

Planning Applications Report

Planning Committee

24th August 2017

**Bolton
Council**

Bolton Council has approved a Guide to Good Practice for Members and Officers Involved in the Planning Process. Appendix 1 of the Guide sets down guidance on what should be included in Officer Reports to Committee on planning applications. This Report is written in accordance with that guidance. Copies of the Guide to Good Practice are available at www.bolton.gov.uk

Bolton Council also has a Statement of Community Involvement. As part of this statement, neighbour notification letters will have been sent to all owners and occupiers whose premises adjoin the site of these applications. In residential areas, or in areas where there are dwellings in the vicinity of these sites, letters will also have been sent to all owners and occupiers of residential land or premises, which directly overlook a proposed development. Copies of the Statement of Community Involvement are available at www.bolton.gov.uk

The plans in the report are for location only and are not to scale. The application site will generally be in the centre of the plan edged with a bold line.

The following abbreviations are used within this report: -

CS	The adopted Core Strategy 2011
AP	The adopted Allocations Plan 2014
NPPF	National Planning Policy Framework
NPPG	National Planning Policy Guidance
PCPN	A Bolton Council Planning Control Policy Note
PPG	Department of Communities and Local Government Planning Policy Guidance Note
MPG	Department of Communities and Local Government Minerals Planning Guidance Note
SPG	Bolton Council Supplementary Planning Guidance
SPD	Bolton Council Supplementary Planning Document
PPS	Department of Communities and Local Government Planning Policy Statement
TPO	Tree Preservation Order
EA	Environment Agency
SBI	Site of Biological Importance
SSSI	Site of Special Scientific Interest
GMEU	The Greater Manchester Ecology Unit

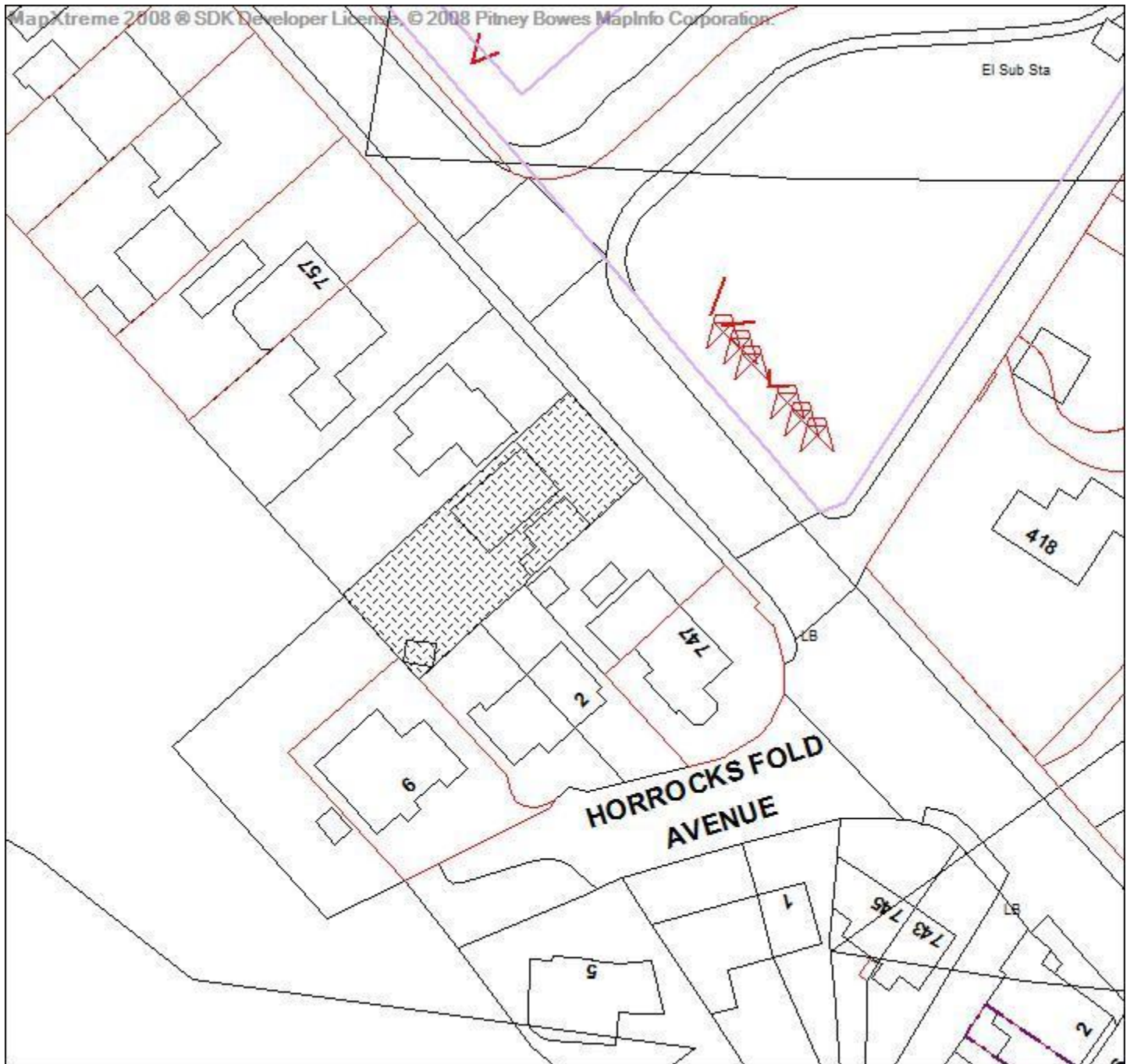
The background documents for this Report are the respective planning application documents which can be found at:-

www.bolton.gov.uk/planapps

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Application number
01124/17



Development & Regeneration Dept
Development Management Section

Town Hall, Bolton, Lancashire, BL1 1RU
Telephone (01204) 333 333

Bolton
Council

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Date of Meeting: 24/08/2017

Application Reference: 01124/17

Type of Application: Full Planning Application
Registration Date: 12/06/2017
Decision Due By: 06/08/2017
Responsible Officer: Lauren Kaye

Location: 751 BELMONT ROAD, BOLTON, BL1 7BY

Proposal: ERECTION OF A FIRST FLOOR REAR EXTENSION AND LOFT CONVERSION WITH INSTALLATION OF ROOFLIGHTS TO FRONT

Ward: Astley Bridge

Applicant: Mrs Pasarello
Agent : Mr McNeil

Officers Report

Recommendation: Approve subject to conditions

Proposal

1. This application seeks permission for the erection of a first floor rear extension and a loft conversion with the installation of roof lights to front.
2. The first floor extension would project 3.5 metres and measure 6.9 metres wide. It would benefit from a pitched roof matching the height of the existing eaves and ridge. It would feature a balcony set in to the extension along with a feature window at the top of the gable serving a bedroom within the loft space. Two roof lights are proposed to the front.

Site Characteristics

3. This is a detached property with a large rear garden and a driveway and garden to the front. It is sited within a row of residential properties on Belmont Road. There is a large, detached, flat roofed garage to the side of the dwelling providing one off-road parking spaces along with three off-road parking spaces on the driveway. To the rear is a garden which backs onto a neighbouring garden and open fields. The site is within the Green Belt.

Policy

4. National Planning Policy Framework (NPPF)
5. Core Strategies Policies: CG3 The Built Environment; CG4 Compatible Uses; OA5 North Bolton.
6. Allocations Plan Policies: CG7AP Green Belt.
7. SPD House Extensions; SPD General Design Principles

Analysis

8. Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise. Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission. Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission. It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.
9. The main impacts of the proposal are:-
- * impact on the character and appearance of the dwelling and the surrounding area
 - * impact on the amenity of neighbouring residents
 - * impact on the purposes and openness of the Green Belt

Impact on the Character and Appearance of the Dwelling and the Surrounding Area

10. Policy CG3 of the Core Strategy states that the Council will conserve and enhance local distinctiveness, ensuring development has regard to the overall built character and landscape quality of the area, and will require development to be compatible with the surrounding area, in terms of scale, massing, grain, form, architecture, street enclosure, local materials and landscape treatment.
11. Policy OA5 of the Core Strategy relates specifically to development in North Bolton and states that the Council will conserve and enhance the character of the existing physical environment, will ensure regard is had to the character of farm complexes, folds, vernacular cottages and the wider open landscape, and will require special attention to be given to the massing and materials used in new development.
12. SPD House Extensions provides general advice on house extensions and offers guidance relating to the effect of extensions on the appearance of the dwelling itself and the effect of extensions on the street scene and the character and appearance of the area.
13. The proposed first floor extension would project 3.5 metres and measure 6.9 metres wide providing a master bedroom and en suite. It would benefit from a pitched roof matching the height of the existing eaves and ridge with a feature window at the top of the gable serving a bedroom within the loft space. Two roof lights are also proposed in the front roof plane. The new master bedroom would benefit from a balcony set in to the extension with a balustrade measuring 1.1 metres high. The proposed balcony would measure 1.2 metres long and 3.7 metres wide and due to the small scale of the balcony it would be considered that its use would be restricted.
14. The proposed extension is sited to the rear of the property and as a result only the side elevation, when approaching the property from the south east of Belmont Road, would be partially visible. There would also be limited visibility from Horrocks Fold Avenue between 747/749 Belmont Road and 2 Horrocks Fold Avenue, where currently the neighbouring property at no. 753 Belmont Road is also visible to the same extent. It is considered that the proposed extension would not be visually obtrusive and would not have a detrimental impact on Horrocks Fold Avenue (which is an unadopted road) and Belmont Road. It is considered overall that the proposed extension would have little impact on the street scene and is therefore considered acceptable.

15. Paragraph 3.9 of the SPD House Extensions 2012 states that, "...one way of additional usable space within a dwelling is to convert the roof space into extra bedrooms or facilities such as bathrooms and studies. This usually involves providing a dormer structure and window as a means of providing necessary room height, light and ventilation to the room." The proposed loft conversion will be within the existing roof and would not include increasing the ridge height. The proposed window in the rear gable will serve the bedroom within the loft conversion and is set back 1.2 metres from the end of the roof plane due to the balcony below. It is considered that the loft conversion would not impact on the appearance of the front elevation which will remain the same from Belmont Road with the exception of two roof lights.
16. The proposed extension would be built in matching materials to the existing house using similar brick work and roof tiles. The access around the property will be retained to allow for the storage on bins on non-collection days.
17. The proposed extension is in proportion and in keeping with the scale and design of the existing property. It is considered that it would not have a detrimental affect on the visual appearance of the street scene or the character and appearance of the area, complying with policy.

Impact on the Amenity of Neighbouring Residents

18. Policy CG4 of the Core Strategy states that the Council will ensure that new development is compatible with surrounding land uses and occupiers, protecting amenity, privacy, safety and security.
19. SPD House Extensions provides general advice on house extensions and offers guidance relating to the impact of extensions on neighbouring properties, particularly in relation to natural light, privacy and overlooking.
20. SPD House Extensions (2012) provides guidance on the suitable interface distances: -
 - between a neighbouring elevation which contains a main room window and a facing wall of a single storey extension which does not. This distance is 9 metres.
 - between a neighbouring elevation which contains a main room window and a facing wall of a two storey extension which does not. This distance is 13.5 metres;
 - between walls on the neighbouring house and the extension (whether single or two storey) which both contain main room windows but which do not directly face each other. This distance is 17 metres;
 - between facing walls on the neighbouring house and the extension (whether single or two storey) which both contain main room windows. This distance is 21 metres.
21. The proposed extension is sited approximately 18.5 metres from the main room window in the neighbouring property to the south, no. 2 Horrocks Fold Avenue which is at a 90 degree angle from the application property. The adjoining neighbouring property, no. 4 Horrocks Fold Avenue is sited approximately 19.5 metres from the proposed extension, also indirectly. It is considered as the balcony is set into the extension, and owing to the distances involved, that any overlooking would be limited. The use of the balcony would also be restricted due to the small size of the balcony (it would only be able to accommodate a couple of chairs). The bedroom window proposed at the top of the gable would be set in and would also have a limited view to these neighbouring properties and is therefore considered acceptable.
22. The neighbouring property to the south west, no. 6 Horrocks Fold Avenue benefits from an extension to their garden to the side and rear (they have purchased land from the farm owner to the rear). As a result, their garden boundary extends across the full width of the application site.

The proposed extension is approximately 17 metres to the rear of the boundary meeting with the neighbouring property's garden. The proposed extension is also approximately 32.5 metres indirectly from the main room window in the rear elevation of 6 Horrocks Fold Avenue. It is considered that 17 metres provides a reasonable degree of privacy to the garden with the interface distance to the neighbouring property far exceeding the minimum requirement. It is also taken into consideration that the neighbouring property is sited on a higher ground level which would also lessen the impact of overlooking from the balcony, along with fencing and mature shrubbery along the common boundary and the neighbouring property benefiting from a large garden area to the side and rear.

23. The adjacent property to the south east, no. 749 Belmont Road is sited 14.5 metres from the side elevation of the application property due to the generous nature of the plots. As a result, the proposed first floor extension would not impinge on any main room windows of this neighbouring property.
24. The adjacent property to the north west, no. 753 Belmont Road benefits from a first floor bedroom extension to the side and original outrigger to rear. The window in the rear elevation of the bedroom extension is considered to be a secondary window to a principal bedroom as the window in the front elevation is the larger of the two (however the rear window would receive more sunlight owing to its orientation). There is also a window at the ground floor in the outrigger which serves the kitchen and window at first floor which serves a bathroom. Therefore, the proposed will not impinge on any main room windows of this neighbouring property and is considered acceptable.
25. The proposed first floor extension due to the orientation of the sun's path would not unduly impact on the natural light to the above neighbouring properties and their main room windows and would not cause overshadowing.
26. It is therefore considered that the proposal will not unduly harm the amenity of neighbouring residents, compliant with Policy CG4 of the Core Strategy.

Impact on the Purposes and Openness of the Green Belt

27. Section 9 of the NPPF states that the Government attaches great importance to Green Belts. The fundamental aim of Green Belt policy is to prevent urban sprawl by keeping land permanently open; the essential characteristics of Green Belts are their openness and their permanence. Paragraph 80 states that Green Belt serves five purposes, which include assisting in safeguarding the countryside from encroachment and assisting in urban regeneration by encouraging the recycling of derelict and other urban land.
28. Paragraph 87 states inappropriate development is, by definition, harmful to the Green Belt and should not be approved except in very special circumstances. Paragraph 89 continues that a local planning authority should regard the construction of new buildings as inappropriate in Green Belt, but exceptions to this include [amongst other things] the extension or alteration of a building provided that it does not result in disproportionate additions over and above the size of the original building. Allocations Plan Policy CG7AP reflects the NPPF's guidance on Green Belt.
29. Given the urban nature of this row of dwellings, the size of the site to comfortably accommodate the proposal and the size of the extension, the proposal, is not considered that the extension would have any detrimental impact on the openness of the Green Belt. To the rear, other properties on the row have been extended to varying degrees, in a non-uniform manner and it is considered that the proposal is not a disproportionate addition.

30. It is considered that the proposal would comply with the National Planning Policy Framework and Allocations Plan Policy CG7AP.

Conclusion

31. For the reasons discussed above it is considered that the first floor rear extension and loft conversion would not harm the character and appearance of existing dwelling or street scene and would not unduly harm the amenity of any neighbouring residents.

32. Members are therefore recommended to approve this application.

Representation and Consultation Annex

Representations

Letters:- Three objections have been received on the following grounds:

- will not be in keeping with the other properties along Belmont Road.
- application 91235/13 was amended as a result of objections and officers rejected the original plans for the rear of the property as they were not in keeping (*officer's comment: 91235/13 was recommended for approval and went to Committee in 2013. It was requested that the proposed Juliet balconies were modified slightly to amend the fenestration to provide more symmetry and the roof hipped. The officer did not request that the balconies were to be removed*).
- all properties have hipped roofs to the rear to keep the open aspect.
- the proposal would change the whole aspect to the rear.
- the balcony and glass wall would overlook the neighbouring properties and gardens (*officer's comment: this is addressed above in the report*).
- breach of rights to private and family life under Article 8 of the Human Rights Act 1998 (*officer's comment: this is not a material planning consideration*).
- changes the property from two storey to three storey.
- block natural light to the neighbouring properties (*officer's comment: the extension would be sited to the north of the neighbouring properties and therefore would not sunlight*).

Elected Members:- the application appears at Planning Committee at the request of Councillor Fairclough.

Consultations

None.

Planning History

None.

Recommendation: **Approve subject to conditions**

Recommended Conditions and/or Reasons

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason

Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. The external surfaces of the extension hereby permitted shall be of a similar colour, texture and size of those of the existing building, and shall be retained thereafter.

Reason

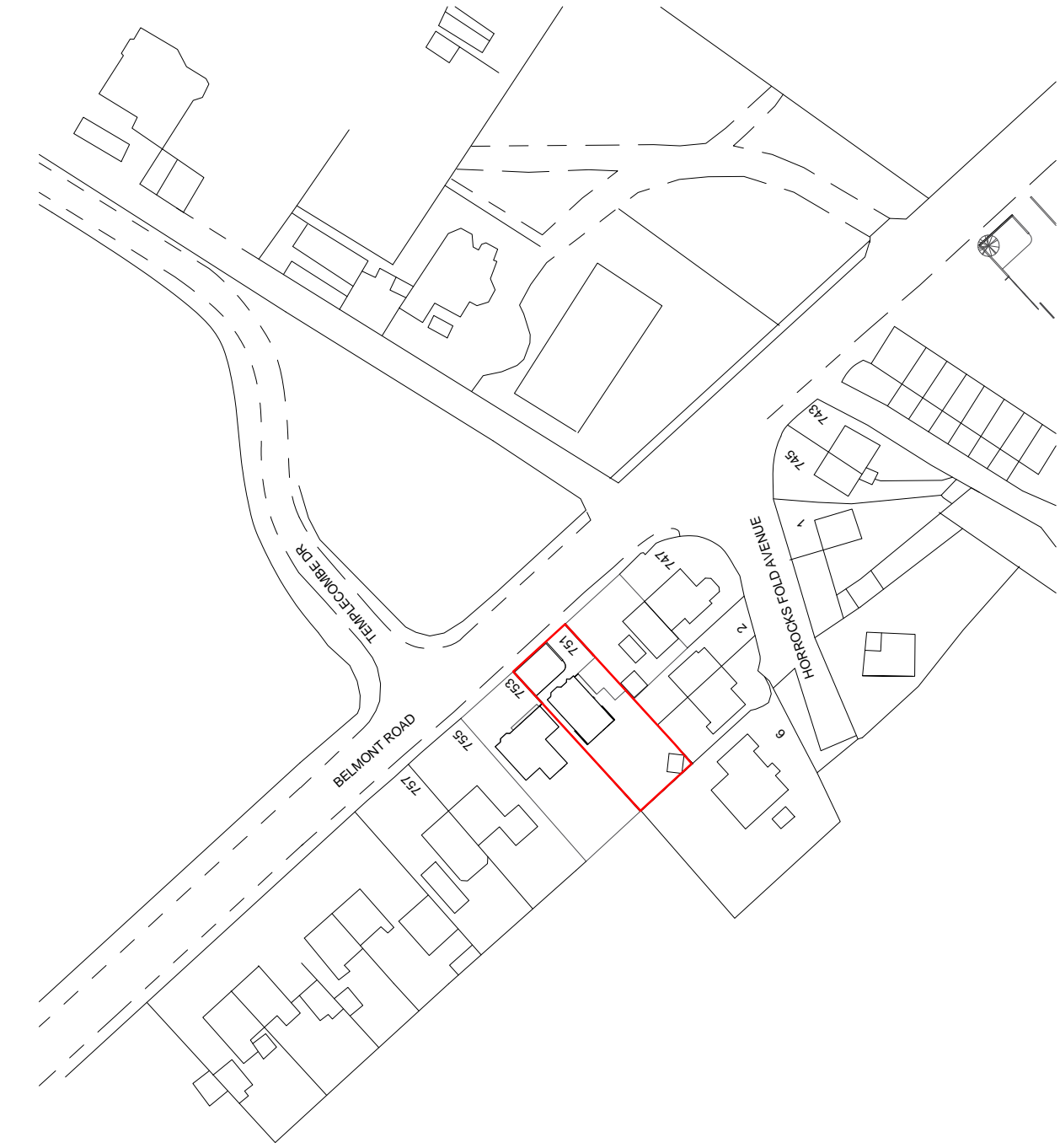
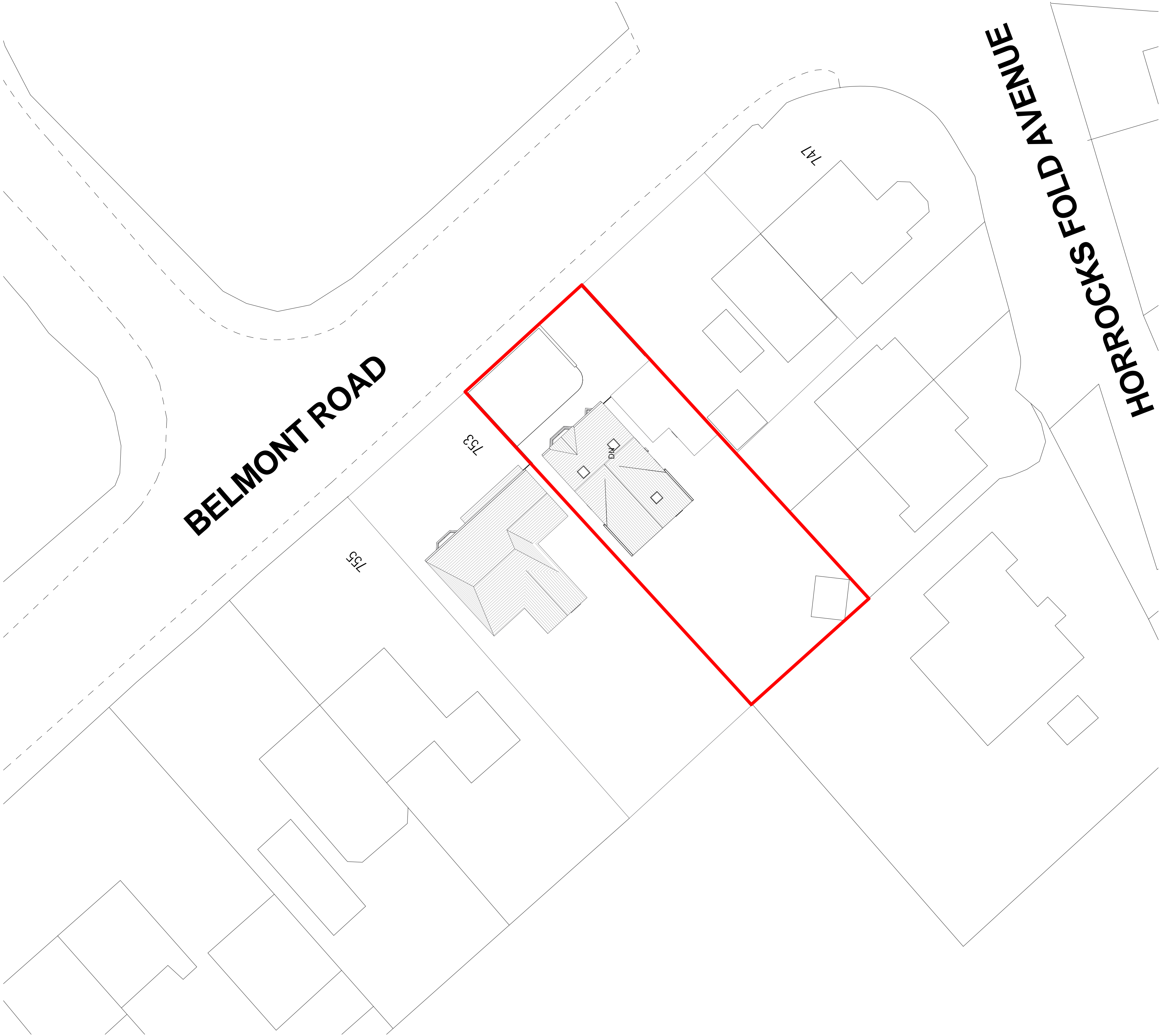
To ensure the development fits in visually with the existing building and safeguards the character and visual appearance of the locality and in order to comply with Core Strategy policy CG3.

