment: Each application must be considered on its own merits, therefore the suitability of this application site has to be considered on its own merits*);
- Γ There is no need for this development (*Officer comment: The Council has a significant shortfall in housing supply*);
- Γ There is no need to demolish two existing bungalows (*Officer comment: The proposed development would result in the net gain of 41 dwellings*);
- Γ Impact on the structure/stability of neighbouring houses from the proposed development (*Officer comment: There is no reason to expect any structural issues to occur, but this in any event would be a private matter*).

3 letters of support have been received (as well as 2 further anonymous letters of support), which raise the following:

- Γ The development would contribute towards the Government's aim to build over 300,000 homes annually;

- Γ Will help meet the housing need for those who cannot afford to own or rent their own home/There is a need for more affordable houses in the area;
- Γ There is a housing shortage and an increasing number of young people have to live with their parents;
- Γ The site is suitable for the development;
- Γ The application site only currently accommodates one house but it is large enough to accommodate many more;
- Γ The development is in keeping with the area/will improve the environment of the surrounding area;
- Γ The site is not Green Belt.

Petitions:- A petition of objection (change.org), containing 647 signatures, has been received. This petition objects to the proposed development for the following reasons:

- Γ Eco-systems will be destroyed;
- Γ Invasion of neighbouring residents' privacy;
- Γ Will cause excess traffic, pollution and noise;
- Γ Extra pressure on local schools and other services;
- Γ Will take away valuable green space;
- Γ The proposed apartment block will not fit in with surrounding properties.

Chris Green MP:- has written in objection to the proposal, for the following reasons:

- Γ The apartment building will create a loss of privacy to neighbouring residents;
- Γ Loss of local green space;
- Γ Felling of trees;
- Γ Increase in traffic on Grizedale Close;
- Γ Insufficient on-site parking proposed.

It is considered that concerns raised, without a specific officer comment in response, have been addressed within the analysis of this report.

Consultations

Advice was sought from the following consultees: Highways Engineers, Drainage Officers, Public Rights of Way Officer, Pollution Control Officers, Tree Officers, Landscape Officers, Greenspace Officers, Housing Strategy Officers, Asset Management and Pupil Place and Planning Officers, Planning Strategy Officers, Greater Manchester Ecology Unit, Lancashire Wildlife Trust, Bolton Ramblers, Peak and Northern Footpaths Society, Open Spaces Society, Greater Manchester Police, Primary Care NHS Bolton, Coal Authority, United Utilities.

Planning History

None for Somerville Bungalow and the adjacent land.

11 Grizedale Close – 14426/80 – Erection of a 7ft 6 high boundary fence at rear – approved June 1980.

Recommendation: Approve subject to conditions

Recommended Conditions and/or Reasons

1. Commencement

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason

Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. **Provision of affordable housing**

No development shall commence unless and until a scheme for the provision of the affordable housing within the development shall be submitted to and approved in writing by the Local Planning Authority. The affordable housing shall be provided in accordance with the approved scheme and shall meet the definition of affordable housing in Annex 2 of the National Planning Policy Framework or any future guidance that replaces it. The scheme shall include:

- i. the number, type, and tenure of the affordable housing to be provided which shall consist of not less than 43 units;
- ii. the arrangements for the management of the affordable housing by an affordable housing provider;
- iii. the arrangements to ensure that such provision is affordable for both first and subsequent occupiers of the affordable housing; and
- iv. the occupancy criteria to be used for determining the identity of occupiers of the affordable housing and the means by which such occupancy criteria shall be enforced.

This condition shall not be binding upon a mortgagee, chargee or any receiver (including an administrative receiver) appointed by such mortgagee or chargee or any other person appointed under any security documentation to enable such mortgagee or chargee to realise its security or any administrator (however appointed) including a housing administrator (each a Receiver) of the whole or any part of the affordable housing or any persons or bodies deriving title through such mortgagee or chargee or Receiver. This condition shall cease to apply (and shall be of no further effect) to any dwelling in respect of which the full equity has been acquired by the individual owner/purchaser and/or their mortgagee OR any dwelling in respect of which a statutory or voluntary right to buy or acquire has been exercised.

Reason

For the avoidance of doubt as to what is approved and in accordance with Policies IPC1 and SC1.3 of Bolton's Core Strategy.

Reason for Pre-Commencement Condition: It needs to be confirmed prior to commencement that the development will be delivered as a 100% affordable housing scheme, as it is currently not subject to any planning obligations on the understanding that it will be delivered by affordable housing provider.

3. **Surface water drainage**

Prior to the commencement of any groundworks, full details of the proposed surface water drainage works shall be submitted to and approved in writing by the local planning authority. Before these details are submitted an assessment shall be carried out to investigate the potential for disposing of surface water by means of a sustainable drainage system in accordance with the principles set out in the National Planning Policy Framework, and the results of the assessment provided to the Local Planning Authority. Where a sustainable drainage system is to be provided, the submitted details shall:

- 1) Provide information about the design storm period and intensity, the method employed to delay and control the surface water discharged from the site and the measures taken to prevent pollution of the receiving groundwater and/or surface waters.
- 2) Include a timetable for its implementation, and
- 3) Provide a management and maintenance plan for the lifetime of the development which shall include the arrangements for adoption by any public authority or statutory undertaker and any other arrangements to secure the operation of the scheme throughout its lifetime.

The approved works shall be implemented in full prior to the occupation of the buildings hereby approved and those works as approved shall be retained thereafter.

Reason

To ensure the site provides satisfactory means of surface water drainage and to comply with policies CG1.5 and CG2.2 of Bolton's Core Strategy.

Reason for pre-commencement condition: The solution for surface water disposal must be understood prior to works commencing on site as it could affect how underground works are planned and carried out.

4. Intrusive site investigations (coal mining legacy)

No development (excluding any demolition or site clearance) shall commence unless and until:

- a) a scheme of intrusive site investigations has been carried out on site to establish the risks posed to the development by past coal mining activity, and;
- b) any remedial works and/or mitigation measures to address land instability arising from coal mining legacy, as may be necessary, have been implemented on site in full in order to ensure that the site is made safe and stable for the development proposed.

The intrusive investigations and remedial works shall be carried out in accordance with authoritative UK guidance.

Prior to first occupation of the development a signed statement or declaration prepared by a suitably competent person confirming that the site is, or has been made, safe and stable for the approved development shall be submitted to the Local Planning Authority for approval in writing. This document shall confirm the methods and findings of the intrusive site investigations and the completion of any remedial works and/or mitigation necessary to address the risks posed by past coal mining activity.

Reason

To ensure the safety and stability of the development, and to comply with Policy CG4.3 of Bolton's Core Strategy.

Reason for pre-commencement condition: Adequate information pertaining to ground conditions and coal mining legacy is required prior to the commencement of any development to enable appropriate remedial and mitigatory measures to be identified and carried out before any building works commence on site.

5. Construction management plan

Prior to the commencement of development, including demolition, ground works or vegetation clearance a Construction Management Plan (CMP) shall be submitted to and approved in writing by the local planning authority. The CMP shall include the following details:

- a) Hours of construction and deliveries;
- b) Details of the precautions to guard against the deposit of mud and substances on the public highway, to include washing facilities by which vehicles will have their wheels, chassis and bodywork effectively cleaned and washed free of mud and similar substances prior to entering the highway;
- c) Dust suppression measures;
- d) Noise emission suppression measures;
- e) Construction routes in and around the site;
- f) Compound locations together with details of the storage facilities for any plant and materials including off-site consolidation if appropriate, the siting of any site huts and other temporary structures, including site hoardings and details of the proposed security arrangements for the site;
- g) Parking of vehicles associated with construction, deliveries, site personnel, operatives and visitors;
- h) Sheeting over of construction vehicles.

Development of each phase or plot shall be carried out in accordance with the approved CMP.

Reason

To ensure that adequate consideration is given to the need to minimise the impact on the road network and reduce pollution, in accordance with Policies P5 and CG4 of Bolton's Core Strategy.

Reason for Pre-Commencement Condition: The site is close to residential properties and therefore considered to be sensitive to potential disturbances during the construction process and these need

to be kept to a minimum to minimise any impact on the sensitive neighbouring uses.

6. **Tree protection fencing**

No demolition, development or stripping of soil shall be started until:

1. The trees and shrubs within or overhanging the site which are to be retained and/or are subject of a Tree Preservation Order (TPO) have been surrounded by fences of a type and in the locations shown on approved plan MY788/GCB/03 Rev G; "Tree Protection Plan"; dated July 2021 (within the approved Arboricultural Report) prior to such works commencing.
2. The approved fencing shall remain in the agreed location (in accordance with BS 5837:2012) until the development is completed or unless otherwise agreed in writing with the Local Planning Authority and there shall be no work, including the storage of materials, or placing of site cabins, within the fenced area(s).

Reason

To protect the health and appearance of the tree(s) and in order to comply with Bolton's Core Strategy policies CG1 and CG3

Reason for Pre-Commencement Condition: Tree protection measures must be implemented prior to the commencement of works to protect the health and appearance of the tree(s) during the construction phase.

7. **CEMP: Biodiversity**

Prior to the commencement of development, including demolition, ground works or vegetation clearance a Construction Environmental Management Plan for Biodiversity (CEMP: biodiversity) shall be submitted to and approved in writing by the local planning authority. The CEMP: biodiversity shall include the following details:

- a) Risk assessment of potentially damaging construction activities.
- b) Identification of "biodiversity protection zones".
- c) Practical measures (both physical measures and sensitive working practices) to avoid or reduce impacts during construction (may be provided as a set of method statements).
- d) The location and timing of sensitive works to avoid harm to biodiversity features.
- e) The times during construction when specialist ecologists need to be present on site to oversee works.
- f) Responsible persons and lines of communication.
- g) The role and responsibilities on site of an ecological clerk of works (ECoW) or similarly competent person.
- h) Use of protective fences, exclusion barriers and warning signs.

Development of each phase or plot shall be carried out in accordance with the approved CEMP: biodiversity

Reason

To ensure that biodiversity is safeguarded during the construction phase and to comply with policy CG1.2 of Bolton's Core Strategy.

Reason for Pre-Commencement Condition: The site contains biodiversity interest and therefore considered to be sensitive to potential disturbances during the construction process.

8. **Invasive species**

Development shall not commence until a scheme for the eradication of the invasive species on the application site (Montbrecia, Rhododendron, Japanese Rose, Wall Cotoneaster, Variegated Yellow Archangel and Three-cornered Leek) has been submitted to and approved in writing by the Local Planning Authority. This shall include a timetable for implementation. Should there be a delay of more than one year between the approval of the scheme and its implementation or the commencement of development then a new site survey and, if necessary, further remedial measures shall be submitted for the further approval of the Local Planning Authority. The scheme shall be carried out as approved and retained thereafter.

Reason

To ensure the safe development of the site and eradication of an invasive species and to comply with policy CG1 of Bolton's Core Strategy.

Reason for pre-commencement condition: The scheme for the eradication of invasive species must be understood prior to works commencing on site as it could affect how works are planned and carried out.

9. Before development commences details of on-site cycle parking to be provided within the site shall be submitted to and approved in writing by the Local Planning Authority. The approved cycle parking shall be implemented in full before the development hereby approved is first occupied or brought into use and retained thereafter.

Reason

To encourage cycle use and provide adequate facilities for cyclists and in order to comply with Bolton's Core Strategy policies S1, P5 and Supplementary Planning Document "Accessibility, Transport and Road Safety".

10. **External materials**

Notwithstanding any description of materials in the application no above ground construction works shall take place until samples or full details of materials to be used externally on the buildings have been submitted to and approved in writing by the Local Planning Authority. Such details shall include the type, colour and texture of the materials. The approved materials shall be implemented in full thereafter.

Reason

To ensure the development reflects local distinctiveness and to comply with policy CG3 of Bolton's Core Strategy.

11. **Doors and Windows**

Prior to any doors or windows being installed, a detailed specification for all doors and windows shall be submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be implemented in full before the development is first occupied and retained thereafter.

Reason

To ensure the development reflects local distinctiveness in order to comply with policy CG3 of Bolton's Core Strategy.

12. **Roads and footways**

No dwelling shall be occupied until the access roads, footways and footpaths leading thereto have been constructed and completed in accordance with drawing ref. 04 P18; "Proposed Site Plan"; revision dated 13.10.21.

Reason

In the interests of highway safety and in order to comply with Bolton's Core Strategy policies S1, P5 and Supplementary Planning Document 'Accessibility, Transport and Road Safety'.

13. **Visibility splay at access**

Prior to the development hereby approved/permitted being first brought into use a visibility splay measuring 2.4 metres by 25.0 metres shall be provided at the junction of the access with Grizedale Close, and subsequently remain free of all obstructions between the height of 1.05 metres and 2 metres (as measured above carriageway level).

Reason

To ensure traffic leaving the site has adequate visibility onto the highway and in order to comply with Bolton's Core Strategy policy S1, P5 and Supplementary Planning Document 'Accessibility, Transport and Road Safety'.

14. **Highways works**

Prior to first occupation of development hereby approved full details of all works within the highway limit and on the proposed roads shall be submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be implemented in full prior to the development being first brought into use and retained thereafter.

Reason

In the interests of highway safety and in order to comply with Bolton's Core Strategy policies S1, P5 and Supplementary Planning Document "Accessibility, Transport and Road Safety".

15. **Vehicle areas**

Prior to the development hereby approved being first occupied or brought into use, a scheme detailing how parts of the site to be used by vehicles are to be laid out, constructed, surfaced, drained and lit shall be submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be implemented in full and thereafter made available for the use of vehicles at all times the development is in use.

Reason

To encourage drivers to make use of the parking and circulation area(s) provided and in order to comply with Core Strategy policies S1 and P5 and Supplementary Planning Document 'Accessibility, Transport and Road Safety'.

16. **Parking**

Before the approved/permitted development is first brought into use the parking as shown in the approved plan drawing ref. 04 P18; "Proposed Site Plan"; revision dated 13.10.21 shall be provided and shall be made available for the parking of cars at all times.

Reason

To ensure that adequate provision is made for vehicles to be left clear of the highway and in order to comply with Bolton's Core Strategy policies S1, P5 and Appendix 3.

17. **Traffic Regulation Orders**

Prior to first occupation of development hereby approved/permitted, all developer obligations shall be fulfilled to enable the Local Highway Authority to carry out a review of Traffic Regulation Orders in the locality where necessary and to enable the Local Highway Authority to consult, advertise, promote and implement Traffic Regulation Orders to mitigate the likely impact of the development by way of waiting restrictions at the site access points from Grizedale Close and turning head provision within the site, and to implement a 20 mph speed limit within the site.

Reason

In the interests of highway safety and in order to comply with Bolton's Core Strategy policies S1, P5 and Supplementary Planning Document "Accessibility, Transport and Road Safety".

18. **Electronic vehicle charging points**

Details of the electric vehicle charging points to be provided within the development shall be submitted and approved in writing by the Local Planning Authority prior to first occupation. The electric vehicle charging points shall be provided in accordance with the approved scheme prior to occupation and shall be maintained for the life of the approved development.

Reason

To safeguard the amenity of the occupiers of the proposed development in respect of atmospheric pollution, and to incorporate high standards of sustainable design within the development, in compliance with policies CG2 and CG4 of Bolton's Core Strategy.

19. **Arboricultural method statement for gas and electricity services**

Prior to the installation of any gas and electricity services, an arboricultural method statement detailing how these elements of the development will be constructed without causing harm or

damage to the trees to be retained on the site shall be submitted to and approved in writing by the Local Planning Authority. The approved measures shall then be implemented fully in accordance with the approved details.

Reason

To ensure the safe development of the site and favourable retention of trees and in order to comply with Bolton's Core Strategy policies CG1 and CG3.

20. **Bat and bird boxes**

The 4no. bat boxes and 1 no. bird box, as shown within approved drawing 6490.01 Rev D; "Landscape Proposal"; revision dated 21.10.21, shall be erected/installed in full accordance with the drawing prior to first occupation of the apartment building and the dwelling approved at plot 3. The boxes shall be retained in their approved locations thereafter.

Reason

To safeguard and enhance biodiversity, and to comply with policy CG1.2 of Bolton's Core Strategy.

21. **External lighting scheme**

Before the development hereby approved is first occupied a scheme shall be submitted to and approved in writing with the Local Planning Authority for all external lighting. The lighting shall be designed to an illumination value of 5 lux at the nearest residential property. The beam angle of any lights directed towards any potential observer should be kept below 70 degrees. Spill shields should also be fitted. The scheme shall also include a "lighting design strategy for biodiversity" which shall:

- i) identify those areas/features on site that are particularly sensitive for bats and that are likely to cause disturbance in or around their breeding sites and resting places or along important routes used to access key areas of their territory, for example, for foraging; and
- ii) show how and where external lighting will be installed (through the provision of appropriate lighting contour plan and technical specifications) so that it can be clearly demonstrated that areas to be lit will not disturb or prevent the bats using their territory or having access to their breeding sites and resting places.

The approved scheme shall be implemented in full before the development is first brought into use and retained thereafter.

Reason

To safeguard the character and appearance of the locality, to prevent light pollution, and to safeguard bats, and in order to comply with Bolton's Core Strategy policies CG1.2, CG3 and CG4.

22. **Landscaping scheme**

Trees, shrubs and hedgerows shall be planted on the site in accordance with the approved landscape scheme [drawing reference: 6490.01 Rev D; "Landscape Proposal"; revision dated 21.10.21] prior to the development being first brought into use. The approved scheme shall be implemented in full and carried out within 6 months of the occupation of any of the buildings or the completion of the development, whichever is the sooner, or in accordance with phasing details included as part of the scheme and subsequently approved by the Local Planning Authority. Any trees and shrubs that die or are removed within five years of planting shall be replaced in the next available planting season with others of similar size and species.

Reason

To reflect and soften the setting of the development within the landscape and in order to comply with Core Strategy policies CG1 and CG3.

23. **Boundary treatment**

Prior to the development being first occupied the approved boundary treatments, as shown on drawings 04 P18; "Proposed Site Plan"; revision dated 13.10.21 and 14 P1; "Boundary Treatment Details"; dated 08.07.21, shall be implemented in full, unless otherwise agreed in writing with the Local Planning Authority, and retained thereafter.

Reason

To ensure adequate standards of privacy and amenity are obtained and the development reflects the landscape and townscape character of the area and in order to comply with policies CG3 and CG4 of Bolton's Core Strategy.

24. **Contaminated land**

The development hereby approved shall be carried out in full accordance with the approved Combined Preliminary Risk Assessment; Geo-Environmental Investigation; Risk Assessment and Remediation Strategy- ref. CL-602-LKC 20 1186-01; dated 21 September 2020; by LK Consult, subject to the following:

- I The remediation strategy for this proposed development is carried out in its entirety, in accordance with the recommendations of this document and to the satisfaction of the Local Planning Authority.
- i) The watching brief for specified aspects of this development is carried out in its entirety, in accordance with the recommendations of this document and to the satisfaction of the Local Planning Authority.
- ii) Any additional or unforeseen contamination encountered during development shall be notified to the Local Planning Authority as soon as practicably possible and a remedial scheme to deal with this approved by the Local Planning Authority.

Upon completion of any approved remediation schemes, and prior to first occupation, a completion report demonstrating that the scheme is suitable for its intended end use shall be submitted to and approved in writing by the local planning authority.

Reason

To safeguard the amenity of the future occupants of the development and to comply with Core Strategy policy CG4.

25. **Arboricultural method statement**

The development hereby approved shall be carried out in full accordance with the method statement contained within "Arboricultural Report (BS 5837:2012)"; ref. PM/FULL/23/09/2021 by Murray Tree Consultancy; dated September 2021. An Arboricultural Supervisor shall be employed to supervise the works.

Reason

To ensure the safe development of the site and favourable retention of trees and in order to comply with Bolton's Core Strategy policies CG1 and CG3.

26. **Geosystem/Geotextile construction**

The car parking area to the north of the apartment building and the eastern car parking space for the dwelling at Plot 1 shall be constructed in accordance with the details as shown in drawing ELL/20078/WH/L/172; "Proposed Geotextile Area"; dated 04/10/2021, to avoid causing harm or damage to the trees proposed to be retained within the site.

Reason

To ensure the favourable retention of trees and in order to comply with Bolton's Core Strategy policies CG1 and CG3.

27. **Woodland management**

The area of protected woodland to the west of the site (protected under Tree Preservation Order Bolton (New Church Road) 1994 and as identified within the approved Management Plan) shall be managed for at least 10 years following completion of the development, in accordance with the approved woodland management plan: "Management Plan (Woodland)"; ref. MG/6490/WMP/AUG21; by TBA Landscape Architects; dated August 2021.

Monitoring reports, as to how the management of the woodland has been carried out in accordance

with the approved management plan, shall be submitted to the Local Planning Authority on an annual basis for the 10 years following completion of the development.

Reason

To safeguard and enhance biodiversity within the development site and to comply with policy CG1.2 of Bolton's Core Strategy.

28. **Approved plans**

The development hereby permitted shall be carried out in complete accordance with the following approved plans:

01 P1; "Site Location Plan"; dated 08.12.19

04 P18; "Proposed Site Plan"; revision dated 13.10.21

06 P4; "Proposed Ground, First and Second Floor Plan - Apartments"; revision dated 20.02.21

07 P4; "Proposed Third Floor and Roof Plan - Apartments"; revision dated 08.07.21

08 P4; "Proposed Elevations - Apartments Option B"; revision dated 22.02.21

10 P6; "Proposed Plans and Elevations - Plots 1 and 2"; revision dated 26.03.21

11 P3; "Proposed Plans and Elevations - Plots 3-5"; revision dated 25.05.20

12 P7; "Proposed Plans and Elevations - Plots 6-8"; revision dated 07.04.21

13 P1; "Proposed Roof Plans - Houses"; dated 08.07.21

14 P1; "Boundary Treatment Details"; dated 08.07.21

17 P1; "Bin Store Details"; dated 08.07.21

6490.01 Rev D; "Landscape Proposal"; revision dated 21.10.21

ELL/20078/WH/L/140 Rev E; "External Works Layout"; revision dated 07.10.21

ELL/20078/WH/L/172; "Proposed Geotextile Area"; dated 04/10/21

Secura Retaining Wall System Specification Sheet; Verdant Solutions; received 30 Jul 2021

Reason

For the avoidance of doubt and in the interests of proper planning.

General Notes
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03: All dimensions should be verified on site before proceeding with the work.
04: TADW Architects shall be notified in writing of any discrepancies.
05: © TADW Limited (UK) 2015

Design Considerations

- Existing tree preservation area on site
- 100% parking provision to apartments (36 spaces)
- 100% parking provision to 2 bed houses, 200% parking provision to 3 bed houses
- Predominantly brick elevations
- Shared ProW from Grizedale Close retained
- Clear easement provided to existing stream
- Generally timber fencing to rear and party fencing in line with SBD.
- Communal bin store for apartments

Fire Safety Considerations (For Apartment Block)

- 4 storey building
- Top storey less than 11m from ground level
- Single common stair within apartment block
- Lobby between apartments and stairwell to be ventilated by 1.5m² free area vent or smoke shaft
- Max. 7.5m travel distance from apartment entrance to stairwell
- Assumed fire pump appliance access to within 45m of all points within each dwelling will be possible

SCHEDULE OF PROPERTIES

2 no. 2 Bed house @ 73.5m ²	
5 no. 3 Bed houses @ 85.2m ²	
1 no. 3 Bed house @ 88.6m ²	
28 no. 1 Bed Flats @ 45.4m ²	
7 no. 2 Bed Flats @ 57.5m ²	

TOTAL 43 no. dwellings

P18	Proposed balconies removed from south facing apartment units	13.10.21	KM	MH
P17	Ash trees T20 and T25 to become monoliths, coronet cut for habitat. Proposed new trees added to reflect TBA Landscape Proposal	06.08.21	KM	MH
P16	Site plan updated. Retaining walls updated. Boundary Type G added.	08.07.21	KM	MH
P15	Tree T8 to be retained. Tree T12 to be removed.	11.06.21	KM	MH
P14	Positions of trees T9, T56 and T57 clarified.	08.04.21	KM	MH
P13	Apartment, houses and road in plan updated. Response to Tree Officer's comments	26.03.21	KM	MH
P12	Site plan amended following Tree Officer's comments	12.03.21	KM	MH
P11	5.5m wide access road to be adopted with 2.0m wide pavements either side	02.03.21	KM	MH
P10	1 Nt 2 Bed apartment omitted to reduce apartment block massing	25.02.21	KM	MH
P9	Boundary treatments updated.	17.02.21	KM	MH
P8	Bollard to prevent vehicular access onto ProW. Tree T68 to be retained as existing. Plot 9 omitted.	12.02.21	KM	MH
Issue	Description	Date	Drawn	Checked

Drawing Status
P - Planning | T - Tender | C - Construction | R - As Record

For Approval

tadw architects

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Client **Watson Homes**

Job **Grizedale Close, Bolton**

Title **Proposed Site Plan**

Scale **1:250 @ A1**

Note - Prints from PDF files may not be to scale, check accuracy against scale

0 5 10 15 25m
1:250

Job Number 021292	Drawing Number 04	Issue P18
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05: © TADW Limited (UK) 2015

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- 100% parking provision to apartments (36 spaces)
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P12	Site plan amended following Tree Officer's comments	12.03.21	KM	MH
P11	5.5m wide access road to be adopted with 2.0m wide pavements either side	02.03.21	KM	MH
P10	1 Nt 2 Bed apartment omitted to reduce apartment block massing	25.02.21	KM	MH
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Issue	Description	Date	Drawn	Checked

Drawing Status
P - Planning | T - Tender | C - Construction | R - As Record

For Approval

tadw architects

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Client **Watson Homes**

Job **Grizedale Close, Bolton**

Title **Proposed Site Plan**

Scale **1:250 @ A1**

Note - Prints from PDF files may not be to scale, check accuracy against scale

0 5 10 15 25m
1:250

Job Number 021292	Drawing Number 04	Issue P18
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General Notes
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04: TADW Architects shall be notified in writing of any discrepancies.
05: © TADW Limited (UK) 2015

MATERIALS SCHEDULE

- Brickwork Type A - Colour Red (Farmstead Antique by Forterra)
- Brickwork Type B - Colour Red (Meadow Red Brick by Forterra)
- Single ply roofing membrane with standing seam effect feature
- Aluminium surround around main entrance, stair lobby and top floor patio doors
- Aluminium frame windows (Colour: Dark Grey RAL 7016)
- Aluminium main entrance door
- Metal frame balcony with glass balustrading


P4	1 Nr. 2 Bed unit omitted, floor to floor levels reduced, and roof design amended to reduce massing. Balconies changed to Juliet balconies on Plots 12 and 21	22.02.21	KM	KM
P3	Entrance area revised, stair added to section	8.06.20	MH	KM
P2	Materials updated	25.05.20	KM	MH
P1	Issued for comments	10.05.20	KM	MH
Issue	Description	Date	Drawn	Checked

Drawing Status
P - Planning | T - Tender | C - Construction | R - As Record

For Approval

tadw

architects



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SK St. Petersgate | Stockport | Cheshire | SK1 1HD

Client

Watson Homes

Job

Grizedale Close, Bolton

Title

Proposed Elevations - Apartments OPTION B

Scale

1:100 @ A1

Note - Prints from PDF files may not be to scale, check accuracy against scale

5

10m

1:100

Job Number

021292

Drawing Number

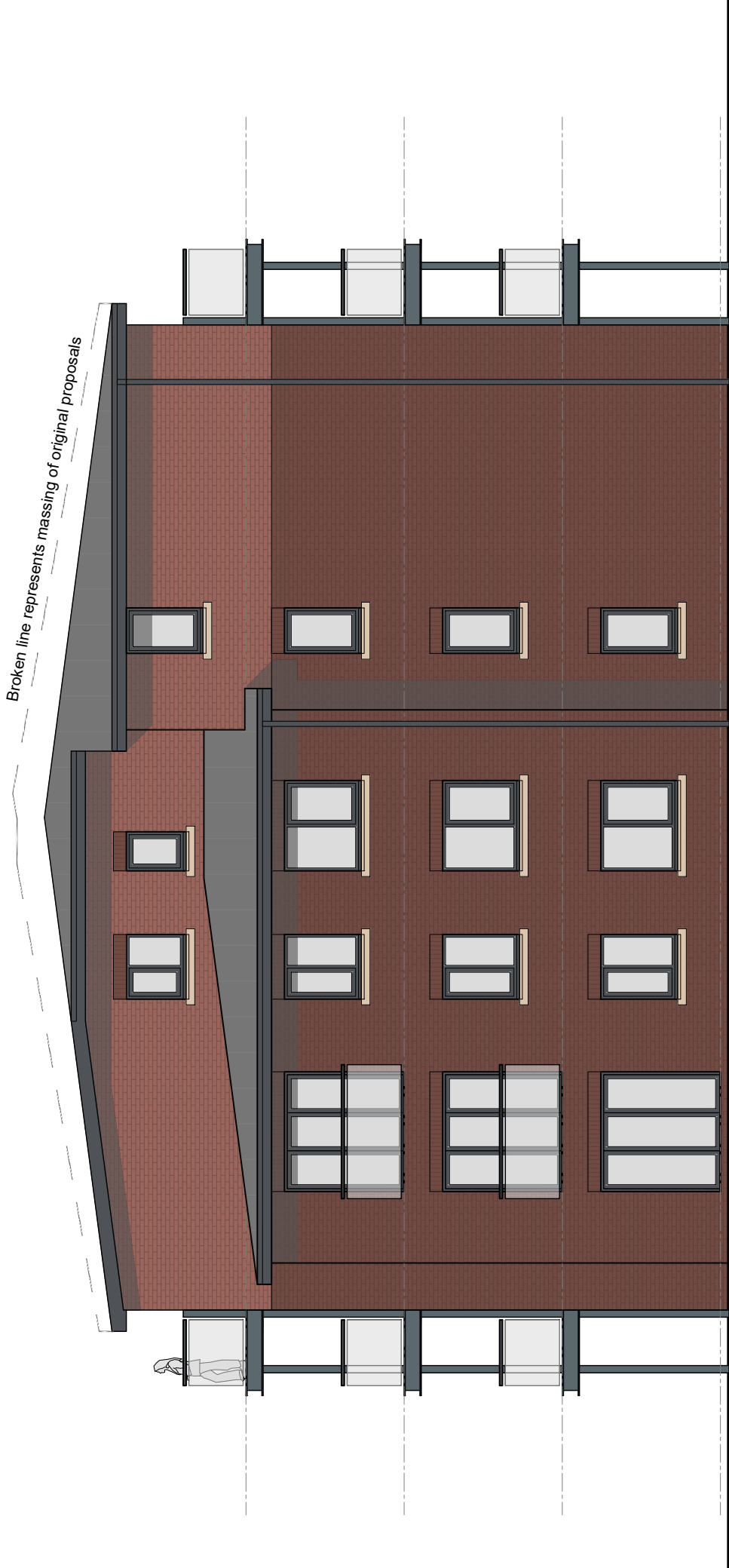
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Issue

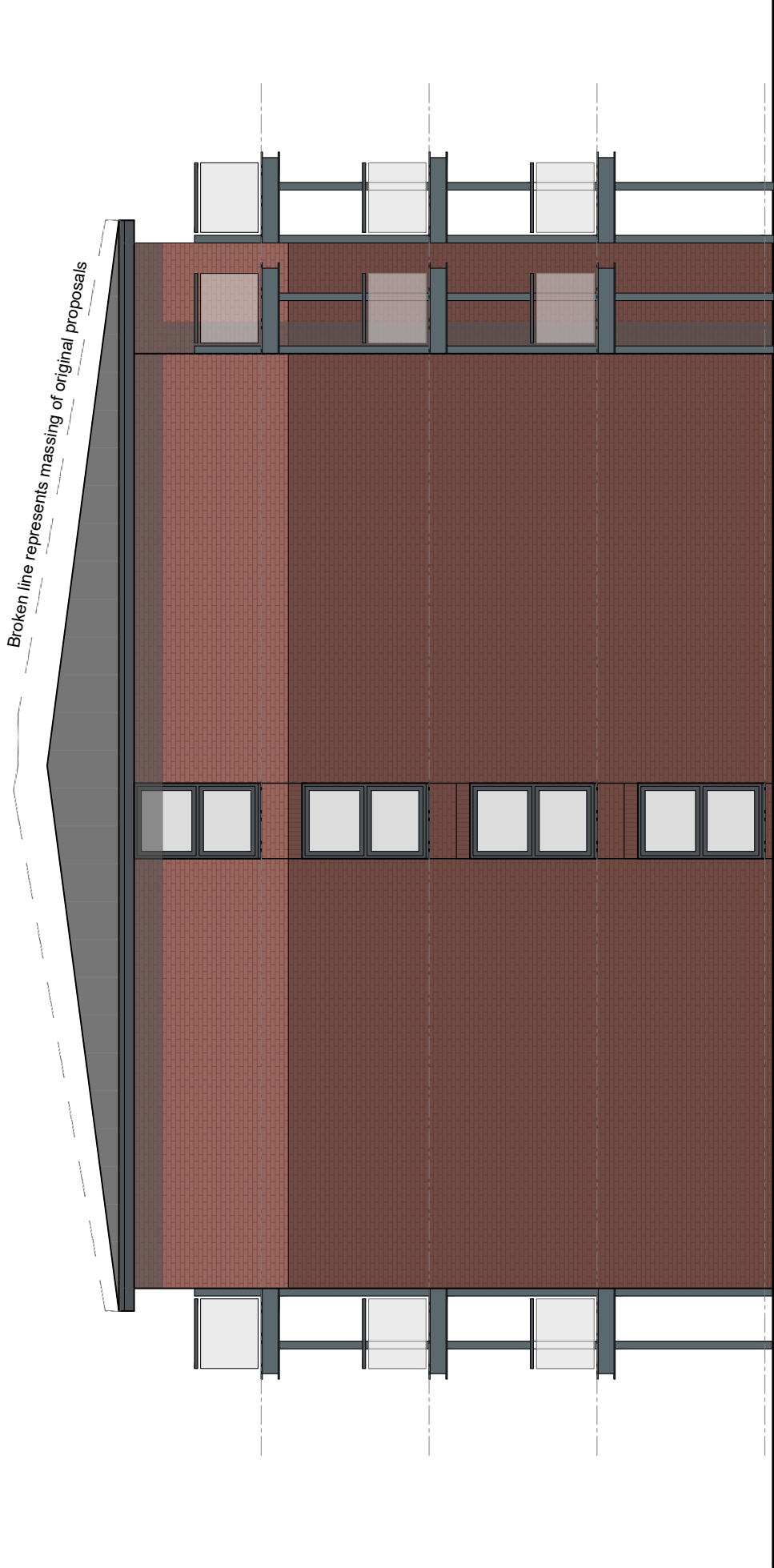
P4



Proposed Front (East) Elevation
01 08
Scale 1:100 @ A1



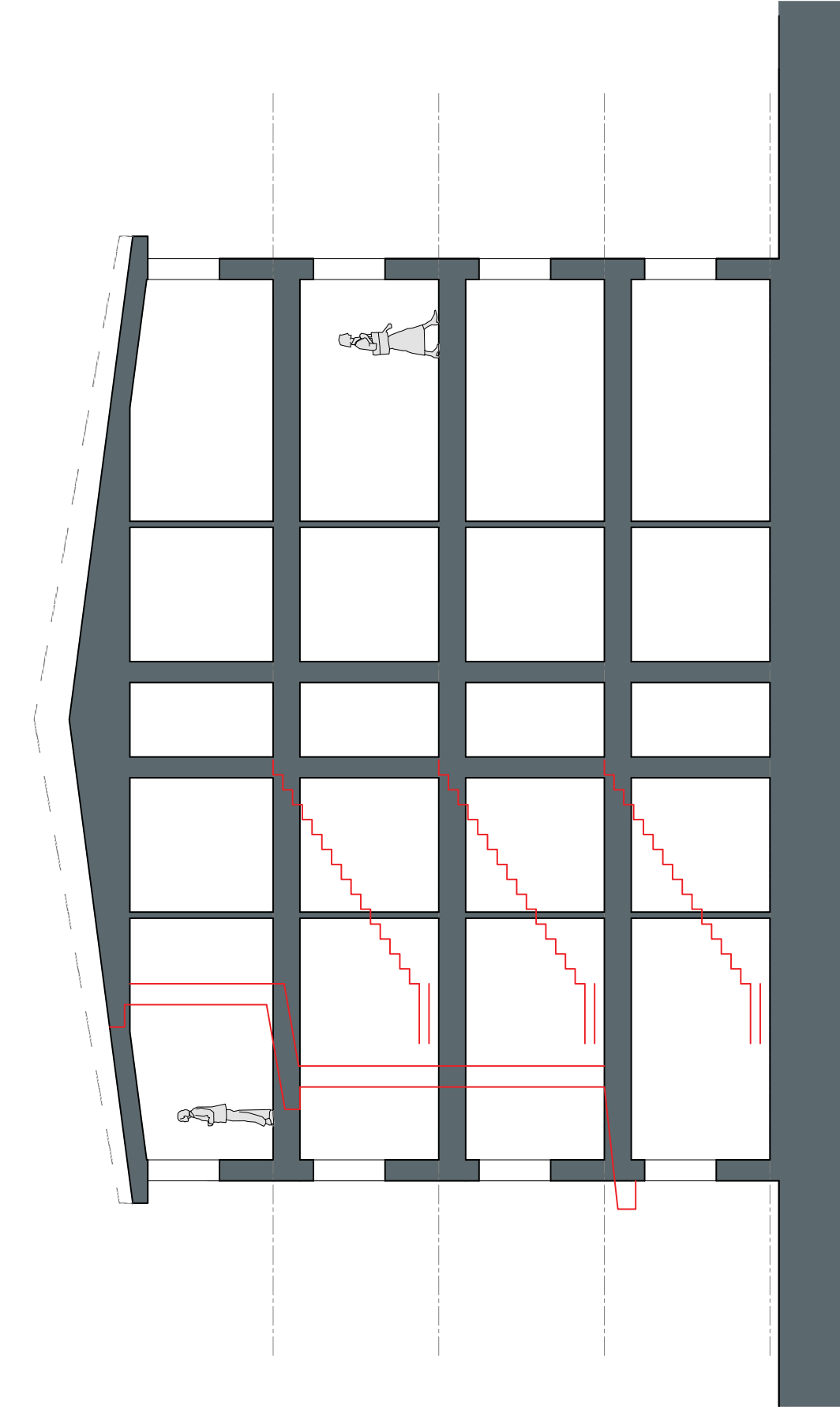
Proposed Side (South) Elevation
02 08
Scale 1:100 @ A1



