

Planning Applications Report

Planning Committee

30 June 2016

**Bolton
Council**

Bolton Council has approved a Guide to Good Practice for Members and Officers Involved in the Planning Process. Appendix 1 of the Guide sets down guidance on what should be included in Officer Reports to Committee on planning applications. This Report is written in accordance with that guidance. Copies of the Guide to Good Practice are available at www.bolton.gov.uk

Bolton Council also has a Statement of Community Involvement. As part of this statement, neighbour notification letters will have been sent to all owners and occupiers whose premises adjoin the site of these applications. In residential areas, or in areas where there are dwellings in the vicinity of these sites, letters will also have been sent to all owners and occupiers of residential land or premises, which directly overlook a proposed development. Copies of the Statement of Community Involvement are available at www.bolton.gov.uk

The plans in the report are for location only and are not to scale. The application site will generally be in the centre of the plan edged with a bold line.

The following abbreviations are used within this report: -

UDP	The adopted Unitary Development Plan 2005
RSS	Regional Spatial Strategy for the North West of England 2008
PCPN	A Bolton Council Planning Control Policy Note
PPG	Department of Communities and Local Government Planning Policy Guidance Note
MPG	Department of Communities and Local Government Minerals Planning Guidance Note
SPG	Bolton Council Supplementary Planning Guidance
SPD	Bolton Council Supplementary Planning Document
PPS	Department of Communities and Local Government Planning Policy Statement
TPO	Tree Preservation Order
EA	Environment Agency
SBI	Site of Biological Importance
SSSI	Site of Special Scientific Interest
GMEU	The Greater Manchester Ecology Unit

The background documents for this Report are the respective planning application documents which can be found at:-

www.bolton.gov.uk/planapps

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Town Hall, Bolton, Lancashire, BL1 1RU
Telephone (01204) 333 333

Bolton Council

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Date of Meeting: 30/06/2016

Application Reference: 95081/15

Type of Application: Full Planning Application
Registration Date: 24/03/2016
Decision Due By: 18/05/2016
Responsible Officer: Jeanette Isherwood

Location: UNITS 1 AND 3, ST PAUL'S MILL, BARBARA STREET, BOLTON, BL3 6UQ

Proposal: CHANGE OF USE FROM INDUSTRIAL UNIT TO COMMUNITY AND FUNCTION SPACE

Ward: Rumworth

Applicant: Mr Jiva
Agent : Smusa1

Officers Report

Recommendation: Approve subject to conditions

Background

This proposal was initially presented to the Planning Committee on the 17th December 2015 but was deferred at the meeting for a site visit and further information.

Ownership issue with regards to the site and the additional parking proposed was identified and as a result the application was held for further clarification. The following changes were made and the consultation process was repeated:-

- Certificate B was submitted identifying all correct ownership
- The additional parking at Swan Mill was deleted and a new site plan with amended red edge was submitted (further consultation was taken on receipt of the corrected plan)

Proposal

A change of use is proposed to use part of the mill building for a community and function facility, which falls into a D2 use classification.

The proposal includes a function and community assembly space together with a serving area and kitchen. The seating capacity of the venue has been detailed as 150 - 250 people.

25 car parking spaces have been allocated within the existing car park that serves St Paul's Mill as a whole.

The applicant has requested that the hours of opening will be 11:00 - 23:00 Monday to Sunday with 22 employees (2 x full time and 20 x part time).

Minor changes to the front and side elevations are proposed to provide a new entrance lobby (to the side) and 2 exit doors to the front.

Site Characteristics

The site is a former mill which lies in the heart of Inner Bolton. To the north and east are residential dwellings, typically laid out in rows of terraces. To the south are other mill buildings, which are put to commercial uses. The adjoining part of the mill is used as a mosque (Goshia Mosque). There are further small units in use within the site.

Policy

National Planning Policy Framework 2012

Core Strategy Objectives - SO2 Achieving Bolton; SO11 Bolton's Built Heritage

Core Strategy Policies CG3.2 Conserve and Enhance Local Distinctiveness; CG3.3 Scale, Massing, Grain and Form; CG4.1 Compatible Uses; SC2.1 Cultural and Community Facilities; RA1 Inner Bolton.

Analysis

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- * Suitability of the Use and Visual Changes
- * Highways Issues
- * Other Matters

Suitability of the Use and Visual Changes

Core Strategy policy SC2.1 states that the Council and its partners will ensure that local cultural activities and community facilities are located in the neighbourhoods they serve.