3. The development hereby permitted shall be carried out in complete accordance with the following approved plans:

L900 Rev A; "Site Plan"; dated 09/06/17
L1000 Rev A; "Proposed Floor Plans"; dated 09/06/17
L1001 Rev A; "Proposed Floor Plans"; dated 09/06/17
L1110 Rev A; "Proposed Elevations - North and South"; dated 09/06/17
L111 Rev A; "Proposed Elevations - East and West"; dated 09/06/17
L1200 Rev A; "GA Proposed Section!; dated 09/06/17

Reason

For the avoidance of doubt and in the interests of proper planning.

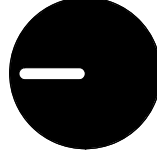


KEY: SITE BOUNDARY

GENERAL NOTES

Contractor to visit site prior to commencement of work and check all dimensions and familiarise themselves with the site conditions. This drawing must then be checked and verified by the contractor prior to work commencing on site.


This drawing is for the purpose of achieving local authority approval only and should not be used as a working drawing. Client must have received written approval from both the local authority and the relevant planning authority prior to the start of construction or commencement of any building works on the relevant site. All workmanship, materials and construction to comply with Building Regulations, British Standards, Codes of Practice and N.H.B.C requirements whether or not specifically stated on these drawings.



REVISION	DATE	DESCRIPTION	ARCHITECT
A	09/04/17	Planning set of drawings	

CHECK ALL DIMENSIONS AND VERIFY ON SITE. REPORT ANY ERRORS OR OMISSIONS.

PLANNING



KMI Architecture + Design

178 BLACKBURN ROAD
BURNLEY, BL1 1AB
T: 01751 444100
E: info@kmiarchitecture.com

JOB TITLE:
751 BELMONT ROAD,
SHARPLES

DRAWING TITLE:
SITE PLAN

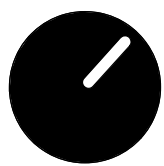
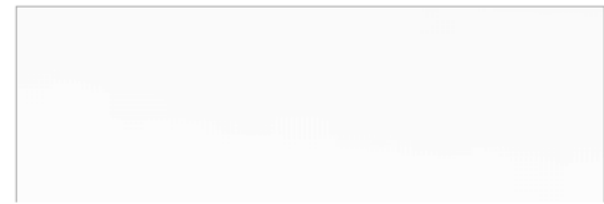
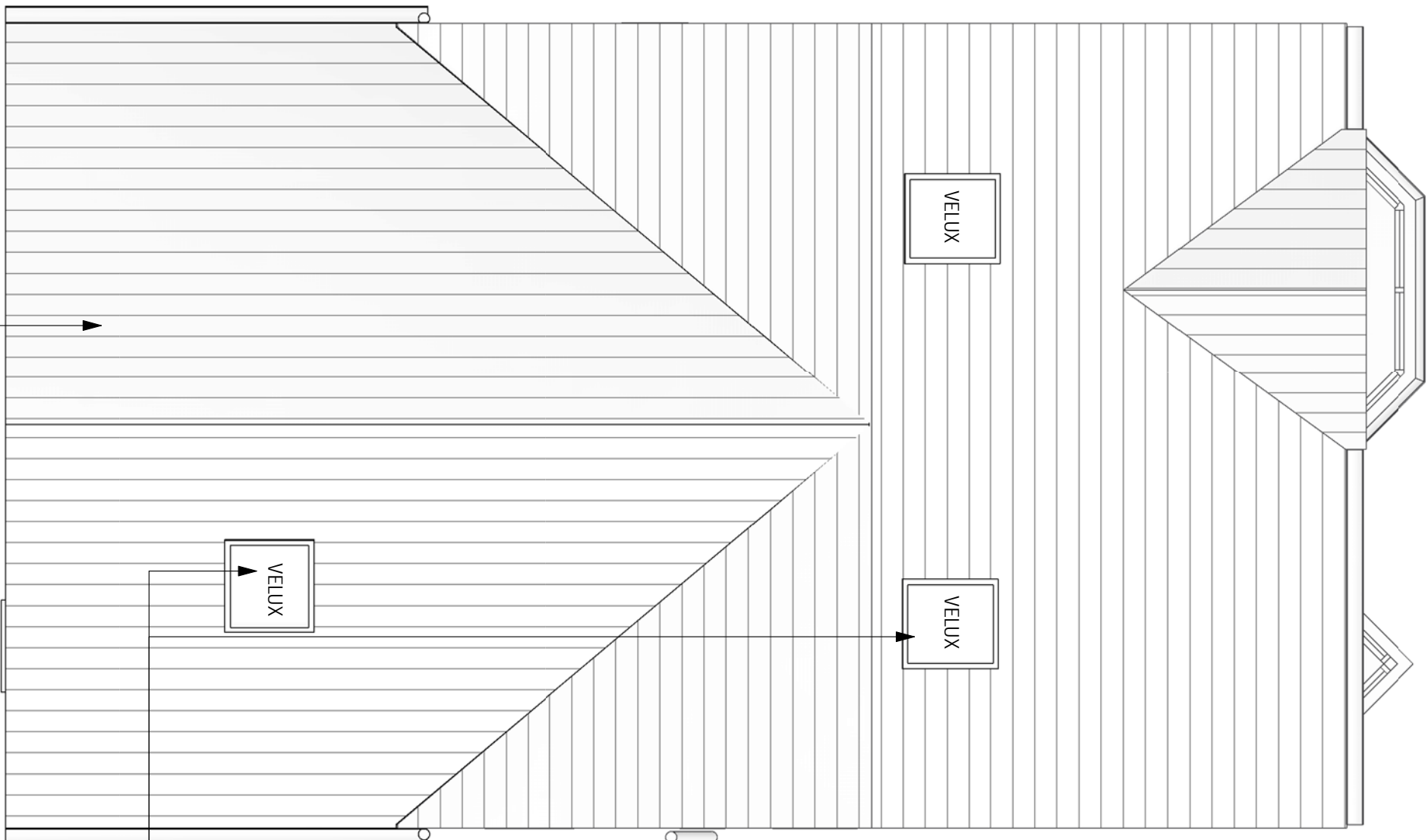
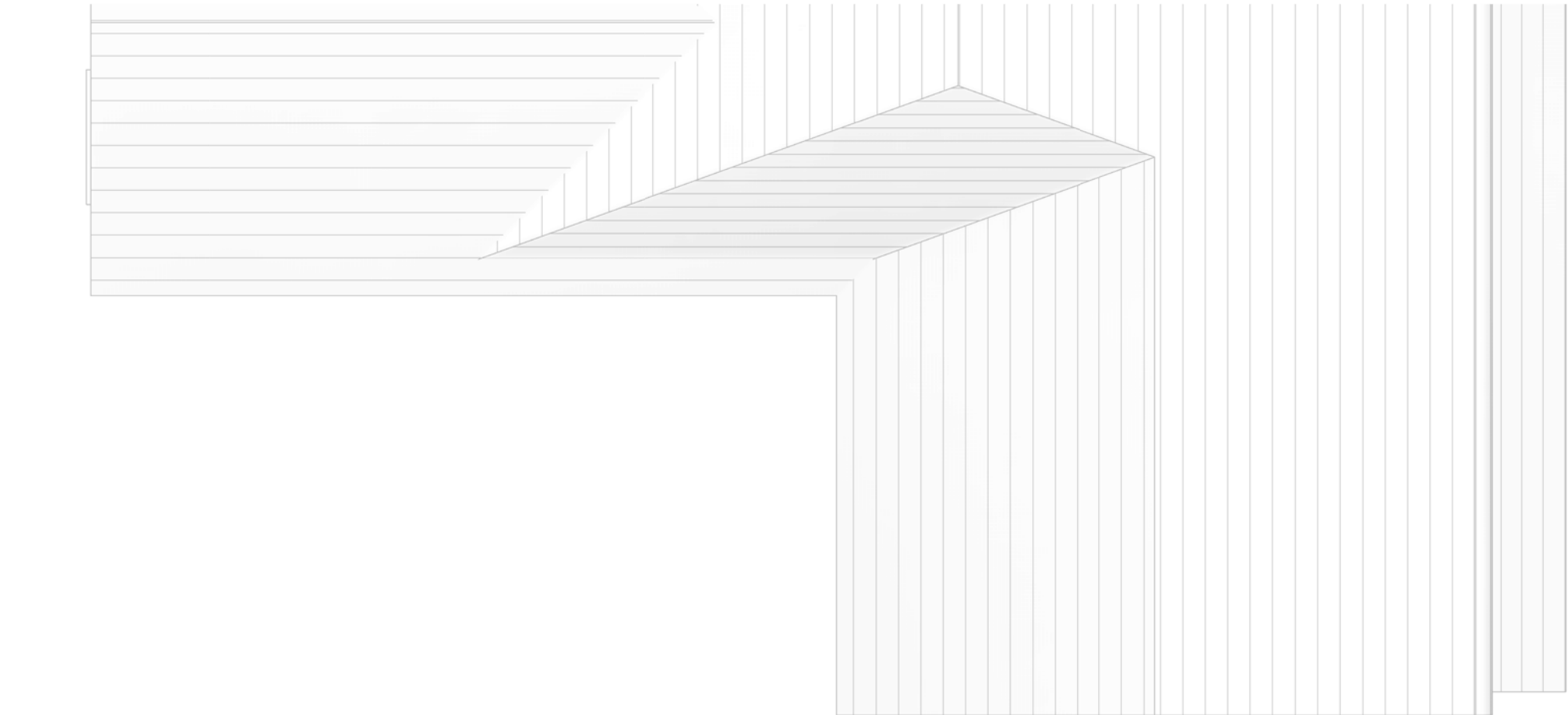
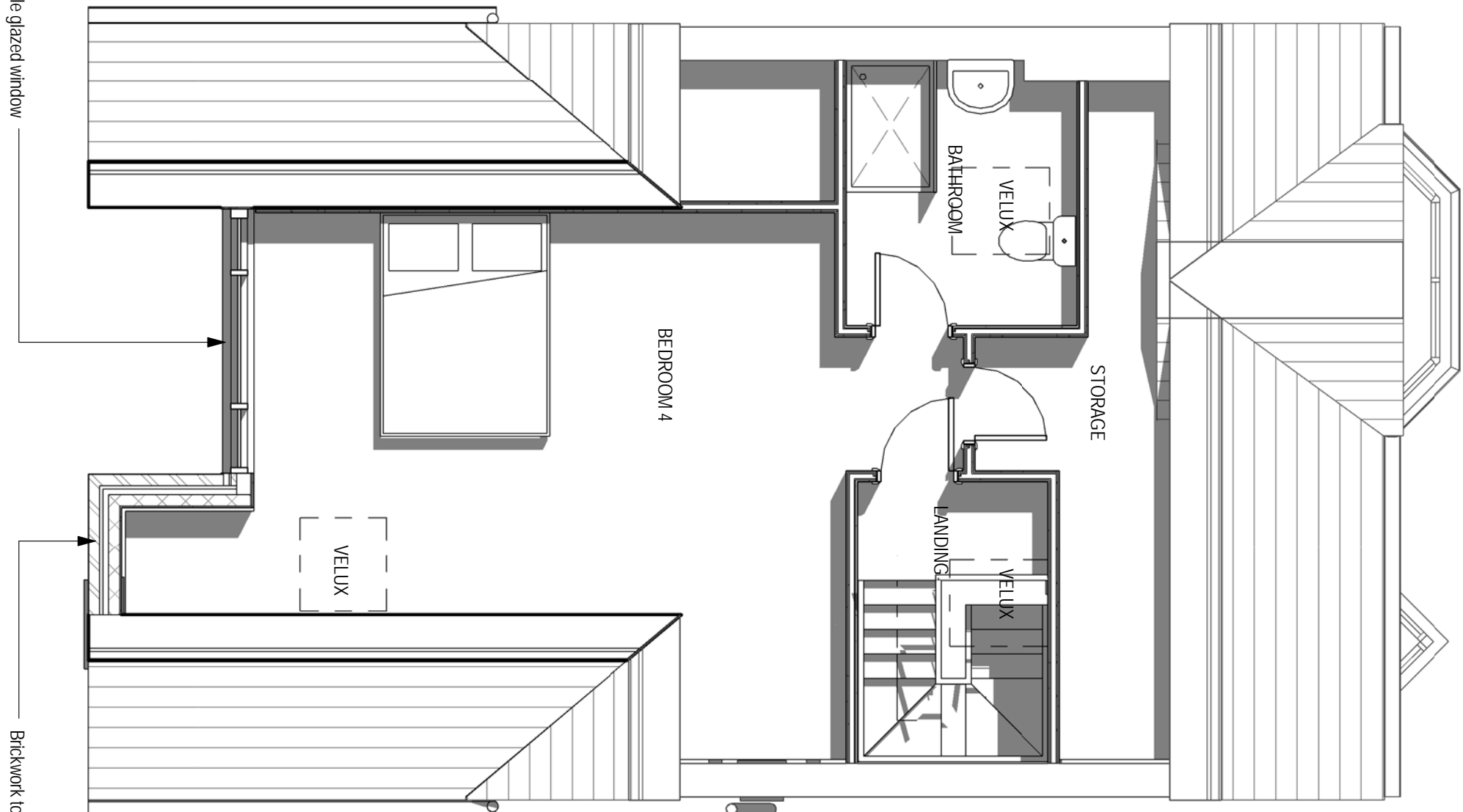
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JOB CODE: K/M01	DRAWING NUMBER: L900
REVISION: A	

1 SITE PLAN

1 : 200

GENERAL NOTES

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This drawing is for the purpose of achieving local authority approval only and should not be used as a working drawing. Client must have received written approval from both the local authority planning and building control departments prior to commencement of any work on the development site. All workmanship materials and construction to comply with Building Regulations, British Standards, Codes of Practice and N.H.B.C requirements whether or not specifically stated on these drawings.



REVISION	DATE	DESCRIPTION	APPROVAL
A	09/06/17	Planning set of drawings	

CHECK ALL DIMENSIONS AND VERIFY ON SITE. REPORT ANY ERRORS OR DISCREPANCIES.

PLANNING

1
1 : 50
PROPOSED SECOND FLOOR

2
1 : 50
PROPOSED ROOF PLAN

178 BLACKBURN ROAD
BLACKBURN, LANCASHIRE
BB1 1JH
E: info@kmiarchitects.com

JOB TITLE:
751 BELMONT ROAD,
SHARPLES

DRAWING TITLE:
PROPOSED FLOOR PLANS

SCALE:
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JOB CODE:
KM01

DRAWING SHEET SIZE:
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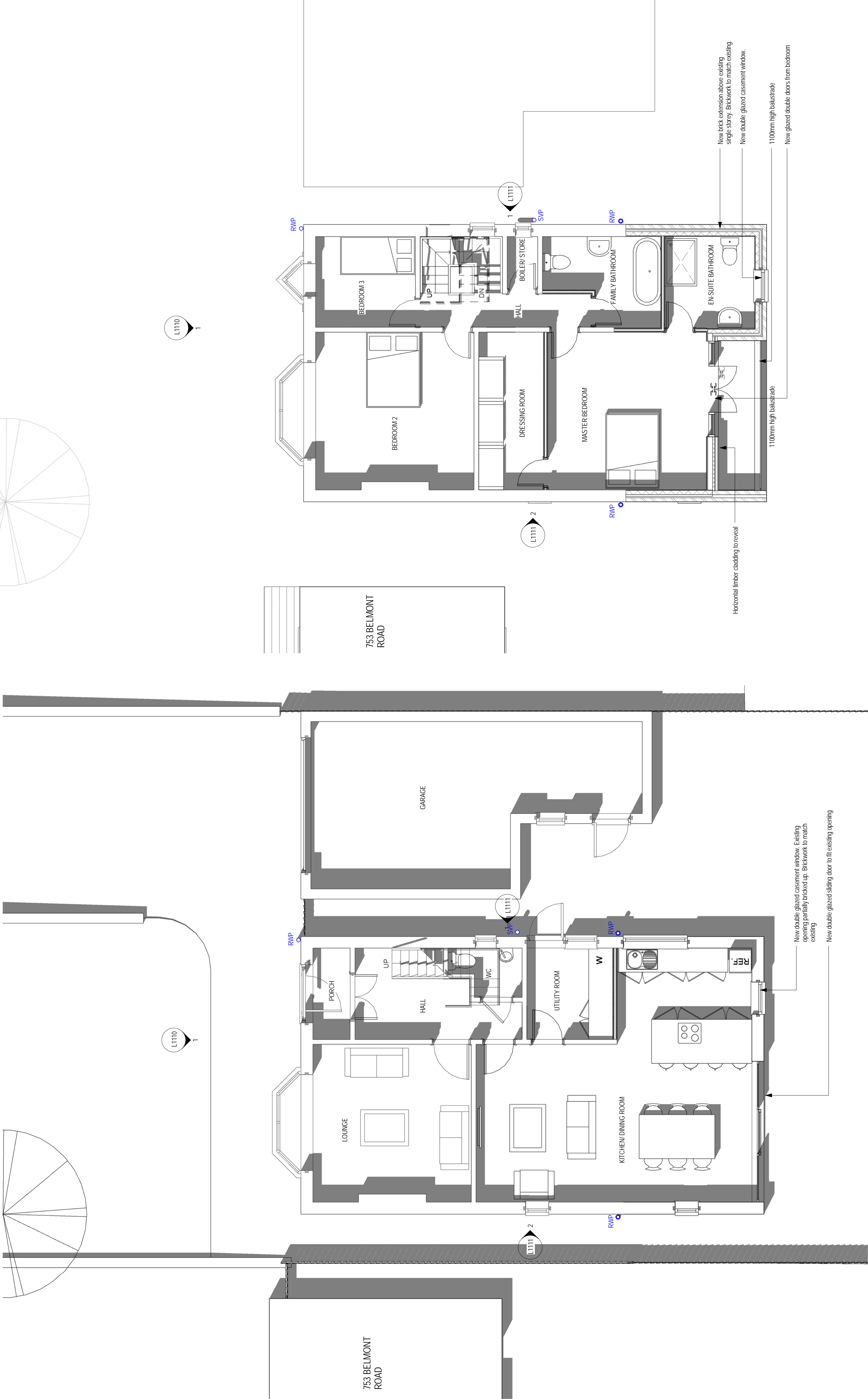
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L1001

REVISION:
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GENERAL NOTES

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1 PROPOSED GROUND FLOOR PLAN

1 : 50

2 PROPOSED FIRST FLOOR PLAN

1 : 50

PLANNING



198 BLACKBURN ROAD
BURNLEY, LANCASHIRE
BB10 1AA
E: info@kmiarchitect.com

JOB TITLE:
751 BELMONT ROAD,
SHARPLES

DRAWING TITLE:
PROPOSED FLOOR PLANS

SCALE:
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DRAWING SHEET SIZE:
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JOB CODE:
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DRAWING NUMBER:
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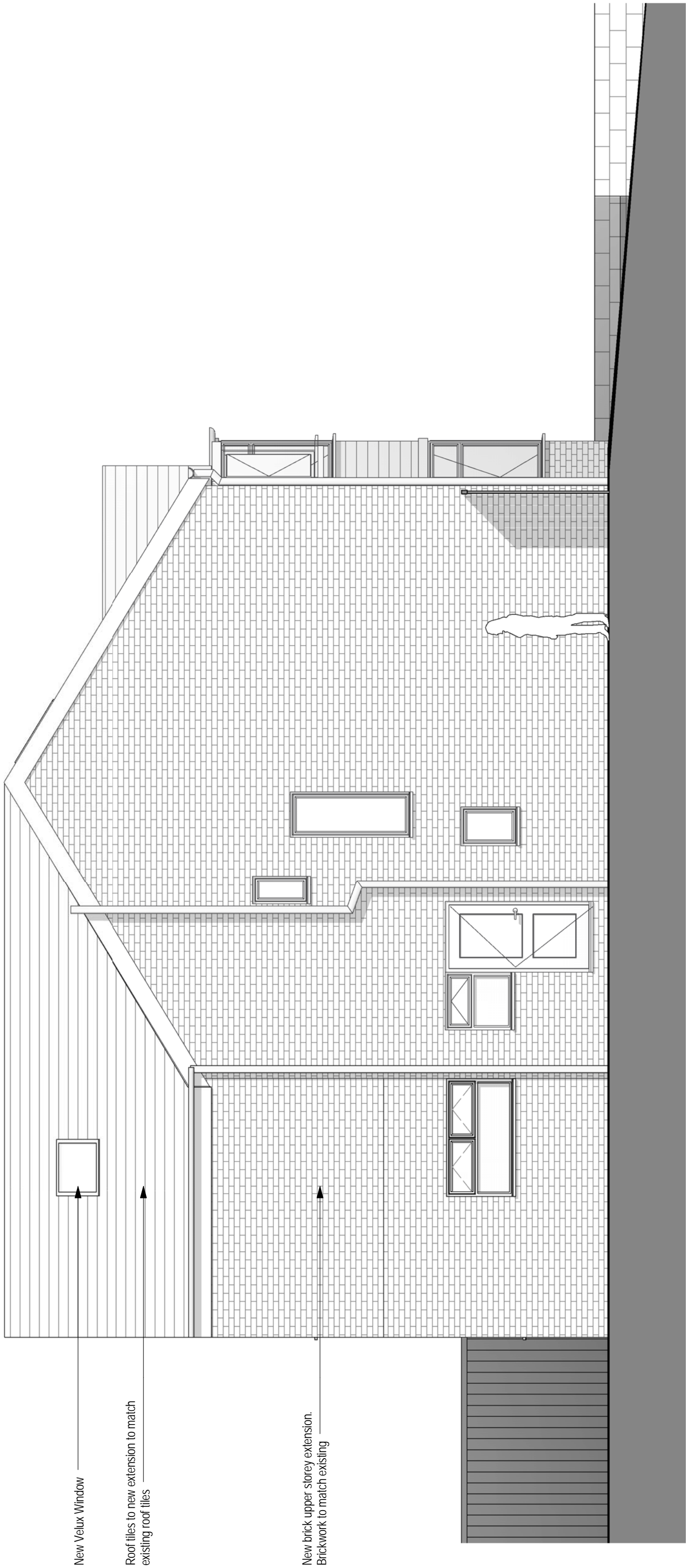
REVISION:
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ARCHITECT