Proposed Side (North) Elevation
03 08
Scale 1:100 @ A1



Proposed Rear (West) Elevation
04 08
Scale 1:100 @ A1



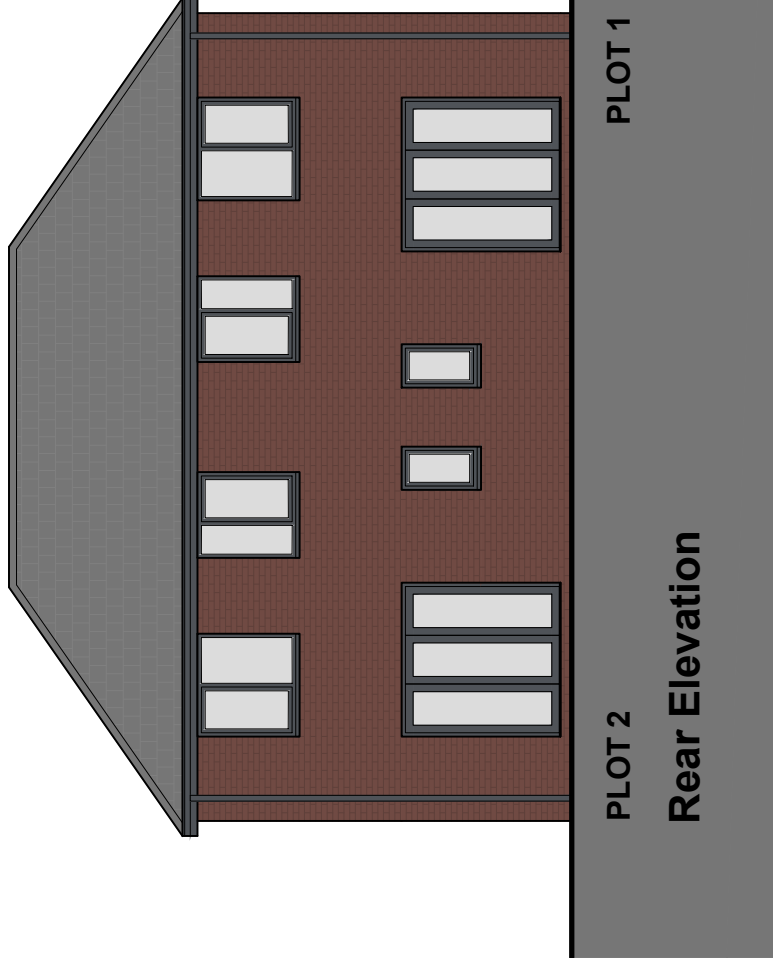
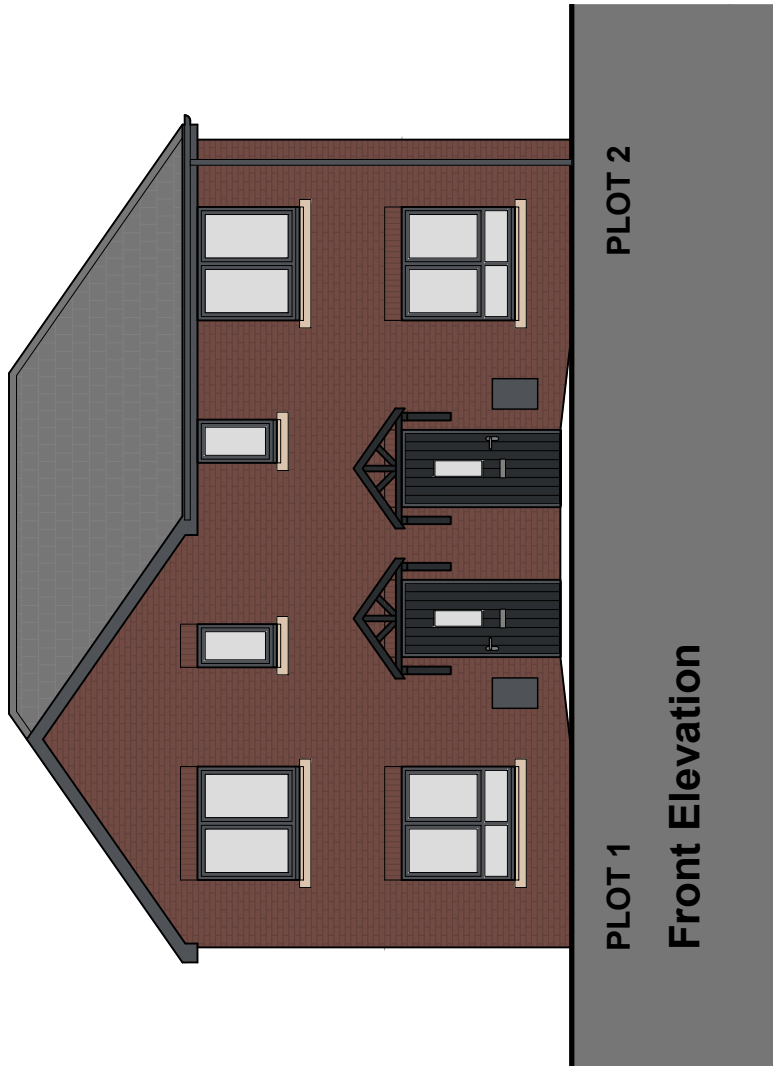
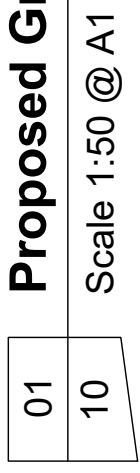
Proposed Section
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Scale 1:100 @ A1



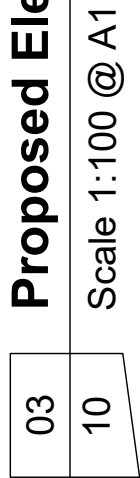
PLOT 1

PLOT 2

Proposed Ground Floor Plan (Plots 1 and 2)



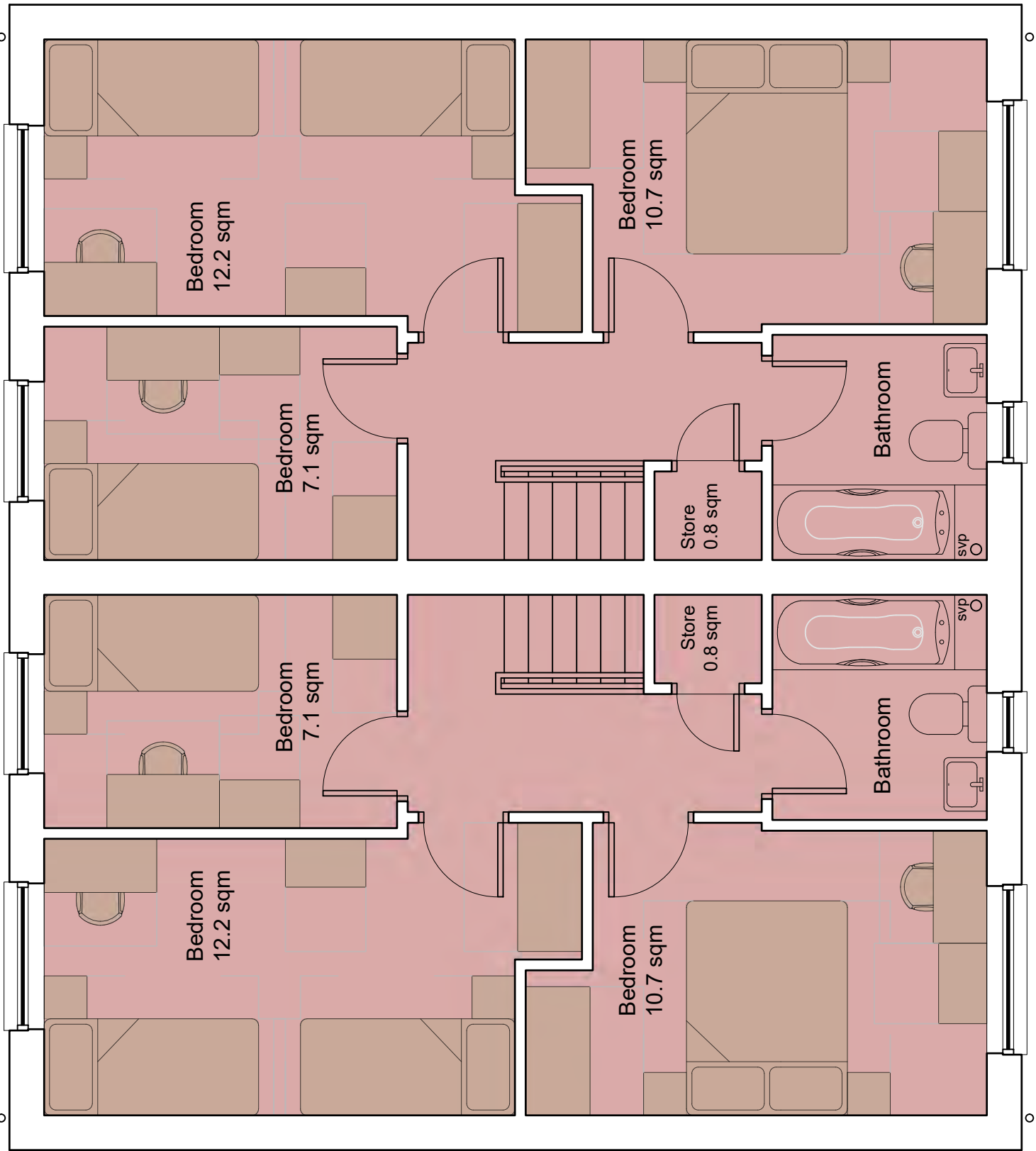
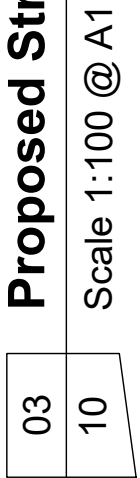
Proposed Elevations (Plots 1 and 2)



Represents outline of four storey apartment block behind



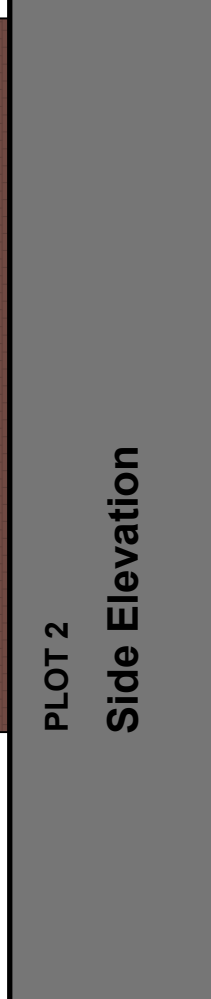
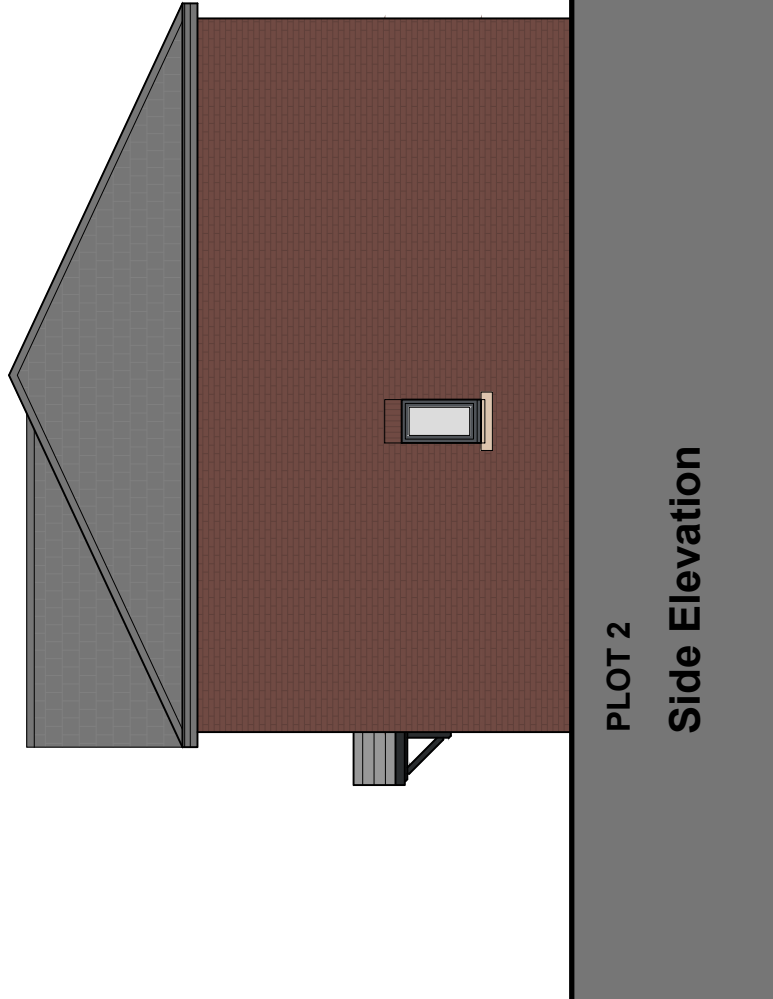
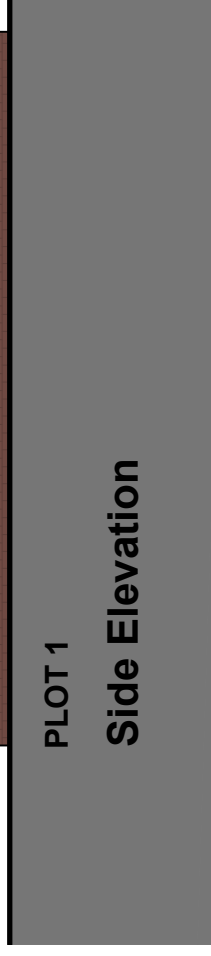
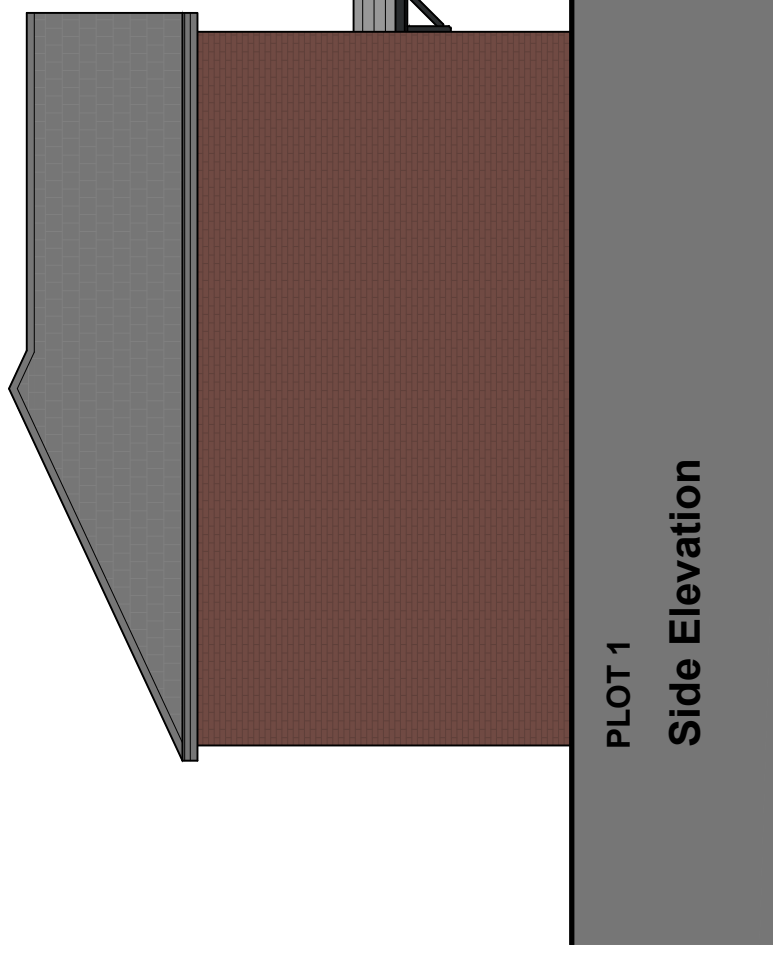
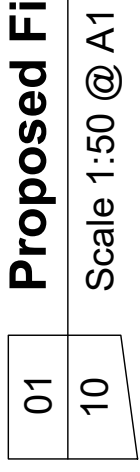
Proposed Street Scene (Plots 1 - 9)



PLOT 1

PLOT 2

Proposed First Floor Plan (Plots 1 and 2)



General Notes
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05: © TADW Limited (UK) 2015

MATERIALS SCHEDULE

Brickwork Type A - Colour Red (Farmstead Antique by Forterra)

Brickwork Type B - Colour Red (Meadow Red Brick by Forterra)

GRP entrance canopy

Roof Finish - Concrete roof tiles (Colour: Dark Grey)

UPVC Windows / bifolding patio doors - Grey (RAL colour 7016)

GRP composite main entrance doors to Secured By Design standard.

Airstone cills

Brick soldier courses

uPVC rainwater goods - Colour Black

All windows and doors to be set back from main face of brickwork to provide 85mm reveals

P6	Apartments, houses and road repositioned & levels amended in response to Tree Officer's comments	26.03.21	KM	MH
P5	Street elevation updated. Plot 9 omitted. Plot 8 house amended.	20.02.21	KM	MH
P4	Updated following Planner's pre-app comments.	15.09.20	KM	MH
P3	Materials updated. Bi-folding doors added	25.05.20	KM	MH
P2	Revised	21.05.20	KM	MH
P1	Issued for comments	10.05.20	KM	MH
Issue	Description	Date	Drawn	Checked

Drawing Status
P - Planning | T - Tender | C - Construction | R - As Record

For Approval

tadw architects



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Client Watson Homes

Job Grizedale Close, Bolton

Title Proposed Plans and Elevations - Plots 1 and 2

Scale 1:50 / 1:100 @ A1

Note - Prints from PDF files may not be to scale, check accuracy against scale



Job Number 021292	Drawing Number 10	Issue P6
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General Notes
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05: © TADW Limited (UK) 2015

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GRP entrance canopy

Roof Finish - Concrete roof tiles (Colour: Dark Grey)

UPVC Windows / bifolding patio doors - Grey (RAL colour 7016)

GRP composite main entrance doors to Secured By Design standard.

Airstone cills

Brick soldier courses

uPVC rainwater goods - Colour Black

All windows and doors to be set back from main face of brickwork to provide 85mm reveals

P3	Materials updated, Bi-folding doors added	25.05.20	KM	MH
P2	Revised	21.05.20	KM	MH
P1	Issued for comments	10.05.20	KM	MH
Issue	Description	Date	Drawn	Checked

Drawing Status
P - Planning | T - Tender | C - Construction | R - As Record

For Approval

tadw architects



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Client Watson Homes

Job Grizedale Close, Bolton

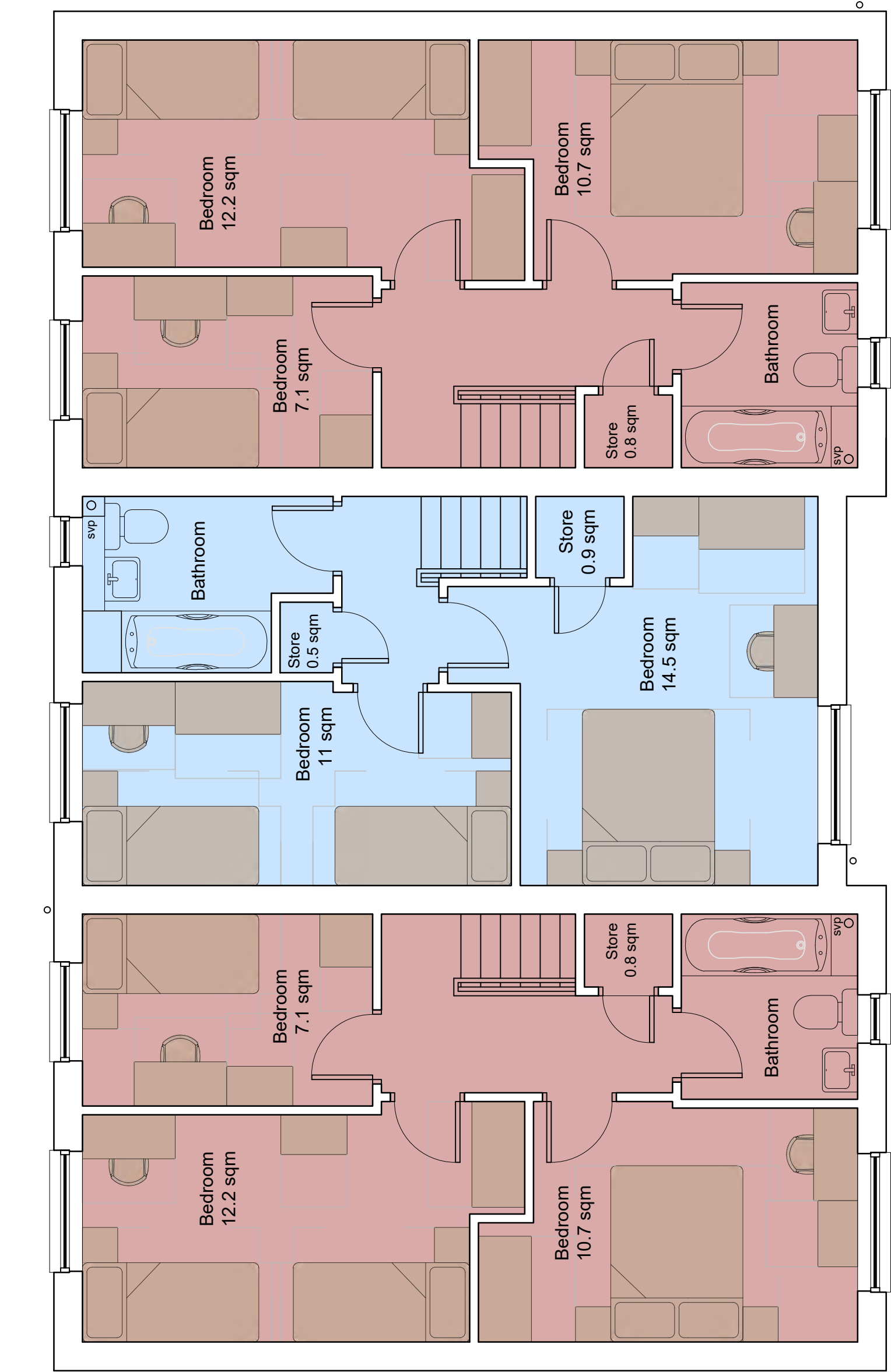
Title Proposed Plans and Elevations - Plots 3-5

Scale 1:50 / 1:100 @ A1

Note - Prints from PDF files may not be to scale, check accuracy against scale



Job Number	Drawing Number	Issue
021292	11	P3



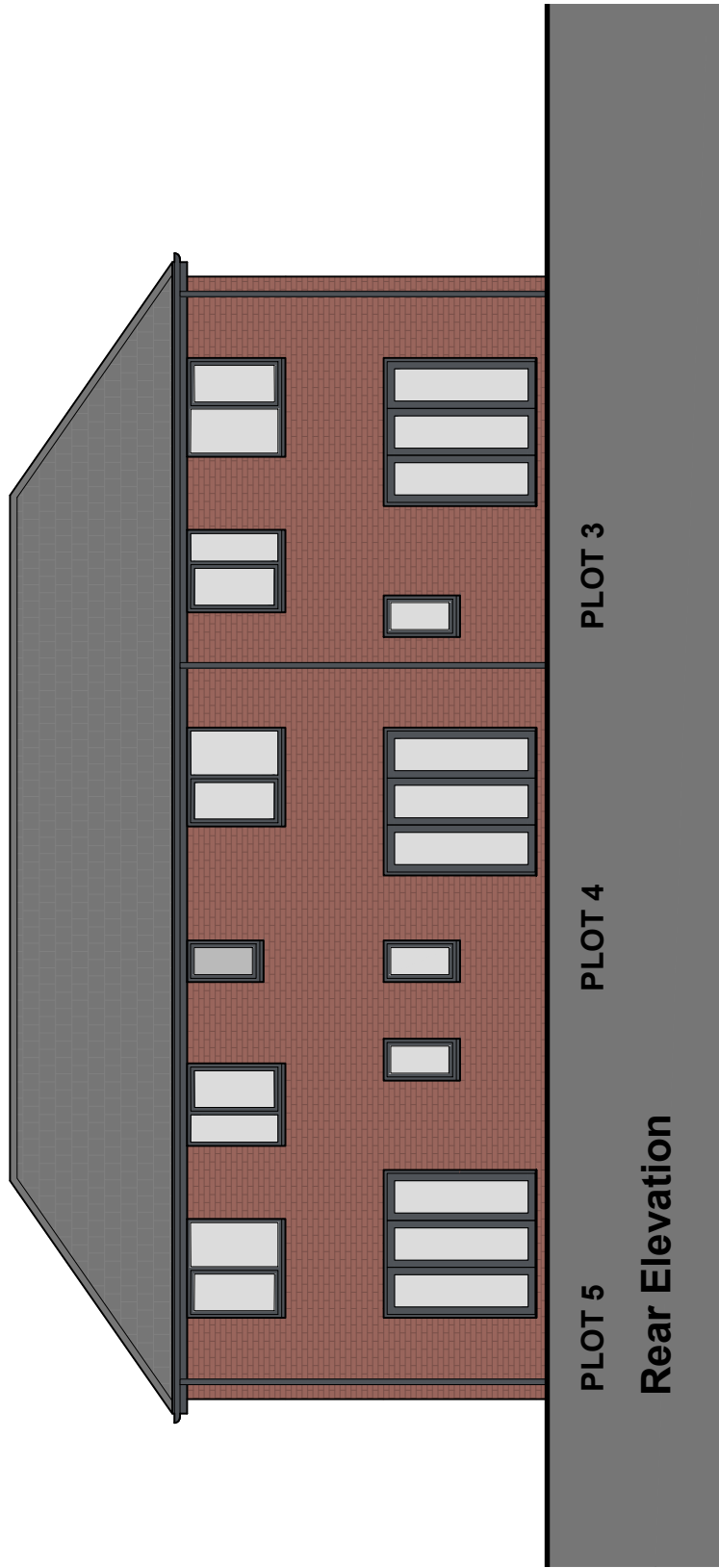
PLOT 3

PLOT 4

PLOT 5

Proposed First Floor Plan (Plots 3-5)

02 11 Scale 1:50 @ A1



PLOT 5
Rear Elevation

PLOT 4

PLOT 3



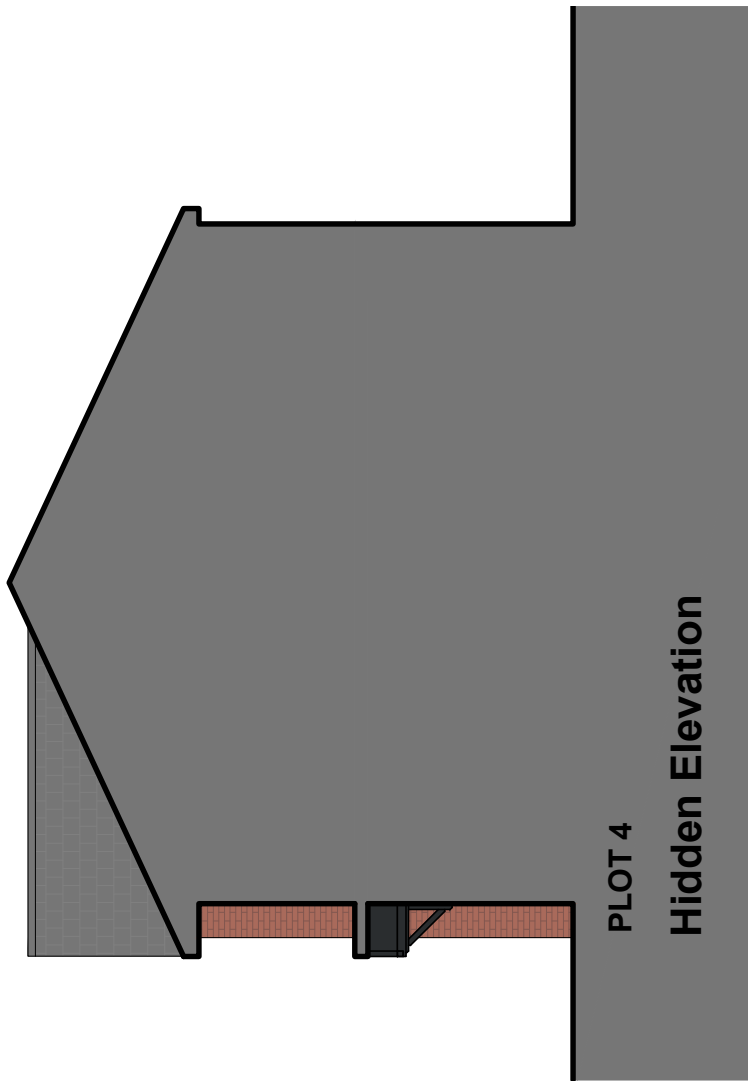
PLOT 3
Front Elevation

PLOT 4

PLOT 5

Proposed Elevations (Plots 3-5)

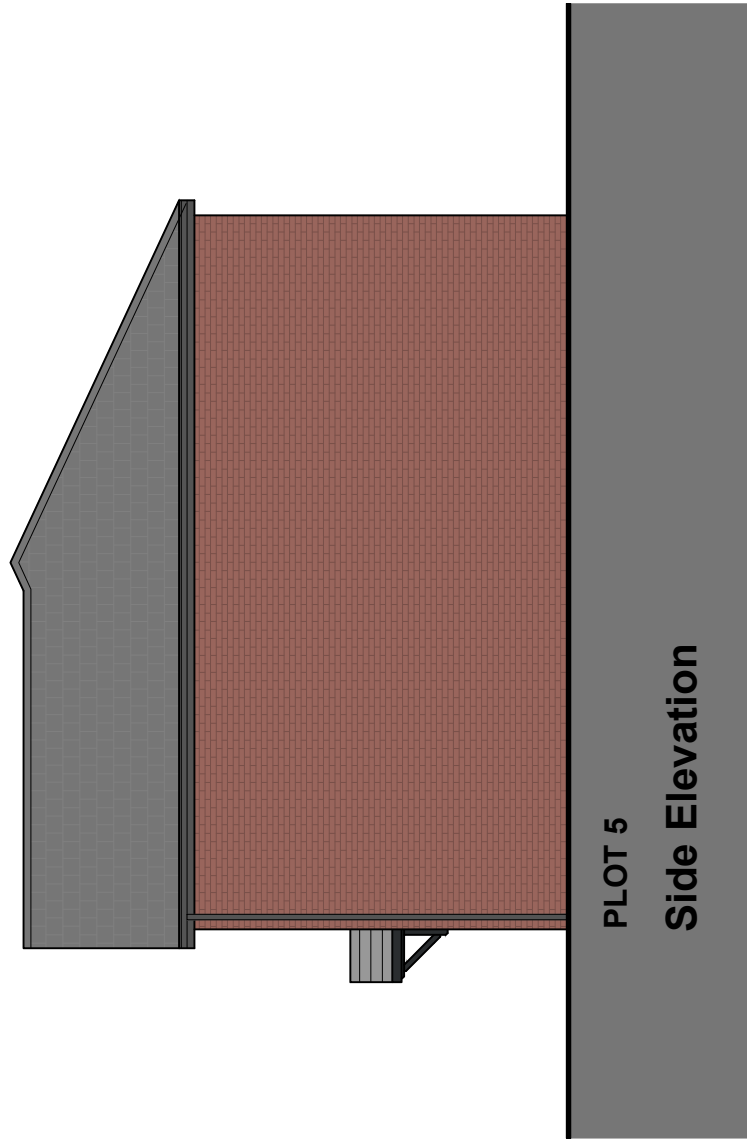
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PLOT 4
Hidden Elevation