Objection has been raised to the proliferation of similar uses in the area. However, this proposal is ideally situated to promote community cohesion, providing facilities within the communities they serve.

Core Strategy policy CG4.1 outlines the Council's stance regarding the protection and preservation of neighbouring occupiers privacy and amenity by ensuring new development is compatible with surrounding land uses and it does not generate unacceptable nuisance, fumes, odours, noise or light pollution.

Several objections have been received to the proposal with one issue being the possibility of noise disturbance with the use of the venue.

The Council's Environmental Health Officers were consulted and had no objections to the proposal subject to several conditions due to the proximity of the site to residential dwellings. Conditions are suggested to ensure there is no undue disturbance and nuisance arising from the centre with regard to light pollution, odour or noise, beyond what is normally expected during daytime and evening hours.

In terms of the proposed use, on the basis of information submitted and technical consultee responses, Officers do not consider the use will unduly affect the amenity of local residents, businesses or the surrounding environment.

One objection has been raised with regards to the loss of local employment. However, the site was previously vacant, its re-use will create 22 new employment opportunities for local people.

Core Strategy policies CG3.2 and CG3.3 require proposals to respect and enhance local distinctiveness and be compatible with the surrounding area in terms of scale, massing, grain, form, architecture and landscape treatment, including hard/soft landscaping and boundary treatment. Changes to the front and side elevations are considered to be minor, in terms of their impact on the street scene.

The proposal is considered to be compliant with Core Strategy policies CG3, CG4, and SC2 subject to conditions.

Highways Issues

Policy S1.2 seeks to promote road safety in new development, whilst policy P5 asks that developments are accessible by different means of transport and take into account the parking standards contained in appendix 3.

Objections have been raised to possible traffic/parking issues that may arise due to the use of the site.

Highways initial response to the amended site plan was as follows:-

- The 25 spaces within the site curtilage are not solely for this proposal and are utilised by other businesses that are located within the premises.
- The over-spill parking provision proposed within Swan Lane Mill is actually non-existent.
- There will be a reliance for on-street parking on Bridgeman Street and within the surrounding residential areas for the proposal to be viable
- We have had a number of complaints from residents relating to parking issues associated with this business which appears to be operating illegally.
- The Highways Authority is not supportive of what is being proposed owing to the level of on-street parking that it will/is generating at this location and the impact that it will potentially have on surrounding existing businesses/facilities and on the amenity value to residents within the surrounding areas.

It is noted that there is congestion at the site and therefore the following measures are proposed to alleviate these issues:-

- The erection of gates to both back streets serving Edgemont Avenue to allow private residents parking
- The incorporation of a Parking Permit Scheme to Edgmont Avenue during the hours of 18:00 - 22:00 Monday to Friday and 11:00 - 15:00/18:00 - 22:00 on Saturdays and Sundays

- The insertion of double yellow lines to the corners of the streets junctioning with Caroline Street to avoid access and egress issues.

Provision is also available for 25 vehicles in the car park, although this is shared with the adjacent local business on the site.

The Highway Engineers have expressed the following reservations to the above proposals:-

- The Highways Authority would be reluctant to support a residents only parking scheme at this location. The scheme implementation would be reliant on the implementation of a traffic regulation order to enforce the proposal, this in turn would be reliant on the satisfactory outcome of a consultation exercise with residents/businesses and stakeholders at that location.
- Both the above would not prevent parking on Bridgeman Street associated with the proposed development which was our main concern.

Affected residents have been consulted on the above scheme and a positive response has been received.

On balance Officers conclude that the proposal is in a highly sustainable location accessible to the surrounding community. The parking measures suggested are considered to alleviate some of the present issues and on this basis the proposal is considered to be acceptable.

Other Matters

Objection has been received to the validity of the final site plan with concerns that the full ownership of the site had not been highlighted. (Area edged in blue)

This issue was brought to the attention of the applicant and a request for a correction was made. No correction has been received to date. However this issue is a legal issue, the site edged red shown on the plan is indeed correct and therefore does not invalidate the application.

Concern has also been voiced that the owners listed on Certificate B have not been issued with Notice. However the applicant confirms that Notice has been sent. This does not affect the validity of the planning application.