GENERAL NOTES

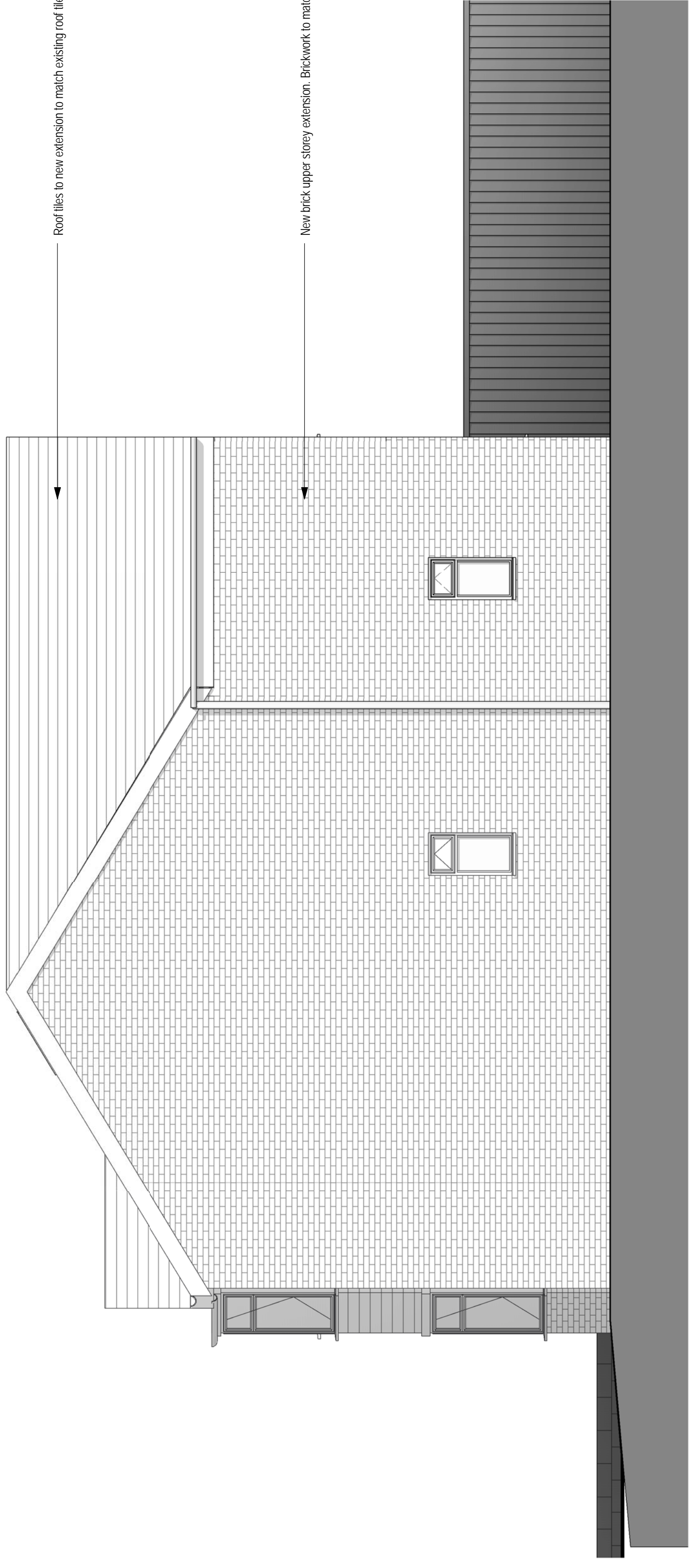
Contractor to visit site prior to commencement of work and check all dimensions and familiarise him/herself with the site conditions. This drawing must then be checked and verified by the contractor prior to work commencing on site.

This drawing is for the purpose of achieving local authority approval only and should not be used as a working drawing. Client must have received written approval from both the local authority planning and building control departments prior to construction. Contractor must comply with all rules on the relevant site. All workmanship, materials and construction to comply with Building Regulations, British Standards, Codes of Practice and N.H.B.C requirements whether or not specifically stated on these drawings.



1 PROPOSED EAST ELEVATION

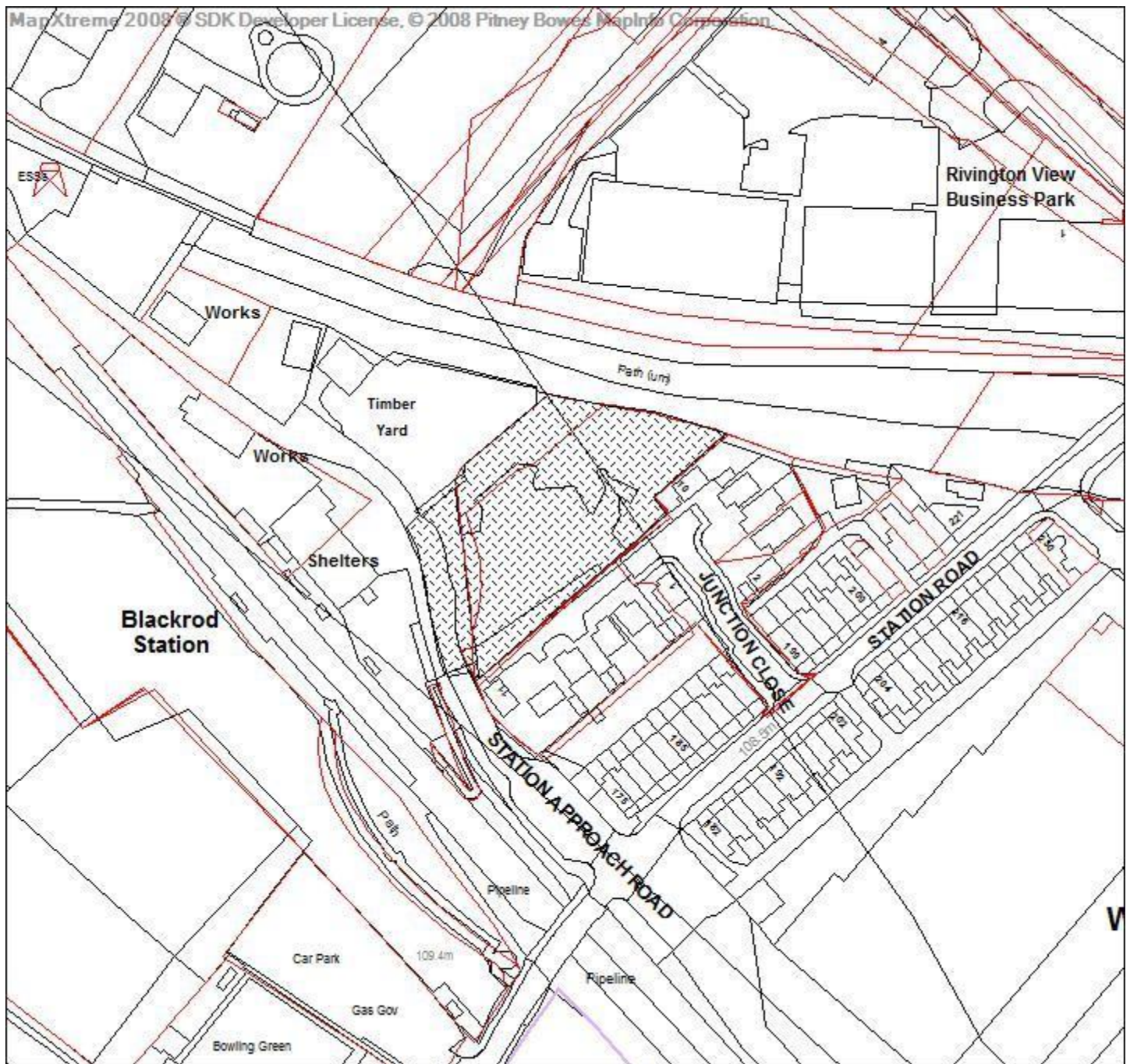
1 : 50



2 PROPOSED WEST ELEVATION

1 : 50

Application number 01247/17



Development & Regeneration Dept
Development Management Section

Town Hall, Bolton, Lancashire, BL1 1RU
Telephone (01204) 333 333

Bolton Council

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Date of Meeting: 24/08/2017

Application Reference: 01247/17

Type of Application: Outline Planning Permission
Registration Date: 27/06/2017
Decision Due By: 25/09/2017
Responsible Officer: Helen Williams

Location: LAND AT STATION YARD, OFF STATION ROAD, BLACKROD, BOLTON, BL6

Proposal: OUTLINE APPLICATION FOR THE ERECTION OF 12NO. RESIDENTIAL DWELLINGS (ACCESS AND LAYOUT DETAILS ONLY)

Ward: Horwich and Blackrod

Applicant: Mr Green
Agent : Mr G Luxton

Officers Report

Recommendation: Approve subject to conditions

Proposal

Outline permission is sought for the erection of 12 dwellings (four detached and eight semi-detached dwellings). Only the access and layout details of the dwellings are currently sought, with the details of appearance, scale and landscaping being reserved matters.

Two previous outline planning applications for the erection of 11 detached dwellings on the site were refused in 2013 and in 2015. The latter application (93108/14) was refused by Planning Committee in January 2015 for the following reason:

"The proposed residential development of the site would be incompatible with the existing neighbouring industrial and commercial land uses and would fail to protect the amenity of future residents of the development, particularly with regard to odours, noise and general disturbance created by the neighbouring industrial and commercial operations, contrary to Policy CG4 of Bolton's Core Strategy."

Application 90454/13 (which 93108/14 was a resubmission of) was refused for the same reason under delegated powers in November 2013.

This latest application differs from the previous refusals in the following main ways:

- * A landscaped earth bund with 1.8 metre high acoustic fencing on top is proposed along the north and west boundaries of the site, as well as close boarded acoustic fencing around the perimeter of the dwellings, whereas the previous proposal (93108/14) only proposed a landscape bund with acoustic fencing at the northern boundary.

- * The applicant has now submitted a noise assessment, an odour assessment and an air quality assessment in support of their application (the previous applications did not submit such supporting information).
- * 12 semi-detached and detached dwellings are now proposed instead of 11 detached dwellings.
- * The redesign of the layout has resulted in fewer dwellings being sited adjacent the northern boundary (with Dickinson's).

Access into the development is proposed off Station Approach Road. The cobbled part of the road to the front of the application site is proposed to be resurfaced and the existing 1.8 metre wide footway is proposed to be extended into and across the frontage of the development.

Each dwelling would have parking provision for two vehicles (with plots 5 and 6 having a garage as well as a driveway).

Site Characteristics

The application site is a vacant piece of land between the houses on Junction Close and the industrial units at Station Yard. The site was previously used for the storage of equipment in association with preparation works for the electrification of the adjacent railway line. A portable building continues to be sited to the north west of the site. The site is bounded by grey palisade fencing, with timber fencing to the south (along the boundary with Junction Close). There is a gated access to the west (onto Station Approach). The land slopes slightly to the north.

The houses at 1 to 9 (odd) Junction Close front the application site and 10 and 11 Junction Close side onto it. There are industrial/commercial units to the west of the site (at Station Yard) and there is a timber yard with stacked pallets immediately to the north west (Gilly's Yard). To the north of the site and across the disused railway line is J. Dickinson & Sons, a large waste transfer business. The buildings at Dickinson's are partly screened by trees along the railway embankment.

Station Yard is accessed via Station Approach off Station Road (B5238). This access road becomes cobbled past 11 Junction Close.

Blackrod Station is to the south west of the site. Horwich Waste Water Treatment Works is approximately 90 metres to the north west of the application site.

Policy

National Planning Policy Framework (NPPF)

Core Strategy Policies: P5 Transport and Accessibility; S1 Safe Bolton; CG3 The Built Environment; CG4 Compatible Uses; SC1 Housing; OA1 Horwich and Blackrod.

SPD General Design Principles; SPD Accessibility, Transport and Safety.

Analysis

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- * impact on urban regeneration
- * impact on the amenity of future residents
- * impact on the amenity of neighbouring residents
- * impact on the character and appearance of the area
- * impact on the highway

Impact on Urban Regeneration

The NPPF encourages the effective use of land by reusing land that has been previously developed (brownfield land) and states that housing applications should be considered in the context of the presumption in favour of sustainable development.

Policy SC1 of the Core Strategy states that the Council will identify a range of housing sites for additional provision and at least 80% of housing development will be on previously developed land. Policy OA1.4 states that the Council will concentrate sites for new housing in Horwich town centre, at Horwich Loco Works and on other sites within the existing urban area.

The application site is previously developed land (as it is gravelled and was used for storage in conjunction with an industrial business) and it is within the urban area. It is considered that the site is within a highly sustainable location, being close to Blackrod railway station, on a main thoroughfare between Blackrod and Horwich, and being close to shops, schools and local services.

It is therefore considered that the proposal would represent an effective re-use of land within the urban area and would provide new housing in a highly sustainable location, compliant with Policies SC1 and OA1.4 of the Core Strategy.

Impact on the Amenity of Future Residents

Policy CG4 of the Core Strategy states that the Council will ensure that new development is compatible with surrounding land uses and occupiers, protecting amenity, privacy, safety and security, and that development does not generate unacceptable nuisance, odours, fumes, noise or light pollution, nor cause detrimental impacts upon water, ground or air quality.

The application site is located within an existing industrial estate, Station Yard, which contains a number of industrial and commercial units. The site is only approximately 30 metres away from J. Dickinson & Sons (a large waste transfer and treatment facility) and Gilly's Yard (a pallet business) immediately adjoins the application site to the north west.

The previous two planning applications (90454/13 and 93108/14) were refused as it was considered by officers that residential development of the application site would be incompatible with the neighbouring industrial uses, and the proposal would fail to protect the amenity of future residents of the development with regard to odours, noise and general disturbance. During the determination of these two applications the Council's Pollution Control Officers raised an objection to the proposals as they considered that future residents of the development would be likely to complain about odours and disturbance from Dickinson's owing to the proximity of the dwellings to the waste treatment facility. Concerns were also raised about the proximity to Gilly's Yard, a business that stores and repairs pallets, as this long established business has no restrictions to their hours of working. The Environment Agency raised concerns regarding the first application (90454/13) owing to odours from

Dickinson's, but did not comment on the second application.

As with the last planning application (93108/14) the applicant is again proposing a landscaped bund with a 1.8 metre high acoustic (close boarded) fence on top along the whole northern edge of the application site, however they are also now proposing that the landscaped bund with acoustic fence is extended along the length of the western boundary of the site, *and* that an acoustic close boarded fence is erected at the perimeter of the dwellings (adjacent the bund).

Unlike the previous two applications the applicant has now commissioned a noise assessment, an odour assessment and an air quality assessment in support of their proposal. All three assessments have been undertaken by Royal Haskoning DHV.

Noise

The noise assessment confirms that the primary source of noise that would affect the proposed development would be operational noise from the neighbouring commercial premises, with other noise sources being road traffic and rail usage. Noise surveys were conducted from the 27th to the 29th April 2017 at a number of locations within the site (to represent where the dwellings would be sited). The assessment found that the noise climate at the site was dominated by distant continuous noise from the motorway, with contributions from intermittent road traffic on Station Road, infrequent rail usage and activities at the nearby commercial premises.

To predict the noise effects at the locations of the proposed dwellings a noise model of the proposed layout was created and calibrated to the highest measured daytime and night-time LAeqT at the application site in order to provide a conservative assessment. Noise levels at the most exposed facades to the site boundary (adjacent the commercial premises and railway line) were chosen for this purpose. Based upon an assumed 15dB attenuation from an open window the calculated noise levels indicated that further mitigation measures would be required for the dwellings proposed on the boundaries. The noise assessment therefore recommends that sound insulation measures are undertaken, such as acoustic glazing and alternative means of ventilation.

In terms of external space (the proposed gardens) the assessment found that it was possible that the noise levels in some gardens could exceed the lower guideline values. The applicant's consultant therefore recommends that acoustic fencing be erected. Following initial concerns from the Council's Pollution Control Officers the applicant has amended their plans further, to propose two areas of acoustic fencing; around the perimeter of the dwellings (garden fences) and along the proposed landscape bunds to the north and west boundaries of the site. When the consultant re-ran the modelling exercise taking into account this additional fencing, the assessment indicates that the predicted noise levels in the gardens would be acceptable.

Odours

The odour assessment recognises that the application site is located in close proximity to J. Dickinson & Sons (a materials recycling facility) and Horwich Waste Water Treatment Works, which both have the potential for the release of odours. The risk of odours from these neighbouring uses has been fully considered. The assessment also acknowledges that there have historically been complaints relating to odour and odour related pests from residential properties neighbouring the application site, however the author states that there are no records within the last three years (from the Council and the Environment Agency) of reported odour events.

The odour assessment has taken into consideration representative meteorological data/conditions for the area and states that the predominant wind direction in the area is from the south and west, with regular winds also coming from the south west. The assessment therefore concludes, as the application site is located to the south and south east of the potential odour sources, the prevailing

wind is likely to disperse odours away from the proposed dwellings.

The assessment does acknowledge that, over the short term, wind can blow from any direction, however the data the author has collected confirms that the application is located upwind of the potential odour sources for a greater number of hours (between the years of 2012 to 2016) than downwind. As such, winds would disperse odours away from the site for the majority of the year, which would reduce the risk of odour impacts at the proposed residential development.

The consultant's examination of records held on the Environment Agency public register indicate that there are measures in place at Dickinson's to reduce and contain potential odour emissions, though it is noted that the Waste Water Treatment Works' processes are not regulated under the Environmental Permitting Regime.

The odour assessment concludes that there would be a "slight adverse" impact on future residents of the proposed residential development, with the risk of odour effects at the dwellings being considered to be "not significant".

Air quality

The air quality assessment has reviewed the existing air quality in proximity to the application site and has assessed the suitability of the site for residential use with regard to air quality and also the potential for the proposed development (during construction) to impact on local air quality. It is noted that the application site is situated approximately 150 metres to the west of a section of the statutory designated Greater Manchester Air Quality Management Area (AQMA).

The assessment has assessed emissions from the M61, Station Road and the A6, along with meteorological data. Concentrations of NO₂, PM₁₀ and PM_{2.5} were predicted at the proposed dwellings and the predicted concentrations were compared to the long-term and short-term objectives to determine the suitability of residential development at the application site. The pollutant concentrations at the proposed dwellings have been predicted to be below the respective air quality objectives and therefore the consultant concludes that the proposed residential development would be acceptable with regard to air quality.

The Council's Pollution Control Officers have reviewed the three supporting assessments and raise no objection to the proposed residential development of the site subject to the proposed mitigation measures (which they recommend are conditioned).

The Environment Agency has been consulted on the application but at the time of writing the officer's report they had provided a comment. Should the Environment Agency comment on the proposal, this will be reported directly to the meeting.

Officers consider that whilst the application site is not the most ideal site for residential development, given its close proximity to neighbouring industrial uses that have the potential to create noise and odour disturbance to residents, there are mitigation measures that can be implemented to reduce this risk of disturbance to an acceptable level. Subject to further details relating to the proposed acoustic fencing, the proposed landscaped bunds along the northern and western boundaries of the site, and acoustic glazing and alternative means of ventilation (which are suggested by conditions), it is considered that the amenity of future residents would not be so unduly harmed as to warrant a refusal of this application.

It is therefore considered, subject to the relevant mitigation conditions, that the proposed development would comply with Policy CG4 of Bolton's Core Strategy.

Impact on the Amenity of Neighbouring Residents

Policy CG4.1 of the Core Strategy states that the Council will ensure that new development is compatible with surrounding land uses and occupiers, protecting amenity, privacy, safety and security. SPD General Design Principles sets out the Council's recommended interface distances between dwellings.

There are residential properties to the immediate south of the application site on Junction Close, a residential cul-de-sac off Station Road. 1 to 9 Junction Close (odd numbers) face the application site and 10 and 11 Junction Close side onto the site.

The front elevations of 3 and 9 Junction Close would face the side elevations of the proposed dwellings at plots 10 and 1 (respectively). As the application is only in outline, the scale and appearance of the dwellings have not been applied for, but it is expected that the dwellings would be two storey in height (as they are proposed to contain three bedrooms) and would not have any main windows situated in their side elevations. The SPD recommends an interface distance of 13.5 metres where a front elevation (containing main windows) faces a side elevation (containing no main windows).

In the case of 9 Junction Close, this interface distance would be exceeded (15.6 metres).

The proposed site plan shows an interface distance of only 11.7 metres between the front of 3 Junction Close and the side of the dwelling proposed at plot 10, however this line is not taken from the centre of the lounge and bedroom windows (the main windows), which would instead mainly overlook the rear garden to plot 10 (as the dwelling would not be sited directly in front of these windows). It is therefore considered that plot 10's relationship with 3 Junction Close is acceptable, particularly as plot 10 would be sited to the north of the existing neighbour (and therefore would not restrict sunlight).

The dwelling at plot 11 is proposed to be sited, side on, to the side elevation of 10 Junction Close. There are no main windows in the side elevation of no. 10. The proposed dwelling would be sited forward of no. 10 but would not infringe upon a line drawn at a 45 degree from the centre of the nearest main window at the front of no. 10. It is therefore considered that the siting of the dwelling at plot 11 would not unduly affect the amenity of the residents of 10 Junction Close.

For these reasons it is considered that the proposed siting of the dwelling would not unduly harm the amenity of any neighbouring resident, compliant with Policy CG4 of Bolton's Core Strategy.