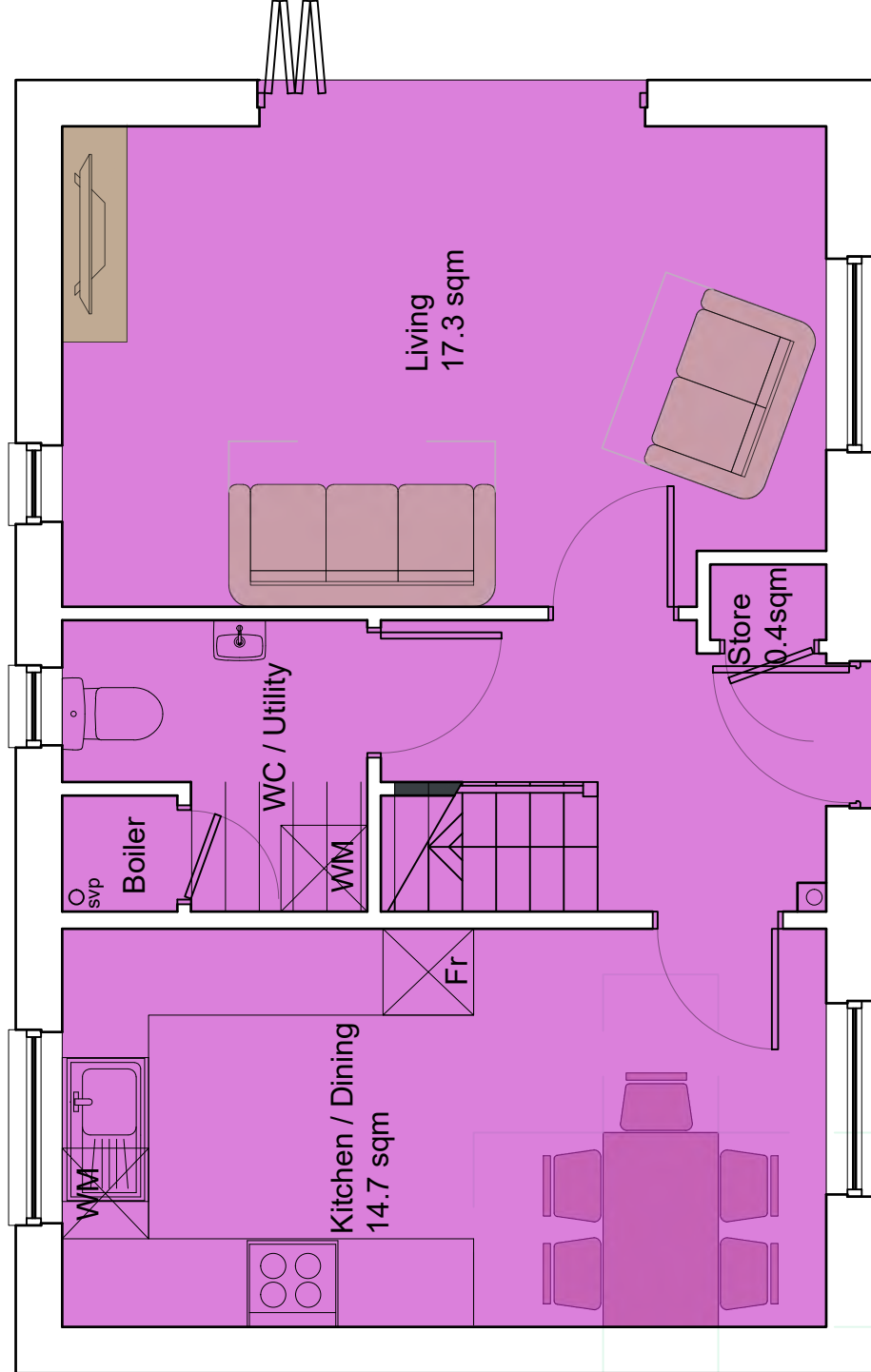
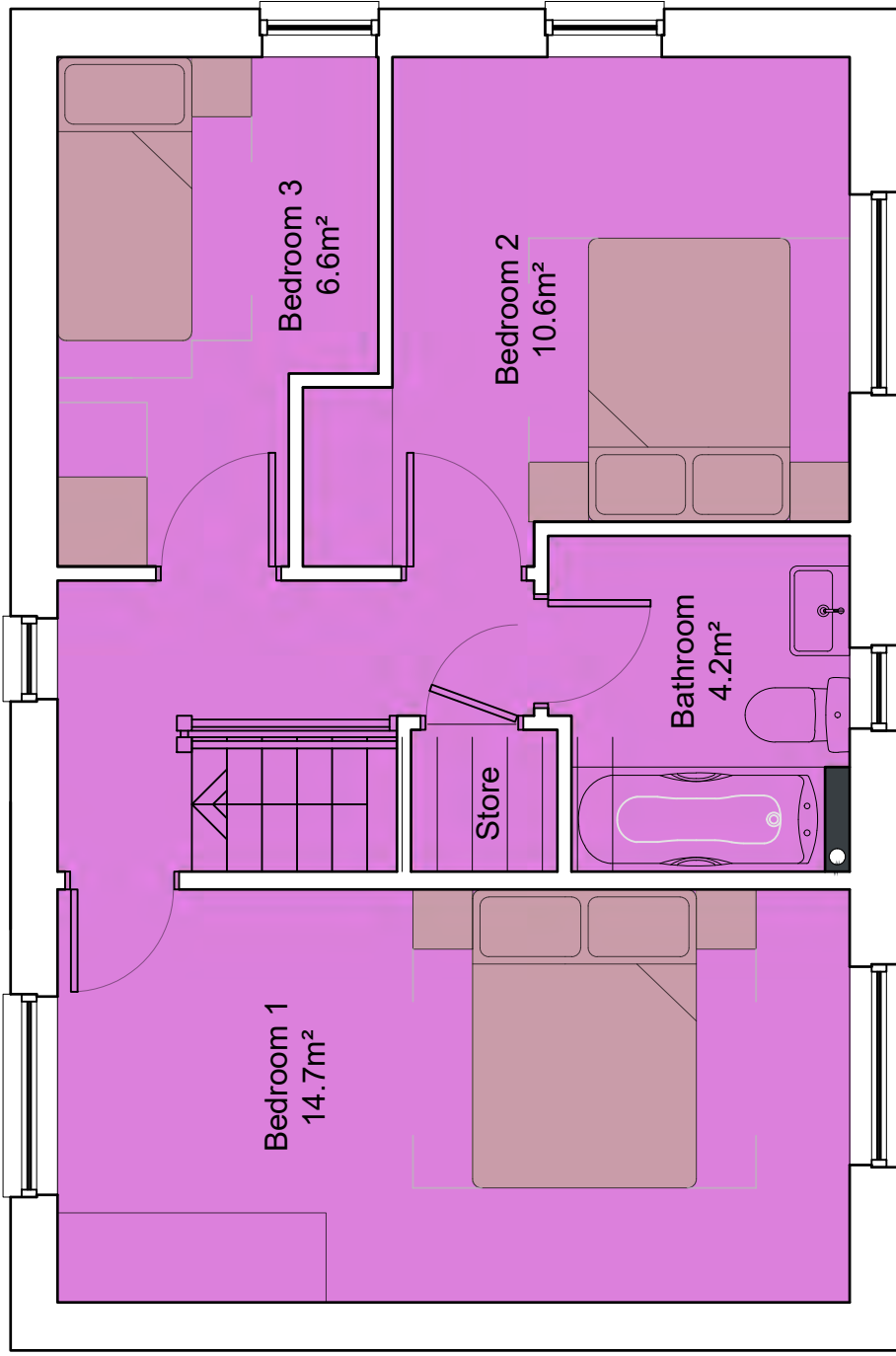
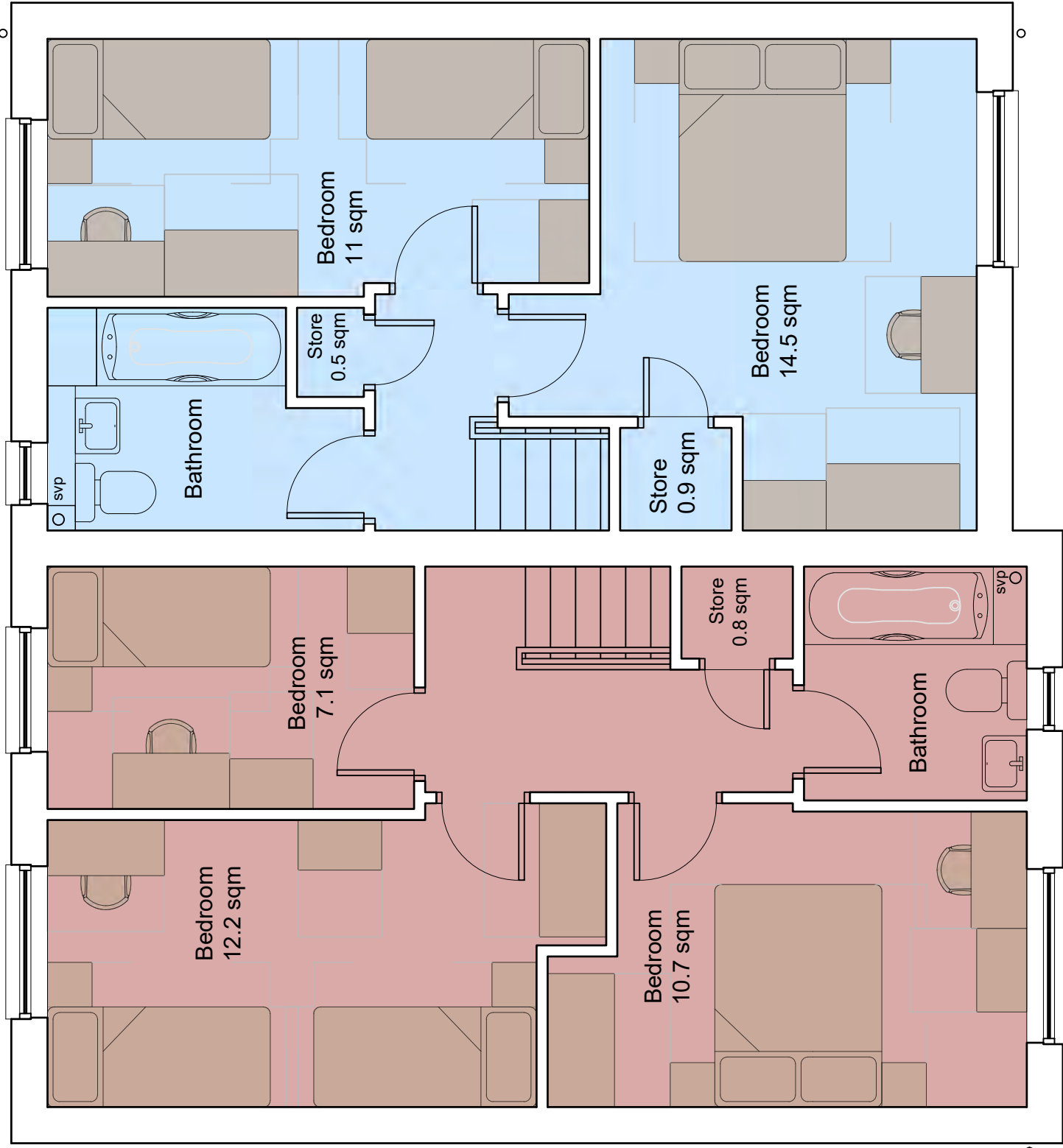
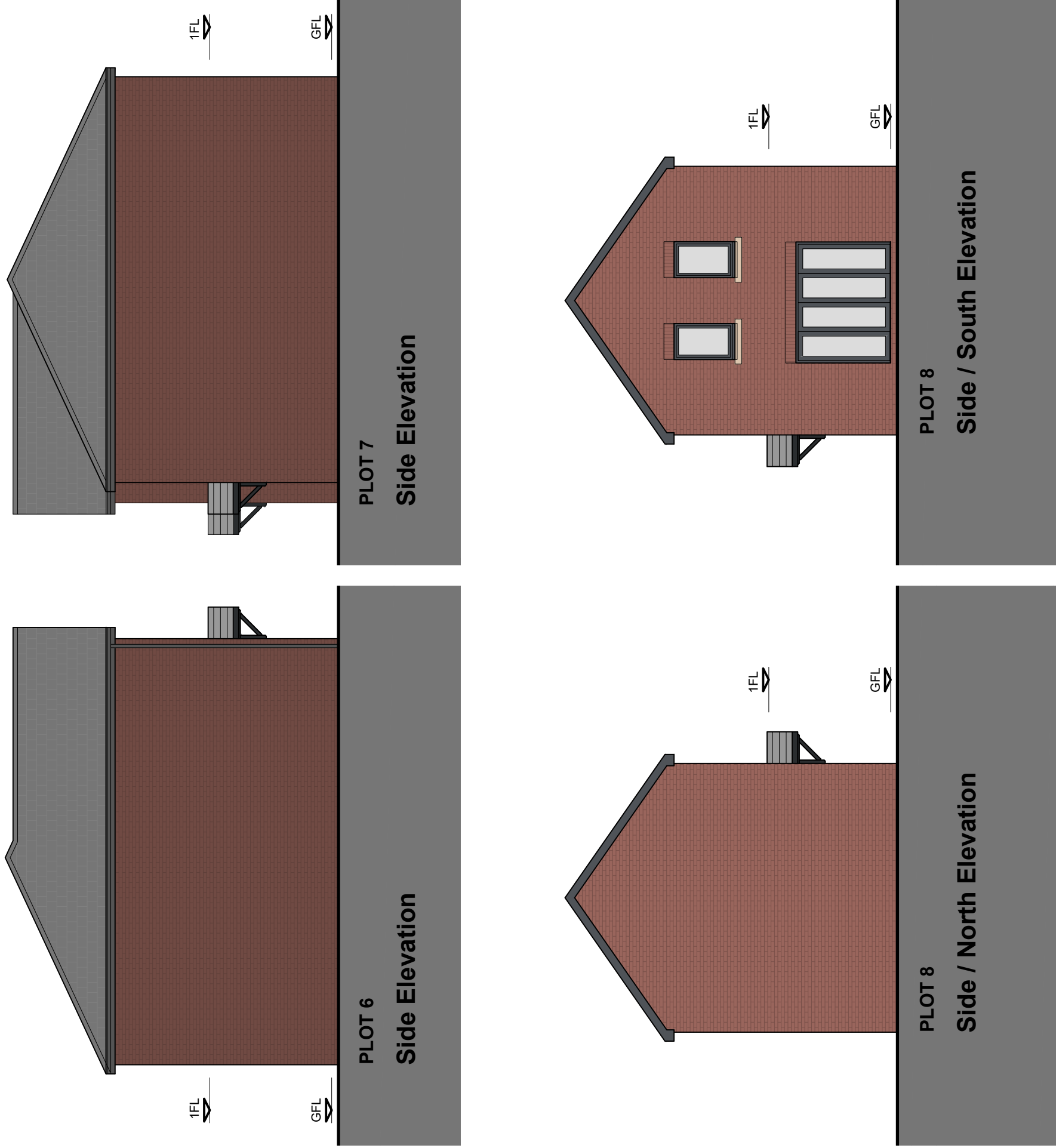


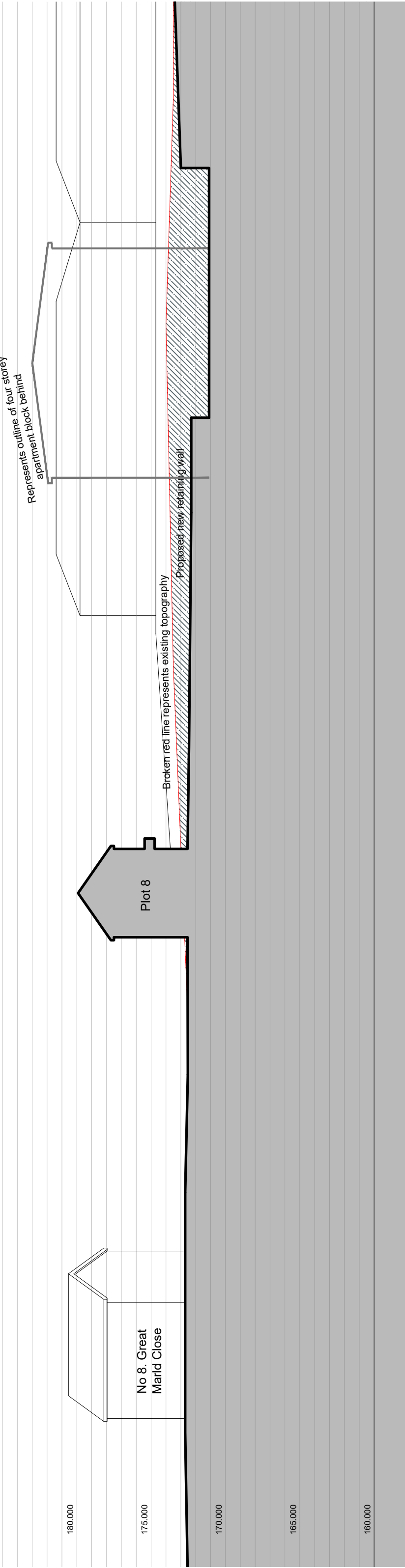
PLOT 3
Side Elevation



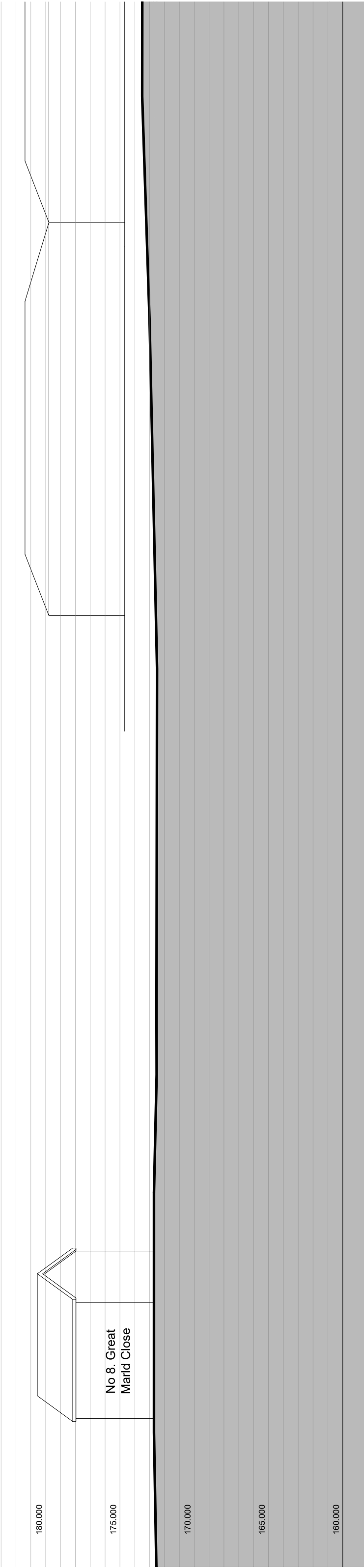
PLOT 5
Side Elevation

<div><div>General Notes</div><div>01 - All dimensions must not be scaled from this drawing. If in doubt, please ask.</div><div>02 - All dimensions are in millimetres unless noted otherwise.</div><div>03 - All dimensions should be verified on site before proceeding with the work.</div><div>04 - TADW Architects shall be notified in writing of any discrepancies.</div><div>05 - © TADW Limited (UK) 2015</div></div>				
<div><div>MATERIALS SCHEDULE</div><div><div>Brickwork Type A - Colour Red (Farnstead Antique by Forterra)</div><div>Brickwork Type B - Colour Red (Meadow Red Brick by Forterra)</div><div>GRP entrance canopy</div><div>Roof Finish - Concrete roof tiles (Colour: Dark Grey)</div><div>UPVC Windows / bifolding patio doors - Grey (RAL colour 7016)</div><div>GRP composite main entrance doors to Secured By Design standard.</div><div>Artstone cills</div><div>Brick soldier courses</div><div>uPVC rainwater goods - Colour Black</div><div>All windows and doors to be set back from main face of brickwork to provide 85mm reveals</div></div></div>				
P7	Proposed finished levels adjusted	07.04.21	KM	MH
P6	Plot 9 omitted. Plot 8 house design amended	17.02.21	KM	MH
P5	Amended following Planner's pre-application advice	15.09.20	KM	MH
P4	Plots 6-9 changed to semi-detached dwellings	25.05.20	KM	MH
P3	Materials updated. Bi-folding doors added	25.05.20	KM	MH
P2	Revised	21.05.20	KM	MH
P1	Issued for comments	10.05.20	KM	MH
Issue	Description	Date	Drawn	Checked
<div><div>Drawing Status</div><div>P - Planning T - Tender C - Construction R - As Record</div></div>				
<div><div>For Approval</div><div><div>tadw</div><div>architects</div><div></div></div></div>				
<div><div>Six St, Petersgate Stockport, Cheshire SK1 1HD</div><div>Ph 0161 477 6158 Fx 0161 480 8342 mail@tadw.co.uk www.tadw.co.uk</div></div>				
Client	Watson Homes			
Job	Grizedale Close, Bolton			
Title	Proposed Plans and Elevations - Plots 6-8			
Scale	1:50 / 1:100 @ A1			
<div><div>Note - Prints from PDF files may not be to scale, check accuracy against scale</div><div><div><div></div><div>1</div></div><div><div></div><div>2</div></div><div><div></div><div>3</div></div><div><div></div><div>5m</div></div><div><div></div><div>1:50</div></div></div></div>				
Job Number	021292	Drawing Number	12	Issue
				P7





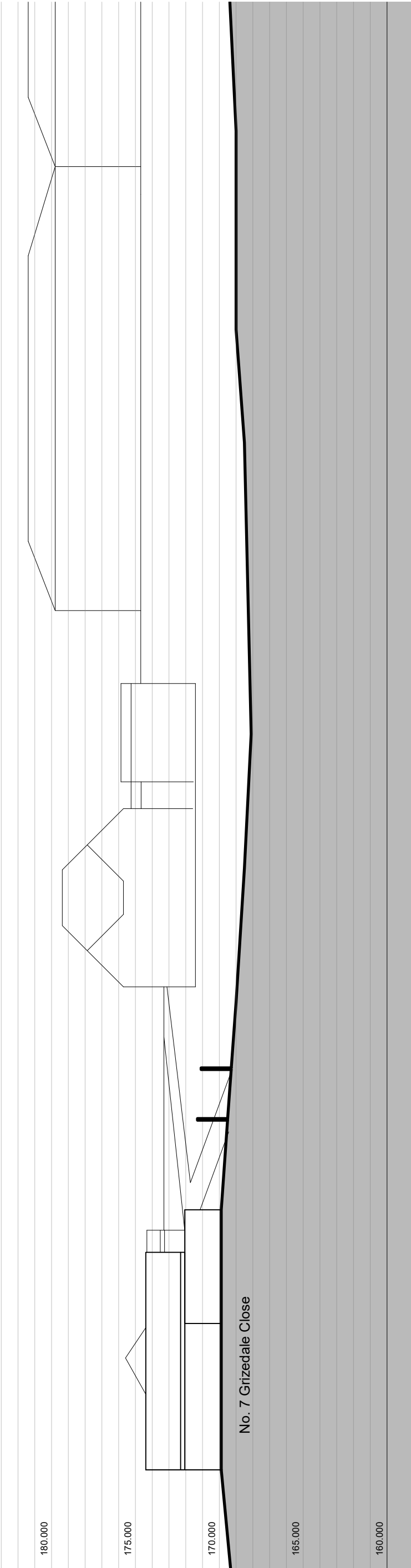
PROPOSED SITE SECTION C-C



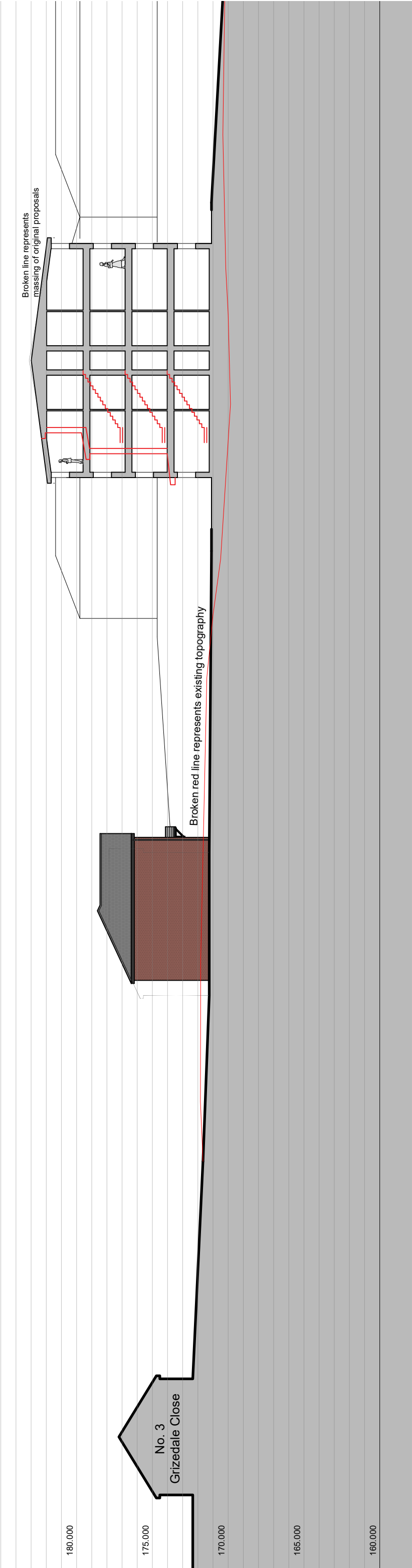
EXISTING SITE SECTION C-C



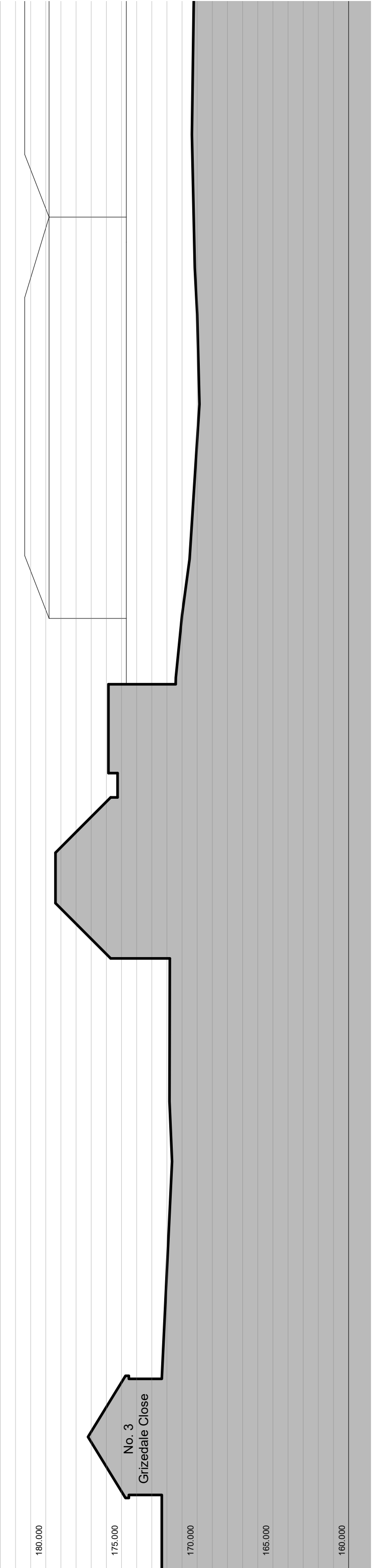
PROPOSED SITE SECTION E-E



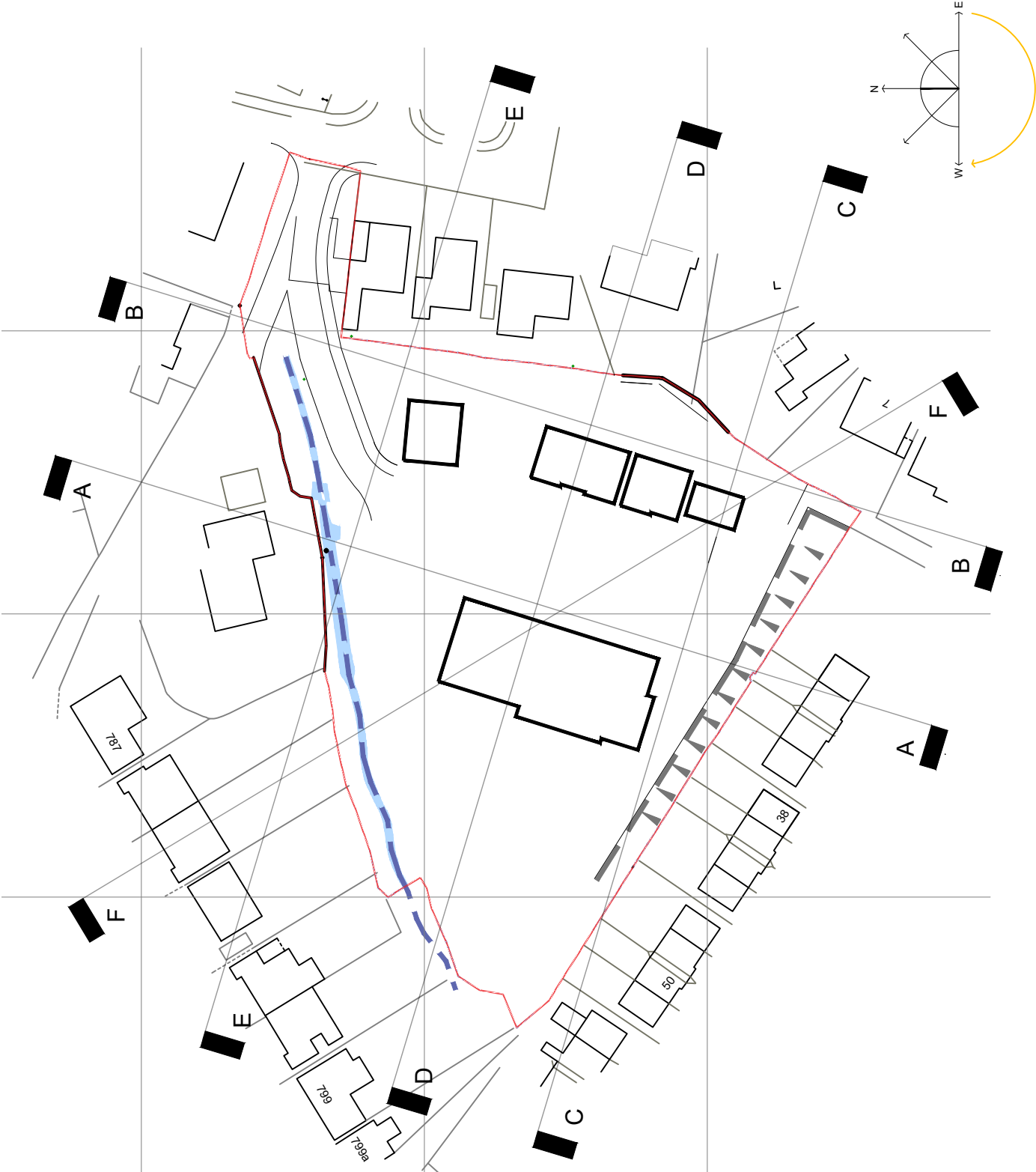
EXISTING SITE SECTION E-E



PROPOSED SITE SECTION D-D



EXISTING SITE SECTION D-D



Note: Due to limited available information on the topographical survey, external levels and heights of properties outside development site boundary are approximate only.

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Issue	Description	Date	Drawn	Checked
P7	Section E-E Indicative levels through No. 7 Grizedale Close updated	20.10.21	KM	KM
P6	Minor amendments	07.04.21	KM	KM
P5	Apartments, houses and road repositioned & levels amended in response to Tree Officer's comments	26.03.21	KM	MH
P4	Apartment block amended to reduce massing.	22.02.21	KM	KM
P3	Levels amended in line with SE drawings	07.12.20	KM	MH
P2	Amended following Planner's pre-application advice	15.09.20	KM	MH
P1	Issued for comments	09.06.20	KM	MH

Drawing Status
P - Planning | T - Tender | C - Construction | R - As Record

For Approval

tadw architects

SK St. Petersburg Stockport Cheshire SK1 1HD
Ph 0161 477 6158 Fx 0161 480 8342 mail@tadw.co.uk www.tadw.co.uk

Client Watson Homes

Job Grizedale Close, Bolton

Title Existing and Proposed Site Sections - (Sheet 2 of 2)

Scale 1:250 @ A1

Note - Prints from PDF files may not be to scale, check accuracy against scale

5 10 15 25m 1:250

Job Number 021292 Drawing Number 16 Issue P7

Application number 11585/21



Directorate of Place
Development Management Section

Town Hall, Bolton, Lancashire, BL1 1RU
Telephone (01204) 333 333

Bolton Council

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Date of Meeting: 11/11/2021

Application Reference: 11585/21

Type of Application: Full Planning Application

Registration Date: 23/06/2021

Decision Due By: 17/08/2021

Responsible Officer: John Dupre

Location: HEATON AND LOSTOCK NEIGHBOURHOOD CHILDRENS CENTRE, NEW HALL LANE, BOLTON, BL1 5LF

Proposal: PROVISION OF A NEW AIR SOURCE HEAT PUMP SUPPLEMENTING EXISTING MECHANICAL SERVICES TO BUILDING

Ward: Heaton and Lostock

Applicant: BOLTON COUNCIL

Agent : AECOM LTD

Officers Report

Recommendation: Approve subject to conditions

Executive Summary

-) The proposal is for a single Air Source Heat Pump (ASHP) unit, mounted on the roof of the building
-) The building is Council owned and the project is Council led.
-) There are no objections from Pollution Control.
-) The proposal is recommended for approval subject to planning conditions.
-) This application was approved by members at the 16th of September 2021 Planning Committee Meeting; however, following the completion of the noise report, it has been necessary to amend the location of the ASHP so that it is roof-mounted.
-) No objections were received to the original proposal. A further consultation has been undertaken with residents on the revised location. This ends on the 11th November.

Background

1. The siting of an air source heat pump at this building was approved under this application reference number by Members at their meeting of 16th of September 2021; however, following the completion of the noise report, it has been necessary to amend the location of the ASHP so that it is roof-mounted.

Proposal

2. Bolton Council applied for grant funding from Public Sector Decarbonisation launched in 2020 and were extremely successful in securing funding. The grant awards are for decarbonisation of operational buildings. the key focus of the funding was around decarbonisation of heat, this can fundamentally be achieved via Heat Pumps, solar thermal and improvements in fabric/controls.

3. The decarbonisation of the Council's building stock is a highly important element in the Council meeting its carbon reduction commitments. The buildings were selected due to their poor carbon performance.
4. This proposal comprises an installation of an Air Source Heat Pump (ASHP) on the roof of the building. The unit would measure 0.71 metres in height, 0.8 metres in width and 0.36 metres in depth.
5. The ASHP will be screened from the nearest residential properties by an acoustic sound barrier. This will be 1.2m in height and will consist of a solid, impermeable screen constructed from a material with a minimum mass per unit area of 15kg/m². Neighbours have been reconsulted on the revised proposal.

Site Characteristics

6. The application site relates to the existing single storey children's centre on the corner of Welbeck and New Hall Lane.
7. The area is residential in character and the site is bounded to the south and east by residential properties. St Thomas of Canterbury RC Primary School is located further to the south east.

Policy

The Development Plan

8. Core Strategy Policies: OA4 West Bolton; CG3 The Built Environment; CG4 Compatible Uses.

Other material considerations

9. National Planning Policy Framework 2021
10. Supplementary Planning Documents: General Design Principles.

Analysis

12. Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise. Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission. Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission. It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.
13. The main impacts of the proposal are:
 - \ impact on the character and appearance of the area
 - \ impact on residential amenity

Impact on the Character and Appearance of the Area

14. Policy CG3 of the Core Strategy states that the Council will expect development proposals to contribute to good urban design, conserve and enhance local distinctiveness and be compatible with the surrounding area, in terms of scale, massing, grain, form, architecture, street enclosure, local materials and landscape treatment. Policy OA3 of the Core Strategy aims to ensure the

character of the existing environment is conserved and enhanced.

15. This proposal comprises an installation of an ASHP on the roof of the building. The unit would be screened from street level and neighbouring properties by a 1.2m acoustic barrier. This would be dark grey in colour.
16. Due to its location and limited size, it is not considered that the ASHP or the acoustic barrier would have an undue impact on the character or appearance of the building or the surrounding area. In view of the above it is considered that the proposed scheme is in line with Core Strategy Policies CG3 and OA3 of the Core Strategy.

Impact on Residential Amenity

17. Policy CG4 of the Core Strategy relates to new development and seeks to ensure that future occupiers and neighbours are provided with a satisfactory level of amenity in terms of space, sunlight, daylight, privacy, aspect and layout.
18. The nearest residential property is 43 Welbeck Road, located immediately 8m to the east of the Children's Centre. Also in close proximity are 75 New Hall Lane and 56 Eastbourne Grove which are located 15m to the south and south-east respectively. Further residential properties are located on the opposite side of New Hall Lane and Welbeck Road.
19. A background noise survey was completed on the 13th to 14th September 2021 to establish a background noise level for ASHP units proposed at the site. Based on this data, plant noise emission limits have been established following the guidance of BS4142: 2014+A1:2019, BS4142:1997 and the Environmental Protection Act of 1990.
20. To meet the plant noise emission limit at 1m from the worst affected window of the surrounding noise sensitive premises, the ASHP is proposed to be located on the roof and a barrier is to be installed around the unit. It is also recommended that the equipment should be controlled so that it does not produce any "distinguishable, discrete or continuous note (whine, hiss, screech, hum, etc.) or distinct impulses (bangs, clicks, clatters or thumps)" at noise sensitive receivers. Normal operation of the equipment is not expected to give rise to such characteristics.
21. A planning condition has been recommended to ensure that the ASHP and sound barrier is installed and operated as set out in the noise report.
22. It is not considered that the ASHP or acoustic barrier would have an overbearing visual impact on any neighbouring property.
23. Having regard to the above, it is considered that the proposal subject to the recommended condition would comply with Policy CG4 of Bolton's Core Strategy.

Conclusion

24. It is considered that the proposal would not have an unacceptable impact on the street scene nor on the character of the existing building and it would not have detrimental impact on residential amenity of neighbouring occupiers (subject to planning conditions), and it is therefore

recommended that members approve the application.

Representation and Consultation Annex

Representations

At the time of writing, no representations have been made. The consultation period ends on 11th November 2021 - should any representations be made, they would be reported at the meeting.

Consultations

Pollution Control – no objection subject to the recommended condition.

Planning History

The site has no relevant planning history.

Recommendation: Approve subject to conditions

Recommended Conditions and/or Reasons

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. **Fixed plant and equipment noise rating**

The rating level (LAeqT), as determined by BS4142:2014+A1:2019 Methods for Rating and Assessing Industrial and Commercial sound, from all sources associated with the development, when operating simultaneously or individually, shall not exceed the background sound levels (LA90) that are specified in table 4.1 of the report by AECOM (ref: 60661834/2, September 2021, rev 01, revised 22/10/21) when measured 1m from the boundary of any noise sensitive receptors. Prior to its first operation, the equipment shall be provided with a 1.2m high acoustic screen constructed from a material that has a minimum mass per unit area of 15kg/m² in accordance with the details shown in Section 4.3 and Fig 4.1 of the aforementioned report and all such screening shall be retained in full thereafter.

Reason

To minimise the impact of noise on the general and residential amenity from the increase and alteration of commercial and/or industrial uses in the area in accordance with Core Strategy Policy CG4 and NPPF.

3. The development hereby permitted shall be carried out in complete accordance with the following approved plans:

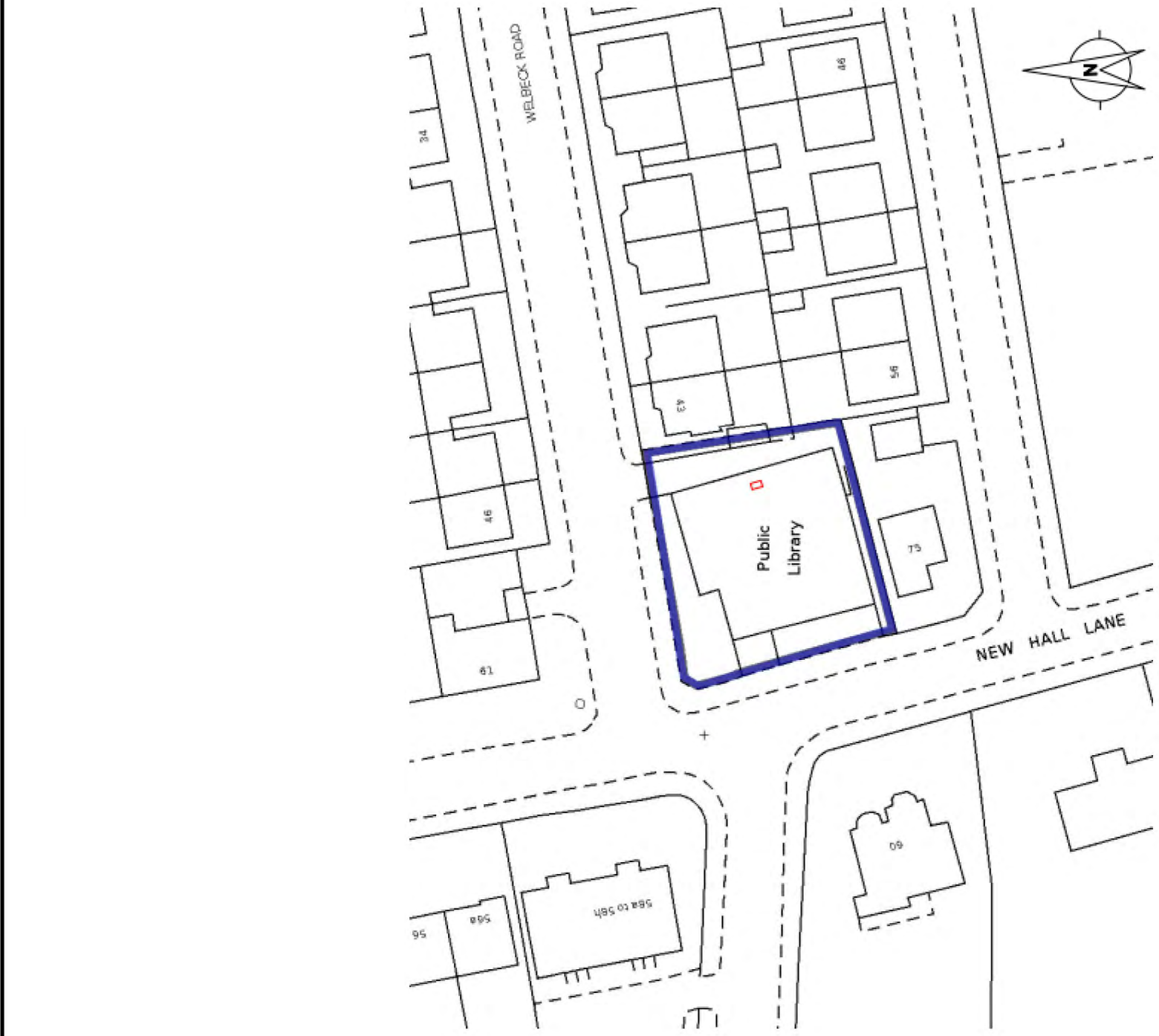
Location Plan - uploaded 02.11.21

Elevation showing proposed location of ASHP - Drwg. No.

60654197_HLC_PLANNING_001_T0 - uploaded 2.11.21

Daikin Altherma 3 Monobloc Air Source Heat Pump Technical Guide - uploaded 23.06.21

Reason: For the avoidance of doubt and in the interests of proper planning.