Conclusion

The proposal is considered to comply with the aforementioned policies. Therefore the application is approved subject to conditions.

Representation and Consultation Annex

Representations

Letters:- Objections have been received from areas throughout Bolton with the main issues being:-

- Proliferation of similar uses in the area
- Traffic and parking issues
- Loss of employment opportunities
- Noise and general disturbance.
- Loss of access to emergency vehicles due to parking congestion.

Several letters of support have also been received.

Consultations

Advice was sought from the following consultees: Highways, Environmental Health.

Planning History

Planning approval for the change of use of part of vacant mill to community and education facility (D1) Ref: 88139/12

Recommendation: Approve subject to conditions

Recommended Conditions and/or Reasons

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason

Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. Before development commences details of all external lighting equipment shall be submitted to and approved by the Local Planning Authority. The lighting in the scheme should be erected and directed so as to avoid nuisance to residential accommodation in close proximity. The lighting should be designed to provide a standard maintained illumination of no more than 5 LUX at the facades of the nearest residential properties. No other lighting equipment may then be used within the development other than as approved by the Local Planning Authority.

Reason

To minimise light intrusion to the residential amenity of the area from the increase and alteration of commercial, industrial and recreational uses in the area and to comply with Core Strategy policy CG4.

3. Prior to the commencement of development full details of the following highway works shall be submitted to and approved in writing by the Local Planning Authority:-
 - The erection of gates to both back streets serving Edgemont Avenue.
 - The incorporation of a Parking Permit Scheme to Edgmont Avenue during the hours of 18:00 - 22:00 Monday to Friday and 11:00 - 15:00/18:00 - 22:00 on Saturdays and Sundays
 - The insertion of double yellow lines to the corners of the streets/back streets junctioning with Caroline Street.

The approved scheme shall be implemented in full prior to the development being first brought into

use and retained thereafter.

Reason

In the interests of highway safety.

4. Before the approved/permitted development is first brought into use no less than 25 car parking spaces with minimum dimensions of 2.5 metres by 5.0 metres shall be provided within the curtilage of the site, in accordance with Drawing Ref: Revised Site Plan 05 dated 08/15 approved by the Local Planning Authority. Such spaces shall be made available for the parking of cars at all times the premises are in use.

Reason

To ensure that adequate provision is made for vehicles to be left clear of the highway and to comply with Core Strategy policy S1 and P5.

5. Before the commencement of the use hereby permitted, a scheme showing details of the means of extraction and filtration of cooking odours and methods to be employed to prevent noise disturbance shall be submitted to and approved by the Local Planning Authority. The approved scheme shall be implemented in full before the use hereby permitted is first commenced and retained thereafter at all times.

Reason

To minimise the impact of noise on the general and residential amenity from the increase and alteration of commercial and/or industrial uses in the area and to comply with Core Strategy policy CG4.

6. The external surfaces of the altered elevations hereby permitted shall be of a similar colour, texture and size of those of the existing building, and shall be retained thereafter.

Reason

To ensure the development visually reflects the existing building and to comply with Core Strategy policy CG3.

7. The premises subject of this consent shall not be open for trade outside the following hours:-

11:00- 23:00 Mondays – Sundays and Bank Holidays

Reason

To safeguard the living conditions of residents and the amenity and character of the area with regard to noise and/or disturbance and to comply with Core Strategy policy CG4.

8. The development hereby permitted shall be carried out in complete accordance with the following approved plans:

Proposed floor plans - 03 - dated 08/15

Proposed elevations - 04 - dated 08/15

Revised site plan - 05 - dated 08/15

Reason

For the avoidance of doubt and in the interests of proper planning.



NOTES:

No scaling or dimensions other than those figured are to be taken from this drawing. All dimensions to be checked on site

This drawing is for use at the named location and for Planning Permission purposes only and as such should not be used at any other location or for any other purposes