Impact on the Character and Appearance of the Area

Policy CG3 of the Core Strategy states that the Council will expect development proposals to contribute to good urban design, conserve and enhance local distinctiveness, ensuring development has regard to the overall built character and landscape quality of the area, and require development to be compatible with the surrounding area, in terms of scale, massing, grain, form, architecture, street enclosure, local materials and landscape treatment. Policy OA1 states that for developments in Horwich and Blackrod the Council will conserve and enhance the character of the existing landscape and physical environment and ensure that development respects street patterns, and the grain and the form of predominant architectural styles.

Only details of the proposed layout and the means of access are applied for within this outline application.

It is considered that the layout of the development is acceptable, and each dwelling would have an ample rear garden. There is also opportunity within the site for the landscaping of communal areas

(which would be addressed within a reserved matters application). The density of the development is reflective of the recently built residential development at Junction Close.

The proposed acoustic fencing on top of the proposed bunds along the northern and western boundaries of the site would give the development an enclosed appearance, however this would be softened by sensitive landscape planting.

It is therefore considered that the proposal complies with Policies CG3 and OA1 of the Core Strategy.

Impact on the Highway

Policy P5 of the Core Strategy states that the Council will ensure that developments take into account [amongst other things] accessibility and parking. Policy S1.2 states that the Council will promote road safety in the design of new development.

Access into the proposed development is from Station Approach, which is proposed to be improved up to the northern boundary of the application site (where it is currently cobbled). An internal road is proposed off the existing access road into the development. No access is proposed from Junction Close.

The Council's Highways Engineers have comments that the traffic generation associated with the proposed development of 12 dwellings would be negligible and should be accommodated without detriment to the operational capacity of the surrounding highway network, and with limited road safety implications. They also note that the site is highly accessible to sustainable transport provision (neighbouring Blackrod Railway Station and being just off a bus route).

Engineers also comment that the proposed internal road layout appears to comply with the Council's standards for adoption and that the number of parking spaces proposed comply with the Council's maximum parking standards.

It is therefore considered that the proposed development would not jeopardise highway safety in the area or the operational capacity of the local highway network, compliant with Policies P5 and S1.2 of Bolton's Core Strategy.

Other matters

As the application site is located within a high risk area the applicant has submitted a coal mining risk assessment. This has been forwarded to the Coal Authority for their comments. Should comments be received, these will be reported directly to the meeting.

A phase 1 contaminated land study has been submitted by the applicant following a request from officers.

The applicant has stated that they will be submitting a Crime Impact Statement prior to the Committee meeting. If this is not received, then one will be required at the reserved matters stage (by a condition of any approval).

No drainage information has been submitted with the application. This would need to be submitted with the reserved matters application.

Local finance considerations

Section 70(2) of the Town and County Planning Act 1990 (as amended) provides that a local planning authority must have regard to a local finance consideration as far as it is material. The following is considered to be a local financial consideration in this case:

New Homes Bonus for the 12 proposed dwellings – this is not a material planning consideration.

Conclusion

Whilst it is considered that the application site is not an ideal location for dwellings given the potential for noise and odour disturbance to future residents, the applicant has proven that there are mitigation measures that could be implemented that would lessen this disturbance, to an extent that the living conditions of the future residents would not be unduly harmed. The proposed development would deliver 12 dwellings on a previously developed site within the urban area, in a highly sustainable location, and would not unduly harm the amenity of neighbouring residents, the character and appearance of the area, or highway safety or capacity.

It is therefore considered, subject to the recommended conditions, that the proposal would comply with the Council's development plan policies. Members are therefore recommended to approve this application.

Representation and Consultation Annex

Representations

Letters:- Ten letters of objection and two letters of support have been received.

The ten letters of objection have been received from neighbouring businesses (J. Dickinson & Sons, Medicina, Gilly's Yard and Greenlands Garage), employees of those businesses and a neighbouring resident at Junction Close. These letters raise the following concerns:

- * The introduction of new residential development within close proximity of Dickinson's and other businesses would be incompatible as the dwellings' amenity would inevitably be negatively impacted on by operations of the businesses;
- * Complaints from the future residents of the development (to the Environment Agency and Bolton Council) would potentially jeopardise the range of activities and materials that are permissible via the EA licence at Dickinson's. This could potentially greatly affect the viability of the business and the employment it generates;
- * The general area is predominantly industrial or emission generating (between the M61 and railway track) - the site would be situated almost centrally within this industrial and commercial area;
- * Gilly's Yard (pallet business) repair pallets on a daily basis which causes some noise due to the generator and air pressure guns. The normal working hours are anything from 5am through to 8-9pm depending how busy they are;
- * Many complaints have been made by residents of Junction Close with regard to Dickinson's. This proposed development would be even closer;
- * Previous applications for the residential development of the site have been refused by officers and the Planning Committee;
- * Little has changed since the last planning applications were refused;
- * Garden areas would not be protected from noise from the neighbouring businesses and motorway;
- * Intermittent noise is often the source of more complaints than general background noise, which the dwellings would be susceptible to, especially within the gardens;
- * Odours from Dickinson's and the Waste Water Treatment Works. Odour from the WWTW is not controlled. Dickinson's have received complaints about odour and odour related flies;
- * Odour nuisance complaints occur regardless of the prevailing wind direction;
- * The pallet yard regularly opens between 06:00 and 06:30 and they burn pallets on site;
- * The measures and assessments that the latest application includes do not overcome the fundamental problem that residential development in this location is simply not compatible with the adjacent land uses;
- * The Council's lack of a 5 year housing land supply does not outweigh the adverse effects that would be created by the proposal;
- * Employees concerned that the proposal would result in the local businesses closing;
- * The development would have an overbearing impact on their house - loss of light and outlook;
- * Increase in traffic and road congestion;
- * The access road is a private road;
- * Increase in pollution;
- * The application site may be contaminated;
- * There is not a demand for any family houses in the area.

Two letters of support have been received from neighbouring residents at Junction Close. These provide the following comments:

- * The previous industrial occupants of the site caused problems for residents of Junction Close. A

- residential development would be more compatible;
- * Residents welcome a development of this calibre

Blackrod Town Council:- raised an objection to the proposed development at their meeting of 7th August 2017 on "environmental grounds including the possibility of coal mining hazards and sewerage works locality."

Consultations

Advice was sought from the following consultees: Pollution Control Officers, Highways Engineers, Drainage Officers, Strategic Development Unit, Economic Strategy Officers, Strategic Housing Officers, the Environment Agency, National Grid, Coal Authority, United Utilities and Greater Manchester Police.

Planning History

Outline planning application 93108/14 for the erection of 11 detached dwellings (access and layout details only) was refused at Planning Committee in January 2015 for the following reason:

"The proposed residential development of the site would be incompatible with the existing neighbouring industrial and commercial land uses and would fail to protect the amenity of future residents of the development, particularly with regard to odours, noise and general disturbance created by the neighbouring industrial and commercial operations, contrary to Policy CG4 of Bolton's Core Strategy."

Outline planning application 90454/13 for erection of 11 detached dwelling (layout and means of access only) was refused under delegated powers in November 2013 for the following reason:

"The proposed development would be incompatible with the existing neighbouring industrial uses and would fail to protect the amenity of future residents of the development, particularly with regard to odours and general disturbance created by the neighbouring industrial operations, contrary to Policy CG4 of Bolton's Core Strategy."

Planning application 53582/98 for the demolition of 197 Station Road and the erection of 19 houses (siting and means of access details only) was refused by the Council in January 1999 and was subsequently dismissed at appeal in November 1999. The application site subject to this latest application was part of the wider application site in this 1998 application. Application 53582/98 was refused on access grounds.

Permission was granted in January 1981 for the development of the site (and surrounding land) for industrial purposes (15853/80).

Recommendation: Approve subject to conditions

Recommended Conditions and/or Reasons

1. Application for the approval of 'Reserved Matters' must be made not later than the expiration of two years beginning with the date of this permission and the development must be begun not later than whichever is the later of the following dates:
 - i) The expiration of four years from the date of this permission, or
 - ii) The expiration of two years from the final approval of the Reserved Matters, or, in the case of approval on different dates, the final approval of the last such matter to be approved.

Reason

Required to be imposed pursuant to Section 92 of the Town and Country Planning Act 1990.

2. Details of the appearance, landscaping and scale (hereinafter called "the reserved matters") shall be submitted to and approved in writing by the Local Planning Authority before any development begins and the development shall be carried out as approved.

Reason

To application is for outline planning permission and these matters were reserved by the applicant for subsequent approval.

3. Prior to the commencement of the development:-

- A Site Investigation report shall be submitted to and approved in writing by the Local Planning Authority. The investigation shall address the nature, degree and distribution of land contamination on site and shall include an identification and assessment of the risk to receptors focusing primarily on risks to human health, property and/or the wider environment; and
- The details of any proposed remedial works shall be submitted to, and approved in writing by the Local Planning Authority. The approved remedial works shall be incorporated into the development during the course of construction and completed prior to occupation of the development or the development being first brought into use; and

Prior to first use/occupation of the development hereby approved:

- A Verification Report shall be submitted to, and approved in writing by, the Local Planning Authority. The Verification Report shall validate that all remedial works undertaken on site were completed in accordance with those agreed by the LPA.

Reason

To safeguard the amenity of the future occupants of the development and to comply with Core Strategy policy CG4.

4. Prior to the commencement of any groundworks surface water drainage works should be implemented in full in accordance with details that have been submitted to and approved in writing with the Local Planning Authority. Before these details are submitted an assessment shall be carried out of the potential for disposing of surface water by means of a sustainable drainage system in accordance with the principles set out in the National Planning Policy Framework, and the results of the assessment provided to the Local Planning Authority. Where a sustainable drainage system is to be provided, the submitted details shall:

- 1) Provide information about the design storm period and intensity, the method employed to delay and control the surface water discharged from the site and the measures taken to prevent pollution of the receiving groundwater and/or surface waters.
- 2) Include a timetable for its implementation, and
- 3) Provide a management and maintenance plan for the lifetime of the development which shall include the arrangements for adoption by any public authority or statutory undertaker and any other arrangements to secure the operation of the scheme throughout its lifetime

Reason

To ensure the site provides satisfactory means of surface water drainage and to comply with policies CG1.5 and CG2.2 of Bolton's Core Strategy.

5. Prior to the commencement of development full details of the locations, height, materials, colour and design of the proposed acoustic fencing shall be submitted to and approved in writing by the Local

Planning Authority. The fencing shall be erected along the top of the proposed landscaped bunds (as approved within drawing no. 17/1158/201) and around the perimeters of the private garden areas. The approved fencing shall be erected as agreed prior to first occupation of the dwellings and shall be retained in situ therefore, unless otherwise agreed in writing by the Local Planning Authority.

Reason

To safeguard the amenity of future residents of the development and to comply with policy CG4 of Bolton's Core Strategy.

6. Prior to the commencement of development full details of the landscaping (trees and shrubs) proposed for the approved landscaped bunds (locations as shown in drawing no. 17/1158/201) and the height and widths of the bunds shall be submitted to and approved by the Local Planning Authority. The approved bunds and landscaping scheme shall be implemented in full prior to the first occupation of the dwellings. Any trees and shrubs that die or are removed within five years of planting shall be replaced in the next available planting season with others of similar size and species. The approved bunds shall be retained to their approved height, width and location unless otherwise agreed in writing by the Local Planning Authority.

Reason

To safeguard the amenity of future residents of the development from noise and odour disturbance, and to comply with policies CG3 and CG4 of Bolton's Core Strategy.

7. Prior to the commencement of development full details of the highway works at Station Approach comprising the making up of the section of the road shown in drawing 17/1158/201 to industrial adoption standards with 2.0 metre footway and 6.7 metre carriageway width shall be submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be implemented in full prior to the development being first brought into use and retained thereafter.

Reason

In the interests of highway safety and in order to comply with Bolton's Core Strategy policies S1, P5 and Supplementary Planning Document "Accessibility, Transport and Road Safety".

8. Prior to the commencement of development hereby approved/permitted, all developer obligations shall be fulfilled to enable the Local Highway Authority to carry out a review of Traffic Regulation Orders around the site where necessary (with the potential implementation of junction edge protection at the site access) and to enable the Local Highway Authority to consult, advertise, promote and implement Traffic Regulation Orders to mitigate the likely impact of the development.

Reason

In the interests of highway safety and in order to comply with Bolton's Core Strategy policies S1, P5 and Supplementary Planning Document "Accessibility, Transport and Road Safety".

9. Prior to the development hereby approved/permitted being first occupied the means of vehicular access to the site from Station Approach shall be constructed to a minimum width of 5.5 metres with 5.0 metres radii and 2 no. 2.0 metres wide footways in accordance with the drawing ref 17/1158/201.

Reason

In the interests of highway safety and in order to comply with Bolton's Core Strategy policies S1, P5 and Supplementary Planning Document 'Accessibility, Transport and Road Safety'.

10. Prior to the development hereby approved/permitted being first brought into use a visibility splay measuring 2.4 metres by 43.0 metres shall be provided at the junction of the site access with Station Approach, and subsequently remain free of all obstructions between the height of 1.05 metres and 2 metres (as measured above carriageway level).

Reason

To ensure traffic leaving the site has adequate visibility onto the highway and in order to comply with Bolton's Core Strategy policy S1, P5 and Supplementary Planning Document 'Accessibility, Transport and Road Safety'.

11. Prior to the occupation of the dwelling houses hereby permitted provision shall be made for the parking or garaging of 2 motor vehicles adjacent to each of the dwelling houses in the area identified for that purpose on the approved plan. Driveways shall be a minimum of 6.0 metres long. Those areas shall thereafter be retained at all times for that purpose. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any Order amending or replacing that Order) other than (a) garage(s), no extensions, porches, outbuildings, sheds, greenhouses, oil tanks, wind turbines or satellite antennae shall be erected within that area.

Reason

To ensure that adequate provision is made for vehicles to be left clear of the highway and in order to comply with Bolton's Core Strategy policies S1, P5 and Appendix 3.

12. Prior to the development hereby approved/permitted being first occupied, all Traffic Regulation Orders required by the Local Highway Authority to mitigate the impact of the development shall be implemented in full.

Reason

In the interests of highway safety and in order to comply with Bolton's Core Strategy policies S1, P5 and Supplementary Planning Document "Accessibility, Transport and Road Safety".

13. Prior to the development being first occupied, details (including a brick or masonry specification and colour scheme) of the treatment to all boundaries to the site not referred to in condition 5 shall be submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be implemented in full prior to first occupation and retained thereafter.

Reason

To ensure adequate standards of privacy and amenity are obtained and the development reflects the landscape and townscape character of the area and in order to comply with policies CG3 and CG4 of Bolton's Core Strategy.

14. Before the first occupation of the dwellings hereby approved, details of the windows (which are to be acoustically double glazed and ventilated) shall be submitted to and approved in writing by the Local Planning Authority. The approved windows shall be installed prior to first occupation.

Reason

To minimise the impact of noise on residential amenity in order to comply with Bolton's Core Strategy policy CG4.

15. No dwelling shall be occupied until the access road(s), footway(s) and footpath(s) leading thereto have been constructed and completed in accordance with drawing ref: 17/1158/201 .

Reason

In the interests of highway safety and in order to comply with Bolton's Core Strategy policies S1, P5 and Supplementary Planning Document 'Accessibility, Transport and Road Safety'.

16. The garages hereby approved/permitted shall be made available at all times for the parking of motor vehicles.

Reason

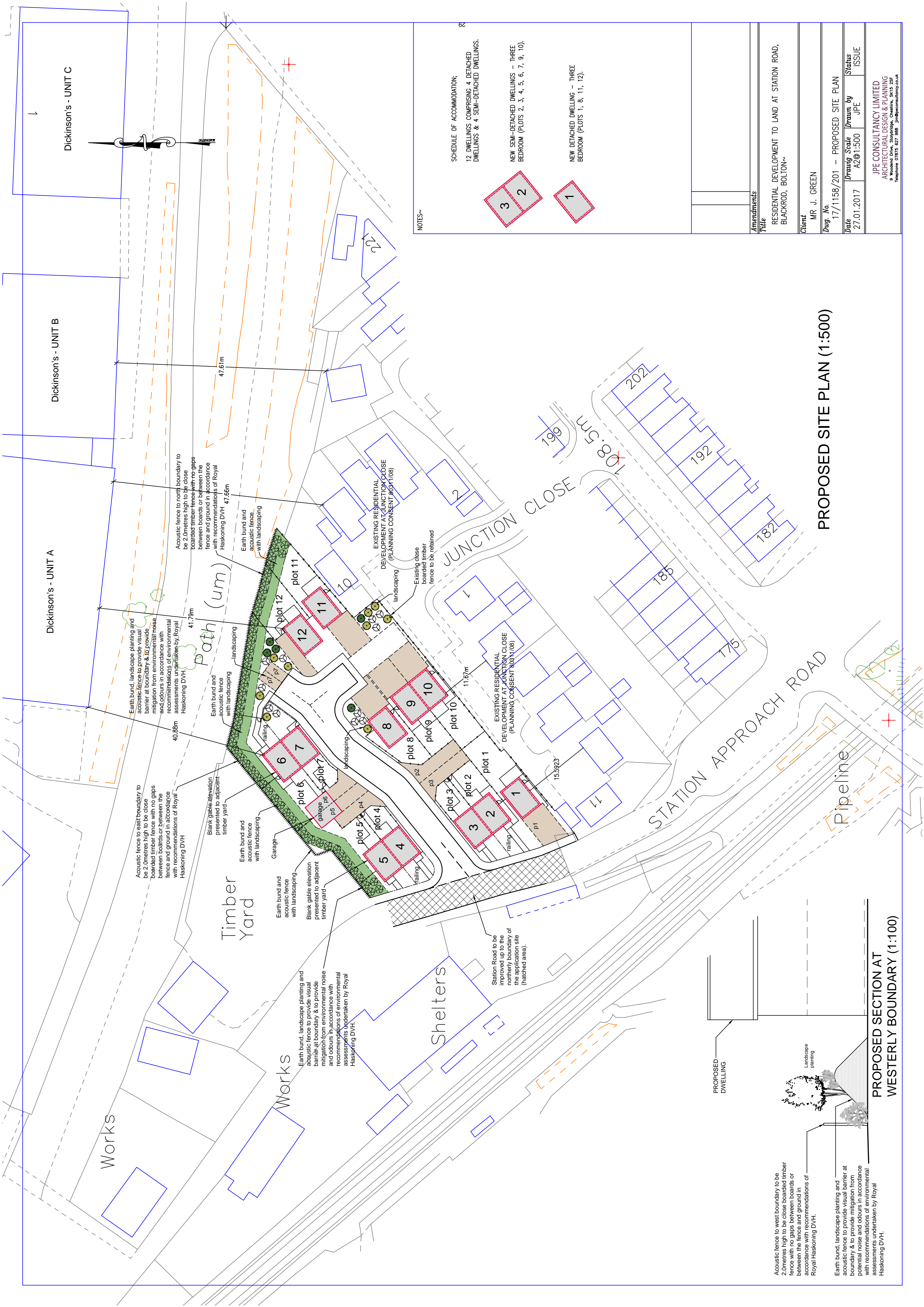
The loss of garage spaces would be likely to lead to an increase in on-street parking to the general detriment of highway safety and in order to comply with Bolton's Core Strategy policies S1, P5, Supplementary Planning Document 'Accessibility, Transport and Road Safety' and Appendix 3.

17. The development hereby permitted shall be carried out in complete accordance with the following approved plans:

17/1158/201; "Proposed Site Plan"; dated 27.01.2017

Reason

For the avoidance of doubt and in the interests of proper planning.



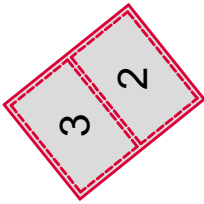
NOTES~

SCHEDULE OF ACCOMMODATION;

12 DWELLINGS COMPRISING 4 DETACHED DWELLINGS & 4 SEMI-DETACHED DWELLINGS.

NEW SEMI-DETACHED DWELLINGS – THREE BEDROOM (PLOTS 2, 3, 4, 5, 6, 7, 9, 10).

NEW DETACHED DWELLING – THREE BEDROOM (PLOTS 1, 8, 11, 12).



Amendments

Title

RESIDENTIAL DEVELOPMENT TO LAND AT STATION ROAD, BLACKROD, BOLTON~

Client

MR J. GREEN

Dwg. No.