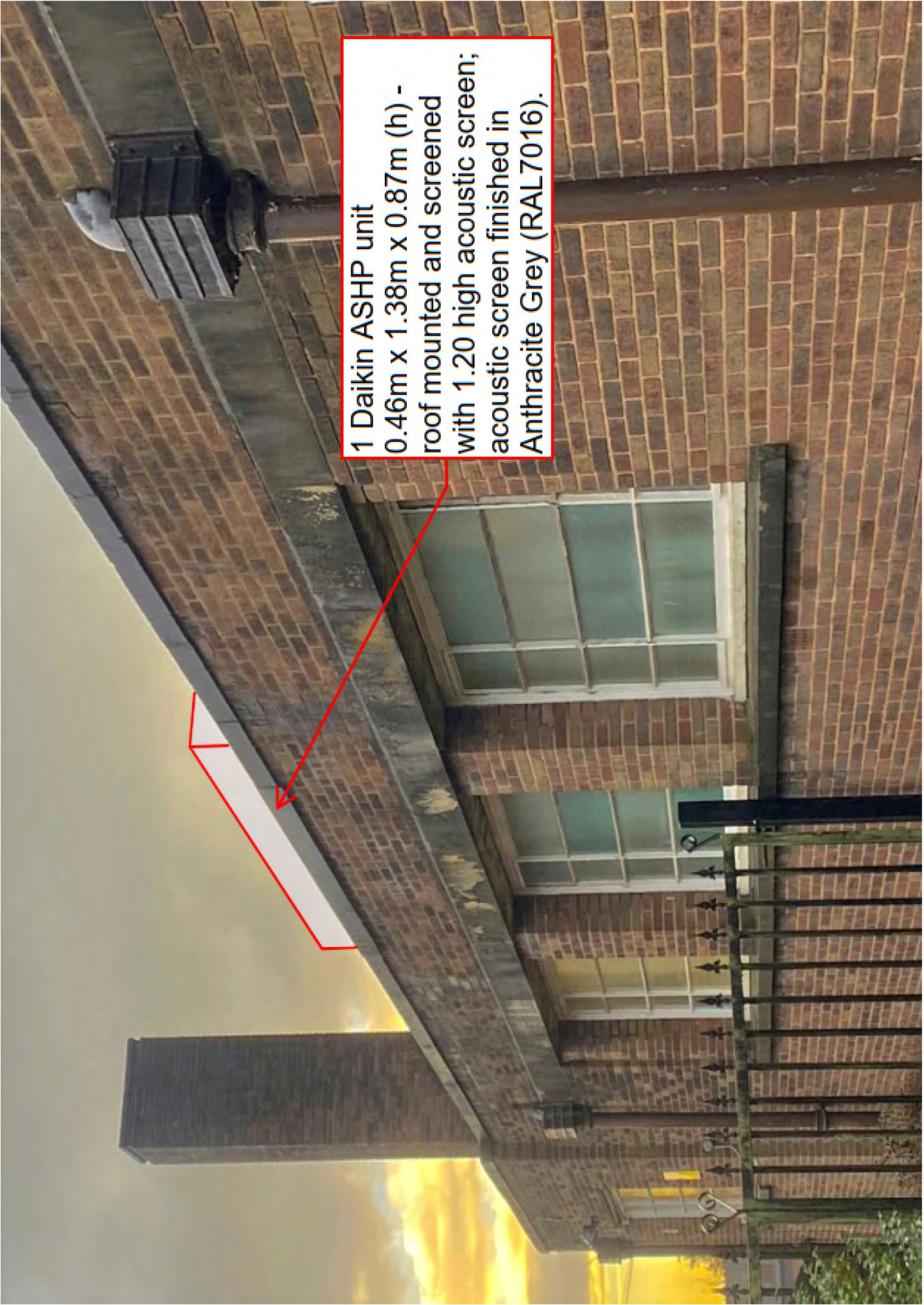
01 - SITE PLAN SHOWING LOCATION OF ASHP UNIT; SCALE 1:1250



02 - BIRD'S EYE VIEW SHOWING LOCATION OF ASHP UNIT ON THE ROOF



03 - EAST ELEVATION SEEN FROM WELBECK ROAD



04 - CLOSE UP OF EAST ELEVATION



PROJECT

GR MANCHESTER
DECARBONISATION

PROJECT:

HEATON LOSTOCK CHILDREN CENTRE

New Hall Lane
Bolton, BL1 5LF

STAGE

Planning

CLIENT

ROBERTSON GROUP

3 The Parks
Haydock
Newton-le-Willows, WA12 0JQ
rfm@robertson.co.uk Email
www.robertson.co.uk

CONSULTANT

AECOM

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2 LEMAN STREET,
LONDON E1 8FA
www.aecom.com

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DESCRIPTION	ISSUE/REVISION	SCALE	NTS

PROJECT NUMBER

60654197

SHEET TITLE

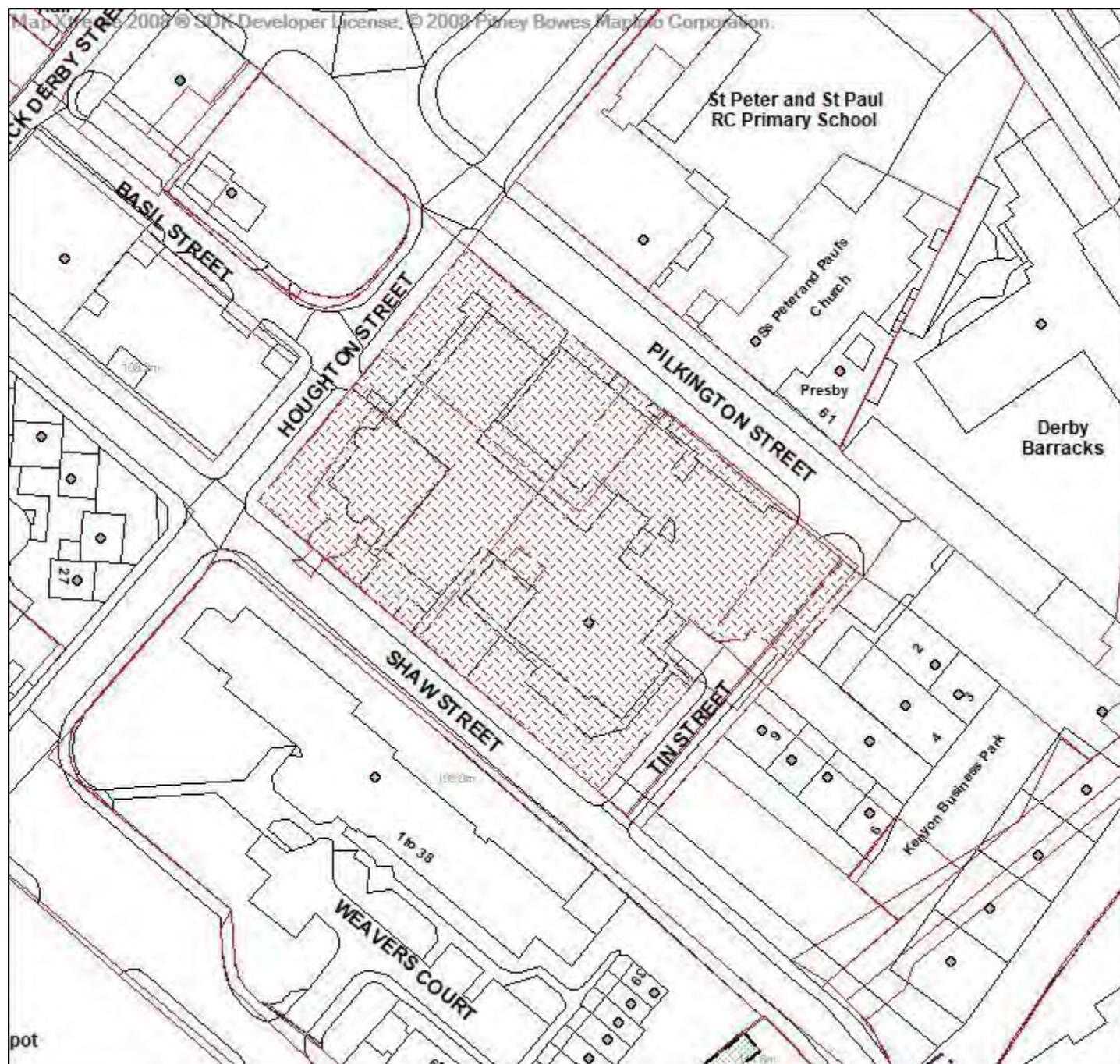
Elevation showing proposed location of
ASHP

SHEET NUMBER

60654197_HLC_PLANNING_001_T0



Application number 11991/21



Directorate of Place
Development Management Section

Town Hall, Bolton, Lancashire, BL1 1RU
Telephone (01204) 333 333

Bolton Council

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Date of Meeting: 11/11/2021

Application Reference: 11991/21

Type of Application: Full Planning Application

Registration Date: 17/08/2021

Decision Due By: 11/10/2021

Responsible Officer: Beth Bradburn

Location: HARVEY EARLY YEARS CENTRE, SHAW STREET, BOLTON, BL3 6HU

Proposal: PROVISION OF NEW AIR SOURCE HEAT PUMP, SUPPLEMENTING EXISTING MECHANICAL SERVICES TO BUILDING

Ward: Great Lever

Applicant: BOLTON COUNCIL

Agent : AECOM LTD

Officers Report

Recommendation: Approve subject to conditions

Executive Summary

-) The building is Council-owned
-) The project is Council-led
-) The proposal is a modest Air Source Heat Pump to be installed on the north eastern elevation of the building
-) There are no objections from Pollution Control
-) No objections have been received from local residents
-) The proposed Air Source Heat Pump would assist in improving the energy efficiency of the application building and reducing CO2 emissions
-) The proposal is recommended for approval subject to planning conditions

Background

1. Bolton Council applied for grant funding from Public Sector Decarbonisation Scheme launched in 2020 and were extremely successful in securing funding. The grant awards are for the decarbonisation of operational buildings. The key focus of the funding was around decarbonisation of heat, this can fundamentally be achieved via Heat Pumps, solar thermal and improvements in fabric / controls.
2. The decarbonisation of the Council's building stock is a highly important element in the Council meeting its carbon reduction commitments. The buildings were selected due to their poor carbon performance.

Proposal

3. This proposal comprises an installation of one Air Source Heat Pump external unit on the north

eastern elevation of the building. The heat pump would be mounted on the external wall 1.2m above floor level and would measure 0.87m in height, 1.38m in width and 0.46m in depth.

4. All the other components shown on the proposed plans are either internal or underground and are not on view externally.

Site Characteristics

5. The application site is a single storey building bounded by Shaw Street, Pilkington Street, Tin Street and Houghton Street. To the north of the site is St Peter and Paul's Primary School and to the east and west are other industrial and business uses. To the south of the site are residential flats.
6. The application site is surrounded by perimeter railings and landscaping and is sited within Great Lever (Core Strategy Policy RA1).

Policy

The Development Plan

7. Core Strategy Policies: CG3 The Built Environment; CG4 Compatible Uses, RA1 Inner Bolton, CG1 Cleaner and Greener, CG2 Sustainable Design and Construction

Other material considerations

8. National Planning Policy Framework 2021
9. Supplementary Planning Documents: General Design Principles.
10. Bolton Council Climate Change Strategy

Analysis

11. Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.
12. Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission. Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission. It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.
13. The main impacts of the proposal are:-
 - \ Impact on the character and appearance of the area
 - \ Impact on residential amenity
 - \ Impact on Sustainability

Site Layout/ Impact on the Character and Appearance of the Area

14. Policy CG3 of the Core Strategy states that the Council will expect development proposals to contribute to good urban design, conserve and enhance local distinctiveness and be compatible with the surrounding area, in terms of scale, massing, grain, form, architecture, street enclosure, local materials and landscape treatment.
15. Policy RA1 of the Core Strategy states that the council and its partners will ensure that development has particular regard to massing and materials used.
16. This proposal comprises an installation of an Air Source Heat Pump external unit on the north eastern elevation of the building. Although the Air Source Heat Pump would be visible from Pilkington Street, it would be sited in excess of 25m from the street due to the separation of the building from the road by the car park of the children's centre. Furthermore, low railings and trees line the perimeter of the site which will also assist in screening the heat pump.
17. The Air Source Heat Pump will also be mounted near the eastern corner of the building adjacent to the boiler room door and would not appear as a prominent feature on the building. All the other components shown on the proposed plans are either internal or underground and are not on view externally.
18. In view of the above it is considered that the proposed scheme is in line with Core Strategy Policies CG3 and RA1 and associated supplementary planning guidance and the NPPF.

Impact on Residential Amenity

19. Policy CG4 of the Core Strategy states that development should not generate unacceptable nuisance, odours, fumes, noise or light pollution, nor cause detrimental impacts upon water, ground or air quality.
20. The proposed Air Source Heat Pump would be located on the side of the building away from any residential properties. The Council's Pollution Control Officer was also consulted and noted that due to its siting on the building, no noise assessment would be required.
21. Having regard to the above, it is considered that the proposal would comply with the standards of amenity in policy CG4 of Bolton's Core Strategy and the NPPF.

Impact on Sustainability

22. Policies CG1 and CG2 of Core Strategy seek to ensure [amongst other things] that development work towards minimising energy requirements, improving energy efficiency, lessening the reliance on fossil fuel-based energy and reducing carbon dioxide (CO₂) emissions as well as maximising the potential for renewable energy development and encourage proposals that contribute towards the renewable energy targets.
23. Bolton Council applied for grant funding from Public Sector Decarbonisation launched in 2020 and were successful in securing funding. The grant awards are for decarbonisation of operational buildings. The key focus of the funding was around decarbonisation of heat which can fundamentally be achieved via Heat Pumps, solar thermal and improvements in fabric/controls. The decarbonisation of the Council's building stock is a highly important element in the Council meeting its carbon reduction commitments. The buildings were selected due to their poor carbon performance.
24. The proposed air source heat pumps would therefore assist in improving the energy efficiency

of the application building and reducing CO2 emissions which would be wholly compliant with policies CG1 and CG2 of the Core Strategy.

Conclusion

It is considered that the proposal would not have an unacceptable impact on the street scene nor on the character of the existing building. The proposed Air Source Heat Pump would also have no impact on residential amenity or neighbouring occupiers and would assist in improving the energy efficiency of the building and reducing CO2 emissions.

Members are requested to approve the application.

Representation and Consultation Annex

Representations

No representations have been made

Consultations

Pollution Control

Planning History

None relevant.

Recommendation: **Approve subject to conditions**

Recommended Conditions and/or Reasons

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason

Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. The development hereby permitted shall be carried out in complete accordance with the following approved plans:

Elevation showing proposed location of ASHP, drawing no. 60654197_HEY_PLANNING_002_REV_T0
Site Location Plan, received 21.08.21

Reason

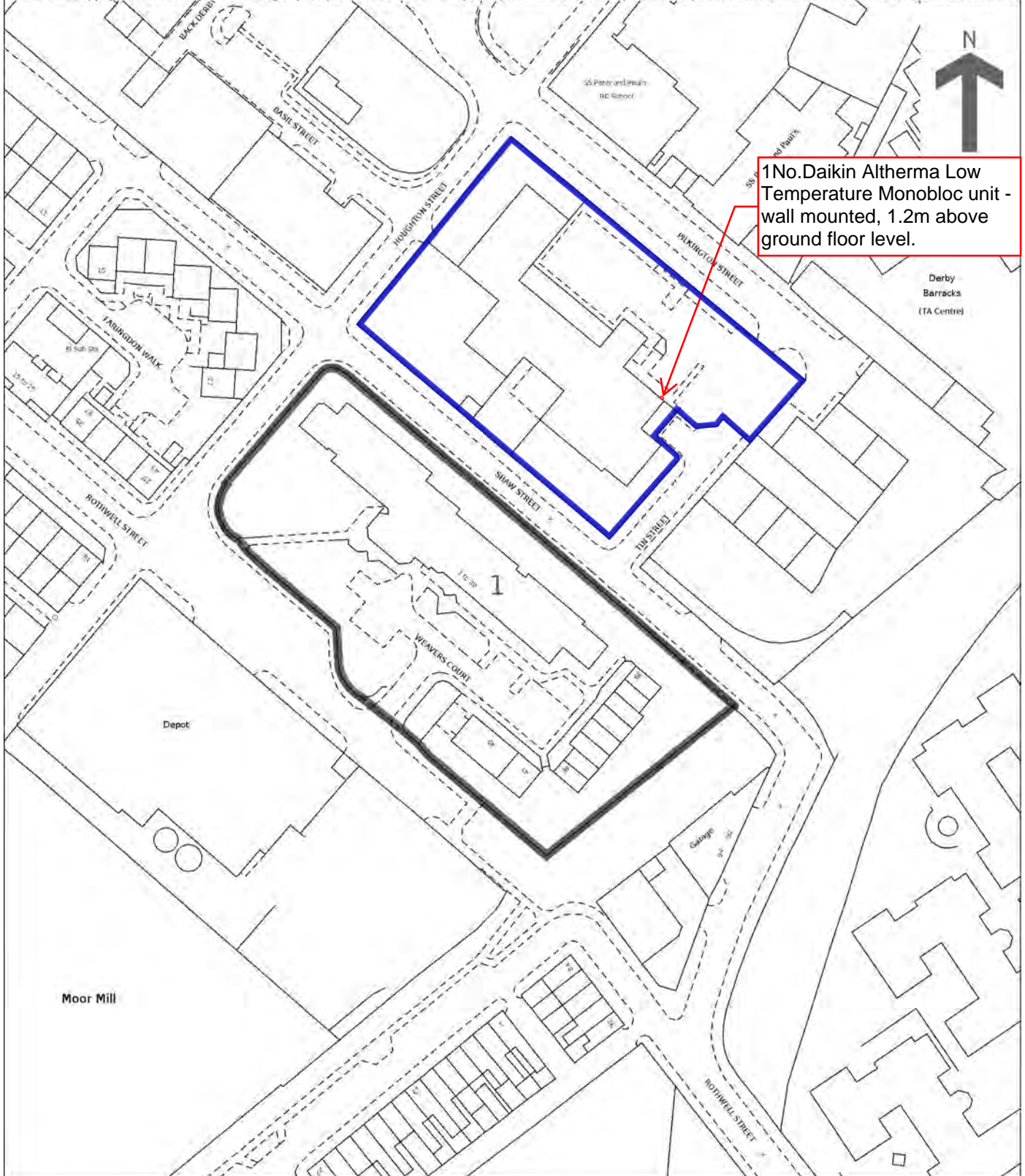
For the avoidance of doubt and in the interests of proper planning.

HM Land Registry
Official copy of
title plan

Title number **MAN49818**
Ordnance Survey map reference **SD7108SW**
Scale **1:1250**
Administrative area **Greater Manchester :**
Bolton



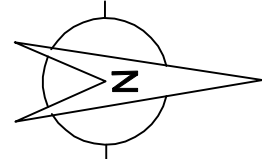
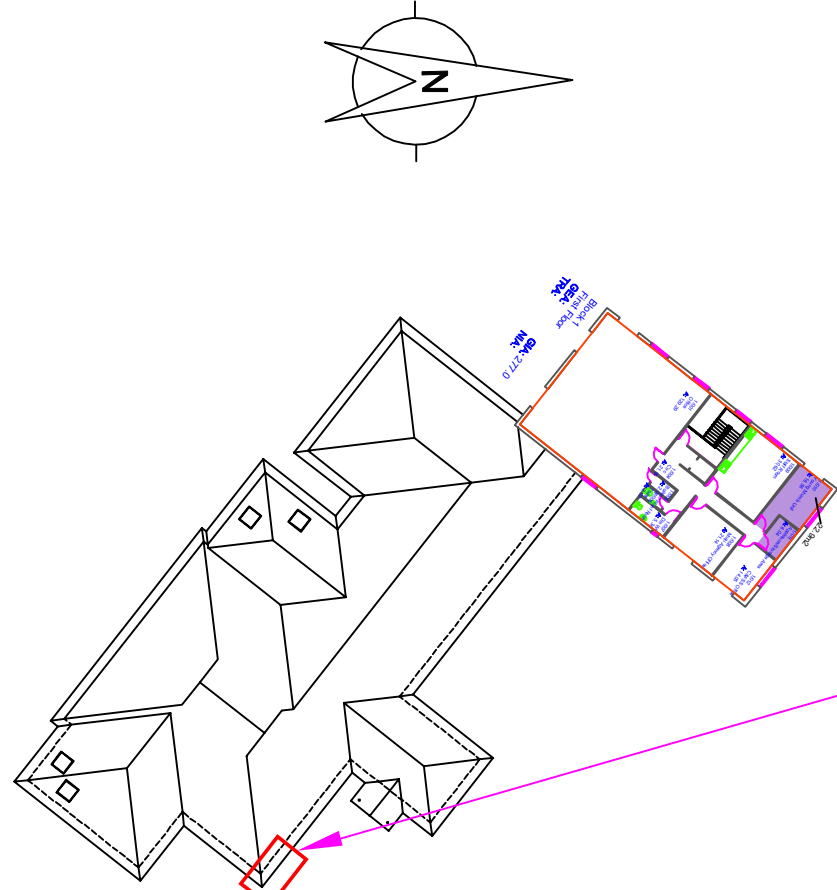
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DESCRIPTION	Checked By	Checked Date	Date	Scale
ISSUE/REVISION				
SCALE	1:20@A1			

2 Site plan showing
location of ASHP



1 ASHP Location:

1no Daikin Atherma
EDLA16D3V3 ASHP
H= 870mm
W= 1380mm
D= 460mm

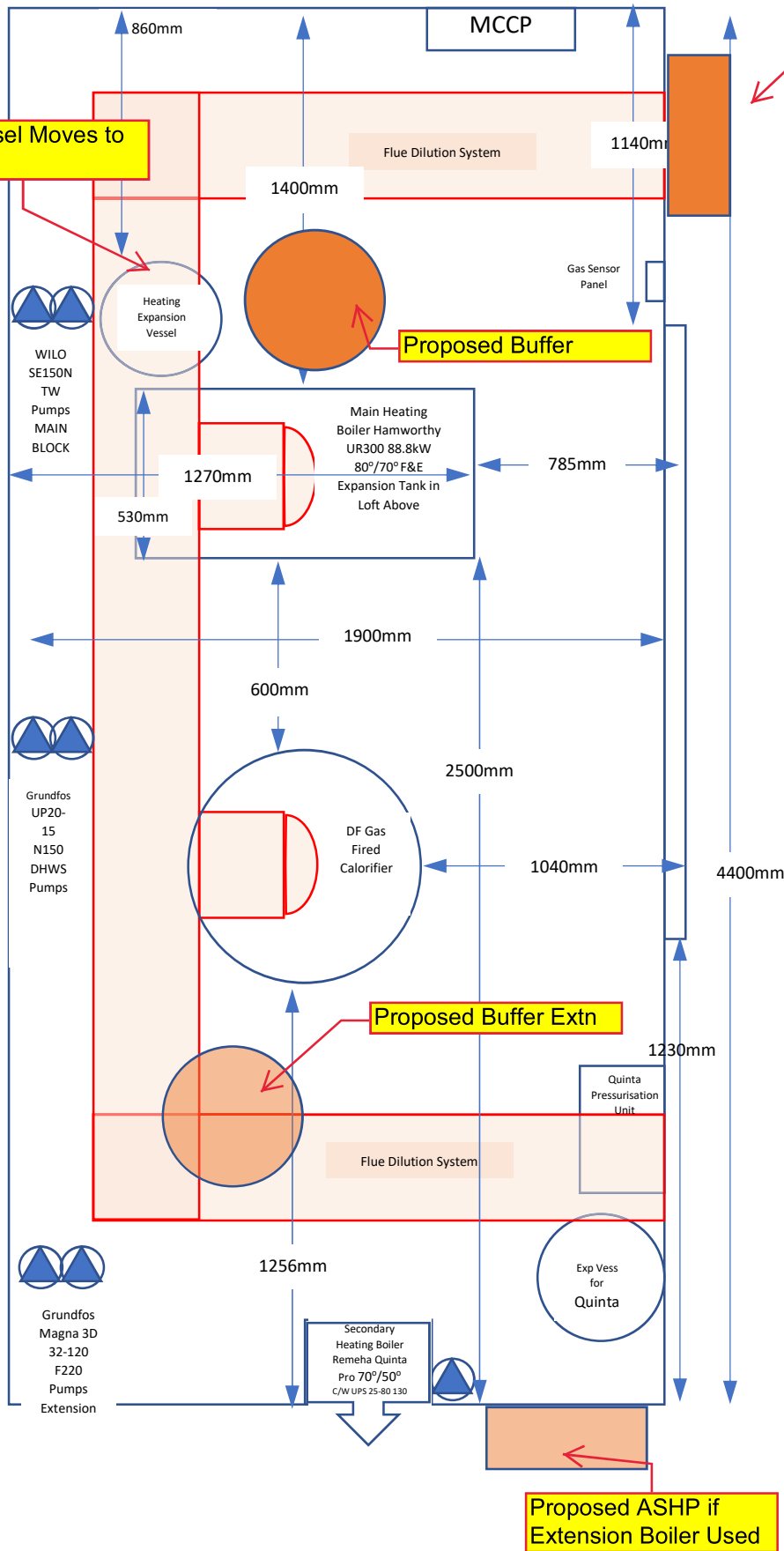


Do Not Scale from Drawing

Harvey Early Years Block Plan

Proposed Position of ASHP Condensing Unit

This Vessel Moves to Corner



Main Blr Room

Main Boiler, Hamworthy UR300 88.8kW,

No data on Quinta Condensing Boiler, (Extension) (730x500x464) All Ratings same size on website outputs (Modulating) 29.8-107kW,

Hardly any Room in Plant Room Buffer **Will** just Fit if we move the Expansion Vessel into corner

Insulation Required

Foil Covered Rigid Glass Fibre

- 4-2" Flanges
- 2-2" Valve Muffs
- 4-1.25" Valve Muffs
- 8-1" Valve Muffs
- 2mtrs 22mm CU Pipework
- Various Aluminium Tape Repairs

1st Floor Boiler Room

Serves 1st floor Only Room is 2.5mx 1.5mx 2.3m has 2 x Alfa InTec 26C 20kW Output Each

Insulation Required

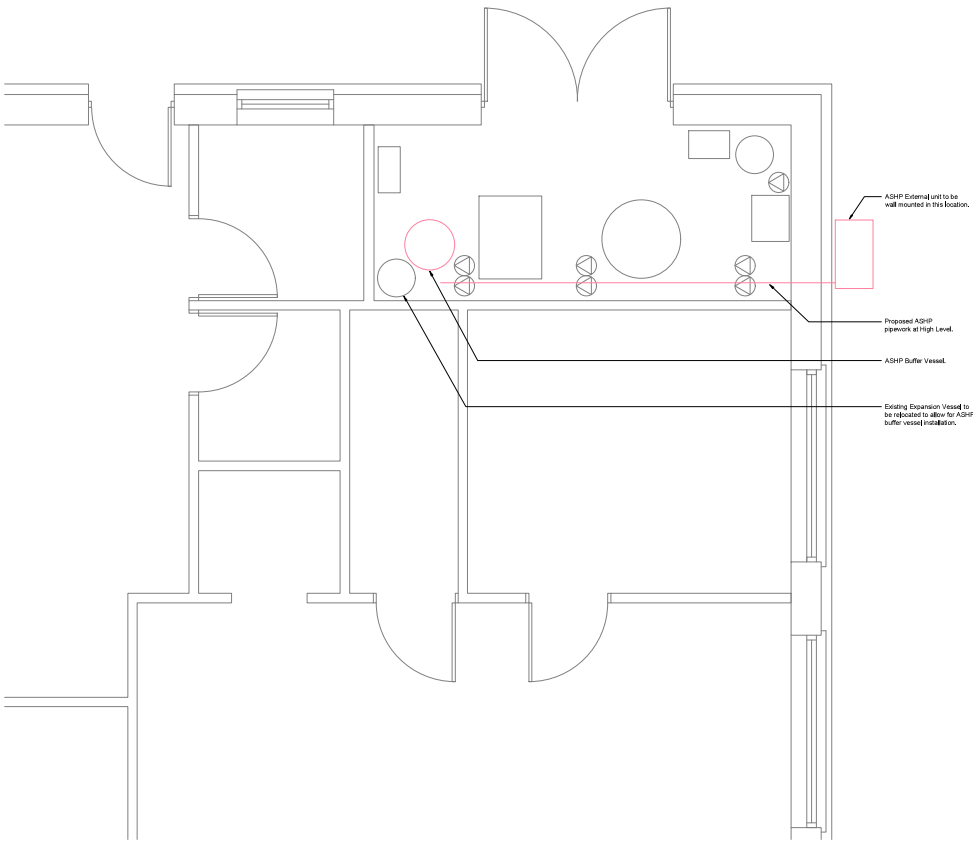
Foil Covered Rigid Glass Fibre

- 2mtrs 2" Pipework
- 1m 1.25" Pipework
- 4 2" Tees
- 4 1.25" Tees
- 15mtrs 22mm CU Pipe

ACU External Unit is Daikin RY45DA7V1 1999 1.8kG R22 Internal Unit is Voidpak FHYB35,45.60.71FK7V1 Room is 6mx4mx2.3m Medical Room

DB Crabtree Polestar 100A
2 Spare 1 Ph Ways
40A Spare Capacity

BMS Is Trend IQ241 within MCCP



ASHP External unit to be wall mounted in this location.

Proposed ASHP pipework at High Level.

ASHP Buffer Vessel.

Existing Expansion Vessel to be repositioned to allow for ASHP buffer vessel installation.



PROJECT
MANCHESTER
DECARBONISATION
TAMESIDE BROUGH

STAGE
FOR INFORMATION

CLIENT
ROBERTSON GROUP

CONSULTANT
AECOM
ALDgate TOWER,
2 LEMAN STREET,
LONDON E1 8FA
www.aecom.com

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NOTES

1. THE CONSULTANT HAS CONDUCTED VISUAL INSPECTIONS OF THE EXISTING BOILER ROOM AND HAS IDENTIFIED THE FOLLOWING DEFECTS:
2. THE CONSULTANT HAS IDENTIFIED THE FOLLOWING DEFECTS:
3. THE CONSULTANT HAS IDENTIFIED THE FOLLOWING DEFECTS:
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REVISION	DATE	BY	CHKD	APPD
1	08/07/20

SCALE : N/A

PROJECT NUMBER
60654197
SHEET TITLE
HARVEY EARLY YEARS
PROPOSED BOILER ROOM LAYOUT
SHEET NUMBER
60654197_HEV_MECH_002_REV_TO

Application number 12097/21



Directorate of Place
Development Management Section

Town Hall, Bolton, Lancashire, BL1 1RU
Telephone (01204) 333 333

Bolton Council

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Date of Meeting: 11/11/2021

Application Reference: 12097/21

Type of Application: Full Planning Application

Registration Date: 31/08/2021

Decision Due By: 29/11/2021

Responsible Officer: Martin Mansell

Location: MOOR LANE BUS STATION, MOOR LANE, BOLTON

Proposal: DEMOLITION OF ALL BUILDINGS AND STRUCTURES AND ERECTION OF A MIXED-USE DEVELOPMENT COMPRISING 208 DWELLINGS (USE CLASS C3), COMMERCIAL FLOORSPACE (USE CLASS E), PUBLIC SQUARE, LANDSCAPING, PARKING, HIGHWAY WORKS AND OTHER ASSOCIATED WORKS.

Ward: Halliwell

Applicant: FSG Moor Lane Bolton Developments Limited

Agent : Euan Kellie Property Solutions

Officers Report

Recommendation: Approve subject to conditions

Executive Summary

-) Consent is sought to construct a new development of 208 new homes including 82 affordable homes together with new commercial space on land comprising the former Moor Lane bus station together with two nearby parcels of land either side of the Job Centre on Blackhorse Street.
-) The proposed development consists of two six-storey apartment blocks on the Moor Lane side of the site, 44 three-storey townhouses in the central portion of the site and two four-storey apartment blocks either side of the Job Centre, behind Le Mans Crescent.
-) The application also proposes new hard and soft landscaping, public realm improvements to the surrounding highways and two linear parks, one of which would run west from the arches of Le Mans Crescent.
-) The application has been subject to a significant degree of design consideration with Officers and external consultees, including two separate engagements with the Design Review process via Places Matter (RIBA NW, Liverpool).
-) Feedback from market traders to the General Manager of Bolton Market has been positive, based on an expanded customer base for their businesses.
-) The site currently has very low biodiversity value and this would be significantly improved by the proposed development.

-) The proposed development would make a significant contribution to the Council's housing supply shortfall.
-) A total of four objections have been received - a formal response from Bolton & District Civic Trust, personal objections from two of its members and finally an objection from a resident of Harwood, supporting the Trust's position on this proposal. No other objections were received

Proposal

1. Full planning permission is sought to construct a new development of 208 new homes including 82 affordable homes together with new commercial space on land comprising the former Moor Lane bus station together with two nearby parcels of land either side of the Job Centre on Blackhorse Street. It would consist of 44 dwelling houses arranged to form two distinct elements and 164 apartments in the form of four elements. New commercial space would be provided to the ground floor of the two four-storey blocks either side of the Job Centre. The proposals also incorporate extensive public realm and landscaping for the benefit of prospective residents and those passing through the development, including two linear parks - one running west from Le Mans Crescent to Moor Lane (referred to as Cheadle Gardens) and a second running south from this new element towards Bolton Market. A one-way road running from Blackhorse Street to Moor Lane would provide access to the townhouse parking.
2. The proposed residential development can be considered to consist of six main elements:-

Element 1 - Apartment block adjacent to Moor Lane (North)

3. This element would rise to six storeys and would be bounded by Moor Lane to the west, by the new east-west linear park to the north, by the new north-south linear park to the east and by the new residential vehicular access route to the south. It would consist of 19 one-bedroom apartments, 29 two-bedroom apartments and 4 duplex three-bedroom apartments - 52 in total. Projecting balconies would be provided to the north, south and eastern elevations to provide access to external amenity space for the majority of the proposed apartments.

Element 2 - Apartment block adjacent to Moor Lane (South)

4. This element would rise to six storeys and would be bounded by Moor Lane to the west, by the new the new residential vehicular access route to the north, by the new north-south linear park to the east and by land associated with Bolton Market to the south. It would consist of 19 one-bedroom apartments, 29 two-bedroom apartments and 4 duplex three-bedroom apartments - 52 in total. Projecting balconies would be provided to the north, south and eastern elevations to provide access to external amenity space for the majority of the proposed apartments.

Element 3 - Four blocks of three-storey houses in the middle of the site (North)

5. These units would be three storeys in height but with a variation in roof line created by a private terrace provided either to the front or rear at second floor level. They would be bounded by the new north-south linear park to the west, by the new east-west linear park to the north, by Blackhorse Street to the east and by the new residential vehicular access route to the south. A total of 20 units are proposed, arranged in four blocks enclosing a gated courtyard of 20 car parking spaces, one per dwelling. These dwellings are designed with flat-roofs in order to discreetly accommodate roof-mounted photovoltaic cells. The private terraces have been arranged in manner which accentuates the height to Blackhorse Street and to the new east-west linear park to the north whilst maximising solar gain. As well as the elevated private terrace, each dwelling would also be provided with a small area of enclosed space at the rear.

Element 4 - Four blocks of three-storey houses in the middle of the site (South)

6. These units would be three storeys in height but with a variation in roof line created by a private

terrace provided either to the front or rear at second floor level. They would be bounded by the new north-south linear park to the west, by the new residential vehicular access route to the north, by Blackhorse Street to the east and by land associated with Bolton Market to the south. A total of 24 units are proposed, arranged in four blocks enclosing a gated courtyard of 24 car parking spaces, one per dwelling. These dwellings are designed with flat-roofs in order to discreetly accommodate roof-mounted photovoltaic cells. The private terraces have been arranged in manner which accentuates the height to Blackhorse Street whilst maximising solar gain. As well as the elevated private terrace, each dwelling would also be provided with a small area of enclosed space at the rear.

Element 5 - Four-storey block, adjacent to the Job Centre (North)

7. This element would rise to four storeys and would be bounded by Blackhorse Street to the west, by the new east-west linear park to the north, by Queen Street to the east and by the Job Centre to the south. It would consist of 24 one-bedroom apartments and 9 two-bedroom apartments - 33 in total. Two commercial units are proposed for the ground floor, one approximately 155 square metres and one approximately 253 square metres, both wrapping around the north-eastern and north-western corners of the block with the entrance to the residential units dividing the two commercial units. This element has been designed with a civic appearance in mind and would feature chamfered corners and a repetitive window arrangement, consistent with other buildings within the civic core of Bolton town centre. Materials are suggested as buff brick, again consistent with the civic core. It is envisaged that this element will provide accommodation for people aged over 55.

Element 6 - Four-storey block, adjacent to the Job Centre (South)

8. This element, sited at the location of the 5 retail properties proposed to be demolished, would rise to four storeys and would be bounded by Blackhorse Street to the west, by Barn Street to the north, by Queen Street to the east and by Ashburner Street to the south. It would consist of 12 one-bedroom apartments and 15 two-bedroom apartments - 27 in total. One large commercial unit is proposed for the ground floor, approximately 397 square metres, with frontages to Blackhorse Street, Ashburner Street and partially to Queen Street. Entrance to the residential units would be via Barn Street. This element has been designed with a civic appearance in mind and would feature chamfered corners and a repetitive window arrangement, consistent with other buildings within the civic core of Bolton town centre. Materials are suggested as buff brick, again consistent with the civic core.
9. All properties would meet the nationally described internal space standards.

Demolition

10. It is proposed that the five single-storey retail units fronting Ashburner Street (directly opposite the former Odeon site) be demolished. The structures proposed for demolition, 52-60 Ashburner Street, lie within the Town Hall Conservation Area at its south-western extremity; however the Building Bolton SPD does not consider these buildings to have any particular townscape value and the Town Hall Conservation Area Character Study considers their impact on the significance of the Conservation Area to be neutral.