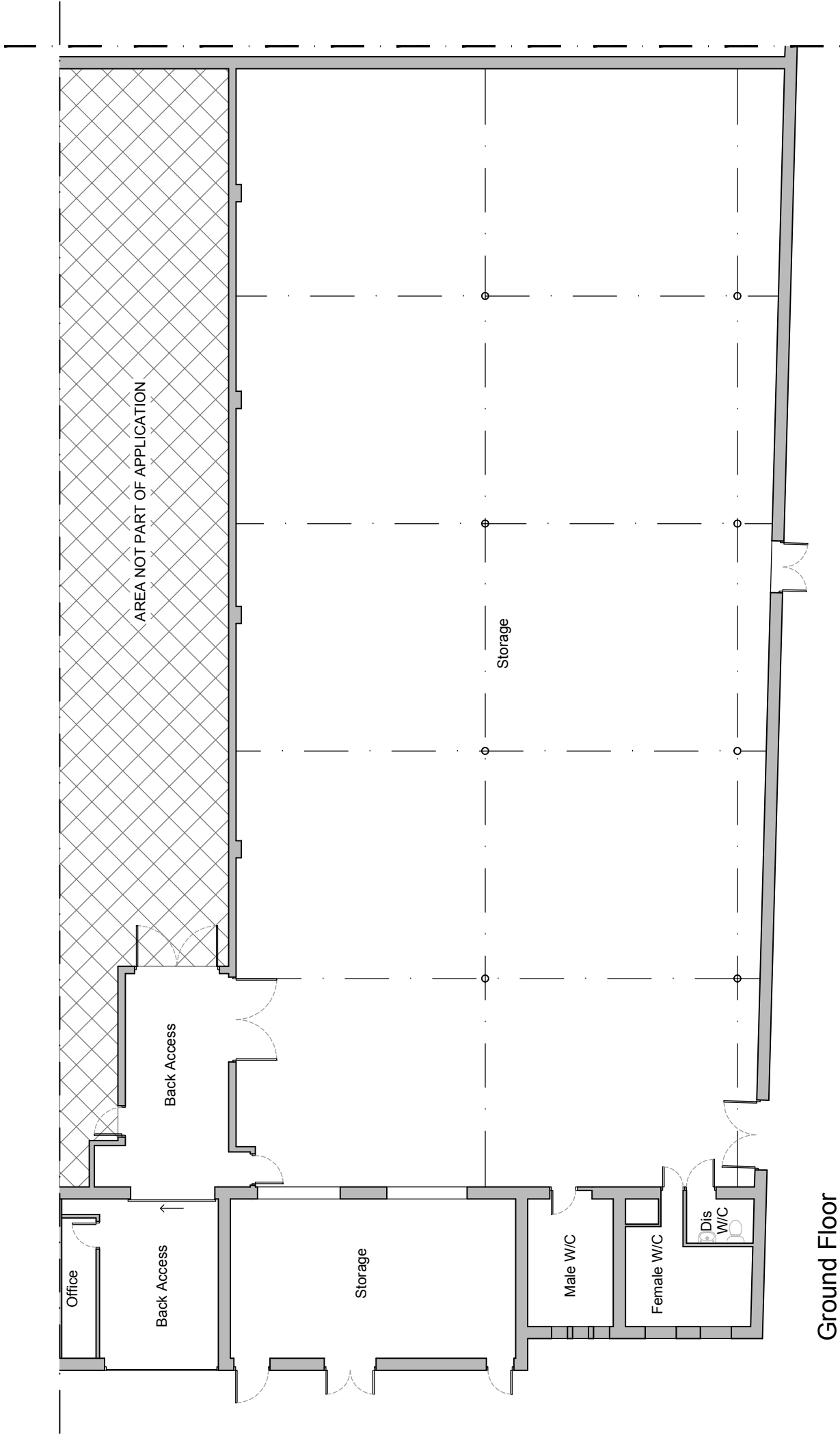
Project Location:

St Pauls Mill
Barbara St
Bolton
BL3 6UQ

Drawing:

00 Site Location

Scale:	Size:	Date:
Varies	A3	08/15



NOTES:

No scaling or dimensions other than those figured are to be taken from this drawing. All dimensions to be checked on site