17/1158/201 – PROPOSED SITE PLAN

Date

27.01.2017

Drawing Scale

A2@1:500

Drawn by

JPE

Status

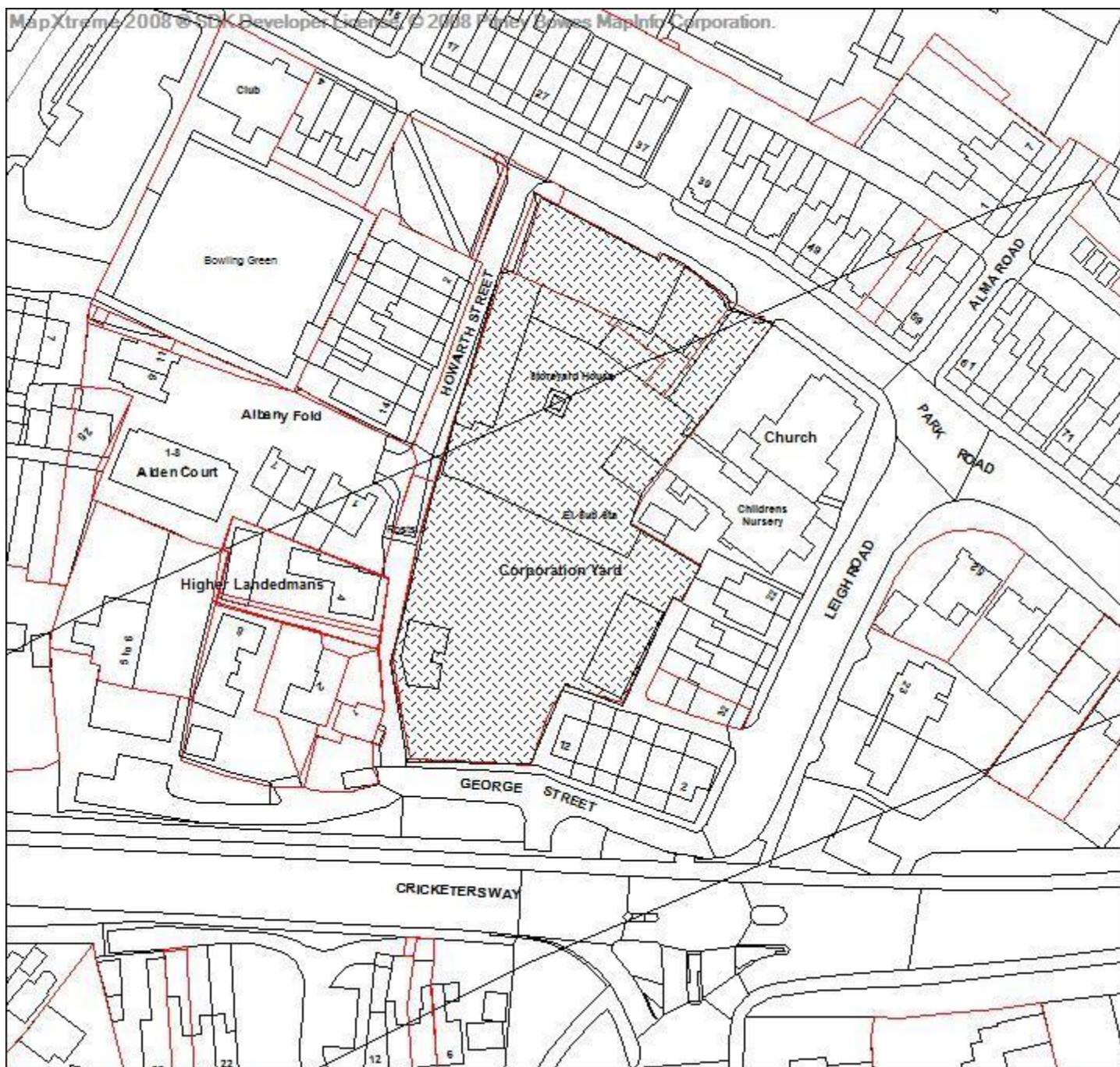
ISSUE

JPE CONSULTANCY LIMITED
ARCHITECTURAL DESIGN & PLANNING
9 Woodend Drive, Stalybridge, Cheshire, SK15 2SF
Telephone 01257 827 985 jpe@jpeconsultancy.co.uk

PROPOSED SITE PLAN (1:500)

PROPOSED SECTION AT WESTERLY BOUNDARY (1:100)

Application number 00940/17



Development & Regeneration Dept
Development Management Section

Town Hall, Bolton, Lancashire, BL1 1RU
Telephone (01204) 333 333

Bolton Council

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Date of Meeting: 24/08/2017

Application Reference: 00940/17

Type of Application: Full Planning Application
Registration Date: 17/05/2017
Decision Due By: 15/08/2017
Responsible Officer: Helen Williams

Location: FORMER CORPORATION YARD, OFF PARK ROAD, WESTHOUGHTON, BOLTON

Proposal: ERECTION OF 21NO. RESIDENTIAL DWELLINGS (2NO. TWO BED AND 19NO. THREE BED) TOGETHER WITH ASSOCIATED WORKS INCLUDING LANDSCAPING AND ACCESS ROADS

Ward: Westhoughton North

Applicant: Bolton at Home
Agent : Nicol Thomas

Officers Report

Recommendation: Approve the application subject to conditions and authorise the Director of Place and Borough Solicitor to complete all the necessary legal formalities in consultation with the Chair of Planning Committee.

Proposal

1. Full planning permission is sought for the erection of 21 two storey dwellings on the site (2 two bedroom dwellings and 19 three bedroom dwellings). 18 of the dwellings are proposed as semi-detached properties with the remaining three forming a row of dwellings (three mews/town houses).
2. The development is effectively split into two parts. 17 of the dwellings would be accessed from Park Road, either directly off Park Road (in the case of plots 1 to 6, which would front the main road) or off the cul-de-sac that would be created centrally within the site. The remaining four dwellings (plots 18 to 21) would face and be accessed off George Street to the south.
3. All of the dwellings are proposed to be constructed from red brick with grey roof tiles.
4. Each dwelling would have driveway parking for two vehicles, and front and rear gardens.
5. The applicant is Bolton at Home, a social housing provider. All 21 dwellings are therefore proposed as affordable units, with 11 of the dwellings currently being proposed for shared ownership and 10 being proposed as rental units.
6. The application is brought before Planning Committee as it is a major application that has received more than six objections. Councillor Kirk-Robinson has also requested that the

application be heard before Committee.

Site Characteristics

7. The application site measures approximately 0.5 hectares and is a former Council depot (now closed). The site contains a number of single and two storey red brick buildings (mainly around the edges of the site) and an area of hardstanding. To the front of the site (adjacent Park Road) is an open area of green space which contains four trees. Three further trees are sited on land adjoining the application site to the north east (within the churchyard, but overhang the site).
8. The existing vehicular access into the site is off Park Road to the north, adjacent to the boundary with the adjacent church and opposite 47 and 49 Park Road.
9. The site has a gradual slope from the north to the south (the southern boundary of the site is approximately 3.5 metres lower than the northern boundary).
10. To the north of the site are the two storey terraced properties along Park Road. The properties are in residential use, with the exception of no. 53 (opposite the church) which is a dentist. To the immediate north east of the site is Westhoughton United Reformed Church, which has its graveyard adjoining the site and an attached children's nursery.
11. Leigh Road runs to the east of the application site and comprises of nine dwellings, six of which (two storey, red brick terraced and semi-detached dwellings at nos. 22 to 32) have their rear elevations facing the site. George Street runs to the south of the site (accessed from Leigh Road) and comprises of six two storey, cream painted terraced dwellings. The application site neighbours the dwellings at nos. 10 and 12 to the north and west.
12. Neighbouring the application site at its south western corner are 1, 2 and 4 Higher Landedmans. 1 and 2 Higher Landedmans is a former farm building dating back to 1609, now converted into two dwellings. The building is not listed but is noted for its heritage value. No. 1 is constructed from red brick with a slate roof and no. 2 is a timber framed building with a thatched roof. No. 4 is a more contemporary bungalow. These dwellings are accessed from George Street.
13. Howarth Street runs to the west of the application site and contains seven two storey, red brick terraced dwellings on the opposite side of the road from the site. Howarth Street leads to Albany Fold; 1 Albany Fold sides on to the application site.
14. There are double yellow lines along the southern side of Park Road. Along the northern side is a single yellow line (waiting restrictions between 0800 and 1800 hours Monday to Saturday) with double yellow lines at the junctions with the side streets. There are also double yellow lines on Leigh Road outside the church and by the side of 52 Park Road. These waiting restrictions are believed to be historic, when Park Road and Leigh Road were main roads through Westhoughton (before Cricketers Way was constructed).
15. The application site is an unallocated, previously developed site in the urban area.

Policy

16. National Planning Policy Framework (NPPF)
17. Core Strategy Policies: P5 Transport and Accessibility; S1 Safe Bolton; CG1 Green Bolton; CG2 Sustainable Design and Construction; CG3 The Built Environment; CG4 Compatible Uses; SC1 Housing; OA3 Westhoughton.

18. SPD Accessibility, Transport and Road Safety; SPD General Design Principles; SPD Affordable Housing.

Analysis

19. Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise. Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission. Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission. It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

20. The main impacts of the proposal are:-

- * principle of residential development on the site
- * loss of informal green space
- * impact on the highway
- * impact on the character and appearance of the area
- * impact on the amenity of neighbouring residents
- * impact on biodiversity
- * impact on drainage

Principle of Residential Development on the Site

21. Section 6 of the National Planning Policy Framework (NPPF) concerns delivering a wide choice of high quality homes. Guidance also seeks to widen opportunities for home ownership and create sustainable, inclusive and mixed communities. Paragraph 49 states that housing applications should be considered in the context of the presumption in favour of sustainable development.

22. A core planning principle of the NPPF is to encourage the effective use of land by reusing land that has been previously developed (brownfield land), provided that it is not of high environmental value.

23. Policy SC1 of Bolton's Core Strategy states that the Council will identify a range of housing sites for additional provision and at least 80% of housing development will be on previously developed land. Policy OA3.3 states that the Council will concentrate sites for new housing in Westhoughton town centre and on other sites within the existing urban area.

24. The application site is a previously developed site within the urban area of Westhoughton. The principle of residential development has already been established following the approval of outline planning application 91146/13 (access details only).

25. It is considered that the application site is in a highly sustainable location, being a short walk from Westhoughton town centre and the local services, shops and facilities within the centre, close to schools and health facilities, and within walking distance of two railway stations.

26. The proposal would provide 21 affordable units which would be managed by Bolton at Home.

27. The proposed development would contribute to the affordable housing supply within Westhoughton and would develop a previously developed site in the urban area, compliant with the aims of the NPPF and Policies SC1 and OA3.3 of the Core Strategy.

Loss of Informal Green Space

28. Policy CG1.4 of the Core Strategy states that the Council will allow some development on informal green spaces in the urban area, provided that it allows for the improvement of remaining green spaces and helps to meet the strategic objectives of housing.
29. The northern part of the application site, fronting Park Road, comprises an area of informal open green space. The trees within this area are to be retained within the development, however the green space would become gardens and driveways to plots 3 to 6 (as well as accommodating the proposed new access into the site).
30. The proposed development helps to meet Bolton Council's strategic housing objectives, by providing affordable housing that meets a recognised need and by focusing new development in the existing urban area (Strategic Objectives 14 and 15 of the Core Strategy).
31. The proposal would also allow for the improvement of remaining green spaces in Westhoughton in that the applicant has agreed to pay a commuted sum of £5,000 towards the improvement works of the toddler play area at Westhoughton Central Park. This money is to be secured through a Unilateral Undertaking.
32. It is therefore considered, subject to the signing of the Unilateral Undertaking, that the proposed development would comply Policy CG1.4 of Bolton's Core Strategy.

Impact on the Highway

33. Paragraph 32 of the NPPF states that development should only be prevented or refused on transport grounds where the residual cumulative impacts of the development are severe.
34. Policy P5 of the Core Strategy states that the Council will ensure that developments take into account accessibility by different types of transport, servicing arrangements and parking (in accordance with the parking standards set out in Appendix 3). Policy S1.2 states that the Council will promote road safety in the design of new development.
35. The Council's Highways Engineers have confirmed that the proposal (a low-density residential development of 21 dwellings) would generate negligible traffic, below that of the extant use of the site as a depot. They state that the proposal would have a limited impact on the operational capacity of the surrounding highway network and would pose limited road safety implications.
36. A new access is proposed into the development off Park Road, with the existing access to the depot being proposed to be closed and made up. In terms of the proposed internal highway layout, Engineers state that the proposal appears to comply with the Council's standards for adoption. A full Section 38 appraisal would however need to be undertaken by the Highways Authority at the post-planning stage.
37. A number of objections and concerns have been raised by neighbouring residents and councillors with regard to the lack of parking for existing residents on the neighbouring roads of Park Road, Leigh Road, George Street and Howarth Street. Residents are concerned that the proposed development would exacerbate this existing parking problem.
38. Two parking spaces are proposed as driveway parking for each dwelling. This complies with the Council's maximum parking standards for 2 and 3 bedroom dwellings and therefore it is considered that the proposed development would not further exacerbate parking problems in the area. Indeed, there would be space within the development for any overspill parking along the access road (on the internal footways). Furthermore, the site is also in a highly sustainable

location (close to Westhoughton town centre) and would be accessible both by bus and train.

39. It is noted that residents and visitors to the area (particularly to the dentist, church and nursery) currently park their vehicles on the former access road into the site and on the pavement on Park Road to the front of the site (both areas within the application site). This informal parking would no longer be available following the development. Officers are aware of the parking problems in the area and that the current waiting restrictions along Park Road displace car parking during the daytime, to the neighbouring residential roads (Leigh Road and Haworth Street) and side streets off Park Road.
40. It is believed that the waiting restrictions (double yellow lines along the southern side of Park Road and single yellow line along the northern side) were implemented at a time when Park Road and Leigh Road were main roads through Westhoughton (before Cricketers Way was constructed as a bypass). It is therefore possible that the current waiting restrictions are no longer required and Highways Engineers are therefore recommending, should the application be approved, that a review of the waiting restrictions within the locality is undertaken. Should the review exercise (which includes consultation with local residents) find that the waiting restrictions in the area are no longer fit for purpose then the applicant would be required to fund their removal/alteration, prior to first occupation of the dwellings.
41. Highways Engineers are also recommending conditions to improve the footway along the eastern side of Haworth Street (so that it is 1.8 metres the whole length (the extra width being accommodated within the application site)), to resurface George Street in front of plots 18 to 21, and to resurface the footway along Park Road.
42. It is considered, subject to the recommended conditions, that the proposed development would not jeopardise highway safety in the area and would provide for sufficient on-site parking, compliant with Policies P5 and S1.2 of Bolton's Core Strategy.

Impact on the Character and Appearance of the Area

43. Section 7 of the NPPF requires good design. The Government attaches great importance to the design of the built environment. Good design is a key aspect of sustainable development, is indivisible from good planning, and should contribute positively to making places better for people.
44. Policy CG3 of the Core Strategy states that the Council will conserve and enhance local distinctiveness, ensuring development has regard to the overall built character and landscape quality of the area, and will require development to be compatible with the surrounding area in terms of scale, massing, grain, form, architecture, street enclosure, local materials and landscape treatment. Policy OA3 relates specifically to developments in Westhoughton and states that the Council will conserve and enhance the character of the existing physical environment.
45. The proposed development would have an active street presence both along Park Road to the north and George Street to the south, with the dwellings proposed at plots 1 to 6 and at plots 18 to 21 fronting on to the roads. The dwellings on George Street would continue the built form along this residential road. The dwellings proposed along the Park Road frontage are proposed to be set back from the street to allow for the retention of all four of the existing mature trees along this frontage: the Council's Tree and Landscape Officers consider that these trees are an important element in the townscape of Park Road, since there are few other trees elsewhere in the vicinity. Furthermore, the proposed lengthy front gardens to plots 1 to 6, and low level railings proposed to the front, would help retain the open character of the existing area of green space to the front of the site.

46. The dwellings proposed within the centre of the development would be sited around the internal road. Each dwelling would have a front garden, driveway parking for two vehicles and ample rear garden space. The low density of the development also allows for sufficient additional tree planting within the development, both within gardens and along the internal road (between plots 1 and 17). A condition requiring the approval of a landscaping scheme of the development is suggested by officers.
47. All dwellings are proposed to be two storeys in height, which respects the existing built character of the area. The dwellings are also proposed to be constructed from red brick and grey roof tiles which would complement the neighbouring red brick terraced dwellings along Park Road, Leigh Road and Haworth Street.
48. Concerns have been raised with regard to the siting, scale and design of the development in relation to the neighbouring dwellings at 1 and 2 Higher Landedmans (which date back to 1609). The dwellings are not listed buildings nor in a conservation area, however officers acknowledge that the dwellings have heritage value (the former farmhouse is considered to be a non-designated heritage asset). It is noted that the proposed dwellings along George Street would be taller than the dwellings at Higher Landedmans (as nos. 1 and 2 have lower ceiling heights), however it is considered that the proposed dwellings would be sited sufficiently far enough away from the properties to not appear incongruous when they are viewed together, nor is it considered that the siting and scale of the dwellings would harmfully block public views of the historic building (with public views of the historic building already being limited owing to the proximity of other neighbouring dwellings and by the presence of fencing and trees at the southern end of the application site).
49. For the reasons discussed above is considered that the proposed development, by virtue of its siting, scale and appearance, would be compatible with the character and appearance of the area, compliant with Policies CG3 and OA3 of Bolton's Core Strategy.

Impact on the Amenity of Neighbouring Residents

50. Policy CG4 of the Core Strategy states that the Council will ensure that new development is compatible with surrounding land uses and occupiers, protecting amenity, privacy, safety and security. SPD General Design Principles sets out the Council's recommended interface distances between dwellings.

31 to 49 Park Road

51. The front elevations of the houses proposed along the front of the development would be approximately 25 metres away from the front of the dwellings on the opposite side of Park Road. The Council's recommended interface standard between two storey dwellings (where principal elevations containing principal windows face each other) is 21 metres, therefore this interface standard is exceeded.

22 to 32 Leigh Road

52. The rear elevations of these terraced dwellings back onto the application site. 22 and 24 Leigh Road are the closest dwellings to the proposed development, with their rear elevations being approximately 23.5 metres away from the rear elevations of the dwellings proposed at plots 16 and 17. Their rear two storey outriggers would be approximately 20.5 metres away from the rears of plot 17, however it is not believed that the rear outriggers contain any main windows. In the event that they do, a shortfall in the recommended interface distance of 0.5 metres is not considered to be great enough to cause an undue harm to the living conditions of these neighbours.

53. The rear elevations of the dwellings at 26 to 32 Leigh Road would be approximately 24 metres away from the rears of the dwellings proposed at plots 14 and 15, which exceeds the recommended interface distance of 21 metres (as set out within the SPD).

10 and 12 George Street

54. The rears of the terraced dwellings at 10 and 12 George Street would overlook the rear garden at plot 14. The rear of 12 George Street would be approximately 13 metres away from the side of the dwelling at plot 14, however 12 George Street does not directly overlook the side of this dwelling and therefore this interface distance is considered to be acceptable.
55. The side elevation of plot 21 would be side on to the blank side elevation of 12 George Street. The siting of plot 21 would also not infringe upon a "45 degree line" from the rear of no. 12 (the "45 degree rule" contained within the SPD). It is therefore considered that this relationship would not harm the amenity of the neighbouring resident.

1, 2 and 4 Higher Landedmans

56. The side elevation of 1 Higher Landedmans (that would face plot 18) contains porch windows and a kitchen window at ground floor (not main windows within the SPD) and a bathroom window (not main window) and a bedroom window at first floor. The bedroom which the window serves also has a window on the northern elevation of the house, which is approximately the same size. It is therefore uncertain as to which bedroom window should be classed as the main window. 1 Higher Landedmans would be approximately 8.7 metres from the side elevation of the dwelling proposed at plot 18 however the proposed dwelling would be behind the building line of no. 1 and therefore the bedroom window in the side elevation would overlook the front garden and driveway to plot 18. The main windows sited in the northern elevation of no. 1 would not be affected by the proposed development. It is therefore considered that the siting of the dwelling at plot 18 would not have an undue affect on the amenity of this neighbour.
57. The front elevation of 2 Higher Landedmans faces the side elevation of the dwelling proposed at plot 18. The side elevation of the dwelling at plot 18 does not contain any main windows, therefore an interface distance of 13.5 metres is recommended within the SPD in this situation: an interface distance of approximately 18.4 metres is achieved here, which exceeds the recommendation.
58. The side of 4 Higher Landedmans does not contain any main windows and therefore it is considered that this neighbour is not unduly affected by the proposal.

1 Albany Fold

59. The side elevation of this neighbour faces the proposed development (the side elevation of plot 11). The side elevation of 1 Albany Fold does not contain any main windows and therefore it is considered that the amenity of this neighbour would not be unduly affected.

2 to 14 Howarth Street

60. The front elevations of the terraced dwellings along Howarth Street face the application site. The dwellings on Howarth Street would be over 22 metres away from the rears of the dwellings proposed at plots 7 to 10: a minimum interface distance of 21 metres is recommended by the SPD in this instance.
61. The dwelling at plot 6 would be sited forward of the side elevation of 2 Haworth Street and therefore it is considered that the siting of this dwelling would not affect the outlook of the neighbour at no. 2.

62. For the reasons discussed above it is considered that the proposed development would comply with the Council's recommended interface distance between dwellings (contained within policy guidance) and that no neighbouring residents' living conditions should be unduly affected by the proposed development. It is therefore considered that the proposal complies with Policy CG4 of Bolton's Core Strategy.

Impact on Biodiversity

63. Policy CG1.2 of the Core Strategy states that the Council will safeguard and enhance biodiversity in the borough by protecting sites of urban biodiversity including trees, woodland and hedgerows from adverse development.

Trees

64. A tree survey and constraints report has been submitted with the application. This confirms that the trees to be felled along the southern, western and eastern boundaries of the site are of low amenity value and are in generally poor form. The Council's Tree Officer agrees that these trees are not worthy of retention and that planting in mitigation of these trees should be undertaken (this is suggested by officers as a condition should the application be approved).
65. The trees along the northern boundary of the site (fronting Park Road) are proposed to be retained within the development. The Tree Officer agrees with the submitted survey that these trees are worthy of retention as they have high amenity value: their retention is therefore welcomed. The Tree Officer also recommends that these trees should be protected in the future by virtue of a Tree Preservation Order.
66. With regard to the proposed driveways and gardens for plots 1, 3, 4, 5 and 6 (which would be accessed off Park Road and would run adjacent the retained trees) the Tree Officer recommends a "no-dig" method of construction (and permeable surfacing for the driveway to plot 6) within the root protection zones. This would be a condition of any approval.
67. It is therefore considered, subject to the recommended conditions, that the proposed development would result in the unacceptable loss of trees from the site.

Wildlife

68. As the proposed development includes the demolition of the existing buildings on the site, a bat, barn owl and nesting bird survey has been submitted with the application. The survey did not find evidence of bat usage on site and assessed that the probability of encountering bats during demolition works to be low. There was potential for use of the site by nesting birds (particularly feral pigeons) but barn owls were considered unlikely.
69. Greater Manchester Ecology Unit has assessed the survey and have raised no objection to the proposed development.
70. It is considered that the proposed development would safeguard the biodiversity of the site, compliant with Policy CG1.2 of Bolton's Core Strategy.

Impact on Drainage

71. Policy CG1.5 of the Core Strategy states that the Council will reduce the risk of flooding in Bolton and other areas downstream by minimising water run-off from new development and ensuring a sequential approach is followed, concentrating new development in areas of lowest flood risk. Policy CG2.2 states that the Council will ensure that developments demonstrate the sustainable management of surface water run-off and that on brownfield sites the rate of run-off should be

50% less than conditions before development.

72. A flood risk assessment and drainage strategy report has been submitted with the application. The applicant currently proposes to discharge surface water run-off to the existing combined sewer system which exists in George Street to the south of the application site, as they consider that the ground conditions appear unfavourable for infiltration/soakaway drainage. Further assessment of the ground conditions would be required within the suggested drainage condition.
73. The development is to be connected to the existing foul sewer system.
74. United Utilities have raised no objection to the proposal.
75. It is considered, subject to a condition requiring further details of the proposed drainage, that the proposed development would comply with Policies CG1.5 and CG2.2 of the Core Strategy.

Other matter

76. The application site falls within the defined development high risk area for coal mining and therefore a coal mining risk assessment has been submitted with the application. The assessment has reviewed the available coal mining and geological information and concludes that the risk is low, but recommends that intrusive site investigation works should be undertaken prior to development in order to establish the exact situation regarding coal mining legacy issues on the site. The Coal Authority raise no objection to the proposal subject to a condition requiring further investigatory works and any required remedial works.