General

11. As well as the relevant plans and elevations, the application is supported by the following documents:- a Design and Access Statement prepared by AEW Architects, a Landscape Statement prepared by Gillespies, Heritage Assessment prepared by Buttress, a Historic Environment Desk Based Assessment (Archaeological Assessment) prepared by Archaeological Research Services Limited, an Air Quality Assessment prepared by Redmore Environmental, a Crime Impact Statement prepared by GM Police Design for Security, a Flood Risk Assessment

prepared by Renaissance, an Outline Drainage Strategy prepared by Renaissance, a Phase 1 Desk Study Assessment Report prepared by Renaissance, a Noise Impact Assessment prepared by Hann Tucker Associates, a Preliminary Ecological Appraisal prepared by E3P, a Nocturnal Bat Survey Report prepared by E3P, a Biodiversity Metric Report prepared by E3P, an Energy Statement prepared by Ridge, a Transport Assessment including Interim Travel Plan prepared by SK Transport Planning, a Framework Construction Management Plan prepared by AEDI and a Statement of Community Involvement prepared by Lexington.

12. A Screening Opinion was sought from Bolton Council pursuant to the Town & Country Planning (Environmental Impact Assessment) Regulations 2017, as amended by the Town and Country Planning and Infrastructure Planning (Environmental Impact Assessment) (Amendment) Regulations 2018. Officers formally confirmed that the development was not EIA development.
13. On the important issue of the delivery of the development, the Applicant, Step Places point out that they are an award winning place making, regeneration specialist having just won Best Residential Scheme, Most Saleable Homes and most importantly Most Sustainable Scheme at the recent Northwest Property awards. They have committed to a speedy delivery and following the receipt of planning will commence detailed design and condition discharge immediately. A target start on site has been set as April 2022 for the main bus station site with work concluding on all three sites by April 2024. During this period they will be working in partnership with the Council to ensure that all works to Black Horse Street and the Market are completed in tandem. Phase 1 would involve the development of the former Moor Lane bus station with Phase 2 comprising work on the Ashburner Street and Queen Street car park sites. Upon completion, 82 of the 208 properties would be transferred to and subsequently managed by Bolton at Home.
14. The submitted Statement of Community Involvement sets out in detail the extensive engagement that the Applicant has had with stakeholders and how this engagement has informed their approach. This included engagement with Council Officers and Elected Members together with external consultees such as Historic England and GM Police. The Applicant's team also presented the scheme via an online meeting to three members of Bolton & District Civic Trust on 27th August, prior to the submission of the application. In addition, 1,008 leaflets were distributed to nearby commercial and residential properties and a website was created so that feedback could be provided online.

Site Characteristics

15. The site measures approximately 1.78 hectares in area and is located within the heart of Bolton town centre, immediately to the east of the civic core. It primarily comprises the former Moor Lane bus station but is irregular in shape as it also includes two parcels of land to the north and south of the Job Centre on Blackhorse Street. It can broadly be considered to be bounded by Moor Lane to the west, by Bolton Courts and Cheadle Square to the north, by Queen Street / Le Mans Crescent to the east and by Ashburner Street / Bolton Market to the south. Blackhorse Street runs north to south through the site, dividing it into two unequal halves. With the exception of the five single storey retail units fronting Ashburner Street that are proposed to be demolished, the site itself contains no buildings and its contextual built form is defined by the western side of Le Mans Crescent, the Job Centre, the Court building and Bolton Market. The site is considered to be highly prominent within Bolton town centre due to the presence of important highways such as Moor Lane, Blackhorse Street and Ashburner Street, together with the close proximity of Bolton Market, Le Mans Crescent and the wider civic core beyond. All parts of the site can be considered to be previously-developed (brownfield) land.
16. The western extremity of the Town Hall Conservation Area runs along the middle of Blackhorse Street before returning east along Ashburner Street. Therefore, Le Mans Crescent, Cheadle

Square and the plots of land north and south of the Job Centre (including the buildings at the junction of Ashburner Street and Blackhorse Street proposed to be demolished) all lie within the Town Hall Conservation Area however all land that formed the former bus station site lies outside of the Conservation Area.

17. For the avoidance of doubt, this report uses the term "Le Mans Crescent" to refer to the building; however, strictly speaking the name of the building is "Civic Centre" and Le Mans Crescent is the road.

Policy

The Development Plan

18. Committee should have regard to the requirements of the development plan as a whole. The following policies are considered to be particularly relevant.
19. Bolton's Core Strategy Development Plan Document (2011) - Strategic Objectives - SO3 - To take advantage of the economic opportunities presented by Bolton town centre and ensure that these opportunities benefit everybody in Bolton, including those people living in the most deprived areas, SO4 - To create a transformed and vibrant Bolton town centre, SO5 - Ensuring Bolton takes full advantage of its location in the Greater Manchester City Region, SO6 Ensuring that transport infrastructure supports all aspects of the spatial vision, SO9 - To reduce crime and the fear of crime, and improve road safety by ensuring that neighbourhoods are attractive and well designed, SO10 To minimise Bolton's contribution to climate change and mitigate and adapt to its adverse effects, SO11 Conserving and enhancing the best of Bolton's built heritage and landscapes, SO12 To protect and enhance Bolton's biodiversity, SO13 To reduce the likelihood and manage the impacts of flooding in Bolton, and to minimise potential flooding to areas downstream, SO14 - Providing housing that meets the needs of everybody, SO15 To focus new housing in the existing urban area, especially in Bolton town centre, SO16 To develop mixed communities which encourage community cohesion and ensure access for all to community and cultural facilities.
20. Bolton's Core Strategy Development Plan Document (2011) - H1 Healthy, P5 Accessibility, S1 Safe, CG1 Cleaner Greener, CG2 Sustainable Design and Construction, CG3 The Built Environment, CG4 Compatible Uses, SC1 Housing, TC5 Cultural Quarter, TC11 Design in Bolton Town centre, IPC1 Infrastructure and Planning Contributions and Appendix 3 - Car parking standards.
21. Bolton's Allocations Plan (2014) - P7AP Strategic Route Network

Other Material Considerations

22. Supplementary Planning Documents - Accessibility, Transport and Road Safety (October 2013), Infrastructure and Planning Contributions (July 2016), Affordable Housing (February 2013), General Design Principles (June 2015), Sustainable Design and Construction (October 2016), Building Bolton (2006), Public Realm Implementation Framework (2007)
23. National Planning Policy Framework (July 2021) - building a strong and competitive economy, ensuring the vitality of town centres, promoting sustainable transport, delivering a wide choice of high quality homes, requiring good design, promoting healthy communities, meeting the challenge of climate change, flood and coastal change, conserving and enhancing the natural environment, conserving and enhancing the historic environment.
24. Relevant National Planning Practice Guidance - Air Quality, Build To Rent, Climate Change, Conserving and Enhancing the Historic Environment, Design, Ensuring the Vitality of Town

centres, Environmental Impact Assessment, Flood Risk and Coastal Change, Health and Wellbeing, Housing, Land Affected By Contamination, Land Stability, Light Pollution, Natural Environment, Noise, Public Rights of Way, Planning Obligations, Renewable and Low Carbon Energy, Travel Plans, Transport Assessments and Statements, Use of Planning Conditions, Viability, Water Supply, Wastewater and Water Quality

25. The Setting of Heritage Assets: Historic Environment Good Practice Advice Note 3 in Planning (Historic England 2017) and Managing Significance in Decision-Taking in the Historic Environment: Historic Environment Good Practice Advice in Planning Note 2 (Historic England 2015), Conservation Principles for the Sustainable Management of the Historic Environment (Consultation Draft, 2017)
26. Planning (Listed Building and Conservation Areas) Act 1990 s. 66 - general duty as respects listed buildings in exercise of planning functions.
27. The Bolton Town Centre Framework was approved by Members of Bolton Council's Cabinet in September 2017 and is a material consideration in the determination of planning applications in Bolton Town centre, particularly within the identified intervention areas.
28. The Bolton Economy, Our Strategy For Growth 2016-2030
29. Consultation closed on Places For Everyone (formerly the Greater Manchester Spatial Framework) on 3rd October 2021. Bolton Council is one of the nine authorities that has committed to preparing a joint development plan. The site is not allocated within the draft of Places For Everyone. Only limited weight can be given to this draft plan; however, it is noted that it promotes the use of brownfield land and town centre regeneration.

Analysis

30. Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with the Development Plan unless material considerations indicate otherwise. Applications which are not in accordance with Development Plan policies taken as a whole should be refused unless material considerations justify granting permission. Similarly, proposals which accord with the Development Plan should be approved unless there are material considerations which would justify a refusal of permission. It is therefore necessary to decide whether this proposal is in accordance with the Development Plan as whole and then take account of other material considerations.
31. The main issues in relation to the proposal are:-
 -) the principle of the development
 -) impact on the character and appearance of the area
 -) impact on the setting of nearby listed buildings, the surrounding Conservation Areas and other Heritage Assets
 -) impact on housing provision
 -) impact on Bolton town centre and Bolton's economy
 -) impact on the road network
 -) impact on ecology, biodiversity and trees
 -) impact on living conditions, future occupiers and existing nearby uses
 -) impact on infrastructure, affordable housing and planning contributions
 -) impact on surface water drainage and flood risk
 -) impact on sustainable construction and energy management
 -) impact on crime reduction

-) impact on land stability, ground conditions and coal mining

The Principle of the Development

32. Policy TC5 of Bolton's Core Strategy states that the Council will redevelop Moor Lane bus station and Cheadle Square for a mixture of employment, cultural, residential and family orientated leisure uses. Bolton Council has had a longstanding ambition to redevelop and regenerate the former Moor Lane bus station site for a mixture of uses including new homes and to deliver an improved pedestrian route west from the Civic Core, most recently expressed within the Bolton Town Centre Framework approved by Members of Bolton Council's Cabinet in September 2017. Strategic Objective 4 and Policy TC5 of Bolton's Core Strategy promote the creation of a transformed and vibrant Bolton town centre, with residential uses playing a significant role in this regeneration.
33. Members will be aware that the Council is currently not able to demonstrate a 5-year supply of deliverable housing sites and that at between 3.3 and 3.7 years this represents a significant shortfall. The proposed development of 208 new homes including 82 affordable homes would significantly contribute to the Council's housing supply and Officers consider this should be given significant weight in the planning balance.
34. The proposed development would deliver on the objectives of the Core Strategy and the Town Centre Framework whilst also making a significant contribution to Bolton's housing supply shortfall.

Impact on the Character and Appearance of the Area

35. Strategic Objective 11 of the Core Strategy aims to conserve and enhance the best of Bolton's built heritage and landscapes and improve the quality of open spaces and the design of new buildings.
36. Core Strategy policy CG3 seeks to ensure that new development proposals contribute to good urban design. This has a number of elements including (amongst other things):
-) conserve and enhance local distinctiveness ensuring development has regard to the overall built character and landscape quality of the area
 -) compatibility with surrounding area – scale, massing, grain, form, architecture, local materials and landscape treatment
 -) conserve and enhance the heritage significance of heritage assets and area
 -) maintain and respect the landscape character of the surrounding countryside and its distinctiveness, being compatible with the nearby landscape character
37. Core Strategy Policy TC11 relates specifically to development in Bolton Town centre and states that the Council and its partners will protect, strengthen and enhance the distinctive townscape qualities of Bolton town centre. Development should conserve and enhance the special nature of the conservation areas and listed buildings; ensure that development along the gateways to the town centre enhances the townscape through the use of high quality design and improved street frontages and pedestrian permeability; ensure streets are designed in accordance with the Public Realm Implementation Framework to achieve a high standard of design which exhibits safety, consistency and accessibility, particularly for pedestrians, cyclists and users of public transport; require development to respect and enhance existing vistas - new architectural 'set pieces' will be supported where the design is of exemplary quality; have regard to the existing hierarchy of

built forms as indicated in the Building Bolton SPD; make efficient and effective use of land in the town centre due to the existing levels of high density development, ensuring that development provides adequate amenity space and privacy, or attractive public areas, where appropriate for the site.

38. Core Strategy Policy TC5 relates specifically to development within the area immediately west of Bolton of Le Mans Crescent; however, it refers primarily to the allocation of uses rather than seeking to promote or discourage particular forms of design or architecture.
39. The National Planning Policy Framework (July 2021) sets out the Government's planning policy requirements. Section 12, "Achieving Well-designed Places" identifies that the creation of high-quality buildings and places is fundamental to what the planning and development process should achieve. Good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities. Paragraph 130 further states that planning policies and decisions should ensure that developments are in accordance with the following:-
 -) will function well and add to the overall quality of the area, not just for the short term but over the lifetime of the development
 -) are visually attractive as a result of good architecture, layout and appropriate and effective landscaping
 -) are sympathetic to local character and history, including the surrounding built environment and landscape setting, while not preventing or discouraging appropriate innovation or change (such as increased densities)
 -) establish or maintain a strong sense of place, using the arrangement of streets, spaces, building types and materials to create attractive, welcoming and distinctive places to live, work and visit
 -) optimise the potential of the site to accommodate and sustain an appropriate amount and mix of development (including green and other public space) and support local facilities and transport networks
 -) create places that are safe, inclusive and accessible and which promote health and well-being, with a high standard of amenity for existing and future users, and where crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion and resilience
40. Paragraph 134 of the NPPF states 'In determining applications, great weight should be given to outstanding or innovative designs which promote high levels of sustainability or help raise the standard of design more generally in the area, so long as they fit in with the overall form and layout of their surroundings.
41. The Government published its National Design Guide on 1st October 2019 as part of the National Planning Practice Guidance.
42. Officers consider that Core Strategy policies CG3 and TC11 are consistent with the NPPF and can be given substantial weight in the consideration of design matters.
43. The Building Bolton SPD contains a detailed analysis of the existing built form of Bolton town

centre and highlights its local distinctiveness. Development proposals are very much welcome but are encouraged to work with rather than against this distinctiveness. It contains design guidance for a series of areas within the town centre, including the application site, and draws attention to matters such as urban form, scale, massing and detail. The application site falls entirely within the Cultural, University and Transport area and the Building Bolton SPD notes the absence of a clear urban focus and public amenity space, with Le Mans Crescent isolating the area from the Civic Core, and the dominance of out-of-town style developments and highway infrastructure. The SPD expects new developments to take their townscape design cues from the adjacent Civic Core.

44. As with other major development proposals within Bolton town centre, proposals at this site have been subject to engagement with the Design Review process via Places Matter (RIBA North West, Liverpool) on two separate occasions in June and August of this year. Officers have been closely involved in this process, attending the meetings with the panel and have been provided with the panel's written responses. Officers are satisfied that the Applicant's design team have demonstrated a high degree of engagement with the panel's comments and recommendations. The scheme has been developed as a result of the panel's suggestions below:-

-) flat roofs incorporated to the townhouses in order to avoid a domestic or suburban appearance in this important civic setting
-) layout of townhouses tightened to enclose courtyards
-) vehicle access route narrowed as far as possible with a single access point at Moor Lane
-) pick up / drop off facility at the end of the proposed east-west linear park removed
-) building line to be consistent and no building to edge forward of the Le Mans Crescent arches
-) more rigorous approach to the siting of the external terraces in order to maximise enclosure to Blackhorse Street and to the new east-west linear park

45. Other recommendations made by the design review panel or by the Council's Conservation and Design Officer have either been adopted or a robust justification has been provided as to why the Applicant's team takes a different view to that of the panel and these justifications have been accepted in full by Officers. It should be noted that the Panel did not raise any concerns or objections over the scale or siting of the two elements immediately to the rear of Le Mans Crescent.

46. Officers note that the application is supported by extensive analysis within the Design & Access Statement which clearly demonstrates that the development would not be unacceptably out of scale within its setting and would not intrude into existing views or the existing hierarchy of built form within the area. A degree of disparity of scale would take place – at six storeys for the apartment blocks adjacent to Moor Lane and three storeys for the central townhouse blocks there will be a noticeable change in scale within the development itself. Nevertheless, there are considered to be a number of factors which point towards the development being compatible with its surroundings in terms of scale - the surrounding townscape is far from uniform and already contains some buildings which exceed the height of their neighbours, including Marsden House to the north, the apartment block at the junction of Spa Road and Garside Street together with the Bolton Courts building. It should also be noted that buildings of a similar scale have been granted planning permission at the site known as "Westpoint" (at the rear of the Garside

Street block) and that this permission is still capable of being implemented. Officers therefore take the view that a certain degree of disparity of scale need not be unacceptable, particular where this is mitigated by good design and quality of materials. This is considered to be the case in relation to the two Moor Lane blocks and it is also noted that a generous amount of space has been provided around the two blocks in order to ensure that they will not appear cramped within their settings.

47. Turning to the centrally-located 44 dwellings, these are considered to represent a modern interpretation of a traditional terraced house. Officers entirely agree with the design review panel's comments that a traditional pitched roof or a gable-front would be inappropriate here, introducing an element of domesticity to this civic setting. Flat roofed buildings are not at all uncommon in Bolton town centre and the three storey scale of the building means that, when viewed from a human scale, the absence of a pitched roof will not appear out of character here.
48. The two blocks either side of the Job Centre take their points of reference from the civic core and arguably from their neighbour, which was itself designed to be consistent with the civic core. The consistency and compatibility is achieved by the adoption of a materials palette that is consistent with the civic core in terms of colour, the regularity of the fenestration and the splayed corners.
49. The transformational works proposed for the surrounding highways in terms of public realm improvements and the narrowing of Blackhorse Street will also significantly improve the character of the area and provide new residential frontages. The proposed development will begin to address some of area's shortcomings identified within the Building Bolton SPD by providing a tighter urban feel, public amenity space within the two new routes, more strongly linking the area with the Civic Core and reducing the dominance of highway infrastructure. In particular, the block located immediately to the rear of Le Mans Crescent is strongly influenced by the character and appearance of the Civic Core without being a pastiche. Servicing is appropriately located in areas lacking in prominence, maximising active frontages throughout the development.
50. Final materials will be subject to a condition, but the indicative information shows a range of materials compatible with the area and provides Officers with sufficient comfort that there is a direction of travel towards high quality materials. The level of detailing is high, including rustication, linear separation and coursing details.
51. The design quality of the scheme, the attention to detail, quality of materials indicated, substantial new planting, public realm improvements and the two new linear parks / pedestrian routes means that the proposal will have a positive and transformative effect on the character and appearance of the area, entirely consistent with the requirements of Policies TC11 and SO11 of Bolton's Core Strategy.

Impact on the Setting of Nearby Listed Buildings, the Surrounding Conservation Areas and other Heritage Assets

52. Strategic Objective 11 seeks to conserve and enhance the best of Bolton's built heritage and landscapes and improve the quality of open spaces and the design of new buildings.
53. Core Strategy policy CG3.3 states that the Council and its partners will seek to ensure that development should be compatible with the surrounding area, in terms of scale, massing, grain, form, architecture and landscape treatment. Policy CG3.4 aims to conserve and enhance the heritage significance of heritage assets and heritage areas recognising the importance of sites. Policy TC11 relates specifically to development in Bolton Town centre and states that the Council

and its partners will protect, strengthen and enhance the distinctive townscape qualities of Bolton town centre. Development should conserve and enhance the special nature of the conservation areas and listed buildings.

54. Chapter 16 of the NPPF "Conserving and enhancing the historic environment" emphasises at paragraph 185 that plans should set out a positive strategy for the conservation and enjoyment of the historic environment. In terms of proposals affecting heritage assets, Paragraph 194 states that during the determination process of application the applicant must describe the significance of any heritage assets affected, including any contribution made by their setting. Paragraph 195 further comments that local planning authorities should identify and assess the particular significance of any heritage assets that may be affected by a proposal taking account of the available evidence and any necessary expertise. The impact of the proposal on the significance of a non-designated heritage asset should be taken into account in determination of an application taking a balanced judgement as to the scale of the harm and the significance of the heritage asset.
55. Paragraph 199 of the NPPF states that 'when considering the impact of a proposed development on the significance of a designated heritage asset, great weight should be given to the asset's conservation (and the more important the asset, the greater the weight should be). This is irrespective of whether any potential harm amounts to substantial harm, total loss or less than substantial harm.'
56. Paragraph 196 states that 'where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal including, where appropriate, securing its optimal viable use'.
57. Paragraph 206 states that 'Local planning authorities should look for opportunities for new development within Conservation Areas and World Heritage Sites, and within the setting of heritage assets, to enhance or better reveal their significance. Proposals that preserve those elements of the setting that make a positive contribution to the asset (or which better reveal its significance) should be treated favourably'.
58. With specific reference to Listed Buildings it is noted that s. 66 of the Planning (Listed Building and Conservation Areas) Act 1990 states that when an LPA is "considering whether to grant planning permission...for development which affects a listed building or its setting, the local planning authority...shall have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses".
59. Section 72 of the Act requires the Local Planning Authority to pay special attention to the 'desirability of preserving or enhancing the character and appearance' of the designated areas. Consequently, it is important to consider the developments contribution to the townscape character of the conservation areas and the impact of the proposed development on this aspect of the wider historic environment.
60. No works are proposed to any listed buildings; however, the site is immediately to the rear of Le Mans Crescent, a Grade II Listed Building. There are other listed buildings within the vicinity of the site (such as the former Beales department stores and the K6 telephone kiosks outside the former Post Office on Deansgate) however these are considered to be so distant as to remain unaffected. Le Mans Crescent also lies within the Town Hall Conservation Area - however, the majority of the application site lies outside of any designated area and is considered to have little or no relationship with any other town centre Conservation Area or the remainder of the Town Hall Conservation Area due to the strong enclosure provided by the dominant western elevation

of Le Mans Crescent.

61. As noted in the Proposal section above, the application is supported by a Heritage Assessment carried out by Buttress. This has been considered by the Council's Conservation and Design and Officer. Consultation was also carried out with Historic England

Demolition

62. It is noted that the Town Centre Framework accepts the demolition of the single storey retail premises fronting Ashburner Street. Whilst this block lies within the Town Hall Conservation Area at its south-western extremity, the Building Bolton SPD does not consider these buildings to have any particular townscape value and the Town Hall Conservation Area Character Study considers their impact on the significance of the Conservation Area to be neutral. It is therefore considered that these buildings can be demolished without any harm to the significance of the Town Hall Conservation Area.

Impact on Bolton Town Hall

63. The Heritage Assessment carried out by Buttress concludes that the development would cause no change to the significance of Bolton Town Hall. No works are proposed to its fabric and there would be no direct or indirect impact on its setting due to the distance of the site from the Bolton Town Hall and the separation of the town hall and its setting caused by Le Mans Crescent. The Civic Centre creates a boundary to the immediate setting of the town hall and the site is not prominently visible from the Town Hall due to this boundary, save for glimpses through the arches.
64. Officers accept this assessment and conclude that the impact on Bolton Town Hall would be neutral and would cause no harm to the asset.

Impact on Le Mans Crescent

65. The Council's Conservation and Design Officer notes the way that building facades front onto streets adopting a regular rhythm of openings referencing the vertical proportions on adjacent buildings and at Le Mans Crescent in a restrained but elegant style. In their view, the design and layout has been carefully considered to provide a focus to the arches of Le Mans Crescent with buildings flanking the south side in a simple contemporary style. No objection is raised by the Council's Conservation and Design Officer in terms of the impact on the historic significance and special character of Le Mans Crescent.
66. No works are proposed to the fabric of Le Mans Crescent. The Heritage Assessment carried out by Buttress makes a distinction between the "setting" of Le Mans Crescent i.e. the land surrounding it and its "heritage setting" i.e. those parts of its setting which were purposefully designed to have an interdependent relationship with Le Mans Crescent and provide an enhancement to its significance. In this case, whilst all the land and buildings surrounding Le Mans Crescent should be considered to form part of its setting, only the land and buildings to its east can reasonably be considered to form its heritage setting. The application site is therefore part of the setting of Le Mans Crescent, but is not part of its heritage setting - which will remain unaffected. The proposed development will create change to the setting of Le Mans Crescent by way of proximity but the assessment concludes that change would be moderately beneficial in heritage terms. The Heritage Assessment considers the vacancy across the majority of the application site and the existing modern buildings that do not respond effectively to their context to be negative influences on the context of Le Mans Crescent and that the existing situation would be improved by the introduction of companion buildings to enclose long views and reinforce the urban grain. Reinstating structures of an appropriate scale and mass, particularly along the eastern boundary of the application site, will contribute to a development of moderate

beneficial impact for the significance of Le Mans Crescent and its setting and would cause no harm to the asset. As can be seen from the comments of the Conservation and Design Officer above, the Council's heritage advisor agrees with this assessment.

67. Historic England were consulted on the application but do not consider it to be necessary for them to provide any comments i.e. they are satisfied that the application can be determined in relation to heritage matters by way of local consultation with the Council's Conservation and Design Officer and with Greater Manchester Archaeological Advisory Service.
68. It is noted that Bolton & District Civic Trust consider that the Heritage Assessment prepared by Buttress misrepresents the impact of the proposed four-storey block on an element of the western elevation of Le Mans Crescent immediately to the north of the arches and made a representation to Historic England on these grounds. It is the Trust's view that this impact could be mitigated by setting back the fourth storey of the proposed new block by 3.5 metres and introducing a green wall and have provided an image to this end. Officers have considered this issue and note that Historic England have not changed their position as a result of this representation and also note that the Council's Conservation and Design Officer considers that the proposed layout will provide a better focus to the arches of the Crescent. Whilst this is to some degree a subjective matter, Officers take the view that both setting back the top storey of the proposed four-storey block and utilising a green wall would actually introduce two alien features into the view experienced from the west of Le Mans Crescent and are satisfied that the new building will not intrude upon the key features of the western elevation of Le Mans Crescent - the openings within the arches themselves. Views of certain other elements of Le Mans Crescent may be partially obscured, depending on exactly where the viewer stands, in the same way that the Job Centre partially obscures Le Mans Crescent from the perspective of someone standing in front of the Job Centre, but clear views of the Crescent will remain open, will be enhanced by the proposed works to form Cheadle Gardens and that Le Mans Crescent will very much remain the dominant feature of the Cheadle Square / Blackhorse Street area. For the avoidance of doubt, Members should be reassured that the front building line of the new block would be set further back from a notional line drawn from the edge of the northernmost arch of Le Mans Crescent.

Impact on the Town Hall Conservation Area

69. No objection is raised by the Council's Conservation and Design Officer in terms of the impact on the historic significance and special character of the Town Hall Conservation Area. The Heritage Assessment carried out by Buttress considers that the most relevant aspects of the Conservation Area that convey attributes of its significance are: urban grain, large plot sizes, fairly uniform scale, dominance of the Town Hall and tower, open and framed views created by the contrast of dense urban grain and openness of public space, and the civic quality of the stone and historic street surfaces. Officer's note that the tallest element of the proposed development - the six-storey blocks at Moor Lane - would be situated furthest away from the edge of the Conservation Area. The townhouses create a dip in scale in centre of the site and the four-storey blocks proposed immediately to the rear of Le Mans Crescent are considered to be of scale that is commensurate with the context, utilising materials that are complementary to the historic civic materials, without seeking to replicate them. It is considered that the design of these two blocks has been informed by the bay and fenestration pattern of Le Mans Crescent. Furthermore, the proposed public realm improvements, especially the new east-west pedestrian route linking the Le Mans Crescent arches with Moor Lane, will better integrate this currently disjointed site with the wider Town Hall Conservation Area. The vacancy of the Queen Street car parking is considered to be harmful to the significance of the Conservation Area whilst the impact of the buildings proposed for demolition is neutral. The vacancy of Moor Lane (indeed the character and appearance of the former Moor Lane bus station during its operational phase) are

considered to be harmful to the significance of the setting Conservation Area. The impact of the proposed development is considered to be beneficial and to cause less than substantial harm due to the introduction of a built context that responds positively to the existing assets and streetscape and encourages the re-use of currently under-used sites partially within the Town Hall Conservation Area and within its context.

70. For the reasons given above, Officers conclude that the proposed development will cause less than substantial harm to the significance of Le Mans Crescent and the Town Hall Conservation Area. Paragraph 202 of the NPPF states that this harm should be weighed against the public benefits of the proposal including, where appropriate, securing its optimum viable use. It is considered that the public benefits of the proposal include (but are not limited to):

-) the provision of 208 dwellings including 82 affordable homes would significantly contribute to the Council's housing supply. The Council can only currently demonstrate between a 3.3 and 3.7 years housing supply, which represents a significant shortfall
-) an increase in town centre living a mix of active ground floor uses and parking within residential blocks serving the residents and the promotion of the historic axis between Bolton Town Hall, Le Mans Crescent and Queens Park, all consistent with the objectives of the Town Centre Framework
-) pedestrian routes proposed across the site
-) this would be a major housing development on a brownfield/previously developed site. The Council is committed to brownfield delivery
-) a "ready-made customer base for Bolton Market, situated on its doorstep
-) construction jobs

It is considered that the public benefits associated with the proposed development outweigh the less-than-substantial harm that the proposed development would have on the significance of the conservation area, compliant with paragraph 202 of the NPPF.

Impact on Archaeological Assets

71. Greater Manchester Archaeological Advisory Service note that the application is supported by an archaeological desk-based assessment and a heritage statement by Buttress (August 2021), which have been undertaken in accordance with Section 16 Paragraph 194 of the NPPF. This describes the historical development of the application site, supported by key historic map extracts. Following consultation with GMAAS, it utilises GM Historic Environment Record data and grey literature reports on relevant pieces of archaeological investigation and key publications. The report is a comprehensive and useful document that allows a clear understanding of the potential archaeological interests and an appropriate strategy to mitigate the harm of development on archaeological interests. GMAAS concurs with the conclusions drawn from the archaeological assessment of the application area, and recommends that the site is subject to intrusive archaeological investigation in advance of development. This should comprise an initial phase of evaluation trenching, followed if necessary by open-area excavation and recording. This should be targeted on the footprint of the various iron forges and foundries and machine works that occupied the application site for much of the 19th century, together a former row of workers' houses along the west side of Blackhorse Street. As any such remains that do survive as below-ground remains are not going to be of national, but of regional or local significance,

GMAAS are content to see the archaeological investigation conditioned on the planning consent. A condition has been imposed to this effect.

72. The impact of the proposal on nearby heritage assets is considered to be acceptable and the proposal is therefore considered to comply with Policies SO11, CG3 and TC11 of Bolton's Core Strategy.

Impact on Housing Provision

73. Strategic Objective 14 of the Core Strategy seeks to provide housing that meets the needs of everybody, reflecting the needs of an ageing population and a growth in the number of households. Strategic Objective 15 of the Core Strategy seeks to focus new housing in the existing urban area, especially in Bolton town centre, Council-owned housing areas and in mixed-use developments on existing older industrial sites.
74. Core Strategy policy SC1 states that the Council will identify a range of housing sites for additional provision of 694 dwellings per annum between 2008 and 2026.
75. Chapter 5 of the NPPF (Feb 2019) "Delivering a sufficient supply of homes", states the Government's objective of significantly boosting the supply of housing within the UK. Paragraph 60 emphasises the importance of a sufficient amount and variety of land can come forward where it is needed, and paragraph 73 states that supplies of large numbers of homes can often be best achieved through larger scale development and that Local Planning Authorities should identify suitable locations for such development where this can help to meet identified needs in a sustainable way.
76. Paragraph 74 requires authorities to maintain a five-year deliverable supply of housing. Bolton currently cannot demonstrate a deliverable supply when measured against this target and is required to apply a 20% buffer on its housing requirement. The presumption in favour of sustainable development set out in NPPF paragraph 11 is therefore engaged.
77. The priority of delivering new housing within Bolton town centre cannot be underestimated. Bolton town centre is anticipated to contribute 10-20% of the Borough's housing supply over the period of the adopted Core Strategy. This is critical to the strategy which seeks to focus development on brownfield sites within the urban area to safeguard the rural outer areas of the borough, Green Belt and Protected Open Land in particular, from proposals for new housing development. Failure of delivery within Bolton town centre would therefore put these areas at greater risk.
78. In addition to the Core Strategy the significant role that new housing will have in the future of the town centre is endorsed by the Council's Bolton Town Centre Framework. It envisages a significant development of apartments on the site of former bus station with a mix of active ground floor uses and parking within residential block serving the town centre and residents. It is clear that the Framework sees a significant increase in the number of residential properties in Bolton town centre as a key factor of its approach. Regeneration here, specifically, residential-led regeneration, will be an important catalyst for the wider Bolton town centre.
79. Bolton cannot currently demonstrate a 5 year housing land supply which makes it vulnerable to speculative housing applications on open land. Approving and delivering schemes within the town centre is vital to being able to resist these and uphold the Council's decisions at appeal. In addition, Bolton has only achieved 60% of its housing target over the last 3 years and being below the 95% threshold, like many authorities, was required to publish a Housing Delivery Test Action Plan in August 2019. This Action Plan identifies that the Bolton Town Centre Framework

offers the opportunity to develop 2000+ units on brownfield sites as well as on publicly owned land and potential numbers are increasing as schemes become clearer. The application site therefore forms an important part of this Action Plan to deliver homes on brownfield land in the town centre area, at a highly accessible location at the heart of Bolton town centre and reasonably close to Bolton Interchange.

80. The provision of new residential development to meet housing need is a material consideration relevant to the determination of this application. The Moor Lane / Cheadle Square proposal provides the opportunity for the delivery of 208 new homes including 82 affordable homes in an accessible location a short distance from key town centre sites such as the Market Place and the rest of the retail core of Bolton town centre. It will make a significant and positive contribution to the Council's housing requirements and land supply, specifically within the town centre where accommodation is currently limited, consistent with Policies SO15 and SC1 of Bolton's Core Strategy. It will contribute to the Government's objective of significantly boosting the supply of housing, as required by the NPPF.
81. The Council's Growth and Regeneration Manager has noted that there is currently a limited housing market in Bolton town centre that focuses mainly on social rent and student accommodation. It is their view that the residential element of the proposed scheme will provide much needed housing. It will help to assist with the diversification of the housing market, attracting economically active residents to the town centre and to increase footfall and spend. This supports the identified need for town centre living and the development of brownfield sites as a priority.
82. In accordance with Policy IPC1 of Bolton's Core Strategy and the Infrastructure and Planning Contributions SPD, as the scheme proposes high quality improvements to the public realm, no contributions have been sought in the form of affordable housing.
83. The proposed development is considered to make a significant and positive contribution to the housing supply in Bolton, consistent with Policies SO15 and SC1 of Bolton's Core Strategy and contributing to the Government's objective of significantly boosting the supply of housing within the UK, as set out within the NPPF.

Impact on Bolton Town Centre and Bolton's Economy

84. Strategic Objective 3 seeks to ensure a 'Prosperous Bolton' by taking advantage of the economic opportunities presented by Bolton town centre and the M61 corridor, and ensure that these opportunities benefit everyone in Bolton, including those people living in the most deprived areas.
85. The Core Strategy notes that Bolton town centre is the principal location for employment in the whole borough with an emphasis on retailing, offices and leisure; it is subject to considerable development pressure, and its role within the borough is a vital one. It goes on to state as an aim that Bolton town centre will continue to be a vibrant mix of uses and will be the principal location for retailing, leisure, cultural and civic activities. It will make a significant contribution to the new jobs to be located in the borough over the plan period years, will be a main location for education, especially for those over 16 years old and will be one of the main locations for new housing development and a focus for transport infrastructure. Office developments will be concentrated in Bolton town centre, especially in the Bolton Innovation Zone, Merchant's Quarter and Church Wharf. The town centre will be the principal location for financial and professional services, and the Innovation Zone will be the location for knowledge-based employment, benefiting from the location of the university and colleges. Good urban design is recognised as being essential for producing attractive, high-quality, sustainable places in which people will

want to live, work and relax. It is of particular importance for creating a successful and flourishing town centre.

86. Policies SO3, SO4 and SO5 of Bolton's Core Strategy are strategic objectives within the "Prosperous Bolton" theme and seek to take advantage of the economic opportunities presented by Bolton town centre and ensure that these opportunities benefit everybody in Bolton, including those people living in the most deprived areas, together with creating a transformed and vibrant Bolton town centre and ensuring that Bolton takes full economic advantage of its location in the Greater Manchester City Region. Policy TC5 states that the Council will redevelop Moor Lane bus station and Cheadle Square for a mixture of employment, cultural, residential and family orientated leisure uses.
87. The NPPF states in paragraph 8 that the planning system has three overarching objectives, one of which is an economic objective. This is defined as being to help build a strong, responsive and competitive economy, by ensuring that sufficient land of the right types is available in the right places and at the right time to support growth, innovation and improved productivity; and by identifying and coordinating the provision of infrastructure. Chapter 6 Building a strong, competitive economy, indicates that planning should help create the conditions in which businesses can invest, expand and adapt. Paragraph 81 particularly emphasises that significant weight should be placed on the need to support economic growth and productivity, taking into account local business needs and wider opportunities for development, building on an area's strength and countering the weaknesses for the future. Paragraph 83 states the importance of recognising and addressing the specific locational requirements of different sectors. The NPPF states at paragraph 86 that planning decisions should support the role that town centres play at the heart of local communities, by taking a positive approach to their growth, management and adaptation. It also recognises that residential development often plays an important role in ensuring the vitality of centres and encourages residential development on appropriate sites.
88. It is considered that the Core Strategy is consistent with the NPPF and can be given significant weight.
89. The economic benefits of the development are considered to include the following:
-) the residents of the 208 new homes will create the opportunity for an expanded customer base for Bolton Market, right on its doorstep.
 -) Following consultation with market traders and feedback to the General Manager of Bolton Market, he concurs with the traders that the proposed residential development will be an asset to the market traders continued growth of their businesses. It was widely agreed by the market traders that the local area will profit when the new dwellings are built because newcomers to any developments in the Town Centre will likely start using the local businesses and services that occupy Bolton market during the build phase and upon occupation by new residents.
 -) there are longstanding ambitions of Bolton Council to bring forward comprehensive regeneration within the town, including the application site as a key component and catalyst for this regeneration. The objectives of these regenerative ambitions are to counter the deprivation currently experienced by local people in the area and to bring forward new opportunities to live, work and socialise in Bolton town centre.
 -) the proposals brought forward with this application will deliver significant investment in new homes, job opportunities, construction work, income to be spent in the local area, business

rates revenues and more.