Local finance considerations

77. Section 70(2) of the Town and County Planning Act 1990 (as amended) provides that a local planning authority must have regard to a local finance consideration as far as it is material. The following is considered to be a local financial consideration in this case:
78. New Homes Bonus for the 21 proposed dwellings – this is not a material planning consideration.

Conclusion

79. The proposed development would deliver 21 new affordable houses in Westhoughton on a highly sustainable, previously developed site within the existing urban area. The proposal also offers an opportunity to review the existing waiting restrictions along Park Road, which currently restrict on-street parking in the vicinity of the site between 0800 and 1800 hours Mondays to Saturdays.
80. It is considered that the proposed development would not jeopardise highway safety, would be compatible with the character and appearance of the area, would safeguard the amenity of neighbouring residents, would safeguard biodiversity and would not increase flooding.
81. Members are recommended to delegate the decision to the Director to secure the off-site commuted sum for improvements to the toddler play area at Westhoughton Central Park, by way of a Unilateral Undertaking.

Representation and Consultation Annex

Representations

Letters:- Seven letters of objection have been received from neighbouring residents, which raise the following concerns:

- * Problems with existing residential parking during the daytime - residents have very little or no parking during the day (owing to waiting restrictions on the surrounding roads) and neighbouring streets are often full with visitors to the Reform Club, dentist or church;
- * More houses will only add to the parking problem in the area;
- * The proposal will reduce places where existing residents can currently park (the current access road into Corporation Yard and the pavement in front of the application site are used for parking);
- * More cars will be forced to park on the neighbouring streets;
- * The parking restrictions on Park Road are no longer required as they date back to when Park Road was a main road (Cricketers Way now takes this traffic); can they be reconsidered?;
- * The road surface of Park Road is poor. Are there any proposals to resurface this road?;
- * Loss of privacy to neighbouring residents;
- * The development will be too tall and too close to 1 Higher Landedmans (the oldest house in Westhoughton) and will significantly reduce daylight to that property;
- * The type B houses are not in keeping with the 100 year old terraced houses along Park Road;
- * Loss of trees;
- * Loss of wildlife. The development should include bird bricks/boxes within the development and trees should be planted inside the boundary fencing. Bats need consideration;
- * Drainage - additional surface water runoff from the new development;
- * Concerns over existing refuse collections along Howarth Street as it is not wide enough (*officer comment: this is an existing issue which would not be further affected by the proposal*);
- * Noise during construction and demolition (*officer comment: this concern can only be given limited planning weight given that some disruption during construction will be inevitable but temporary*);
- * There may be asbestos present in the buildings to be demolished (*officer comment: this matter would be covered under building regulations*).

Three letters of comment have been received, which raise the following points:

- * Fine, as long as the proposal to keep the existing trees on Park Road are kept in place. The proposal to retain the semi-mature trees is excellent news;
- * How much of the existing green field on Park Road will be lost? (it is a pleasant feature) (*officer comment: the green space that is to be retained is to be utilised as front gardens for plots 1 to 6*);
- * No objection to the dwellings, but have concerns with parking in the area;
- * It is essential that residents have a place to park during the day as Park Road has parking restrictions during daytime hours;
- * Residents currently use the entrance of the former Corporation Yard and the pavement in front of the site for parking;
- * Could there be a review of parking restrictions either side of the access road?;
- * What type of housing is being proposed?
- * There is an emergency access/gateway for the nursery adjacent the site which needs to be maintained;
- * The drainage within the yard needs to be protected and maintained;
- * As the site will no longer be private there are safeguarding and security issues for pre-school children at the pre-school;
- * Potential damage to church property (*officer comment: any damage would be a civil matter (not*

a material planning consideration)).

Westhoughton Town Council:- raised no objection but requested that the opportunity is taken to improve parking in the area by removing double yellow lines on Park Road (from "Tuck In" to Alma Road and on both side of Park Road beyond Leigh Road).

Elected Members:- Cllr. Kirk-Robinson has requested that the application be heard before Committee and has raised objection to the proposal on the following grounds:

- * The development will have a severe effect on local wildlife, particularly protected birds and bats that nest/roost in the area (by the demolition of buildings and the destruction of ivy);
- * The development will block views of/hide the oldest building in Westhoughton which neighbours the site (1 and 2 Higher Landedmans);
- * The amount of light reaching 1 and 2 Higher Landedmans will be adversely affected by the development, particularly as these dwellings have low ceilings and small windows;
- * Some of the development will be within 8 feet of the neighbouring dwellings on Higher Landedmans;
- * Parking on George Street and Leigh Road is already insufficient and cars already park in streets on both sides of the application site. the new development will reduce places where cars will be able to park resulting in further parking problems in the area;
- * Vehicles regularly park in the turning area on George Street because of insufficient space on that road. This access will be required by the development;
- * George Street is already heavily used as parking for carers and other staff who work and access the special needs residences to the rear of higher Landedmans;
- * Parking provision within the site is inadequate;
- * The proposed dwellings are oversized when compared with the older neighbouring properties;
- * The ground is likely to contain a number of pollutants that would be harmful to residents;
- * Not enough neighbours have been consulted on the plans.

Consultations

Advice was sought from the following consultees: Highways Engineers, Tree Officers, Landscape Officers, Drainage Officers, Pollution Control Officers, Strategic Housing Officers, Strategic Development Unit, Economic Development Officers, Corporate Property Officers, Greater Manchester Ecology Unit, Primary Care NHS, the Coal Authority, Greater Manchester Police, United Utilities.

Planning History

Outline planning permission 91146/13 was granted at Planning Committee in February 2014 for the residential development of the site (access details only).

2.4 metre high palisade security fencing was approved in January 2005 (69677/04).

Recommendation: **Approve the application subject to conditions and authorise the Director of Place and Borough Solicitor to complete all the necessary legal formalities in consultation with the Chair of Planning Committee.**

Recommended Conditions and/or Reasons

1. The development hereby permitted shall be begun before the expiration of two years from the date of this permission.

Reason

Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. Prior to the commencement of the development:-

- * A Site Investigation report shall be submitted to and approved in writing by the Local Planning Authority. The investigation shall address the nature, degree and distribution of land contamination on site and shall include an identification and assessment of the risk to receptors focusing primarily on risks to human health, property and/or the wider environment; and
- The details of any proposed remedial works shall be submitted to, and approved in writing by the Local Planning Authority. The approved remedial works shall be incorporated into the development during the course of construction and completed prior to occupation of the development or the development being first brought into use; and

Prior to first use/occupation of the development hereby approved:

- A Verification Report shall be submitted to, and approved in writing by, the Local Planning Authority. The Verification Report shall validate that all remedial works undertaken on site were completed in accordance with those agreed by the LPA.

Reason

To safeguard the amenity of the future occupants of the development and to comply with Core Strategy policy CG4.

3. Prior to the commencement of development a scheme of intrusive site investigations for the site shall be submitted to and approved in writing by the local planning authority. The approved scheme of intrusive site investigations shall be undertaken prior to the commencement of development and a report of findings arising from the investigations (and a scheme of remedial works if necessary) shall then be submitted to and approved in writing by the local planning authority. Any approved remedial works shall be implemented in full in accordance with an approved timeframe.

Reason

The coal mining legacy of the area poses a potential risk to the proposed development and further investigation is required to establish the exact situation regarding coal mining legacy issues on the site, and to comply with policy CG4 of Bolton's Core Strategy.

4. Prior to the commencement of any groundworks surface water drainage works should be implemented in full in accordance with details that have been submitted to and approved in writing with the Local Planning Authority. Before these details are submitted an assessment shall be carried out of the potential for disposing of surface water by means of a sustainable drainage system in accordance with the principles set out in the National Planning Policy Framework, and the results of the assessment provided to the Local Planning Authority. Where a sustainable drainage system is to be provided, the submitted details shall:

- 1) Provide information about the design storm period and intensity, the method employed to delay and control the surface water discharged from the site and the measures taken to prevent pollution of the receiving groundwater and/or surface waters.
- 2) Include a timetable for its implementation, and
- 3) Provide a management and maintenance plan for the lifetime of the development which shall include the arrangements for adoption by any public authority or statutory undertaker and any other arrangements to secure the operation of the scheme throughout its lifetime

Reason

To ensure the site provides satisfactory means of surface water drainage and to comply with policies CG1.5 and CG2.2 of Bolton's Core Strategy.

5. No demolition, development or stripping of soil shall be started until:

1. The trees within or overhanging the site which are to be retained have been surrounded by fences of a type to be agreed in writing with the Local Planning Authority prior to such works commencing.
2. The approved fencing shall remain in the agreed location (in accordance with BS 5837:2012) until the development is completed or unless otherwise agreed in writing with the Local Planning Authority and there shall be no work, including the storage of materials, or placing of site cabins, within the fenced area(s).
3. No development shall be started until a minimum of 14 days written notice has been given to the Local Planning Authority confirming the approved protective fencing has been erected.

Reason

To protect the health and appearance of the tree(s) and in order to comply with Bolton's Core Strategy policies CG1 and CG3.

6. Prior to the commencement of development of any works on site, the developer shall submit a method statement detailing how the following elements of the site will be constructed without causing harm or damage to the protected trees found on the site to the Local Planning Authority. The specified areas are:

- (i) No-dig method statements for the hard and soft works (driveways, footpaths and gardens) within the root protection zones of the trees to be retained;
- (ii) Permeable surfacing details for the driveways proposed within the root protection zones of the trees to be retained.

No development or site clearance shall take place until the Local Planning Authority has agreed the measures in writing, and these measures shall then be implemented fully in accordance with the approved details.

Reason

To ensure the safe development of the site and favourable retention of trees and in order to comply with Bolton's Core Strategy policies CG1 and CG3.

7. Prior to the commencement of development hereby approved/permitted, all developer obligations shall be fulfilled to enable the Local Highway Authority to carry out a review of Traffic Regulation Orders in the locality where necessary and to enable the Local Highway Authority to consult, advertise, promote and implement Traffic Regulation Orders to mitigate the likely impact of the development.

Reason

In the interests of highway safety and in order to comply with Bolton's Core Strategy policies S1, P5 and Supplementary Planning Document "Accessibility, Transport and Road Safety"

8. Prior to the development hereby approved/permitted being first occupied, all Traffic Regulation Orders required by the Local Highway Authority to mitigate the impact of the development shall be implemented in full.

Reason

In the interests of highway safety and in order to comply with Bolton's Core Strategy policies S1, P5 and Supplementary Planning Document "Accessibility, Transport and Road Safety".

9. Prior to the commencement of development full details of the highway works at Harrison Street comprising the dedication of land to highway and the widening of the footway to 1.8 metres over the redundant building footprint shall be submitted to and approved in writing by the Local Planning

Authority. The approved scheme shall be implemented in full prior to the development being first brought into use and retained thereafter.

Reason

In the interests of highway safety and in order to comply with Bolton's Core Strategy policies S1, P5 and Supplementary Planning Document "Accessibility, Transport and Road Safety".

10. Prior to the commencement of development full details of the highway works at Park Road comprising:

- (i) the resurfacing of the footway across the site frontage
- (ii) the implementation of vehicle access crossings (VAC) to facilitate access to the proposed driveways at plots 1, 3, 4 and 6

shall be submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be implemented in full prior to the development being first brought into use and retained thereafter.

Reason

In the interests of highway safety and in order to comply with Bolton's Core Strategy policies S1, P5 and Supplementary Planning Document "Accessibility, Transport and Road Safety".

11. Prior to the commencement of development full details of the highway works at George Street comprising:

- (i) the resurfacing of the section fronting plots 18 to 21
- (ii) the relocation of street lighting to facilitate the development

shall be submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be implemented in full prior to the development being first brought into use and retained thereafter.

Reason

In the interests of highway safety and in order to comply with Bolton's Core Strategy policies S1, P5 and Supplementary Planning Document "Accessibility, Transport and Road Safety".

12. Prior to the commencement of development the details (samples if required) of the type and colour of materials to be used for the external walls and roof to be agreed with the Local Planning Authority. The approved materials shall be implemented in full thereafter.

Reason

To ensure the development reflects local distinctiveness and to comply with policy CG3 of Bolton's Core Strategy.

13. Prior to the development hereby approved/permitted being first occupied the means of vehicular access to the site from Park Road shall be constructed to a minimum width of 5.5 metres with 5.0 metres radii and 2 no. 2.0 metres wide footways in accordance with the drawing ref M4121(PL)01 Rev H.

Reason

In the interests of highway safety and in order to comply with Bolton's Core Strategy policies S1, P5 and Supplementary Planning Document 'Accessibility, Transport and Road Safety'

14. Prior to the development hereby approved/permitted being first occupied the existing vehicular access onto Park Road shall be closed to vehicles and the existing highway (kerbing and footway)

made good to adoptable footway standards. There shall thereafter be no means of vehicular access to or from Park Road, other than as shown on drawing ref: M4121(PL)01 Rev H.

Reason

In the interests of highway safety and in order to comply with Bolton's Core Strategy policies S1, P5 and Supplementary Planning Document 'Accessibility, Transport and Road Safety'.

15. Prior to the development hereby approved being first occupied or brought into use, a scheme detailing how parts of the site to be used by vehicles are to be laid out, constructed, surfaced, drained and lit shall be submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be implemented in full and thereafter made available for the use of vehicles at all times the development is in use.

Reason

To encourage drivers to make use of the parking and circulation area(s) provided and in order to comply with Core Strategy policies S1 and P5 and Supplementary Planning Document 'Accessibility, Transport and Road Safety'.

16. Prior to the occupation of the dwelling houses hereby permitted provision shall be made for the parking 2 motor vehicles adjacent to each of the dwelling houses in the area identified for that purpose on the approved plan. Those areas shall thereafter be retained at all times for that purpose. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any Order amending or replacing that Order) other than (a) garage(s), no extensions, porches, outbuildings, sheds, greenhouses, oil tanks, wind turbines or satellite antennae shall be erected within that area.

Reason

To ensure that adequate provision is made for vehicles to be left clear of the highway and in order to comply with Bolton's Core Strategy policies S1, P5 and Appendix 3.

17. Trees and shrubs shall be planted on the site in accordance with a landscape scheme to be submitted and approved in writing by the Local Planning Authority prior to the development being first brought into use. The approved scheme shall be implemented in full and carried out within 6 months of the occupation of any of the buildings or the completion of the development, whichever is the sooner, or in accordance with phasing details included as part of the scheme and subsequently approved by the Local Planning Authority. Any trees and shrubs that die or are removed within five years of planting shall be replaced in the next available planting season with others of similar size and species.

Reason

To reflect and soften the setting of the development within the landscape and in order to comply with Core Strategy policies CG1 and CG3.

18. Prior to the development being first occupied or brought into use, details (including a brick or masonry specification and colour scheme) of the treatment to all boundaries to the site shall be submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be implemented in full prior to first occupation and retained thereafter.

Reason

To ensure adequate standards of privacy and amenity are obtained and the development reflects the landscape and townscape character of the area and in order to comply with policies CG3 and CG4 of Bolton's Core Strategy.

19. Foul and surface water shall be drained on separate systems.

Reason

To secure proper drainage and to manage the risk of flooding and pollution, and to comply with policy CG1.5 of Bolton's Core Strategy.

20. No dwelling shall be occupied until the access road(s), footway(s) and footpath(s) leading thereto have been constructed and completed in accordance with drawing ref: M4121(PL)01 Rev H.

Reason

In the interests of highway safety and in order to comply with Bolton's Core Strategy policies S1, P5 and Supplementary Planning Document 'Accessibility, Transport and Road Safety'.

21. The development hereby permitted shall be carried out in complete accordance with the following approved plans:

M4121(PL)01 Rev H; "Proposed Site Layout"; dated Mar 2017

M4121(PL)10; "House Type A"; dated Mar 2017

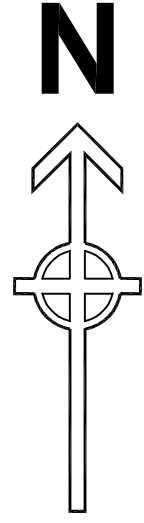
M4121(PL)11 Rev A; "House Type B"; dated Mar 2017

Reason

For the avoidance of doubt and in the interests of proper planning.

Site Layout

Park Road
Westhoughton, Bolton



Schedule Of Accommodation			
Type	Accommodation	Area	No.
A	2 bedroom 4 person house	70sqm	2
B	3 bedroom 5 person house	81sqm	16
B1	3 bedroom 5 person house (dual aspect)	82.5sqm	3
		TOTAL	21

Boundary Treatments

- Type A
900mm high ins railings with litter plate plinth detail
- Type B
1500mm high vertical board timber fence
- Type C
1800mm vertical board timber fence
- Type D
1500mm timber fence with 300mm trellis to top
- Type E
2100mm vertical board timber fence
- Type F
1500mm timber fence with 600mm trellis to top
- Type G
900mm high brick wall with 900mm high vertical board timber fence to top



Location designated for future garage



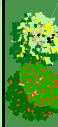
Proposed Shared Ownership Plot



Concrete paving flags



Turfed lawn area



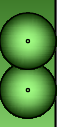
Low level shrub planting (to be flex-pave or similar driveway solution where located under existing trees)



Tarmac parking space



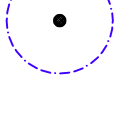
Tarmac highway



Proposed tree planting



Handstanding for storage of external wheeled bins



Existing Tree Root Protection Area - all works within these zones must utilise hand-dig techniques

Site Layout
Scale 1:200

Finished floor levels are indicative only

nicol thomas
architects project managers construction cost consultants CDM co-ordinators
Registered in England and Wales, Reg No. 2140639
Quality Assured to BS EN ISO 9001:1994 Certificate Number GB 4723
Heyside House, Blackshaw Lane, Heyside, Royton, Oldham, OL2 6NS
t01706 390088 f01706 290089 eodham@nicolthomas.com
Also at Birmingham (Registered office)

Revision	Date	Details
A	Apr 17	Layout amended to keep proposed buildings out of existing tree root protection areas
B	Apr 17	Layout amended to keep plots 1 and 2 further from existing tree RPA.
C	Apr 17	Designer identified construction hazards added to drawing.
D	Apr 17	Car parking arrangements amended for plots 3-6 following discussions with Highways
E	May 17	Layout amended following meeting with Council Tree Officer in order to retain the existing trees along the Park Road frontage
F	May 17	Layout amended to accommodate improvement to existing footpath on Howarth Street to 1800mm wide at request of LTA Highways department

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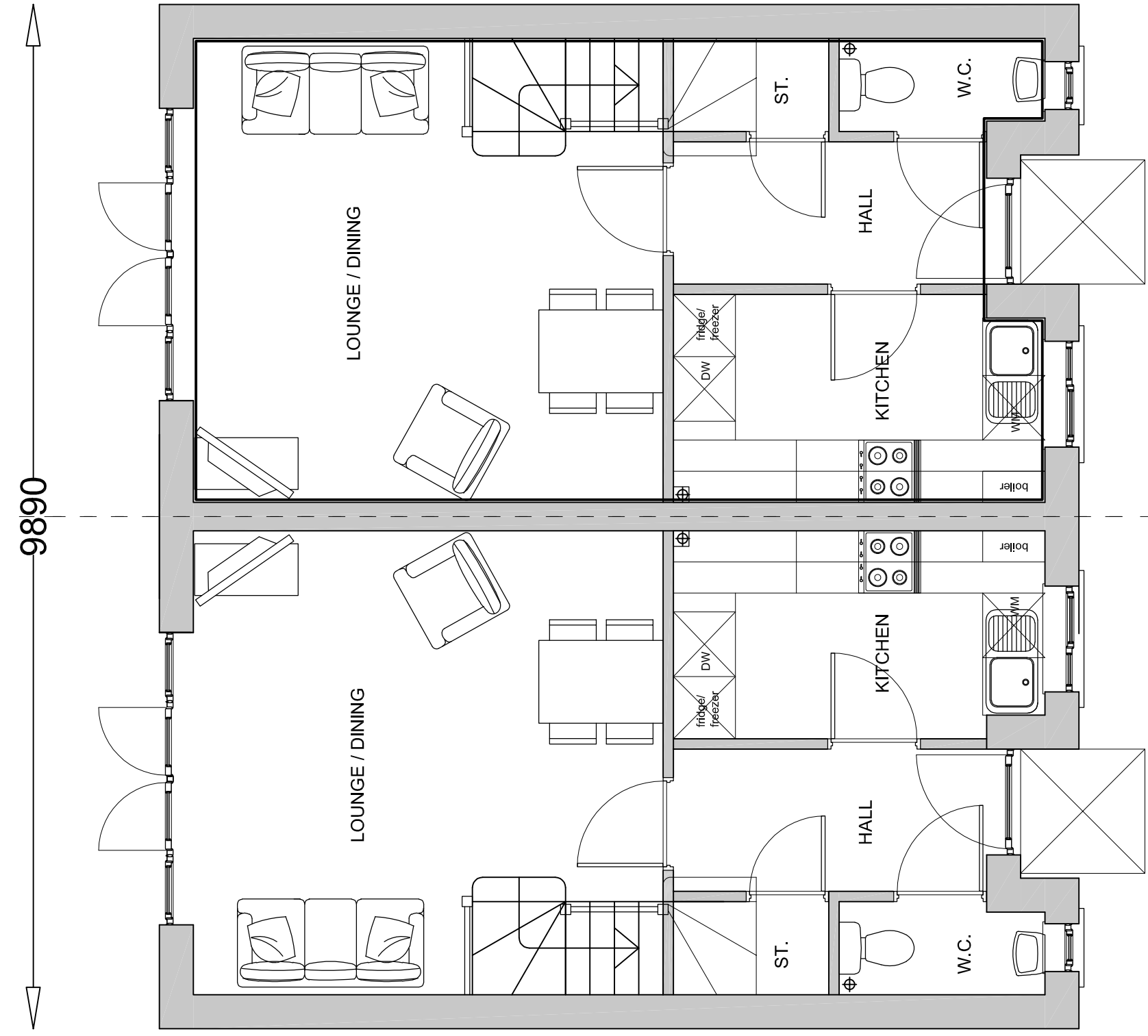
CONSTRUCTION (DESIGN AND MANAGEMENT) REGULATIONS 2015 DESIGNERS' IDENTIFICATION AND ASSESSMENT OF HAZARDS WHICH CAN BE REASONABLY FORESEEN, AND AS NOTED ON CONSTRUCTION DRAWINGS.

ITEM	HAZARD
1	Public access to construction activities where site is adjacent to roads, footpaths and public areas.
2	Contact with buried services within site
3	Contact with contaminated ground
4	Contact with overhead cables
5	Contact with underground water
6	Works to existing roads, footpaths adjacent to site, outside perimeter fencing.
7	Works executed close to existing structures, walls fences etc.
8	Works executed close to existing graveyard

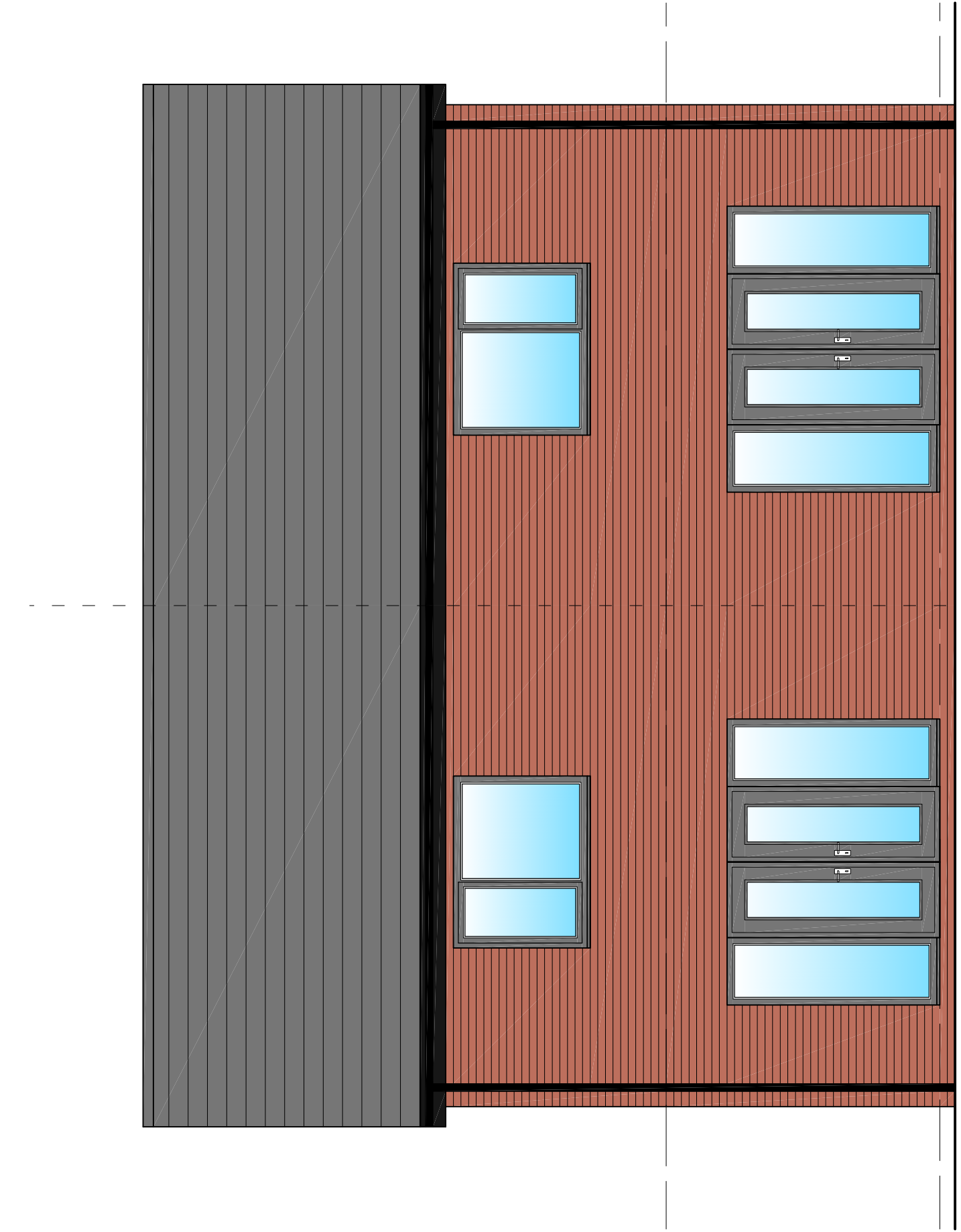
H	July 17	Highway footpath increased to 2m either side of new access road at request of Engineer
G	July 17	Plots 18-21 pushed back to create clearance to side of 18 for drainage easement



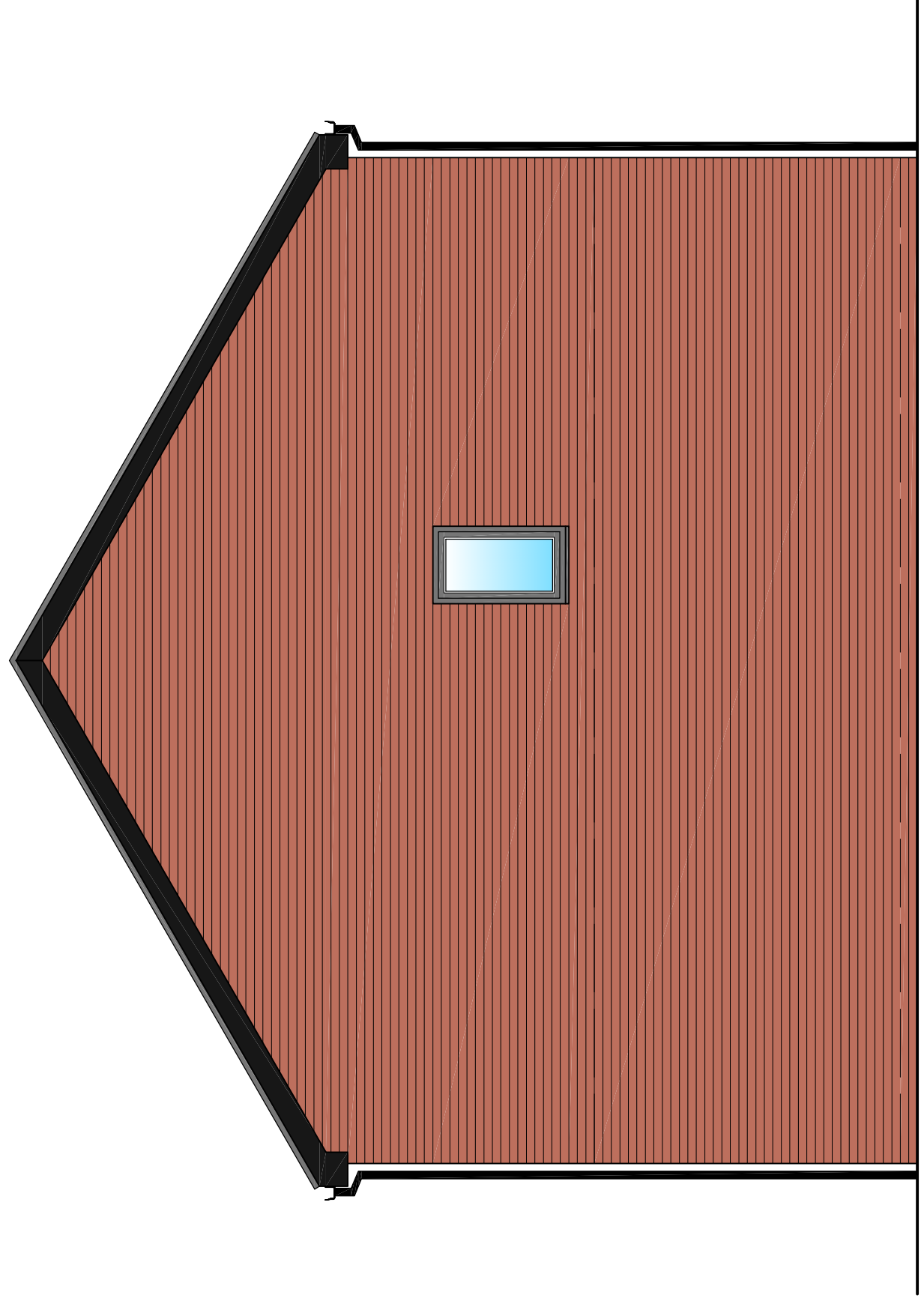
First Floor Plan



Ground Floor Plan

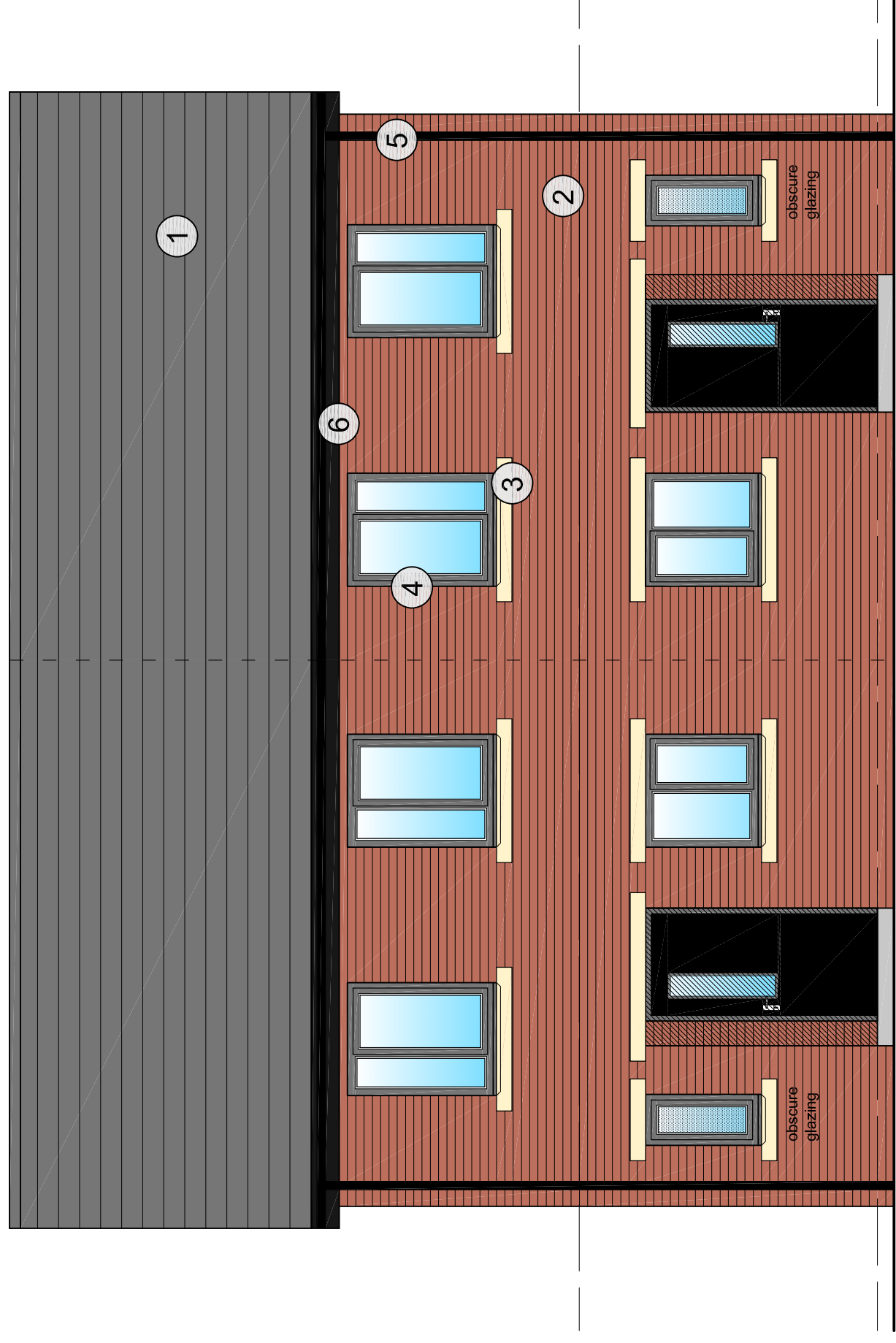


Rear Elevation

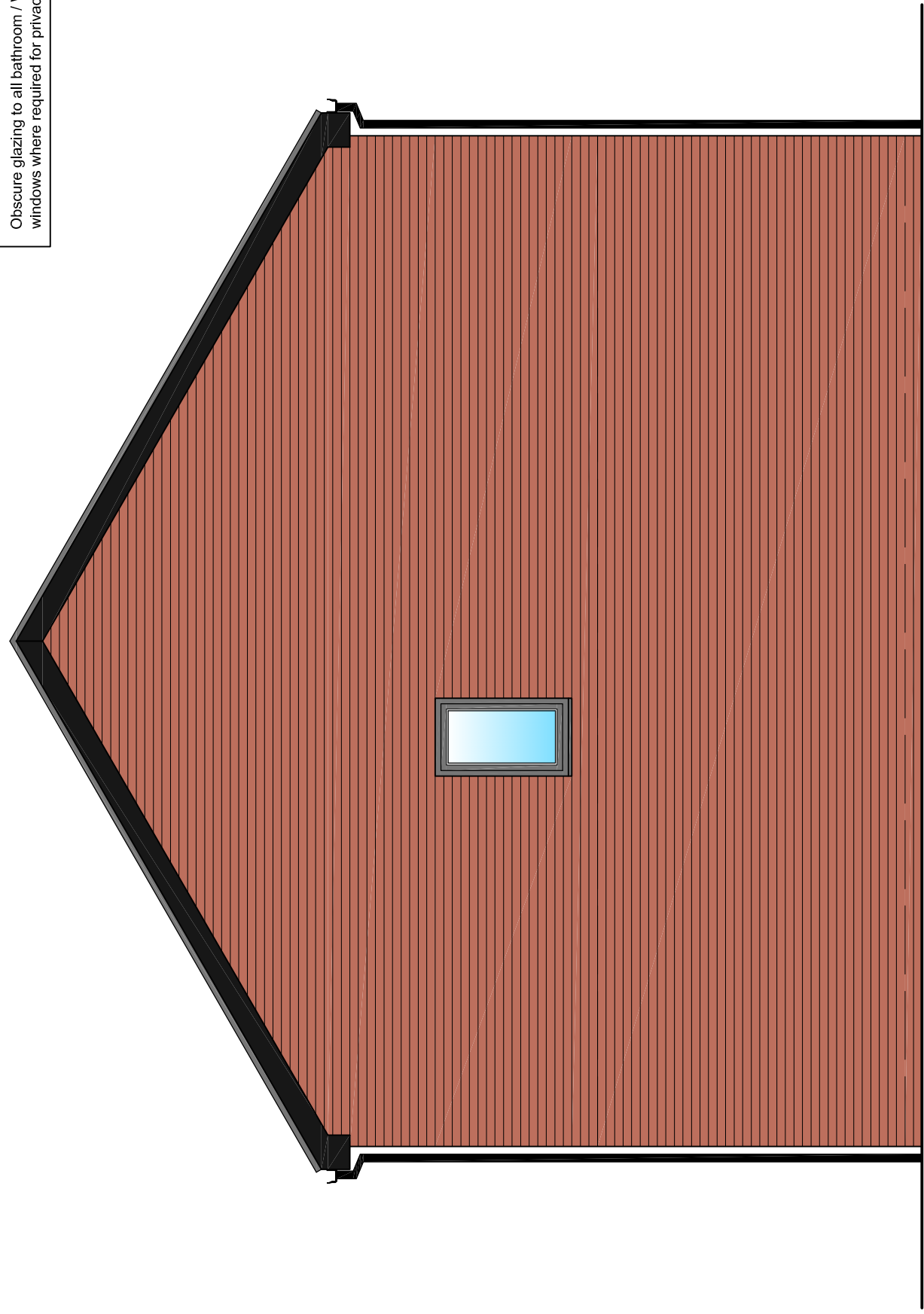


Gable Elevation

Materials Legend	
①	Interlocking concrete roof tiles.
②	Facing brickwork
③	Artstone Cills and Heads
④	UPVC window frames. Colour Grey
5	UPVC rainwater goods. Colour Black
6	UPVC fascias and soffits. Colour Black
NB All materials/colours to be approved by LA Obscure glazing to all bathroom / WC windows and gable windows where required for privacy	



Front Elevation



Gable Elevation

Revision	Date	Details

nicol thomas architects project managers construction cost consultants CDM co-ordinators
Registered in England and Wales. Reg No. 2140639
Quality Assured to BS EN ISO 9001:1994 Certificate Number GB 4723
Heyside House Blackshaw Lane Heyside Roydon Oldham OL2 6NS
t01706 290088 f01706 290089 e:oldham@nicolthomas.com
Also at Birmingham (Registered office)
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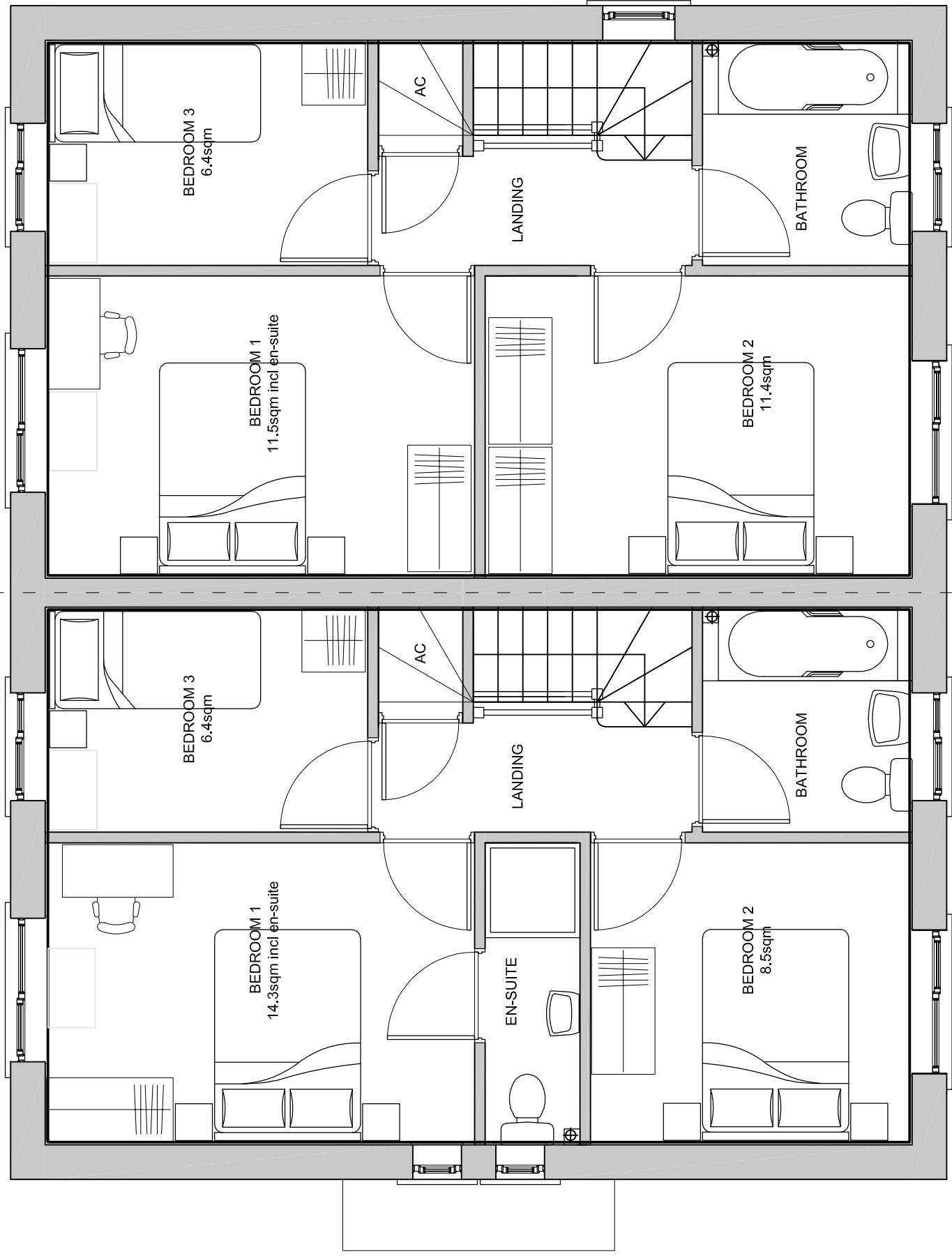
Client:	Casey Group		
Job:	Park Road, Westhoughton		
Drawing title:	House Type A		
Drawing Number: (Job number)	M4121	(PL)	10
Revision:			
Scale:	1:50 @ A1		
Date:	Mar 2017		
Drawn by checked by:	RW		