-) improved commercial floorspace accommodating new jobs and footfall, together with the construction jobs and a major capital investment into Bolton.
-) these substantial socio-economic benefits will create improved living conditions for local people and encourage continued comprehensive regeneration within the local area, thus meeting the ambitions of the regeneration vision set by Bolton Council and for local people.
-) within the context of The Bolton Economy: Our Strategy for Growth 2016-2030 the Council has adopted a Town Centre Strategy including a masterplan framework and key intervention areas. These documents set out Bolton's ambition and vision to achieve a now £1.5bn regeneration of the town centre and sustain its immediate future to the benefit of the wider Borough and its residents, supported by £100m direct investment from the Council. The application site is included as a key intervention area and the proposal would deliver on the Town Centre Framework objectives for this area – a step change in the number of new homes in the area together with improvements to the River Croal.
-) the Town Centre Strategy and the proposed investment acknowledges the importance of the town centre, as well as the challenges faced and the opportunities presented, including the need to diversify beyond a traditional retail offer. A key element of these strategies is to increase the quantity and quality of residential development in the town centre attracting more people to generate demand through household spend and improve its vitality, viability and sustainability. The Civic and Retail Core of Bolton town centre is of strategic importance in achieving these objectives and supporting sustained growth of Bolton, including as a regional centre and within the wider Northern Powerhouse
-) positively transforming the image and profile of the area through a critical mass of development. Also ensuring a comprehensive regeneration approach to the site through strategic public investment alongside the private investment by the Applicant, leading to the opportunity for further inward investment in the rest of the town centre
-) the delivery of 208 dwellings where there is currently a limited market in Bolton town centre that focuses mainly on social rent and student accommodation. This PRS housing will attract economically active residents to the town centre and to increase footfall and spend
-) provision of new areas of public realm within the development and the significantly enhanced links between the civic core of the town centre, Moor Lane and Bolton Market.

90. It is noted that the Town Centre Framework promotes the following objectives for the Cheadle Square / Moor Lane area:-

-) significant development of apartments on the site of the former bus station aimed at town centre living
-) a mix of active ground floor uses and parking within residential blocks serving the residents
-) retention and promotion of the historic axis between Bolton Town Hall, Le Mans Crescent and Queens Park
-) pedestrian routes proposed across the site

-) promotion of the central location of this area and the grand civic architecture of Le Mans Crescent

91. It is considered that the proposed scheme delivers on all these objectives.
92. Planning Officers note the economic and social benefits of the proposal which are very significant material considerations in the determination of this application. By increasing the housing offer within Bolton town centre (in accordance with the Town Centre Framework), providing employment opportunities, improving the public realm and increasing investor confidence, the impact on Bolton town centre is considered to be significant and beneficial, in accordance with Policies SO3, SO4, SO5 and TC5 of the Core Strategy.

Impact on the Road Network

93. Core Strategy Strategic Objective 6 seeks to ensure that transport infrastructure supports all the aspects of the spatial vision and that new development is in accessible locations and makes the best use of existing infrastructure. In addition, Core Strategy Strategic Objective 9 aims to improve road safety by ensuring that neighbourhoods are attractive and well designed.
94. Core Strategy policy P5 and S1 seek to ensure that new development proposals take account of accessibility of transport prioritising pedestrians, cyclists, public transport users over other motorised vehicle users, design developments to be accessible by public transport, servicing arrangements, sufficient parking, transport needs of people with disabilities. Major trip generating developments would need to be supported by a Transport Assessment. Core Strategy policy S1 seeks to ensure that the Council and its partners will promote road safety in the design of new development and also target expenditure on road safety to locations with the worst safety record.
95. Appendix 3 of the Core Strategy provides car, cycle, motorcycle and disabled parking standards for a range of new development proposals. In addition, guidance contained within the Accessibility, Transport and Safety SPD covers a range of highways related matters including provision of facilities for people with disabilities, provision for pedestrians, cyclists, public transport and car parking. In addition, the SPD provides guidance on highway design, Transport Assessments/Statements, Travel Plans, Infrastructure provision and the means for securing such provision.
96. Chapter 9 of the NPPF relates to the promotion of sustainable transport. Paragraph 104 stresses the importance of development proposals addressing transportation issues at an early stage in a development in order to identify the potential impacts of a proposal and look at opportunities to reduce them together with promoting more sustainable methods of transport. Paragraph 105 states that the planning system should actively manage patterns of growth in support of these objectives, focusing on locations that will limit the need to travel offering a genuine choice of travel modes, with the overall aim being to reduce congestion and emissions. Paragraph 107 states that planning policies should support an appropriate mix of uses across an area, and within larger scale sites, minimise the number and length of journeys needed for employment, shopping, leisure, education and other activities.
97. Officers consider that whilst Core Strategy policies P5 and S1 were adopted prior to publication of the NPPF, they are consistent with the NPPF and can be given significant weight.

Traffic Generated by the Proposed Development

98. The Council's Highway Engineers note that the proposal will benefit from 57 parking spaces along with new landscaping and public realm proposals together with highway improvement works.

The Applicant's transport consultant (SK Transport Planning Ltd) has submitted a transport assessment/travel plan in order to justify this development proposal from transport and highways impact perspective. The likely traffic from the proposed development has also been modelled through the Bolton Town Centre AIMSUM model developed by Transport for Greater Manchester. The transport work therefore appears robust in its assessment of the traffic impact and the rationale used in its preparation appears plausible and justifiable. According to the Transport Assessment and modelling work, the level of traffic impact associated with development will be marginal and should be accommodated with minimal additional impact on road safety, amenity and the operational capacity of the surrounding highway network. It is therefore compliant with the requirements contained within current planning policy guidance.

Parking

99. Whilst the 57 off-road parking spaces proposed falls below the Council's maximum parking standards for residential and commercial development, the transport work demonstrates that the site is highly accessible to sustainable modes of transportation and the town centre of Bolton and its amenities. This provides a justification for the level of parking provision proposed to support this development and again meets the recommendations for encouraging sustainable transport contained within current planning policy guidance.

New Road

100. The proposed vehicular access route to the central part of the site will be from Blackhorse Street, egressing onto Moor Lane via a one-way system. The junction onto Moor Lane has been modelled and indicates that with the level of additional development traffic the junction should work without any undue concerns. This section of access road could be brought forward for adoption and will require Traffic Regulation Orders to enforce the one-way system, the extension of the 20mph speed limit and parking restrictions in order to prevent obstruction. The Transport Assessment includes tracking analysis using industry standard software in order to demonstrate accessibility for refuse collection and servicing. This analysis appears to indicate that accessibility for these size of vehicles is achievable.

Stopping Up

101. The Transport Assessment includes a plan indicating areas of highway to be improved, stopped-up or adopted, including a reference to the stopping-up of redundant sections of highway and Back Barn Street and the narrowing parts of Blackhorse Street under Section 247 and 253 of the Town and Country Planning Act. This will need to be pursued by the Applicant through the National Casework Team for the Department for Transport. The stopping-up of sections of Blackhorse Street will be reliant on the proposed highway improvement scheme for that location which will be implemented by the Local Highway Authority. The red edge of the site includes the signal junction access to the market car park which is to be relocated by the Council in order to accommodate this scheme and relevant colleagues in Asset Management and Highway Design are aware of this proposal.
102. Highway Engineers confirm that they have no objection to the proposed development. The impact on the road network is considered to be acceptable in terms of vehicle movements and to be beneficial in terms of an improved pedestrian environment and improved permeability and to comply with policies P5 and S1 of Bolton's Core Strategy and the Accessibility, Transport and Road Safety SPD.

Impact on Ecology, Biodiversity and Trees

103. The aim of Core Strategy Strategic Objective 12 is to protect and enhance Bolton's biodiversity. Core Strategy policy CG1.2 seeks to ensure that the Council and its partners will safeguard and enhance biodiversity in the borough by protecting sites of urban biodiversity including trees,

woodland and hedgerows from adverse development and improving the quality and interconnectivity of wildlife corridors and habitats.

104. Chapter 15 of the NPPF "Conserving and enhancing the natural environment" states at Paragraph 174 that planning decisions should contribute to and enhance the natural and local environment. This can be achieved by providing net gains for biodiversity and by preventing new and existing development from contributing to, being put at unacceptable risk from, or being adversely affected by, unacceptable levels of soil, air, water or noise pollution or land instability. This goes on to state that wherever possible, development should help to improve the local environment such as air and water quality. Under the subheading Habitats and biodiversity, developments should protect and enhance biodiversity and geodiversity (Paragraph 179). Paragraph 180 further comments that opportunities to incorporate biodiversity improvements in and around developments should be encouraged, especially where this can secure measurable net gains for biodiversity.
105. Officers consider that Core Strategy Strategic Objective 12 and Core Strategy policy CG1.1 are consistent with the NPPF and can be given significant weight. Additionally, the relevant sections of the NPPF provide a more detailed consideration of biodiversity and ecological matters and should be taken into account.
106. The application is supported by a Preliminary Ecological Appraisal, a Nocturnal Bat Survey Report and a Biodiversity Metric Report, all prepared by E3P. Greater Manchester Ecology Unit and the Council's Greenspace Officers were consulted on the application and their responses have been taken into account in this section of the report.

Invasive Species

107. Consultation with Greater Manchester Ecology Unit did not identify any records of invasive species within a 1 km search area and no invasive species were identified during the survey.

Bats

108. Only one group of buildings is proposed to be demolished, the five single-storey retail units fronting Ashburner Street and the submitted Nocturnal Bat Survey has assessed the likelihood of the building's use or occupation by bats. Greater Manchester Ecology Unit consider that the survey appears to have used reasonable effort to inspect the structures both internally and externally for the presence of bats and assessed the likelihood that bats would use the structure at other times for roosting. The building inspection found no evidence of recent or historic usage of bats. It was concluded that the structure has low potential to support bats at other times and consequently further activity surveys were required. The activity surveys were undertaken at an appropriate time of year, in suitable weather conditions and an adequate number of surveyors were present. It appears that reasonable effort was undertaken. No bats were observed emerging or re-entering the property and although bats were recorded elsewhere in the locality attesting to the suitability of the surveys, GMEU have no reason to contradict the findings of the survey and it is therefore considered that the buildings can be demolished without further work and without harm to protected species. That said, bats and their roosts, even when unoccupied, are protected at all times even when disturbance occurs as the result of an otherwise lawful activity such as a planning permission - it is therefore suggested that the Applicant's attention is drawn to the legislation via an informative attached to any permission if granted. If bats are found or suspected at any time work should cease until advice has been sought and implemented from the licensed ecologist.

Nesting Birds

109. GMEU advise that clearance of the trees and shrubs on site should avoid the bird breeding

season (March – August inclusive) unless it can be demonstrated that no nesting activity is present as all nesting birds are protected while on the nest with eggs or dependant young by way of the Wildlife & Countryside Act 1981. In this case, GMEU are satisfied that an informative can be used to draw the applicant's attention to this matter.

Existing Trees

110. The Council's Tree and Woodland Manager notes that existing trees at the site located around the Queen Street car park have been surveyed in the manner recommended by BS5837. The report notes that the trees are a mix of Low Value (Category C) and Moderate Value / Quality (Category B) specimens with a single Category U specimen present (T5). No category A trees were scheduled.

111. The proposals show the removal of the following trees:

T1 Whitebeam (Category B) a very prominent tree. The tree is to be removed from northern boundary as a result of conflicts with the proposed development footprint.

T2 Norway Maple (Category B) is also a very prominent tree. This tree to be removed from northern boundary as a result of conflicts with the proposed development footprint.

T4 Whitebeam (Category C) is a prominent tree but has been given a lower classification due to its poor structural form and lean. This tree to be removed from northern boundary as a result of conflicts with the proposed development footprint.

T5 (Elder) - Category U. Tree to be removed from north-western corner owing to poor form and conflicts with proposed development footprint.

G1 (mixed shrubs and self-seeded trees) - Category C. Site boundary shrub and self-seeded tree cover to be removed as a result of the proposed layout configuration.

G2 - partial (mixed) - Category C. Portion of eastern boundary shrub and self-seeded tree cover to be removed as a result of the proposed layout configuration.

112. The Arboricultural Impact Assessment notes that tree group G3, aligning the southern site boundary, will be retained, maintaining a degree of screening and canopy coverage from the adjoining car park area south of the site and retention of these trees is welcomed and supported. Extreme care should be taken where any resurfacing within the indicated root protection zones of the trees takes place. A Rowan tree is shown for retention on the Tree Removals and Retention Plan; however, the Applicant has since confirmed that this will in fact need to be removed as the proximity to the new building is too close for ensure a healthy habitat, the tree is located in the construction zone for the building it will need to be removed in order to implemented the new public realm scheme

113. The proposals will therefore impact on all the on-site trees within the red line boundary with the only trees within the assessment that are shown to be retained are the trees are group G3 which are all off-site trees. If the application is to be approved there will be a requirement for tree planting in mitigation of trees lost to the development. The indicative landscaping scheme provides for 50 new trees and the Council's Landscape Regeneration Manager notes that this is only a planting strategy drawing at present rather than a detailed planting plan, therefore they cannot comment specifically on the suitability of the plant and tree species listed at this time. Nevertheless, they are satisfied that this can be resolved via a condition to submit detailed planting plans/soft works specifications in due course.

114. Officers consider that whilst the trees around the Queen Street car park will be lost to the proposed development, this will be compensated for by the proposed landscaping which - whilst currently in only indicative form - demonstrates a direction of travel that will result in a beneficial outcome provided that a detailed landscaping scheme is required to be submitted. The good

advice of the Council's Landscape Regeneration Manager will be communicated to the Applicant so that it can be taken into account in the detailed design. Other conditions recommended by the Council's Tree and Woodland Manager requiring the protection of trees to be retained will be imposed in full.

115. The impact on trees is considered to be acceptable.

Biodiversity Gains

116. National and local planning policy encourages developments to take opportunities to incorporate biodiversity improvements in and around developments, especially where this can secure measurable net gains for biodiversity. At present, there is no clearly defined target for such gains - if a development will compensate for any biodiversity harm that it causes whilst making the situation better, even by a small but measurable amount - then this is generally considered to be acceptable. The forthcoming Environment Bill will set a target of 10% improvement over the existing situation for all development.

117. The submitted Biodiversity Metric Report suggests that the development, together with its proposed landscaping, public realm and other enhancements, will lead to a biodiversity gain of 262% - which is clearly far in excess of the 10% improvement target likely to be brought into force by the Environment Bill. Both GMEU and this Council's Greenspace Officers consider this 262% gain to be somewhat overstated - GMEU note that it has not been calculated in accordance with the methodology adopted by all ten Greater Manchester authorities (though it is in line with other accepted methodologies) and the Council's Greenspace Officers point to a degree of "double-counting" due to including the retention of trees that are outside the application site or are in reality to be removed. Nonetheless, both consultees accept that the proposals will result in a significant uplift in biodiversity relative to the existing situation.

118. Officers take the view that calculation of biodiversity gain is a relatively new and evolving practice and consider that, notwithstanding the above, it is clear that the proposed landscaping, public realm and other enhancements, will lead to a significant improvement in biodiversity at the site. Other than the group of trees around the Queen Street car park, the site can be considered to be almost sterile in terms of biodiversity and the works associated with the development will significantly improve this situation. Conditions will be imposed to ensure the delivery of the proposed landscaping together with the provision of bat and bird boxes within the buildings themselves. Subject to such a condition, the scheme is considered to have the potential to achieve a biodiversity net gain, which aligns with emerging government guidance and the recently announced Environment Bill.

119. For the reasons above, the impact of the proposal on ecology, biodiversity and trees is considered to be acceptable and the proposal would lead to biodiversity gain, compliant with Core Strategy Strategic Objective 12, Core Strategy Policy CG1.1 and the relevant national policies.

Impact on Living Conditions, Future Occupiers and Existing Nearby Uses

120. Core Strategy policy CG4 seeks to ensure that new development proposals are compatible with surrounding land uses and occupiers, protecting amenity, privacy, safety and security. It also seeks to ensure that development does not generate unacceptable nuisance by way of odours, fumes, noise or light pollution nor cause detrimental impacts upon water, ground or air quality. Development proposals on land affected by contamination or ground instability must include an assessment of the extent of these issues and any possible risks. Development will only be permitted where the land is, or is made suitable for the proposed use.

121. The NPPF contains in paragraph 8 an environmental objective that development should minimise pollution. Paragraph 174 states that planning decisions should prevent new and existing development from contributing to, being put at unacceptable risk from, or being adversely affected by, unacceptable levels of soil, air, water or noise pollution. Development should, wherever possible, help to improve local environmental conditions such as air and water quality.
122. Paragraph 188 of the NPPF states that "the focus of planning policies and decisions should be on whether proposed development is an acceptable use of land, rather than the control of processes or emissions (where these are subject to separate pollution control regimes). Planning decisions should assume that these regimes will operate effectively. Equally, where a planning decision has been made on a particular development, the planning issues should not be revisited through the permitting regimes operated by pollution control authorities.
123. Officers consider that Core Strategy policy CG4 is consistent with the NPPF and has significant weight in the determination of this application as has the NPPF guidance itself.
124. The key issues are considered to be noise and pollution from the nearby transport infrastructure and potentially from evening activities such as those at the Victoria Hall, together with the living conditions experienced by future occupants in terms of internal layout and outlook.

Noise

125. Paragraph 185 of the NPPF states that planning decisions should ensure that new development is appropriate for its location taking into account the likely and cumulative effects of pollution (including noise) on health, living conditions and the natural environment, as well as the potential sensitivity of the site or the wider area to impacts that could arise from the development. They should mitigate and reduce to a minimum potential adverse impacts resulting from noise from new development and avoid noise giving rise to significant adverse impacts on health and the quality of life
126. Paragraph 187 states that planning decisions should ensure that new development can be integrated effectively with existing businesses and community facilities such as places of worship, pubs, music venues and sports clubs. Existing businesses and facilities should not have unreasonable restrictions placed on them as a result of development permitted after they were established. Where the operation of an existing business or community facility could have a significant adverse effect on new development (including changes of use) in its vicinity, the applicant (or 'agent of change') should be required to provide suitable mitigation before the development has been completed.
127. The application is supported by a Noise Impact Assessment prepared by Hann Tucker Associates. The survey recognises that the dominant noise sources around the proposed development were noted to be road traffic noise from Moor Lane and Blackhorse Street, with intermittent noise from deliveries to Bolton market, which also has an associated refuse compound and waste compactor (though these are to be moved, see "Odour" section below). In addition to this, plant noise was audible at roof level from the Job Centre building at the east of the site, this was not audible over road traffic noise during the daytime but was clearly audible during the night-time attended survey.
128. Officers note that, in comparison to other parts of Bolton town centre such as Bradshawgate and Churchgate, the application site area lacks genuine night-time uses (i.e. after midnight), nor is it used as a main route between such uses and that the area is actually exceptionally quiet during the night-time hours, relative to parts of other town and city centres. The Noise Impact

Assessment is consistent with this characterisation, noting that the Hen and Chickens was operational with busy outdoor seating and low-level amplified music playing, but was not audible at the development site, nor was any noise associated with the Greyhound public house. The Albion public house has been closed for some time and is unlikely to reopen, however the Noise Impact Assessment still takes the potential impacts of a reopened Albion into account.

129. The Noise Impact Assessment is still in the process of being considered by the Council's Pollution Control Officers who have some questions over the proposed approach to mechanical ventilation. Officers are fully aware of this process and are entirely satisfied that the unresolved matters do not relate to the principle of the development i.e. the noise environment in the vicinity of the application site is not capable of preventing residential development in principle, however it may affect the final wording of any relevant condition. Further progress on this matter will be reported at the meeting.
130. For the reasons given above, it is considered that the noise sensitive uses proposed (residential) can be accommodated at this location without harm to the living conditions of future occupants and without placing unreasonable burdens or restrictions on the ability of existing evening economy uses to operate.

Impact on Nearby Uses

131. The surrounding uses are primarily commercial or administrative, with the exception of Paderborn Court on the other side of Moor Lane. The Construction Environmental Management Plan condition required by Pollution Control colleagues is considered to be sufficient to ensure that, during the demolition and construction phase, the living conditions of nearby residential uses are not unacceptably affected and existing business do not have unreasonable restrictions placed on their ability to operate. During the operational phase, it is not considered that, subject to conditions relating to the control of plant noise, any of the proposed uses are capable of having any significant negative impact on existing nearby uses. The uses proposed – primarily residential, with limited commercial space – are all entirely typical town centre uses and already exist comfortably elsewhere within Bolton town centre.

Living Conditions of Future Occupiers

132. Mindful of the previous comments of elected members in this regard, Officers have ensured that all units would be compliant with the national standards for internal space and are satisfied that the internal layouts are acceptable and that an appropriate range and mix of dwelling types would be provided. Officers also note that the majority of units would have access to their own private amenity space, whether this is a rear garden, a balcony or a recessed terraced with all units having access to shared the shared amenity space created by the two linear parks. The close proximity of the site to the exceptionally good provision of amenity space at Queens Park should also be taken into account. As Officers have had significant involvement in the design process from the outset, they are satisfied that great care and attention has been given to the efficiency of the internal space, avoiding "wasted" and unused living space such as halls or internal circulation space.

Air Quality

133. The National Planning Practice Guidance contains a section on Air Quality and provides considerations to be addressed during the planning process, detailing the target emission reductions that the UK have committed to and the implications air quality can have on public health, habitats and biodiversity. Planning applications should have regard to the current and future air quality and assess any changes that might significantly change the air quality during the construction or operational phases. It states that air quality assessments should be proportionate to the nature and scale of the development proposed and the potential impacts,

taking into account existing air quality conditions, ideally with the scope agreed with the Local Authority. The assessment should provide details of any proposed mitigation if required in order to make the development acceptable in air quality terms.

134. An Air Quality Assessment was prepared by Redmore Environmental to ascertain the existing air quality situation and the potential demolition, construction and operational impacts. Potential air quality impacts from fugitive dust emissions during the construction phase were assessed as a result of demolition, earthworks, construction and trackout activities. The assessment considers that the use of good practice control measures would provide suitable mitigation for a development of this size and nature and reduce potential impacts to an acceptable level. Due to the proximity of Moor Lane there is the potential for the exposure of future residents to elevated pollution levels. Dispersion modelling was therefore undertaken in order to predict concentrations across the proposed development site as a result of emissions from the highway network. Results were subsequently verified using local monitoring data. The dispersion modelling assessment indicated that predicted pollution levels were below the relevant criteria at all locations across the development. The site is therefore considered suitable for the proposed use from an air quality perspective.
135. Pollution Control Officers are still in the process of considering the air quality assessment, however its findings appear to be reasonable. Conditions could be imposed to require a construction management plan to ensure that dust is controlled during the construction phase and limits on emissions from any gas boilers associated with the development will mitigate the impact of the development on air quality.

Odour

136. Pollution Control colleagues initially raised concerns over the potential for odours associated with the waste compound of Bolton Market to affect the living conditions of future occupiers. However, Economic Development colleagues confirm that the waste compound will be moved to the southern side of Bolton Market early in the new year. Pollution Control advise that this removes their concerns, subject to a condition that this be carried out prior to the occupation of any of the dwellings.
137. In summary, the proposal complies with the requirements of both the development plan and NPPF in terms of being compatible with surrounding land uses and occupiers and protecting amenity.

Impact on Infrastructure, Affordable Housing and Planning Contributions

138. Core Strategy policy H1 seeks to ensure that new development proposals contribute appropriately through planning contributions to meet the health needs that they generate.
139. Core Strategy policy IPC1 states that the Council will seek to ensure that developers make reasonable provision or contribute towards the cost of appropriate physical, social and green infrastructure which are required by the proposed development and/or to mitigate the impact of the development to ensure the development is made acceptable in planning terms and achieves the objective of sustainable development. For new residential development specific contributions are generally sought for affordable housing, open space provision and maintenance, health and well-being, education and community facilities. For major development within Bolton town centre with a non-residential element, the policy requires contributions for public realm improvements. Contributions will apply unless it can be demonstrated by the applicant that the scheme would not be viable if contributions were sought or offered. However, Policy IPC1 also states that "within Bolton town centre it is anticipated that the Council will primarily seek contributions to secure high-quality improvements to the public realm".

140. The application contains details of transformative improvements to the public realm within and adjacent to the application together with the delivery of two linear parks, one of which would partially deliver on the long standing aim of providing a link between the civic core and Queens Park.

141. As these transformational improvements to the public realm will be required by conditions, the proposed development is considered to be fully compliant with the Council's approach to public realm improvements as set out within Policy IPC1 of the Core Strategy and accordingly no further developer contributions are sought.

Impact on Surface Water Drainage and Flood Risk

142. Strategic Objective 13 aims to reduce the likelihood and manage the impacts of flooding in Bolton, and to minimise potential flooding to areas downstream.

143. Core Strategy policy CG1.5 seeks to ensure that the Council and its partners will seek to reduce the risk of flooding in Bolton and other areas downstream by minimising water run-off from new development and ensuring a sequential approach is followed, concentrating new development in areas of lowest flood risk.

144. Paragraph 167 of the NPPF states that when determining any planning applications, local planning authorities should ensure that flood risk is not increased elsewhere and that where appropriate, applications should be supported by a site-specific flood-risk assessment. Paragraph 169 states that major developments should incorporate sustainable drainage systems unless there is clear evidence that this would be inappropriate. The systems used should take account of advice from the lead local flood authority, have appropriate proposed minimum operational standards, have maintenance arrangements in place to ensure an acceptable standard of operation for the lifetime of the development and where possible, provide multifunctional benefits.

145. Officers consider that both Core Strategy policy SO1 and CG1 are consistent with the NPPF and can be given substantial weight.

146. The applicant has submitted a Flood Risk Assessment and Drainage Strategy which aim to ensure compliance with the requirements of the NPPF, Core Strategy policy CG1 and the Sustainable Design and Construction SPD.

Flood Risk

147. The entire site is located in flood zone 1, with therefore the lowest risk of flooding from fluvial sources. This means that it is not necessary to apply the sequential or exception tests.

148. The development is therefore considered to be acceptable in principle from a flood risk perspective, provided that the development takes reasonable measures to reduce flood risk elsewhere via the adoption of sustainable urban drainage measures.

Surface Water Runoff

149. The Council's policies seek to ensure a minimum 50% betterment in flow rates from a newly developed brownfield site – in other words, that the rate of water leaving the site should be at least 50% lower than the previous situation. The policy is also to seek discharge as high up the drainage hierarchy as is reasonably possible:-

-) discharge into the ground via infiltration

-) to a nearby surface water body such as a river, culverted river or other watercourse
-) to a dedicated surface water sewer, highway drain, or another drainage system that takes only surface water
-) to a combined sewer that takes both surface water and foul flows

150. At this stage, it is not clear which of the four above options will be chosen. Infiltration may potentially be possible but it will require further investigation into the permeability of the ground below the site - if this is mostly clay, in common with much of Bolton town centre, infiltration will not be appropriate. As to the second preferred option, the River Croal is located 153 metres to the north and is therefore not considered to be an appropriate option for direct drainage. The most likely options are therefore either infiltration or a connection to the surface water drain at Blackhorse Street, or possibly a combination of both, depending on the information provided by further survey work.

151. In terms of achieving the 50% reduction in flow speeds compared with the existing situation, the Applicant is proposing an engineered solution ("grey SuDS") in the form of underground attenuation tanks rather than a more natural solution that is capable of delivering multi-functional benefits ("green SuDS"). Officers note that national policy encourages the use of sustainable urban drainage systems that deliver multi-functional benefits "where possible" and that both United Utilities and Bolton & District Civic Trust consider that the proposal in its current form represents a missed opportunity to properly integrate the proposed landscaping and public realm improvements with the proposed solution for managing surface water in order to deliver multi-functional benefits.

152. In reaching a view on the issue of what form of SuDS might be appropriate here, Officers consider it important to take into account the reason why the Government encourages forms of SuDS which deliver multi-functional benefits over those which do not, even in cases where both systems would equally attenuate surface water flows from the site. Because of the nature of those multi-functional benefits "green" forms of SuDS (such as ponds and swales etc) have the potential to deliver biodiversity, public amenity and visual improvements together with enhancing the setting of a development. However, in this instance, for reasons set out in the section above relating to biodiversity, character and appearance and other parts of this report, the development would deliver substantial improvements in terms of biodiversity, the amenity of residents and visitors and to the character and appearance of the area in any case, even with the option of underground attenuation tanks. Furthermore, it is noted that paragraphs 50 to 86 of the national planning policy guidance on flood risk state that, when deciding whether or not a particular form of sustainable urban drainage system is possible or indeed appropriate, it is entirely reasonable for Local Planning Authorities to take matters such as cost (including construction, maintenance, ownership and opportunity costs). The Applicant has provided a justification for the choice of a "grey" SuDS option described in these terms and this has been accepted by Officers.

153. Subject to a condition that infiltration be fully discounted at the detailed design stage, the proposed development is considered to fully comply with national and local flood risk policies and would deliver biodiversity, public amenity and visual improvements in other ways, even if a "grey" SuDS option were to be adopted.

Impact on Sustainable Construction and Energy Management

154. Strategic Objective 10 seeks to minimise Bolton's contribution to climate change and mitigate and adapt to its adverse effects.

155. Core Strategy policy CG2 seeks to ensure that new development proposals contribute to

sustainable development, being located and designed to mitigate any adverse effects of the development and adapt to climate change by incorporating high standards of sustainable design and construction principles. There is a requirement on developments of 5 residential units or more or where proposals result in the creation of 500 sq. m of non-residential floorspace to achieve:

- (i) a Level 3 of the Code for Sustainable Homes or the "very good" BREEAM standard;
- (ii) Incorporate decentralised, renewable or low carbon energy sources to reduce CO2 emissions of predicted regulated and unregulated energy use by at least 10%;
- (iii) Demonstrate the sustainable management of surface water run-off from developments.

156. Further guidance is contained within the Sustainable Design and Construction SPD. This document has been through the relevant public consultation process and is now a fully adopted SPD.

157. The NPPF states that the purpose of the planning system is to contribute to the achievement of sustainable development which can be summarised as meeting the needs of the present without compromising the ability of future generations to meet their own needs.

158. Officers consider that Core Strategy policy CG2 carries substantial weight in the assessment of the environmental sustainability of the proposed development. Guidance contained both within the NPPF and also the Council's Sustainability SPD also carry significant weight as they provide additional detail to guide the assessment of planning applications.

159. The site is directly adjacent to the Civic & Retail Core of Bolton town centre and provides all residents and users of the site with easy access to a wide variety of amenities, leisure facilities and employment opportunities. It is approximately 10 minutes' walk to and from Bolton's transport interchange and accessibility will be improved via the proposed transformative public realm works.

160. The application is supported by an Energy Statement prepared by Ridge which assesses the proposed from a sustainable construction and energy management perspective. The statement discounts biomass, biogas, wind, ground sources, combined heat and power and solar thermal for justified reasons but considers that air source heat pumps and photovoltaics may be appropriate in order to reduce the overall CO2 emissions of the scheme. The statement goes on to note that passive measures (location, orientation etc) together with a fabric-first "best practice" approach are unlikely meet the Council's 10% betterment target over current Building Regulation standards on their own and therefore other measures will be required. As noted above, air source heat pumps and photovoltaics are considered to be appropriate solutions and are therefore proposed to be incorporated at the site to meet the Council's policy requirement. On this last point, the roof masterplan documents submitted indicate the potential for extensive photovoltaic coverage for available roof planes within the development via the use of flat roofs.

161. Officers are satisfied that the Applicant is committed towards the incorporation of sustainable design and construction methods to achieving wherever feasible an exceedance of national and local policy requirements in respect of energy efficiency and carbon emission reduction targets. Further details are proposed to be conditioned, including provision for the charging of electric vehicles, to ensure these measures are duly considered and as the design evolves. It is therefore considered that the proposal would comply with the relevant development plan policy, guidance in NPPF and the Sustainable Design and Construction SPD.

Impact on Crime Reduction

162. Section 17 of the Crime and Disorder Act 1998 requires all local authorities to exercise their functions with due regard to their likely effect on crime and disorder, and to do all they reasonably can to prevent crime and disorder. The prevention of crime and the enhancement of community safety are therefore material considerations that a local authority should take into account when exercising its planning functions under Town and Country Planning legislation.
163. Bolton's Core Strategy notes that the town's recorded crime levels are reducing but perception of crime remains high. The Core Strategy's most important direct contribution to reducing crime and the fear of crime is through the design of new development. A contributory factor to improving safety is to create and sustain a sense of place that enables people to identify with their surroundings or locality. This can be achieved through appropriate layout, high quality architecture, good lighting and landscape design. In considering design, the Council takes into account national advice as currently set out in the Government publication "Safer Places – the Planning System and Crime Prevention." Ensuring that the whole of the civic and retail core is safe and attractive for pedestrians will be a priority.
164. Policy SO9 of Bolton's Core Strategy is a strategic objective and seeks to, amongst other things, reduce crime and the fear of crime, by ensuring that neighbourhoods are attractive and well designed. Policy S1 is part of the "Safe Bolton" Sustainable Community Strategy theme and states that the Council and its partners will ensure that the design of new development takes into account the need to reduce crime and the fear of crime. Policy CG4 of Bolton's Core Strategy seeks to ensure that new development protects, amongst other things, safety and security.
165. The National Planning Policy Framework states that planning decisions should aim to achieve places which are safe and accessible, so that crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion – for example through the use of clear and legible pedestrian routes, and high-quality public space, which encourage the active and continual use of public areas.
166. The application is supported by a Crime Impact Statement, carried out by Greater Manchester Police's Design For Security team and issued in November 2020. GM Police note a number of positive aspects of the scheme:-
-) the mix of uses within the scheme, promoting activity and informal surveillance in appropriate places
 -) increasing the resident population of the town centre
 -) variation in the type and size of the proposed dwellings
 -) a distinction between public, semi-private and private spaces
 -) provision of secure car parking
 -) provision of some private amenity space
 -) improving the public realm in and around the development and creating spaces for the public to enjoy
 -) active managements of the development by Bolton At Home

167. The Crime Impact Statement goes on to recommend that the scheme should achieve Secured By Design accreditation, required by a condition. It should also be noted that the plans have been amended to show a 1.2 metre railing running along the boundary between the new east-west pedestrian route and the Courts site.

168. The development is considered to comply with Policies SO9 and S1 of Bolton's Core Strategy.

Impact on Land Stability, Ground Conditions and Coal Mining

169. Core Strategy policy CG4 seeks to ensure that new development proposals on land affected by contamination or ground instability must include an assessment of the extent of these issues and any possible risks. Development will only be permitted where the land is or is made suitable for the proposed use.

170. Paragraph 174 of the NPPF states that planning decisions should contribute to and enhance the natural and local environment by preventing new and existing development from contributing to, being put at unacceptable risk from, or being adversely affected by, unacceptable levels of soil, air, water or noise pollution or land instability. They should also contribute by remediating and mitigating despoiled, degraded, derelict, contaminated and unstable land, where appropriate. Decisions should ensure that a site is suitable for its proposed use taking account of ground conditions and any risks arising from land instability and contamination. It also advises that where a site is affected by contamination or land stability issues, responsibility for securing a safe development rests with the developer and / or landowner.

171. Officers consider that Core Strategy policy CG4 is consistent with the NPPF and carries substantial weight.

Land Contamination

172. The application is supported by a Phase 1 Desk Study Assessment Report prepared by Renaissance. The report notes that the site has an industrial past including its use for foundries, iron and steel works, mills, stone and timber yards with associated railway lines. More recently, the site has been used as a car park and bus station with an electricity substation in the south. The surrounding area also has significant industrial heritage including a garage and gasworks to the west, brass works to the east and cotton mill to the south. Based on the proposed residential development, the overall risk to human health is considered to be moderate. Further survey work in the form of a Phase II intrusive survey is recommended and would be required by a condition.

173. At the time of writing, the Council's Pollution Control Officers are still considering the submitted information and progress will be reported at the meeting.

Coal Mining Legacy Issues

174. In respect of coal mining and land stability issues the Coal Authority notes that the application site does not fall within the defined Development High Risk Area and is located instead within the defined Development Low Risk Area. This means that there is no requirement for a Coal Mining Risk Assessment to be submitted. The Coal Authority's Standing Advice would be added to the decision notice as an informative note to the applicant in the interests of public health and safety.

175. Officers consider that the proposed land use and development of the site are appropriate and whilst further site investigation work and a detailed remediation strategy are required to secure the safe development of the site in order to comply with Core Strategy CG4, this can be conditional upon the approval.

Conclusion

176. The development proposal under consideration is a significant regeneration scheme that has the potential to make a positive and lasting contribution to the vitality and viability of Bolton town centre in terms of increasing the town centre residential population. The social benefits of a significant increase in housing numbers in this location need to be taken into account, contributing to delivering a significant part of the shortfall in housing delivery in terms of demonstrating a five-year supply. Other social benefits will flow from the design quality of the scheme and the confidence that this demonstrates in the housing marking within Bolton town centre. The environmental impacts, in particular the impact on the Le Mans Crescent and the Town Hall Conservation Area have been minimised and will in fact lead to some enhancements in terms of a more cohesive street scene and adding focus to the arches. The public realm improvements and the two linear parks including an enhanced pedestrian route to the west of the town centre will have a transformative impact on the character and appearance of the application site, its immediate surroundings and that the way that this part of Bolton town centre functions.
177. The application is supported by a comprehensive suite of technical documents covering issues such as sustainability, the impact on biodiversity and the development's resilience to flood risk. These have been assessed by the relevant technical consultees and their responses have been analysed within the report.
178. It is considered that the proposals before Members represent an opportunity to make significantly better use of this inefficiently utilised part of the Bolton town centre, to increase the residential population of Bolton town centre, contribute to the housing supply deficit and to increase confidence in the ongoing regeneration of the heart of the Borough.

Representation and Consultation Annex

Consultation

Consultation letters were sent to commercial premises and residential properties in the immediate vicinity of the site, including occupants of Paderborn Court and Jubilee House opposite. Five site notices were displayed around the site, each prominently located on a road frontage, including one outside Bolton Market

Representations

In a lengthy letter to the Leader of Bolton Council, Bolton & District Civic Trust have submitted some details of an alternative scheme for the development of this site which, in their view, addresses certain shortcomings of the submitted scheme. Officers are entirely clear that it is the legal duty of the Local Planning Authority to consider the schemes that are submitted to them via a planning application, rather than considering any alternative schemes. If the Civic Trust wish the Local Planning Authority to fully consider their alternative scheme it would be open to them to, as the Applicant has done, submit a planning application and pay the relevant application fee. In other words, the scheme before Planning Committee needs to be considered wholly upon its own merits and not in relation to any alternative scheme. It should also be noted that the Applicant clearly wishes to deliver the scheme currently before the Council and intends to commence work on site in April 2022. It is reasonably assumed that, despite their best intentions, Bolton & District Civic Trust would not be in a position to deliver the indicative scheme that they have provided within their representation.

Nevertheless, it is possible to determine certain implicit criticisms of the current planning application within the narrative provided by the Trust in support of their alternative scheme. Officers considers it to be appropriate that Members be informed of these criticisms in reaching their decision on the current application, together with Officers' views on these issues, as below:-

-) Victoria Square is over-used and therefore a new public event space should be provided to the west of Le Mans Crescent

Officer's note - Officers do not consider that Victoria Square is over-used. It is some of the most important civic and communal space in Bolton and should be used to its fullest extent

-) Bolton town centre lacks public green space and therefore new public green space should be provided within the site

Officer's note - it is true that Bolton town centre lacks public green space, largely limited to Nelson Square and the grounds of St Peter's Church. However, it does benefit from extensive and high quality green open space in the form of Queen Park, a Grade II listed park. Queens Park is around 350 metres from the site, or less than 5 minutes walk away. It is true that pedestrian routes between much of Bolton town centre and Queens Park are presently poor and dominated by vehicular traffic, but the scheme seeks to address this shortcoming by delivering an improved route from the rear of Le Mans Crescent. The Council is also seeking to improve connectivity in other ways - for example, the Bolton At Home / Irwell Valley development at Chorley Street delivers a new route through to Queen Park.

-) The new landscaping should be combined with the management of surface water to provide multi-functional benefits via a sustainable urban drainage system

Officer's note - as noted in the "impact on surface water drainage and flood risk" section above,

whilst underground attenuation is proposed rather than the more natural option promoted by Bolton & District Civic Trust, many of the biodiversity and amenity benefits that such a scheme would provide will be delivered by the scheme in ways other than via the management of surface water. A condition will ensure that the scheme delivers on the Council's policy requirement for slowing surface water flows from the site by 50% in comparison to the existing situation

- \ Urban food growing should be provided at the site in order to address food security

Officer's note - the NPPF strongly promotes the effective use of land in sustainable locations for new housing at a density appropriate for its location. Whilst the benefits of urban food growing are noted and accepted it is not considered that they are capable of outweighing the benefits of making as much use of the site as possible for housing

- \ This brownfield site should be considered as if it were a greenfield site

Officer's note - this is a brownfield site

- \ The scheme appears to be driven by short-term economics and a desire to satisfy certain social housing goals

Officer's note - as noted in the relevant sections above, the scheme would deliver on a number of economic and housing policy objectives

- \ The whole area between Ashburner Street and Deansgate should be considered

Officer's note - the site edge is as defined within the submitted documents and described above, however it is considered that the impact on the wider area has been considered by the Applicant's team, by consultees such as Places Matter and Historic England and by Officers, including the Council's Conservation and Design Officer

- \ The two blocks to Moor Lane should be better articulated in order to break up their massing as they currently look like warehouses

Officer's note - Places Matter raised no objection to the articulation of these blocks. A simple design, reminiscent of the area's industrial past, is considered to be the appropriate approach here

- \ Every apartment needs to be provided with private external space

Officer's note - the majority of units would have access to their own private amenity space, whether this is a rear garden, a balcony or a recessed terraced with all units having access to shared the shared amenity space created by the two linear parks. The close proximity of the site to the exceptionally good provision of amenity space at Queens Park should also be taken into account. Projecting balconies are not currently typical of Bolton town centre and whilst they are considered to be acceptable for the Moor Lane blocks, they would be considered to be an entirely alien feature for the two blocks proposed to the rear of Le Mans Crescent. It is also noted that Bolton & District Civic Trust do not appear to have incorporated balconies or terraces for the Cheadle Gardens or Ashburner Street blocks within their alternative proposals

- \ Green wall technology should be provided throughout

Officer's note - Bolton & District Civic Trust's promotion of green wall technology is well known from their representations on other schemes but Officers remain of their view that it is not consistent with

the distinctive character of Bolton town centre, in particular in such as sensitive location as this

- \ Community spaces should be provided to the ground floor

Officer's note - presumably in order to ensure that their scheme is commercially viable, the Applicant wishes to use the ground floor of the Cheadle Gardens and Ashburner Street blocks as commercial space

- \ The terraces should run east-west, not enclose the rear parking area

Officer's note - a scheme showing east-west blocks was initially proposed to the panel at Places Matter. However, the panel felt that this resulted in blank ends to each block and therefore promoted the use of blocks enclosing the rear space in order to maximise overlooking and activity levels in all directions, not just north and south

- \ The rear parking area should not be provided, this should be private garden space

Officer's note - the Applicant was encouraged by Officers and by the Places Matter panel to reduce parking provision and has made some positive steps in this direction. They remain of the view, however, that a degree of parking provision is essential for the marketability of the proposed homes. Officer's have accepted this and note that there is no mandatory provision in either national or local planning policy for entirely traffic-free developments

- \ The parking provision is too high

Officer's note - whilst one space per dwelling has been provided for each of the townhouses, the provision across the site is actually around 52 spaces for 208 dwellings, or 1 space per 4 dwellings

- \ The vehicular access should be one-way

Officer's note - the vehicular access is one-way

- \ Blackhorse Street should be reduced to a minimal width with a "shared space" design

Officer's note - the final specification of the works for Blackhorse Street and other highway and public realm improvements will be worked up in details and resolved by condition. It is intended that Blackhorse Street will be narrowed as far as possible within the constraints identified by the Council's Highway Officers in terms of managing expected vehicle flows, including buses accessing Bolton Interchange

- \ The top storey of the apartment block adjacent to Cheadle Square should be set back and provided with a green wall

Officer's note - neither Historic England nor the Council's Conservation and Design Officer have raised concerns over the impact on this block on Le Mans Crescent. Officers take the view that both setting back the top storey of the proposed four-storey block and utilising a green wall would actually introduce two alien features into the view experienced from the west of Le Mans Crescent and are satisfied that the new building will not intrude upon the key features of the western elevation of Le Mans Crescent - the openings within the arches themselves.

- \ The proposed scheme does not do justice to the potential prestige of the site and underplays the future economic benefits to the town

Officer's note - for the reasons expressed within this report, Officers consider that the proposal complies with national and local planning policies and should therefore be approved on its own merits rather than in comparison to some other alternative scheme

A member of Bolton & District Civic Trust, objecting in a personal capacity, has raised the following issues:-

-) an exclusively social housing scheme is the wrong solution for this most prestigious town centre site

Officer's note - this is not an exclusively social housing scheme. Only 82 of the proposed 208 units are proposed to be transferred to Bolton At Home.

-) if you add together the approved or pending housing applications in the centre, there are something like 1000 public and private apartments 'in the pipeline', ranging from 20 storey penthouses to low-rise affordable houses

Officer's note - it remains the case that Bolton cannot currently demonstrate a 5-year supply of housing, as required by national planning policy.

-) where are the matching social and environmental measures to make Bolton Town Centre a place where people actually want to live?

Officer's note - it is not the responsibility of this development or this developer to achieve these objectives alone. Nevertheless, as noted in the report above, the development will achieve a number of important objectives.

-) producing an 'affordable housing' enclave right in front of our Grade II Town Hall and Le Mans Crescent sends out all the wrong signals to potential businesses and house builders/buyers looking to push things further 'upmarket'

Officer's note - this is not an affordable housing enclave.

-) It 'short-changes' the future residents if there are no shops or social facilities for them.

Officer's note - a wide range and number of shops and social facilities would be available to future residents a short walk away in Bolton town centre.

-) despite repeated requests to Officers, we have not been told how and why Bolton At Home were chosen as the developer. Being our most prestigious town centre site, one would have expected some form of design/tender competition with a detailed brief containing specific requirements in terms of design and content. The choice of BAH, by definition, automatically restricts the content of the development to 'social housing' with all the design limitations that go with it.

Officer's note - Bolton At Home are not the developer. The Applicant is Step Places though as noted above, 82 of the 208 units would be transferred to Bolton At Home. In any case, the choice or identity of a developer is not considered to be a material planning consideration.

-) There is already a large social housing scheme approved for Central Street, again without any supporting facilities. To produce the same again on Moor Lane would create an over-emphasis on this type of housing stock in the town centre. We need to attract a broader demographic.

Officer's note - The scheme at Central Street contains no social housing, it is entirely private-sector rental. Commercial uses were included within the Deansgate frontage.

-) Having met with BAH and its development team, it was clear that their terms of reference were to produce a scheme that first and foremost met the requirements of their waiting list and building cost parameters. This has led to a disjointed design of mid-rise and low-rise blocks built of brick rather than stone to match the Crescent with a central core devoted to car parking rather than public open space.

Officer's note - notwithstanding the fact that Bolton At Home are not the developer, it is not considered to be in any way unreasonable for a developer to design a scheme with their future customers in mind or to limit their expenditure to their building cost parameters. For the reasons expressed above, the scheme is not considered to be disjointed and brick is considered to be an entirely acceptable building material here. Car parking is proposed, but is very limited and would be hidden by the enclosure of the surrounding dwellings.

-) A mixed use scheme would have afforded the opportunity to have shops, business/workspace units and social facilities on the ground floor helping to create both interest and a sense of community through shared activity.

Officer's note - this is arguably a mixed use, given that three large commercial units are proposed to the ground floor of the two eastern blocks. It would not be appropriate to increase this mix further as the Council's strategy is to consolidate the existing retail, business and social facilities of Bolton town centre, not expand them further to the west.

-) The architects have failed to create any sense of place or historic building setting for this critical approach to the town centre. The building design is bland and seeks to use variations in brick colour as the only way to highlight the non-descript building blocks

Officer's note - for the reasons expressed in the "Impact on the Character and Appearance of the Area" section above, Officer's respectfully take a different view to that expressed by the objector

-) The 4 storey building next to Cheadle Square blocks the view of the Crescent and the two 6 storey blocks at the western end create a barrier between the development and Moor Lane.

Officer's note - for the reasons expressed above, Officers consider that the dominance of Le Mans Crescent will remain unchallenged. Alternative views of the Crescent will remain.

-) The 2 terraced courts at the heart of the scheme are a complete anti-climax and, with surface car parking at their core, are the opposite of what you would expect to see in a 21st century town centre design.

Officer's note - as expressed above, the parking associated with the scheme is actually fairly limited and would be largely hidden from view.

-) Where is the flair that an appreciation of 21st century climate-resilient, 'green' architecture could bring?

Officer's note - as expressed in the report, the proposed complies with national and local planning policies on sustainable design and resilience to the effects of climate change.

- \ The N-S pocket park is in the wrong place with the wrong orientation (over-shadowed) and there is a lack of imagination in considering what Cheadle Square could become as a public events area rather just another featureless space.

Officer's note - it is true that the Moor Lane blocks would overshadow the north-south linear park in the afternoons and summer evenings; however, the low-rise nature and layout of the townhouses means that it will benefit from good sunlight in the morning and its north-south alignment and the absence of tall buildings to the south means that it will benefit from excellent sunlight penetration in the middle of the day. A solar study has been provided to demonstrate this.

- \ Taking out the unnecessary access roads and creating a traffic-free, 'green' environment should have been the main starting point for any scheme on this site. It would have made such a difference to the outcome.

Officer's note - the Applicant was encouraged by Officers and by the Places Matter panel to reduce parking provision and has made some positive steps in this direction. They remain of the view, however, that a degree parking provision is essential for the marketability of the proposed homes. Officer's have accepted this and note that there is no mandatory provision in either national or local planning policy for entirely traffic-free developments.

A second member of Bolton & District Civic Trust, objecting in a personal capacity, repeats many of the issues raised by their colleagues above, but has also raised the following issues:-

- \ sandstone should be used for the block adjacent Cheadle Square, consistent with Le Mans Crescent

Officer's note - it is considered to be important to differentiate the site from Le Mans Crescent, rather than copy its materiality, therefore a sandstone-coloured brick is considered to be the appropriate choice

- \ provision for social housing is separated from the private market housing and provision for older people is in a separate block

Officer's note - The submitted Design & Access Statement states that "homes and streets will be tenure-blind, so that it is not easy to differentiate between homes that are private and those that are shared ownership or rented". Officer's concur that there is nothing specific in the appearance of any of the buildings that would lead any person to conclude which are for social rent, which are private and which are aim are older people. There would also be nothing to prevent an older person seeking to live on a different part of the site. It's true to say that they are spatially separate, but are for sound reasons of ownership and ease of management, but in terms of design or appearance there is nothing to distinguish the types of tenures at all

- \ Blackhorse Street should be decommissioned for vehicles as far as possible

Blackhorse Street remains an important part of the town centre's bus gyratory system - buses from the west, north and east of the Borough all make use of Blackhorse Street (and Great Moor Street and Newport Street beyond) in order to access the bus station. Re-routing all these buses along Moor Lane with its busier junctions would have the potential to limit the attractiveness of bus use in Bolton

- \ the apartment blocks at Moor Lane are too tall

Officer's note - as noted in the relevant section above, there will be a noticeable change in scale between the apartment blocks adjacent to Moor Lane and the central townhouse. Nevertheless, the surrounding townscape is far from uniform and already contains some buildings which exceed the height of their neighbours, including Marsden House to the north, the apartment block at the junction of Spa Road and Garside Street together with the Bolton Courts building. It should also be noted that buildings of a similar scale have been granted planning permission at the site opposite. Officers therefore take the view that a certain degree of disparity of scale need not be unacceptable, particular where this is mitigated by good design and quality of materials. This is considered to be the case in relation to the two Moor Lane blocks and it is also noted that a generous amount of space has been provided around the two blocks in order to ensure that they will not appear cramped within their settings.

) measures for generating power have not been sufficiently considered

Officer's note - as noted in the relevant section above, the application is supported by an Energy Statement which demonstrates that the Council's policy requirement of a 10% betterment target over current Building Regulation standards would be achieved via the use of roof-mounted photovoltaic panels and air source heat pumps

) more attention should be given to working from home in the form of purpose design live/work units

Officer's note - there is not considered to be any reasons by which the proposed internal layout could not support working from home, without needing to be specifically designed for this purpose

A representation was made by a trader within Bolton Market, expressing concerns about how the works would impact on their business. However, these matters have since been resolved and the scheme now has the support of the market traders

A resident of Harwood has made a representation to say that they agree with Bolton & District Civic Trust and Bolton Civic Trust's objection and that our impressive Town Hall and Crescent should be embellished by a pleasant garden area to the rear, a green lung for the centre, not housing and car parking. Bolton has many sites available for housing, I note the Central Street area and the Bank Street area are being used.

Officer's note - as expressed above, Bolton cannot currently demonstrate a 5-year supply of housing and therefore to say that Bolton has many sites available for housing is incorrect. Furthermore, for the reasons contained within the Officer's report, the application is considered to comply with national and local planning policies and there is no requirement to consider an alternative scheme such as the use of the site as a park.

Consultations

Advice was sought from the following consultees: Greater Manchester Ecology Unit, Environment Agency, Drainage and Technical Support, Trees and Woodland Manager, Coal Authority, Highways Engineers, Greenspace Management (Wildlife Liaison officer), Landscape Architects, Environmental Health officers, Strategic Housing Service, Economic Strategy, Bolton District Civic Trust, Primary Care NHS Bolton, Asset Management and Planning (Education), Strategic Development Unit, Greater Manchester Police Design For Security.

Planning History

Prior Notification was accepted in May 2018 for the demolition of the former bus station to allow for future development (03354/18)

There are no other relevant planning applications at the site.

Recommendation: Approve subject to conditions

Recommended Conditions and/or Reasons

1. **Commencement**

The development hereby permitted shall be begun before the expiration of **two** years from the date of this permission.

Reason

Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004. A shorter period than the default three years is imposed, as permitted by paragraph 76 of the National Planning Policy Framework and as required by Bolton's Housing Delivery Test Action Plan (August 2019).

2. **Surface Water - Detailed Design - Pre-commencement**

Prior to the commencement of any development other than demolition, full details of the proposed surface water drainage works shall be submitted to and approved in writing by the local planning authority. The submitted details shall:

- 1) Provide information about the design storm period and intensity, the method employed to delay and control the surface water discharged from the site and the measures taken to prevent pollution of the receiving groundwater and/or surface waters.
- 2) Include a timetable for its implementation, and
- 3) Provide a management and maintenance plan for the lifetime of the development which shall include the arrangements for adoption by any public authority or statutory undertaker and any other arrangements to secure the operation of the scheme throughout its lifetime

The approved works shall be implemented in full prior to the occupation of the buildings hereby approved and those works as approved shall be retained thereafter.

Reason

To ensure the site provides satisfactory means of surface water drainage and to comply with policies CG1.5 and CG2.2 of Bolton's Core Strategy.

Reason for pre-commencement condition:

The solution for surface water disposal must be understood prior to works commencing on site as it could affect how underground works are planned and carried out.

3. **Archaeology - Prior To Commencement**

No development works shall take place until the applicant or their agents or their successors in title has secured the implementation of a programme of archaeological works in accordance with a Written Scheme of Investigation (WSI) which has been submitted to and approved in writing by the local planning authority. The WSI shall cover the following:

1. A phased programme and methodology of investigation and recording to include:
 -) archaeological evaluation trenching;
 -) pending the results of the above, targeted excavation (subject to an updated WSI).
2. A programme for post-investigation assessment to include:
 -) production of a final report on the results of the investigations and their significance.
3. Deposition of the final report with the Greater Manchester Historic Environment Record.

4. Dissemination of the results of the archaeological investigations commensurate with their significance.

5. Provision for archive deposition of the report and records of the site investigation.

6. Nomination of a competent person or persons/organisation to undertake the works set out within the approved WSI.

All archaeological work should be undertaken by a suitably qualified and experienced archaeological contractor, funded by the applicant. GMAAS will approve the WSI and also monitor the archaeological works on behalf of Bolton Council.

Reason

In accordance with NPPF policy 16, paragraph 205: To record and advance understanding of the significance of any heritage assets to be lost (wholly or in part) and to make this evidence (and any archive generated) publicly accessible.

Reason for Pre-commencement Condition - it would not be possible to record any evidence once works had commenced.

4. **Construction Environmental Management Plan**

Prior to the commencement of development, including demolition, ground works or vegetation clearance a Construction Environmental Management Plan (CEMP) shall be submitted to and approved in writing by the local planning authority. The CEMP shall include as relevant the following details:

Amenity & Pollution

- a) an intrusive pre-demolition asbestos survey in accordance with HSG264 and a mitigation plan for the demolition phase
- b) Hours of work and of deliveries / removal of demolition waste
- c) Measures for the suppression of noise and fugitive dust
- d) Proposals to minimise the impact on the reasonable activities of surrounding properties including, but not limited to, Bolton Market, Bolton Courts, the Job Centre and the residents of Paderborn Court

Highways

- e) Details of the precautions to guard against the deposit of mud and substances on the public highway, to include washing facilities by which vehicles will have their wheels, chassis and bodywork effectively cleaned and washed free of mud and similar substances prior to entering the highway
- f) Routes for vehicles / heavy machinery and around the site including swept path analysis
- g) Compound locations together with details of the storage facilities for any plant and materials including off-site consolidation if appropriate, the siting of any site huts and other temporary structures, including site hoardings and details of the proposed security arrangements for the site
- h) Parking of vehicles associated with demolition construction, deliveries, site personnel, operatives and visitors,
- i) Sheeting over of construction vehicles.
- j) Proposals to minimise the impact on the free flow of pedestrian and vehicular traffic on Blackhorse Street, Moor Lane, Ashburner Street and other nearby highways

General

- k) Display of an emergency contact number
- l) Location, removal and recycling of waste
- m) Evidence of the consideration of the adoption of an appropriate standard such as CLOCS or Considerate Constructors

The development shall then be carried out in accordance with the approved CEMP.

Reason

To ensure that adequate consideration is given to the need to minimise the impact on the road network and reduce pollution, in accordance with Policies P5 and CG4 of Bolton's Core Strategy.

Reason For Pre-commencement Condition

It would not be possible to properly mitigate the impacts of demolition or construction once each phase had commenced.

5. **Trees - Prior to Commencement**

No demolition, development or stripping of soil shall be started until:

1. The trees and hedgerows within or overhanging the site which are to be retained have been surrounded by fences of a type to be agreed in writing with the Local Planning Authority prior to such works commencing.
2. The approved fencing shall remain in the agreed location (in accordance with BS 5837:2012 Trees in relation to design, demolition and construction – Recommendations until the development is completed or unless otherwise agreed in writing with the Local Planning Authority and there shall be no work, including the storage of materials, or placing of site cabins, within the fenced area(s).
3. No development shall be started until a minimum of 14 days written notice has been given to the Local Planning Authority confirming the approved protective fencing has been erected.

Prior to the commencement of development of any works on site, the developer shall submit a method statement detailing how the following elements of the site will be constructed without causing harm or damage to the protected trees found on the site to the Local Planning Authority. The specified areas are:

4. Arboricultural Method Statement – for any ground treatments within the root protection zones of G3, G2 and T3 if required. (See the Arboricultural Impact Assessment: Construction Stage Mitigation section 3.7. where it notes "It will be necessary to demonstrate how the above and below ground structures of retained tree cover will be protected during the site preparation and construction phases of development in accordance with BS5837:2012. It is therefore recommended that a full Arboricultural Method Statement (AMS) is prepared to discharge applicable and suitably worded pre-commencement planning Condition(s)."

No development or site clearance shall take place until the Local Planning Authority has agreed the measures in writing, and these measures shall then be implemented fully in accordance with the approved details.

Reason: To protect the health and appearance of the tree(s).

Reason for Pre-commencement condition - it would not be possible to protect trees shown for retention once development had commenced

6. **Sustainability and Air Quality Measures - Prior To Construction Phase**

Prior to the commencement of any above ground works (other than site hoarding, signboards, compound, demolition, vegetation strip and stripping / crushing of tarmac), details of how the development shall reduce CO2 emissions of predicted energy use of the development by at least 10% below the baseline of Building Regulations Part L standards (taking into account the recommendations of the submitted Energy Strategy Report dated 2nd August 2021 v2.1 by Ridge) shall be submitted to and approved in writing by the local planning. The details shall also demonstrate that the releases associated with any gas-fired boilers shall not exceed 40mgNOx/kWh.

The development shall be completed in accordance with the approved details and all such measures retained in full thereafter.

Reason

In the interests of tackling climate change and in order to comply with policies CG2.2 and CG4 of Bolton's Core Strategy and the Sustainable Design and Construction SPD.

7. **Biodiversity Net Gain - Post-demolition, Prior To Above Ground Works**

No above ground works (other than site hoarding, signboards, compound, demolition, vegetation strip and stripping / crushing of tarmac) shall commence until details of proposed improvements to urban biodiversity at the site have been submitted to and approved by the Local Planning Authority. The proposed improvements shall take account of the recommendations of Section 6 of the Preliminary Ecological Appraisal Report (Reference: 80-565-R1-2, August 2021, E3P) and the Biodiversity Metric Report (Reference: 80-565-R3-1, August 2021, E3P) and include the consideration

of bat or swift boxes incorporated within the external fabric of the buildings, where appropriate. Such details as are agreed shall be implemented in full prior to the development first being brought into use and retained in full thereafter.

Reason

In accordance with Policy CG1.2 of Bolton's Core Strategy and in order to implement the net gains for biodiversity required by Paragraphs 174 and 180 of the National Planning Policy Framework.

8. **Secured By Design Accreditation - Prior To First Occupation**

No dwelling shall be occupied unless and until evidence that the development has been built in accordance with the recommendations contained within section 3.3 of submitted Crime Impact Statement dated 31/08/2021, ref: 2021/0293/CIS/01 Version C and evidence of a Secured By Design accreditation has been submitted to and approved by the Local Planning Authority. Such measures as are incorporated within the development in the interests of crime reduction shall be retained in full thereafter.

Reason

In the interests of crime reduction and Policies CG3 and S1 of Bolton's Core Strategy.

9. **Materials and Other Details - Prior To Construction Phase**

Notwithstanding any description of materials in the application no above ground construction works shall take place until details of the following have been submitted to and approved in writing by the Local Planning Authority. Such details shall include the type, colour and texture of the materials. The approved materials shall be implemented in full thereafter.

-) Samples of all external materials including sample panels of brickwork to show mortar colour and brick detailing.
-) Cross sections drawings at 1:50 scale through the facades of new buildings to show detail and depth of reveals and parapet treatment.
-) Detail specification of windows, balconies and doors.
-) Details of boundary treatments to include elevations, and sections.
-) Details of external lighting and siting.
-) Samples/ details of hard surfacing materials.
-) Details of shopfronts and signage.

Reason

To ensure the development reflects local distinctiveness and to comply with policies CG3 and TC11 of Bolton's Core Strategy.

10. **Public Realm & Landscape - Detail Design and Management - Prior to Construction Phase, Delivered Prior To First Occupation**

Prior to the commencement of any above ground works (other than site hoarding, signboards, compound, demolition, vegetation strip and stripping / crushing of tarmac) or in accordance with any phasing plan agreed in writing by the Local Planning Authority, a detailed Landscape Delivery & Management Scheme to cover all landscaped areas including associated public realm improvements shall be submitted to and approved in writing by the Local Planning Authority. The Scheme shall include, but not be limited to, matters of:

-) Details of materials and finishes to hard surfaces in public and publicly accessible areas;
-) Detail of trees and shrubs to be planted / green infrastructure;
-) Details and materials of walls, fences, boundary treatments;
-) Details of proposed lighting structures;
-) Details of proposed seating, bollards, bins, cycle racks, grilles and other street furniture; and
-) Details of public realm improvements to internal and surrounding highways
-) Details of the applicant's proposals for the future maintenance of the landscape areas in the form of a Delivery & Management Statement

The scheme shall be consistent with the documents listed below:-

Landscape Masterplan – P20274-00-001-GIL-0100 Rev 03
Vegetation Loss – P20274-00-001-GIL-0400 Rev 01
Planting Strategy – P20274-00-001-GIL-0401 Rev 01
Landscape Statement – P20692-00-001-701-00 dated August 2021

Such details as are approved shall be implemented in full prior to any part of the development being brought into use or in accordance with any phasing plan agreed in writing by the Local Planning Authority and retained thereafter. All new planting shall be carried out during the planting season October/March inclusive, in accordance with the appropriate British Standard BS4428:1989, (as amended, revoked or re-enacted), immediately following start of works, unless otherwise agreed in writing by the Local Planning Authority. Any plants or trees found damaged, dead or dying in the first five years are to be duly replaced and the scheme thereafter retained.

Reason

To ensure the provision of an acceptable standard of public realm and landscaping in accordance with Policies TC11, CG3 and IPC1 of Bolton's Core Strategy, together with the Public Realm Implementation Framework.

11. Electric Vehicle Charging - Prior To First Occupation

No dwelling shall be occupied unless and until full details of electric vehicle charging (EVC) infrastructure (including appropriate cable provision and provision for charging points) has been submitted to and approved in writing by the Local Planning Authority. The approved EVC infrastructure shall be put in place prior to the occupation of any dwelling and shall be retained thereafter.

Reason

In the interests of improving local air quality and providing sustainable development, pursuant to the NPPF and policy CG4 of Bolton's Core Strategy.

12. Land Contamination - Following Demolition

Prior to the commencement of any phase of the development, other than demolition:-

-) A Site Investigation report shall be submitted to and approved in writing by the Local Planning Authority relating to the site. The investigation shall address the nature, degree and distribution of land contamination on site and shall include an identification and assessment of the risk to receptors focusing primarily on risks to human health, property and/or the wider environment; and
-) The details of any proposed remedial works for the site as a whole shall be submitted to, and approved in writing by the Local Planning Authority. The approved remedial works shall be incorporated into the development during the course of construction and completed prior to occupation of the development or the development being first brought into use; and

Prior to first use/occupation of the development hereby approved:

-) A Verification Report shall be submitted to, and approved in writing by, the Local Planning Authority. The Verification Report shall validate that all remedial works undertaken on site were completed in accordance with those agreed by the LPA.

Reason

To safeguard the amenity of the future occupants of the development and to comply with Core Strategy policy CG4.

Reason for Pre-Commencement Condition:

Any works on site could affect contamination which may be present and hinder the effective remediation of any contamination causing a risk to the health of future occupiers and harm to the environment, hence the initial investigation must be carried out prior to the commencement of any works on site.

13. **Noise - Details Prior to Construction Phase**

All residential units shall be constructed to achieve within the building envelope (windows, floor, ceilings and walls) provision of sound attenuation against external noise including road traffic noise and noise from deliveries at Bolton Market that achieves the following internal noise levels with windows shut and other means of ventilation provided if necessary:

- a) Living room – daytime 07:00 to 23:00 hours 35 LAeq 16 hours
- b) Dining room / Area – daytime 07:00 to 23:00 hours 40 LAeq 16 hours
- c) Bedroom – daytime 07:00 to 23:00 hours 35 LAeq 16 hours and night time 23:00 to 07:00 hours 30 LAeq 8 hours and 45 L_{Amax} (not to be exceeded more than 15 times per night). Details of the acoustic insulation shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of above ground works at each relevant phase or plot. The acoustic attenuation measures and, if necessary, alternative ventilation shall be implemented in accordance with the approved details prior to first use or occupation of the phase or plot and retained thereafter.'

Details of construction methods and materials, window specifications and ventilations details necessary to achieve the above shall be submitted to and approved in writing by the Local Planning Authority prior to the commencements of development other than demolition. The development shall then be carried out entirely in accordance with those details and retained as such thereafter.

Reason

To safeguard the living conditions of occupiers from noise pollution in accordance with Policy CG4 of Bolton's Core Strategy.

14. **Noise - Relationship between residential and non-residential - Prior To Construction Phase**

Prior to the commencement of any above ground works at any phase or plot that includes both a non-residential use / commercial use and a residential use or would result in a relationship of close proximity a scheme specifying the provision to be made to control noise transmission / break out from the non-residential / commercial unit(s) that may affect any proposed residential units, shall be submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be implemented in full prior to the development being brought into first use or occupation and retained thereafter.

Reason

To safeguard the living conditions of occupiers from noise pollution in accordance with Policy CG4 of Bolton's Core Strategy.

15. **Windows & Doors - Prior To Their Installation**

Prior to any external doors or windows being installed at any phase or plot, a detailed specification of their materials, appearance, colour, finish, texture, profile and depth of reveal together shall be submitted to and approved in writing by the Local Planning Authority. The approved specification shall be implemented in full before any of the residential units of that phase or plot are first occupied and shall be retained thereafter.

Reason

To ensure the development reflects local distinctiveness in order to comply with Policies CG3 and RA1 of Bolton's Core Strategy.

16. **Affordable Housing, 82 units - Prior To First Occupation**

No dwelling shall be occupied unless and until a scheme for the provision of affordable housing as part of the development has been submitted to and approved in writing by the Local Planning Authority. The affordable housing shall be provided in accordance with the approved scheme and shall meet the definition of affordable housing in Annex 2 of the National Planning Policy Framework or any future guidance that replaces it. The scheme shall include:

- i. the number, type, and tenure of the affordable housing to be provided which shall consist of not less than 82 units;
- ii. the arrangements for the management of the affordable housing by an affordable housing provider;
- iii. the arrangements to ensure that such provision is affordable for both first and subsequent

occupiers of the affordable housing; and
iv. the occupancy criteria to be used for determining the identity of occupiers of the affordable housing and the means by which such occupancy criteria shall be enforced.

This condition shall not be binding upon a mortgagee, chargee or any receiver (including an administrative receiver) appointed by such mortgagee or chargee or any other person appointed under any security documentation to enable such mortgagee or chargee to realise its security or any administrator (however appointed) including a housing administrator (each a Receiver) of the whole or any part of the affordable housing or any persons or bodies deriving title through such mortgagee or chargee or Receiver. This condition shall cease to apply (and shall be of no further effect) to any dwelling in respect of which the full equity has been acquired by the individual owner/purchaser and/or their mortgagee OR any dwelling in respect of which a statutory or voluntary right to buy or acquire has been exercised.

Reason

In accordance with Policy IPC1 of Bolton's Core Strategy.

17. Highway Works - Prior To First Occupation

No part of the development shall be occupied or otherwise brought into use unless and until details of the following have been submitted to and approved in writing by the Local Planning Authority

-) a scheme detailing how parts of the site to be used by vehicles are to be laid out, constructed, surfaced, drained and lit
-) details of the repair/reinstatement of surrounding highway surfaces arising from the demolition/construction phases of development
-) details of the areas to be stopped up under S247 of the Town and Country Planning Act as indicated under Drg No SK22124-005
-) areas of highway improvements under S278 of the Highways Act including the new access points from Moor Lane and Blackhorse Street. Level of work to be agreed by the Local Highway Authority as part of the S278 process
-) amendments to existing Traffic Regulation Orders at that location including the promotion of speed limits, a one-way system and parking restriction to be agreed with the Traffic Management Section of Bolton Council and funded by the applicant
-) a servicing and management strategy for the commercial / non-residential floorspace

Such details as are approved shall be implemented in full prior to the development being first brought into use and retained thereafter.

Reason

In the interests of highway safety and in order to comply with Bolton's Core Strategy policies S1, P5 and Supplementary Planning Document "Accessibility, Transport and Road Safety".

18. Waste Compactor Relocation - Prior to First Occupation

No dwelling shall be occupied unless and until the existing waste compound and compactor between the applicaiton site and Bolton Market has been been permantently relocated to the New St side of the Market

Reason

To prevent harm to future occupants by way of noise, pursuant to Policy CG4 of Bolton's Core Strategy.

19. Plant Noise - Compliance

The rating level, as defined in BS4142:2014, from fixed plant and equipment installed within any phase or plot within the development shall not exceed the measured daytime and/or night time background sound level at the closest sensitive premises at any time.

Reason

To minimise the impact of noise on the general and residential amenity from the increase and alteration of commercial and/or industrial uses in the area and in order to comply with Core Strategy policy CG4.

20. **Approved Plans - Compliance**

The development hereby permitted shall be carried out in complete accordance with the following approved plans:

11485-AEW-XX-SIDR-A-0501 Site Location Plan P2
11485-AEW-XX-SI-DR-A-0502 Existing Site Plan P7
11485-AEW-XX-SI-DR-A-0503 Demolition Plan P2
11485-AEW-XX-SI-DR-A-0510 Proposed Master Plan (Ground Floors) P8
11485-AEW-XX-RF-DR-A-0511 Proposed Master Plan (Roof Plan) P6
11485-AEW-XX-SI-DR-A-0512 Aspirational / Illustrative MasterPlan P3
11485-AEW-XX-SI-DR-A-0513 Proposed Boundary Treatment P3
11485-AEW-XX-SI-DR-A-0520 Existing Site Sections / Elevations (Sheet 1) P2
11485-AEW-XX-SI-DR-A-0521 Existing Site Sections / Elevations (Sheet 2) P2
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 Vegetation Loss – P20274-00-001-GIL-0400 Rev 01
 Planting Strategy – P20274-00-001-GIL-0401 Rev 01
 Landscape Statement – P20692-00-001-701-00 dated August 2021

Reason

For the avoidance of doubt and in the interests of proper planning.

Scale Bar TO BE USED ONLY AS GUIDANCE

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0m	10m	20m	30m	40m	50m
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0m	10m	20m	30m	40m	50m
1:100	1:200	1:500	1:1000	1:2000	1:5000
0m	10m	20m	30m	40m	50m

NOTES

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- 11485-AEW-XX-XX-A03-A-0001

CDM 2015

Client notified of duties: July 2020
Principal Designer: AEW

Unless stated below, all known hazards have been highlighted on the drawing.