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House Type B1 / B

3 bed 5 person house @ 82.5 / 81sqm

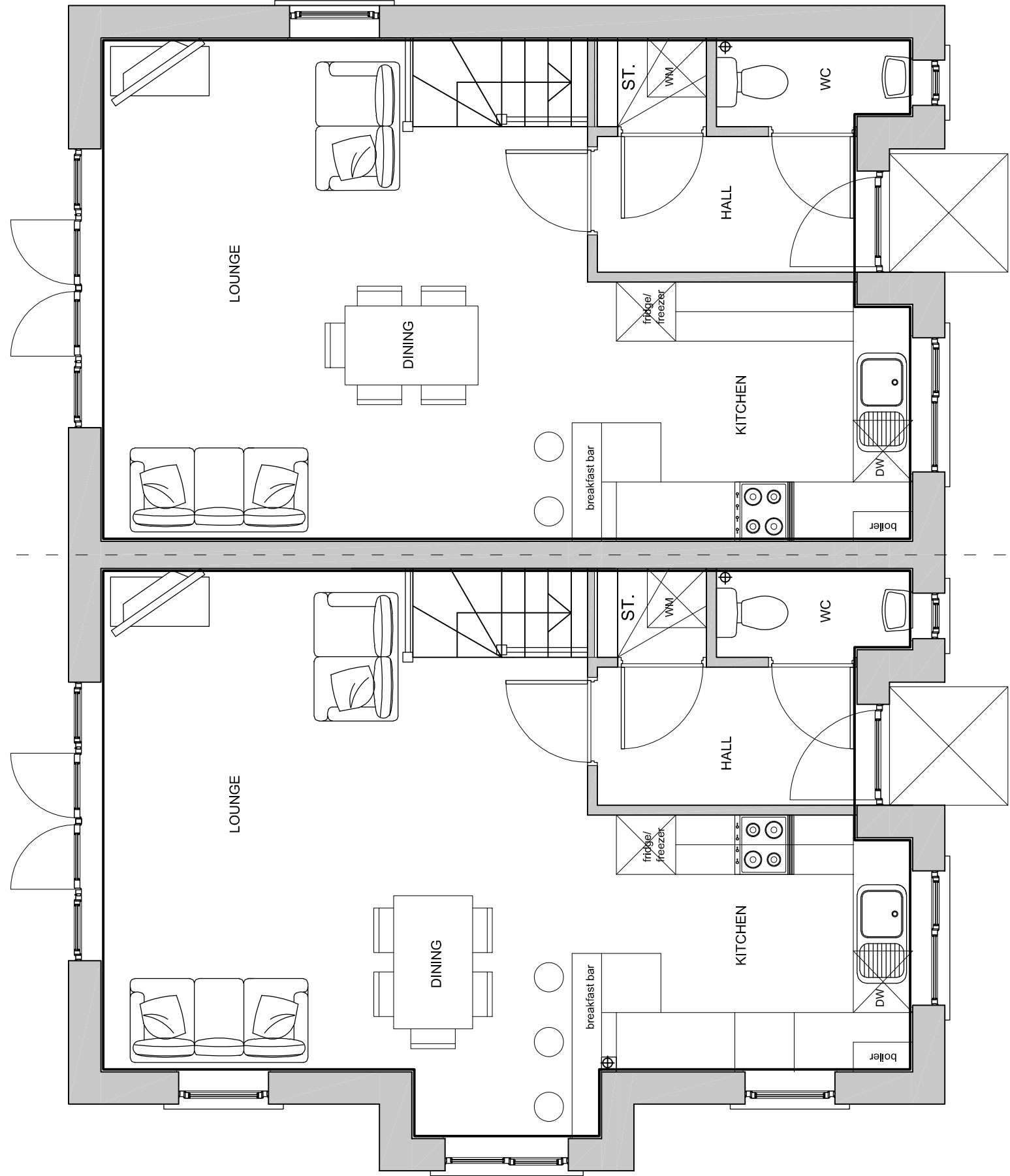
Park Road
Westhoughton, Bolton



First Floor Plan
En-Suite option

Standard option

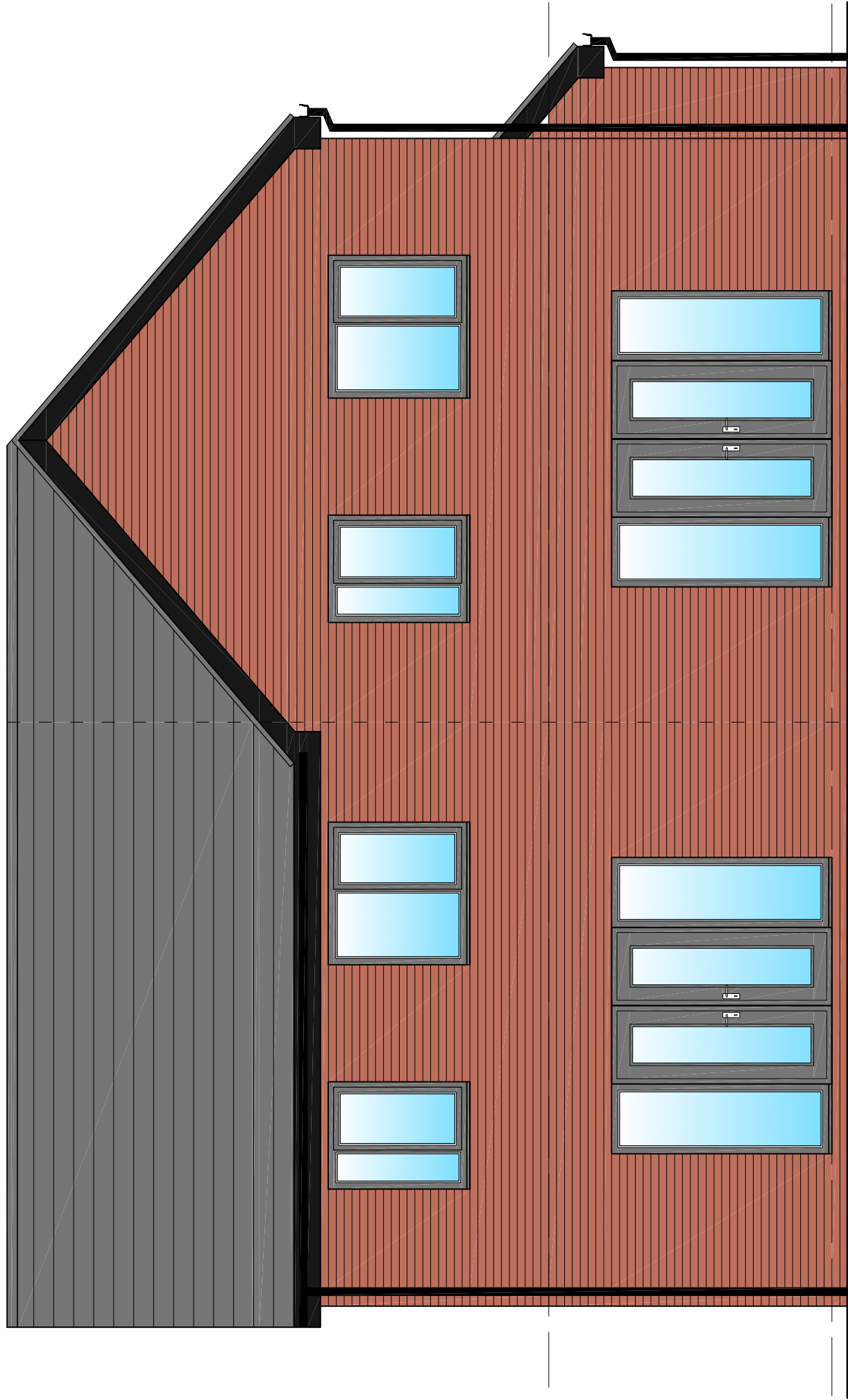
11128



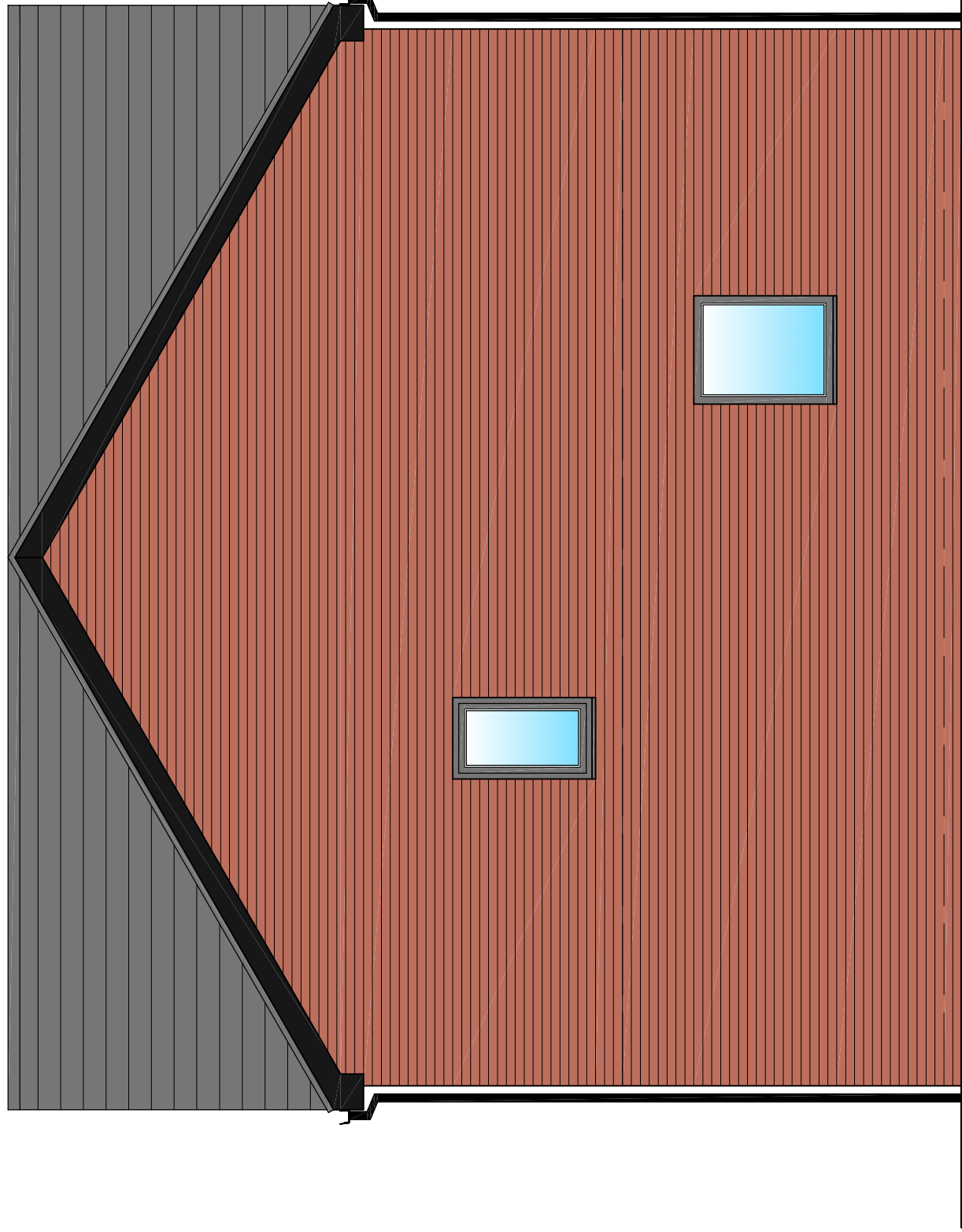
Ground Floor Plan
Dual aspect unit (B1)

Standard unit (B)

8878

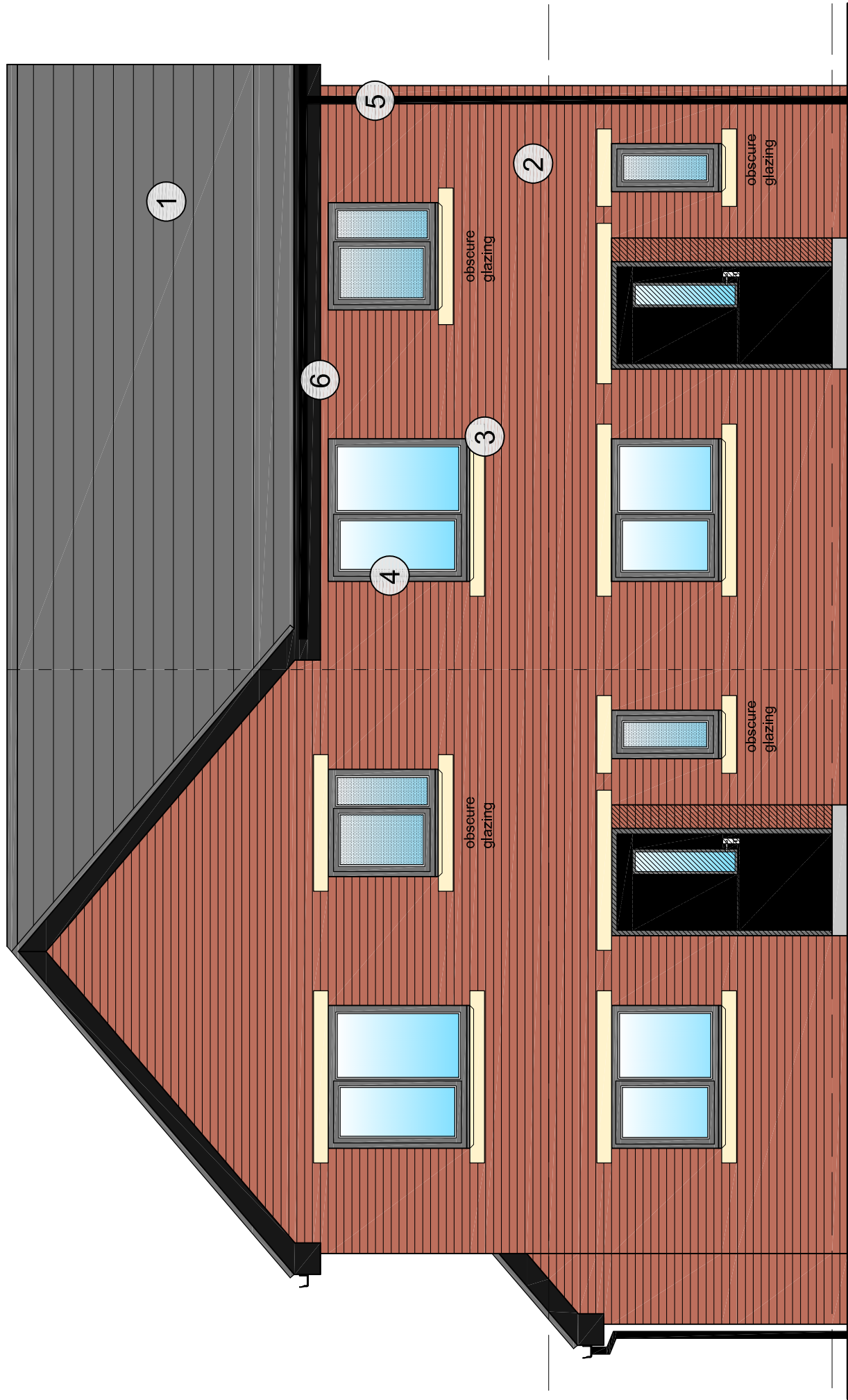


Rear Elevation

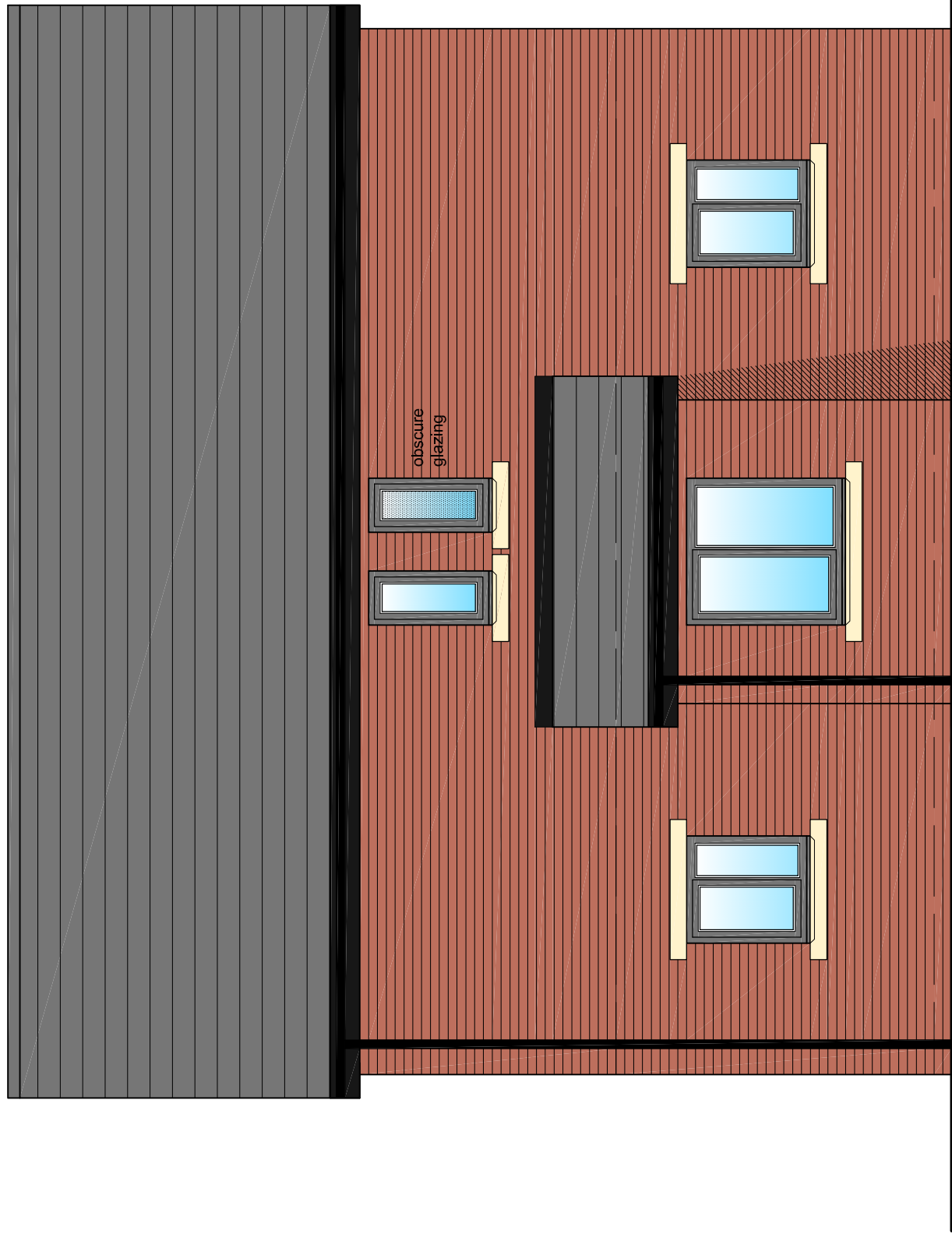


Gable Elevation
Standard unit

Materials Legend	
1	Interlocking concrete roof tiles.
2	Facing brickwork
3	Artstone Cills and Heads
4	UPVC window frames. Colour Grey
5	UPVC rainwater goods. Colour Black
6	UPVC fascias and soffits. Colour Black
NB All materials/colours to be approved by LA	
Obscure glazing to all bathroom /WC windows and gable windows where required for privacy	



Front Elevation
Dual aspect unit



Gable Elevation
Dual aspect unit

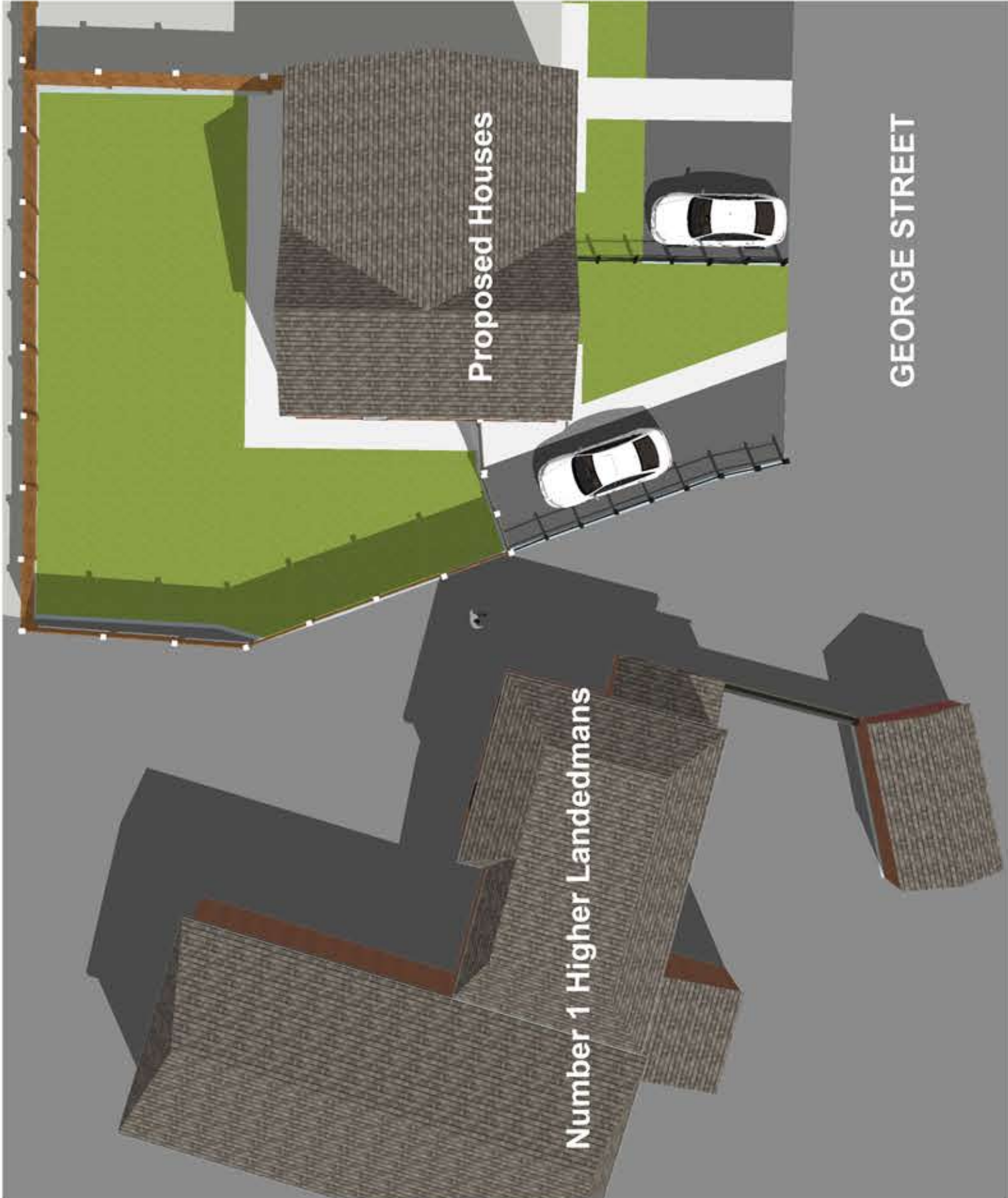
Revision	Date	Details
A	Apr 17	Windows added to first floor gable of type B1

nicol thomas
architects project managers construction cost consultants CDM co-ordinators
Registered in England and Wales. Reg No. 2140639
Quality Assured to BS EN ISO 9001:1994 Certificate Number GB 4723
Heside House, Blackshaw Lane, Heyside, Royton, Oldham, OL2 6NS
t01706 290088 f01706 290099 e:oldham@nicolthomas.com
Also at Birmingham (Registered office)
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Client:		Casey Group	
Job:		Park Road, Westhoughton	
Drawing title:		House Type B	
Drawing Number: (Job number)		M4121 (PL)	11
Revision:		A	
Scale:		1:50 @ A1	
Date:		Mar 2017	
Drawn by/ checked by:		RW	

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Relationship between proposed development and number 1 Higher Landedmans



Revision	Date	Details

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architects project managers construction cost consultants CDM co-ordinators
Quality Assured to BS EN ISO 9001:1994 Certificate Number GB 4723
Heyside House Blackshaw Lane Heyside Royton Oldham OL2 6NS
t:01705 290088 f:01705 290099 e:oldham@nicolthomas.com
Also at Birmingham (Registered office)
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Client: Bolton at Home				
Job: Park Road, Westhoughton				
Drawing title: 3D Massing Images				
Drawing Number: (Job number)		M4121	(PL)	15
Revision:				
Scale: n.t.s. @ A1				
Date: Aug 17				
Drawn by/checked by: SAR/W				