Development Boundary

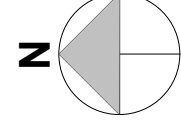


P8	20/10/21	Amended Block 3 Position	DAL	BR
P7	02/08/21	Issued with Red Line Remodel	RBH	BS
P6	31/08/21	Issued For Planning - Client	DAL	BS
P5	25/08/21	Issued For Planning	HC	BR
P4	12/08/21	Drawing Number updated to match planning register	RBH	BS
P3	11/08/21	Access to parking area accessible parking to Block 01 & 02	RBH	BS
P2	09/08/21	Access to parking area	DAL	BR
P1	03/08/21	Initial Planning Issue	DAL	BR
Rev	Date	Description	Drawn	Checked
			By	By

Status	Purpose of Issue
S2	For Planning
drawing stage	Stage 3
client	

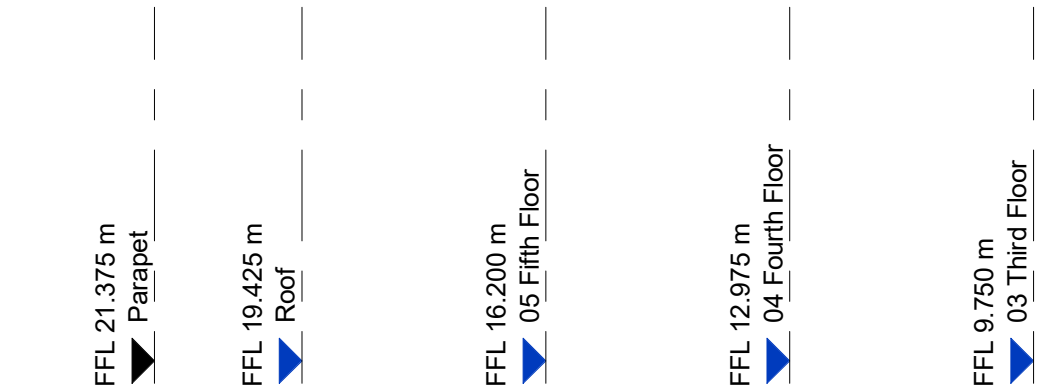
FSC MOOR LANE BOLTON DEVELOPMENTS LIMITED

Bolton, Moor Lane



Site Plan - Proposed - Ground Floor

date: 03.08.2021
scale @ A1: 1:500
drawn: DAL
checked: BR



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This drawing contains the following model files: -

CDM 2015

Client notified of duties: **July 2020**
Principal Designer: **AEW Architects**

Unless noted below, all known hazards have been highlighted on the drawing:

- [illegible]



East Elevation

Scale @ 1 : 100



West Elevation

Scale @ 1 : 100

NEW ACCESS ROAD

date	scale@ A1	20/08/21	drawn	BS
scale@ A1	As indicated		checked	BR

aew architects
0161 214 4370
www.aewarchitects.com

cts.com

ON I

Moor Lane FSG

project

**FSG MOOR LANE BOLTON
DEVELOPMENTS LIMITED**

drawing title

Block 02 - Elevations Sheet 1 of 2



cts.com

ON I

Moor Lane FSG

drawing title

Block 02 - Elevations Sheet 1 of 2



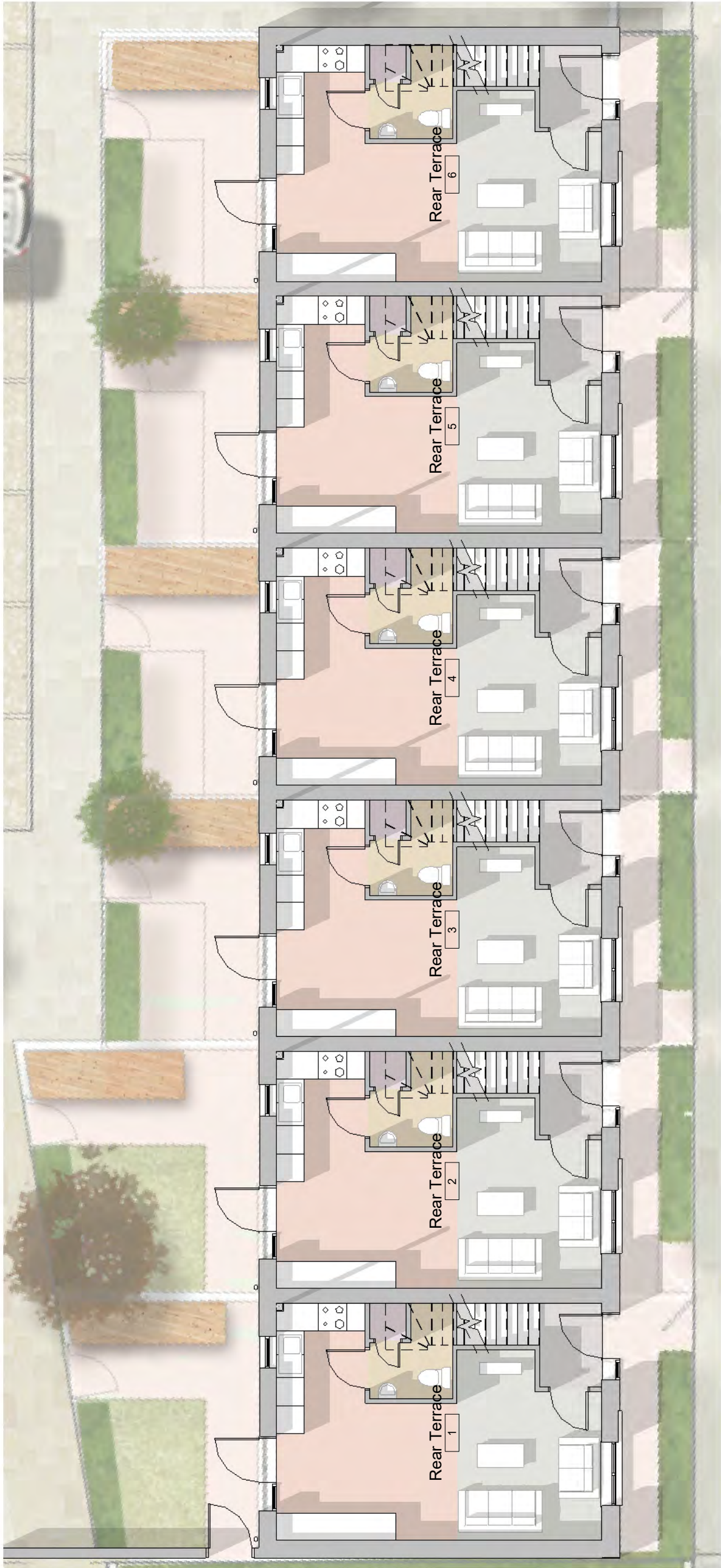
cts.com

ON I

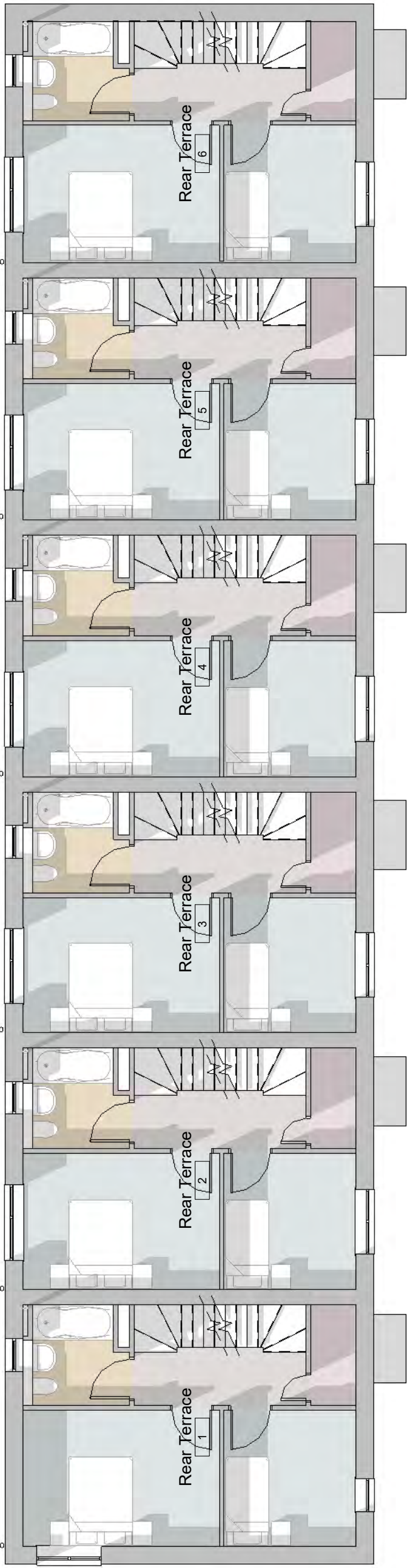
Moor Lane FSG

drawing title

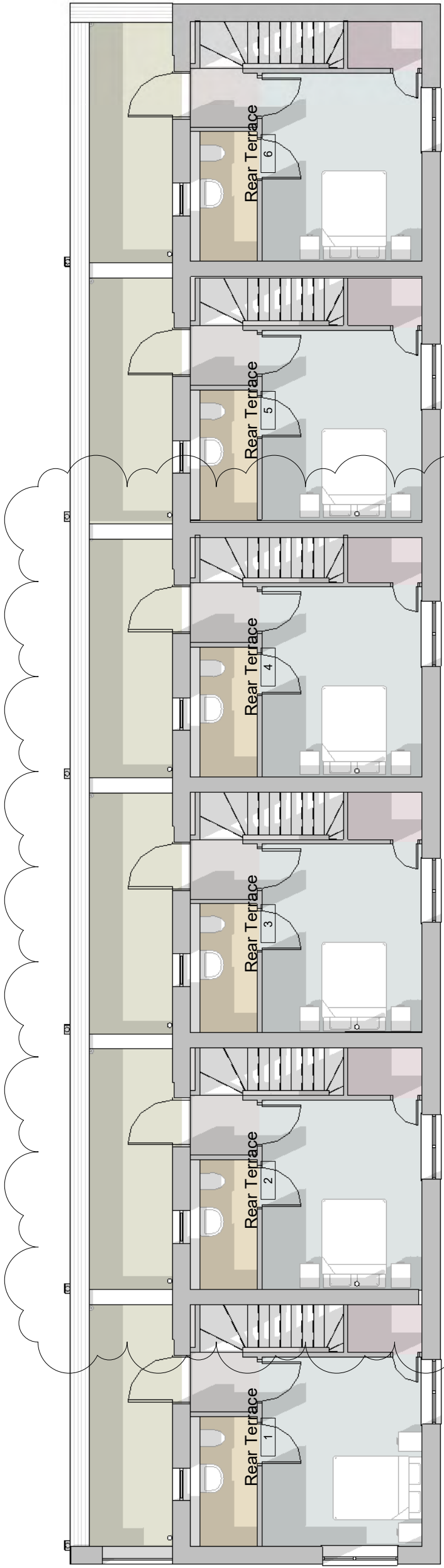
Block 02 - Elevations Sheet 1 of 2



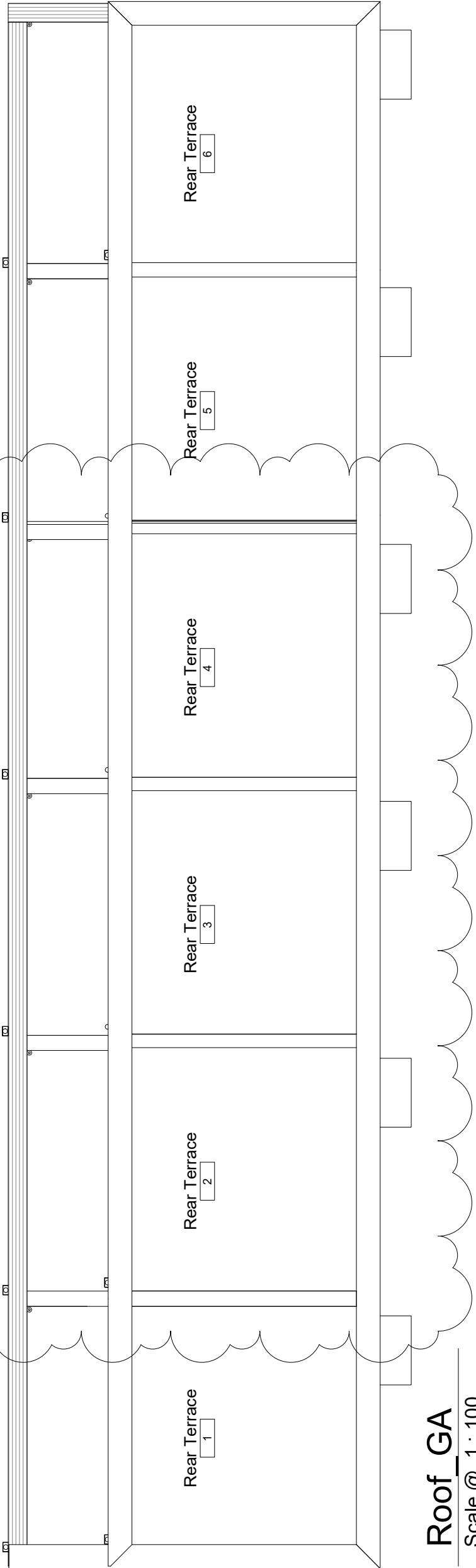
00 GA
Scale @ 1 : 100



01 GA
Scale @ 1 : 100



02 GA
Scale @ 1 : 100



Roof GA
Scale @ 1 : 100

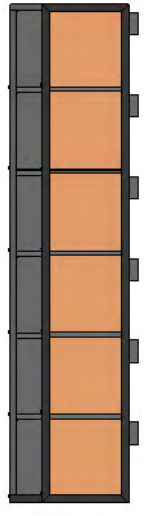
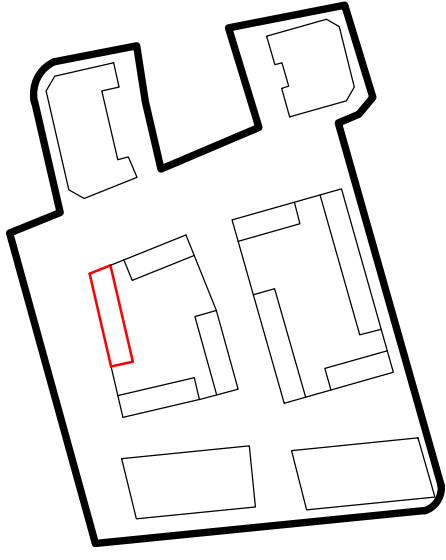
Scale Bar TO BE USED ONLY AS GUIDANCE									
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mm	mm	mm	mm	mm	mm	mm	mm	mm	mm
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NOTES

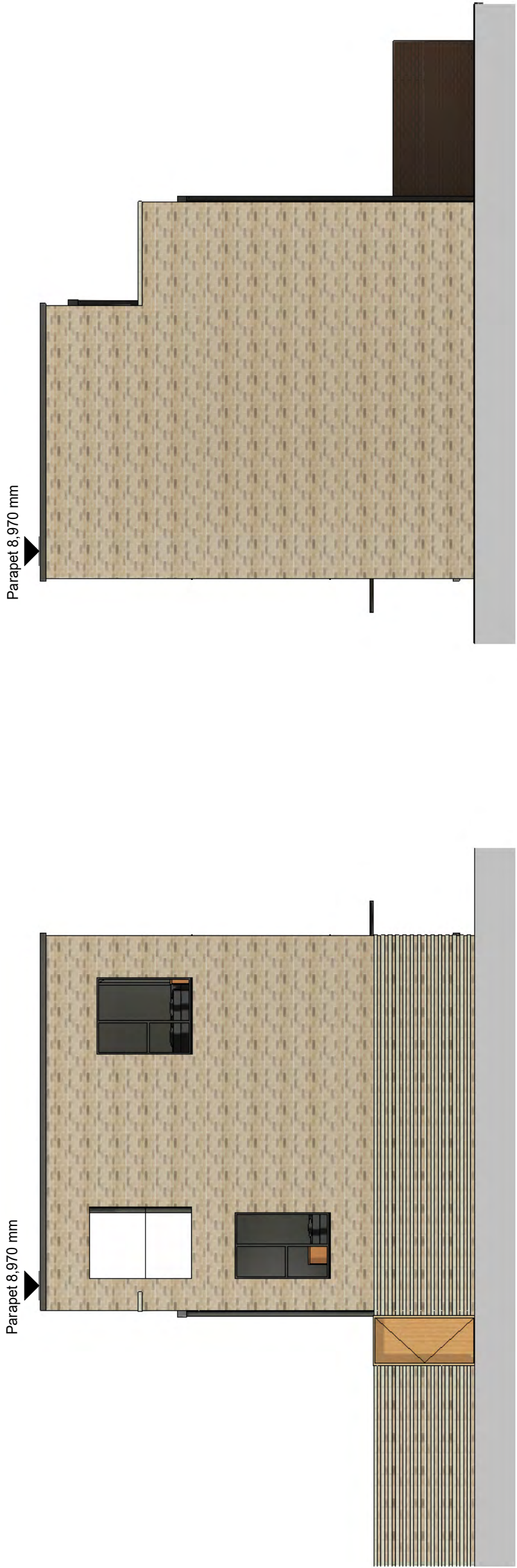
- All dimensions and levels are to be checked on site.
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11485-AEW-05-XX-M3-A-0005 Block 05

CDM 2015

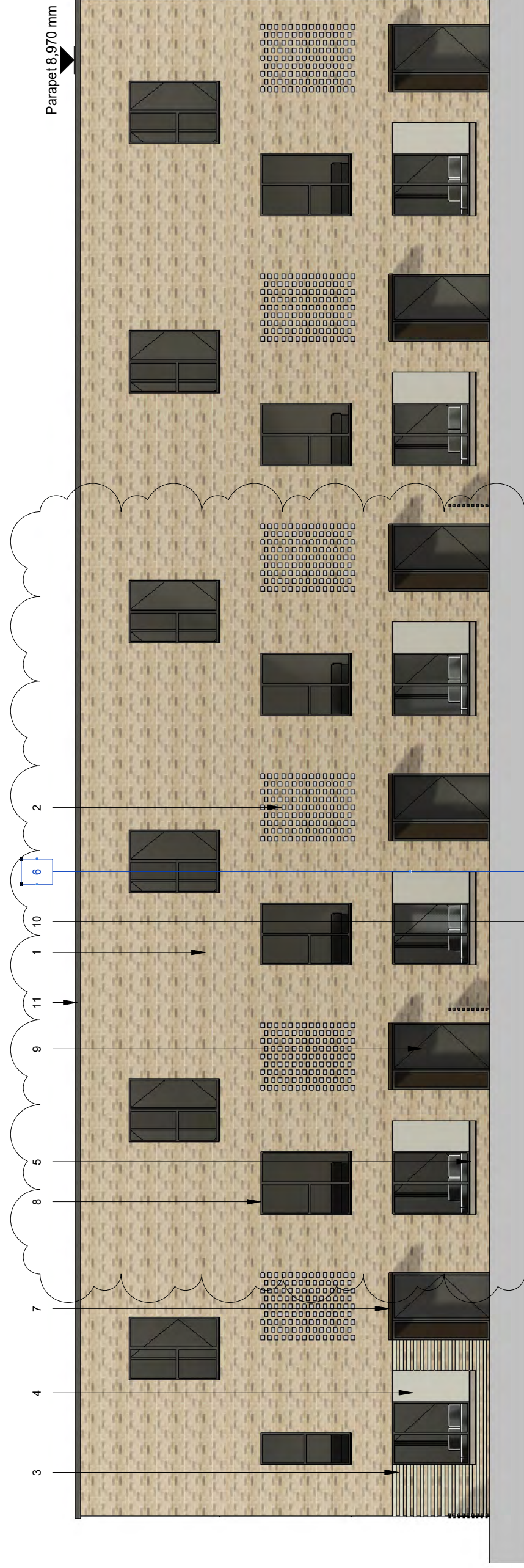
Client notified of duties: July 2020
Principal Designer: AEW
Unless stated below, all known hazards have been highlighted on the drawing.



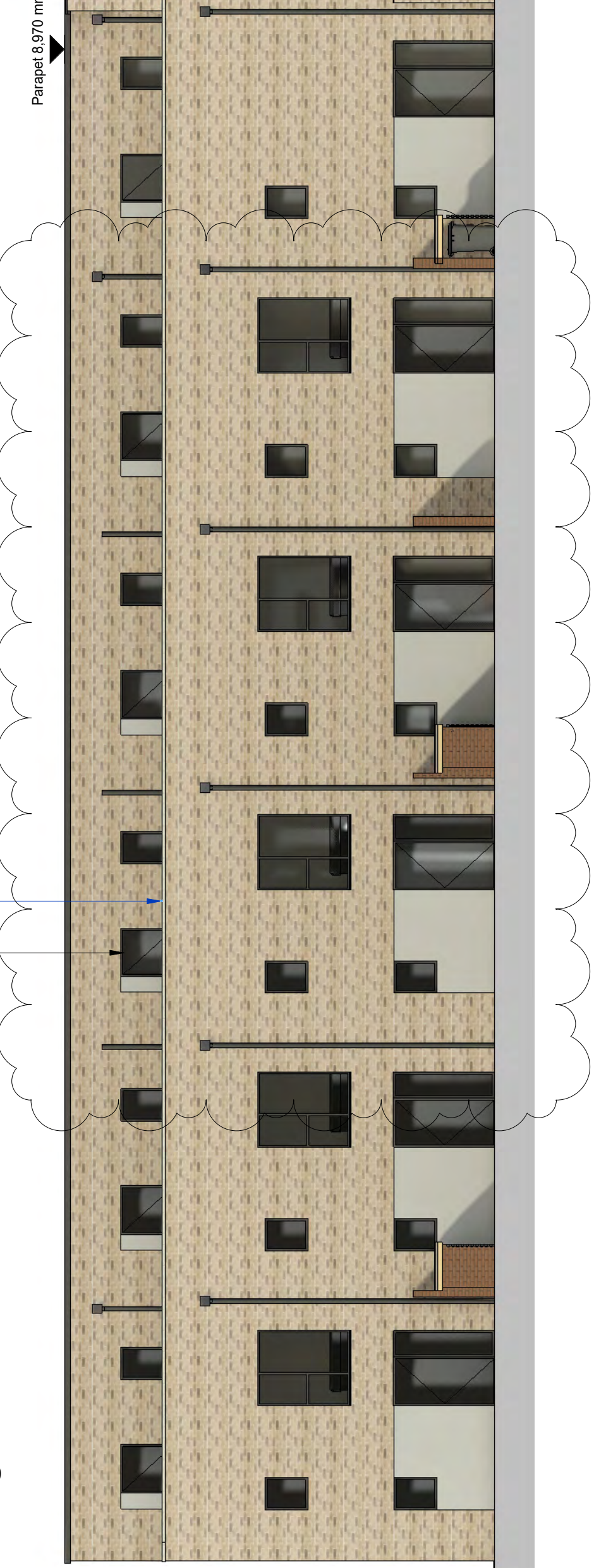
Key
Scale @ 1 : 500



GA Side 1
Scale @ 1 : 100



GA Front
Scale @ 1 : 100



GA Rear
Scale @ 1 : 100

Materials Key

To be read with detailed section for further clarity

- Walls
Generally to be clad in a brown multi facing brick in horizontal running bond
- Feature panelwork snap header detailing in projecting 10mm panels contrasting white brick
- Alternating courses: Brown multi facing brick / white facing brick in horizontal running bond
- Stone / limitation panel
- Stone / limitation stonework cill to news windows
- Stone / limitation stonework capping
- Pressed metal canopy
- Windows, doors & louvers
All windows and doors to have polyester powder coated aluminium frames in dark grey
- Solid doors to have polyester powder coated steel in dark grey
- Glazed aluminium door with PPC frame in dark grey
- Roof
Capping to brickwork to be polyester powder coated aluminium in dark grey

Rev	Date	Description	Drawn	Checked	By
P07	19/10/21	Pkg 2.4 amended to Rear Terrace Units	DAL	BR	BR
P06	08/09/21	Issue 2 tag updated to front terrace	DAL	BR	BR
P05	31/08/21	Issued for Planning - Client	DAL	BS	BS
P04	25/08/21	Issued for Planning - Client	HC	BR	BR
P03	04/08/21	Issued for comment prior to planning submission	HC	BR	BR
P02	04/08/21	Issued for comment prior to planning submission	DAL	BR	BR
P01	21/07/21	Initial issue	DAL	BR	BR

Status	Purpose of Issue
S2	For Information
Stage 3	
drawing stage	client

FSC MOOR LANE BOLTON DEVELOPMENTS LIMITED

Moor Lane Bolton

Block 05 - Plans and Elevations

date 21/07/21
scale @ A1 As indicated
drawn checked
DAL BR

P07

11485-AEW-05-ZZ-DR-A-0810

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FOR ILLUSTRATIVE PURPOSES ONLY

11485 - BOLTON, MOOR LANE
INDICATIVE CGI - Aerial View

DATE 17.06.2021
11485-AEW-SI-XX-VF-540_(P1)(S2)_Aerial Indicative CGI

Aew



FOR ILLUSTRATIVE PURPOSES ONLY

11485 - BOLTON, MOOR LANE
INDICATIVE CGI - View From Cheadle Square towards Black Horse Street

DATE 17.06.2021
11485-AEW-SI-XX-VF-541_(P1)(S2)_Cheadle Square Indicative CGI





FOR ILLUSTRATIVE PURPOSES ONLY

11485 - BOLTON, MOOR LANE
INDICATIVE CGI - View along Cheadle Gardens from Moor Lane

DATE 23.08.2021
11485-AEW-SI-XX-VF-542_(P1)(S2)_Cheadle Gardens Indicative CGI





FOR ILLUSTRATIVE PURPOSES ONLY

11485 - BOLTON, MOOR LANE
INDICATIVE CGI - View of Block 1 from Moor Lane

DATE 17.06.2021
11485-AEW-SI-XX-VF-542_(P1)(S2)_Moor Lane Indicative CGI



Application number 12322/21



Directorate of Place
Development Management Section

Town Hall, Bolton, Lancashire, BL1 1RU
Telephone (01204) 333 333

Bolton Council

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Date of Meeting: 11/11/2021

Application Reference: 12322/21

Type of Application: Full Planning Application

Registration Date: 08/10/2021

Decision Due By: 02/12/2021

Responsible Officer: Janine Logan

Location: 80 SOMERTON ROAD, BOLTON, BL2 6LL

Proposal: ERECTION OF FIRST FLOOR EXTENSIONS AT FRONT, SIDE AND REAR TOGETHER WITH NEW OPENINGS AND CONVERSION OF EXISTING GARAGE

Ward: Breightmet

Applicant: Mr M Dean

Agent : RA Design & Project Management Ltd

Officers Report

Recommendation: Approve subject to conditions

Executive Summary

- \ The application is before Committee as the applicant is Councillor Dean.
- \ This householder application has been submitted for the erection of first floor extensions at front, side and rear together with new openings and conversion of existing garage.
- \ No objections have been received.
- \ Members are recommended to approve this application.

Proposal

1. Proposed first floor side extension: would project 4.4 metres over the existing garage and be 9.1 metres long with a cross gable roof linking into the existing 5.3 metres to the eaves, 7.4 metres to the ridge of the main room and 6.5 metres to the ridge of the front gable feature.
2. Proposed first floor rear extension: would project 2.3 metres over the kitchen and dining room and be 7.4 metres wide with a cross gable roof 5.3 metres to the eaves and 7.1 metres to the ridge.
3. Proposed garage conversion: would include removal of garage doors, bricking up and installation of a window.

Site Characteristics

4. This application relates to a semi-detached, two storey property on Somerton Road. This section of Somerton Road is characterised by semi detached, two storey dwellings.

Policy

Development Plan

Core Strategies policies: P5 Transport and Accessibility; CG1.2 Urban Biodiversity; CG3 The Built Environment; CG4 Compatible Uses; RA3 Brightmet.

Other material considerations

5. SPD House Extensions and SPD General Design Principles

6. National Planning Policy Framework (NPPF)

Analysis

7. Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise. Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission. Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission. It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations. Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission. Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission. It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

8. The main impacts of the proposal are considered to be:-

- Γ Impact on the Character and Appearance of the Surrounding Area
- Γ Impact on the Amenity of Neighbours
- Γ Impact on Parking

Impact on the Character and Appearance of Surrounding Area

9. Policy CG3 of the Core Strategy states that the Council will conserve and enhance local distinctiveness, ensuring development has regard to the overall built character and landscape quality of the area, and require development to be compatible with the surrounding area in terms of scale, massing, grain, form, architecture, street enclosure, local materials and landscape treatment.

10. Policy RA3 refers specifically to requiring development to introduce increased levels of high quality, distinctive design that will enhance the local area, and conserve and enhance the character of the historic registered Tonge Cemetery.

11. Paragraph 5.1 of the House Extensions SPD 2012 states that the siting, design or layout of a house extension should not have an unacceptable effect on the wider street scene or the character of the locality. This might occur for example where extensions protrude in front of a clear and consistent building line, where roof heights are proposed to be increased, or where side extensions might create a "terracing effect". It may be the case that an extension represents a significant change in the overall consistency of house types which might impact more noticeably on the local character than house extensions in more mixed areas or where a range of extension types and styles already exist, prompting the need for a more sensitive design approach.

12. The surrounding area is one of two storey, semi detached properties.

13. Paragraph 3.2 of the House Extensions SPD 2012 states that the height of an extension should not exceed the height of the existing house, so that it does not dominate the host property.
14. Paragraph 3.8 of the House Extensions SPD 2012 states that pitched roofs are the most suitable for extensions especially if they are visible from the street. Two storey extensions in particular look best with a pitched roof. The proposed roof to the extension should generally be subordinate to the existing main roof.
15. Paragraph 5.3 of the House Extensions SPD 2012 states that front extensions (which include porches) can unduly impact on the existing street scene. However - if carefully designed to reflect and respect the appearance of the host building whilst also appearing subordinate to it - extensions to the front of dwellings may be acceptable, particularly where they can be justified in respect of creating variety and continuity within the street scene.
16. The proposed extensions would be visible from the highway, however the eaves and ridge would link into the main roof with a cross gable roof feature with a ridge set lower than the existing, therefore the proposed would appear subordinate to the host dwelling. The proposed first floor front/side extension would form a gable feature at the front over the existing garage and it is considered that this feature would be in keeping with the area as the neighbouring property of 82 Somerton Road also has a front gable. The proposed development would therefore not form an incongruous feature and would have minimal impact on the character and appearance of the surrounding street scene.
17. Paragraph 3.3 of the House Extensions SPD 2012 states that extensions should respect and be sensitive to the proportions of the existing house or any particular features such as:
 - \ Decorative brickwork or eaves
 - \ Size and style of windows
 - \ Stone detailing
 - \ Materials in terms of colour, coursing, texture and pointing
 - \ Specific features such as bay windows, steep sloping roofs, bargeboards etc.
18. The proposed windows in the front elevation would reflect the size and style of the windows of the host dwelling and would therefore be in keeping. It is considered that the proposed would have minimal impact on the character and appearance of the surrounding street scene.
19. The external materials for the proposed would be similar to the existing materials of the host dwelling including brick and stone cladding. It is therefore considered that the proposal would have minimal impact on the character of the existing dwelling and the surrounding street scene.
20. Paragraph 3.13 of the House Extensions SPD 2012 states that for semi-detached properties applications that involve the encroachment onto existing private garden or other amenity space should not result in the unacceptable reduction of such space. As a rule of thumb, 50 square metres is considered to be a reasonable minimum for this style of property; reduction below such a level is likely to constitute an overdevelopment of the site and harm the living conditions at the dwelling. In this instance the proposed first floor extensions would not increase the footprint of the existing property and would therefore have no impact on the existing amenity space of the application property.
21. Access would remain to the rear of the application property for the storage of refuse bins on none collection days.

22. In view of the above, it is considered that the proposed development would be in keeping with the host dwelling house and would not detract from the character and appearance of the street scene. Therefore, it is considered that the proposal would be in accordance with policies CG3 and RA3 of the Core Strategy.

Impact on the Amenity of Neighbouring Residents

23. Policy CG4 of the Core Strategy states that the Council will ensure that new development is compatible with surrounding land uses and occupiers, protecting amenity, privacy, safety and security and does not generate unacceptable nuisance, odours, fumes, noise or light pollution.
24. 67 to 73 Somerton Road are set to the front of the application property. The main room window in the front elevation of the proposed first floor side extension and garage conversion would be more than 25 metres away from the main room windows in the front elevations of these neighbouring properties. Paragraph 4.7 of the House extensions SPD 2012 states that between facing walls on the neighbouring house and the extension (whether single or two storey) which both contain main room windows the recommended interface distance is 21 metres. In this instance the recommended interface distance is exceeded by 4 metres. The proposed first floor rear extension would be shielded by the host dwelling and have no impact on these neighbouring properties, therefore the proposed is considered acceptable.
25. 78 Somerton Road is adjoining the north side of the application property (the other half of the semi pair). The proposed first floor rear extension would project 2.3 metres over the kitchen and dining room and would sit behind the 45 degree angle from the main room windows in the rear elevation of this neighbouring property as well as being set 3.5 metres from the shared boundary. Paragraph 4.14 of the House Extensions SPD 2012 states that single storey rear extensions of up to 3 metres in length (taken from the original rear elevation of the property) on semi-detached houses, will normally be acceptable (where they are not already permitted development). Similarly sized two storey rear extensions will normally be acceptable if set away from any shared boundary by at least 2 metres. This proposed element therefore complies with these recommendations. The proposed first floor side extension would be partially shielded by the host dwelling with the front section extending to the front elevation of the existing garage. This element would sit behind the 45 degree angle from the middle of the main room windows in the front elevation of this neighbouring property, therefore it is considered that this element would have no impact. The proposed is therefore considered acceptable.
26. 82 Somerton Road is set to the south side of the application property. The proposed first floor side and rear extensions would sit behind the 45 degree angle from the main room windows in the rear elevation of this neighbouring property therefore having minimal impact. The proposed side extension would extend to the existing garage's front elevation however this would not impinge on the main room windows in the front elevation of this neighbouring property. The windows in the side elevation of the proposed facing this neighbouring property would service two en-suites and would be conditioned to contain obscure glazing, it is therefore considered acceptable.
27. 2 and 4 Highbridge Close are set to the rear of the application property at an angle. The main room windows (as defined by Paragraph 4.4 of the House Extensions SPD 2012) in the rear elevation of the proposed first floor rear extension would have an indirect relationship and be more than 17 metres away from the main room windows in the rear elevations of these neighbouring properties. Paragraph 4.7 of the House Extensions SPD 2012 states that between walls on the neighbouring house and the extension (whether single or two storey) which both contain main room windows but which do not directly face each other the recommended

interface distance recommended is 17 metres. In this instance the proposed complies with the recommended interface distance. The proposed first floor side extension would not extend beyond the existing rear elevation therefore it is considered that this element would have no more impact on these neighbouring properties than the current situation. The proposed is therefore considered acceptable.

28. 4 Bideford Drive is set to the south west of the application property at a right angle, the windows in the proposed first floor side and rear extensions would be more than 15 metres away and would have no direct relationship with the main room windows in this neighbouring property's rear elevation, therefore it is considered that the proposed would have minimal impact. The windows in the side elevation of the proposed facing this neighbouring property would service two en-suites and would be conditioned to contain obscure glazing. The proposed is therefore considered acceptable.
29. In view of the above, it is considered that the proposed development would not unduly impact upon the amenity of neighbours and is therefore in accordance with policy CG4 of the Core Strategy.

Impact on Parking

30. Policy P5 of the Core Strategy states that the Council will ensure that developments take into account [amongst other things] parking, in accordance with the parking standards set out in Appendix 3. Policy S1.2 states that the Council will promote road safety in the design of new development.
31. SPD House Extensions provides general advice on house extensions and offers guidance relating to the impact of extensions on highway safety.
32. Highways provided the following comment: *No objections on highway grounds to what is being proposed under this application subject to 3 off-road parking spaces being provided in order to support the level of development proposed and the conversion of the integral garaging provision to a habitable room.*
33. Existing and Proposed Plans – Drawing Number - RAD/2440/21/1 – Date Drawn September 2021 indicates two car parking spaces within the curtilage of the site and will be conditioned.
34. The number of bedrooms will increase from 3 to 5 bedrooms, however two car parking spaces would be provided within the curtilage of the site. Due to the design and siting of the development, additional parking spaces are not available.
35. There are a number of surrounding dwellings that benefit from extensions increasing the number of bedrooms without additional parking spaces.
36. There are several major bus routes on Somerton Road close to the junction with Bideford Drive, within a three minute walk of the application property which service Bolton Interchange, Darcy Lever, Bradshaw and Bury.
37. There are no parking restrictions with the immediate vicinity.
38. It is therefore considered that the proposed would comply with policy P5.

Conclusion

39. The proposal is considered to comply with the relevant planning policies, in that it is considered

that the proposed development would be in keeping with the host dwelling house and would have minimal impact on the character and appearance of the street scene.

40. Members are therefore recommended to approve the application.

Representation and Consultation Annex

Representations

Letters:- No letters of objection received.

Elected Members:- Cllr. Dean is the applicant therefore the application is to be determined by Planning Committee.

Planning History

16574/81 - Erection of a single storey extension at front of house to provide a front porch and extended garage - Approved.

13202/79 - Erection of a single storey extension at rear of house - Approved.

Recommendation: **Approve subject to conditions**

Recommended Conditions and/or Reasons

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason

Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. Notwithstanding the submitted details, upon first installation of the first floor en-suite windows in the side elevation of the extension, hereby approved, facing 82 Somerton Road, shall be either non opening or with openings more than 1.7 metres above the finished floor level and fitted with obscure glazing whose obscuration level is 5 on a scale of 1-5 (where 1 is clear and 5 is completely) of the Pilkington Glass scale (or equivalent) and shall be permanently retained in that condition thereafter.

Reason

To protect the privacy, outlook and living conditions of neighbouring occupiers and in order to comply with Core Strategy policy CG4.

3. Notwithstanding the submitted details, the external surfaces of the extensions hereby permitted shall be of a similar colour, texture and size of those of the existing building, and shall be retained thereafter.

Reason

To ensure the development fits in visually with the existing building and safeguards the character and visual appearance of the locality and in order to comply with Core Strategy policy CG3.

4. The development hereby permitted shall be carried out in complete accordance with the following approved plans:

RAD/2440/21/1/RevA; "Existing & Proposed Plans"; revision dated 21/10/21

Reason

For the avoidance of doubt and in the interests of proper planning.

RECEIVED PLANNING APPEALS from 30/09/2021 to 02/11/2021

10199/21 **158 MARKLAND HILL LANE, BOLTON, BL1 5PB**

RETROSPECTIVE APPLICATION FOR THE ERECTION OF A RAISED PATIO AND SUN LOUNGE AT REAR

Decision date: **06-Apr-2021**

Decision: **Refused**

Decision level: **Delegated**

Appeal start date: **28-Jun-21**

Appeal ref: **APP/N4205/W/21/3276118**

DECIDED PLANNING APPEALS from 30/09/2021 to 02/11/2021

10199/21 **158 MARKLAND HILL LANE, BOLTON, BL1 5PB**

RETROSPECTIVE APPLICATION FOR THE ERECTION OF A RAISED PATIO AND SUN LOUNGE AT REAR

Decision date: **06-Apr-2021**

Decision: **Refused**

Decision level: **Delegated**

Appeal decision date: **13-Oct-2021**

Appeal decision: **Dismissed**

Appeal ref: **APP/N4205/W/21/3276118**

Summary of decided planning appeals

	Dismissed	Total
Total	1	1

RECEIVED ENFORCEMENT APPEALS from 30/09/2021 to 02/11/2021

Appeal start date:

Appeal ref:

DECIDED ENFORCEMENT APPEALS from 30/09/2021 to 02/11/2021

Decision date:

Decision:

Appeal ref:

Summary of decided enforcement appeals