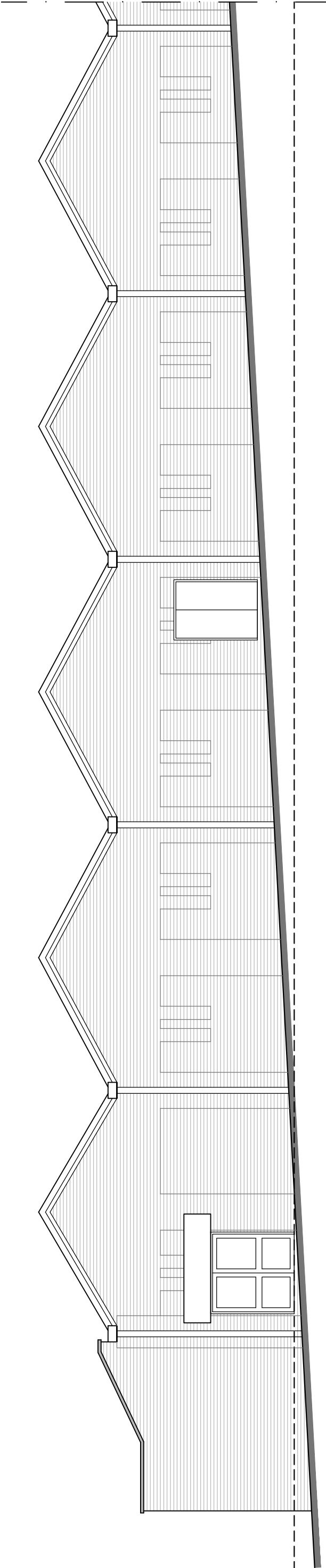
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Project Location:
St Pauls Mill
Barbara St
Bolton
BL3 6UQ

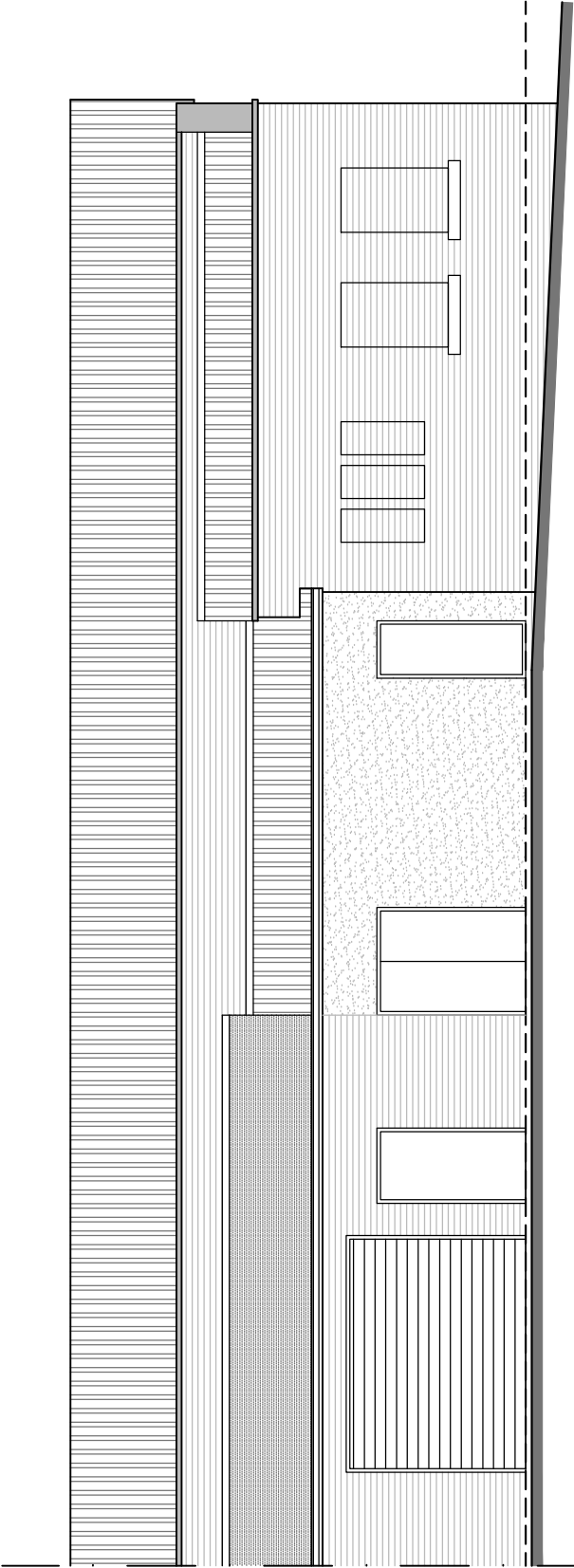
Drawing:

01 Existing Plan

Scale:	Size:	Date:
1:150	A3	08/15



Front Elevation



Side Elevation

NOTES:

No scaling or dimensions other than those figured are to be taken from this drawing. All dimensions to be checked on site

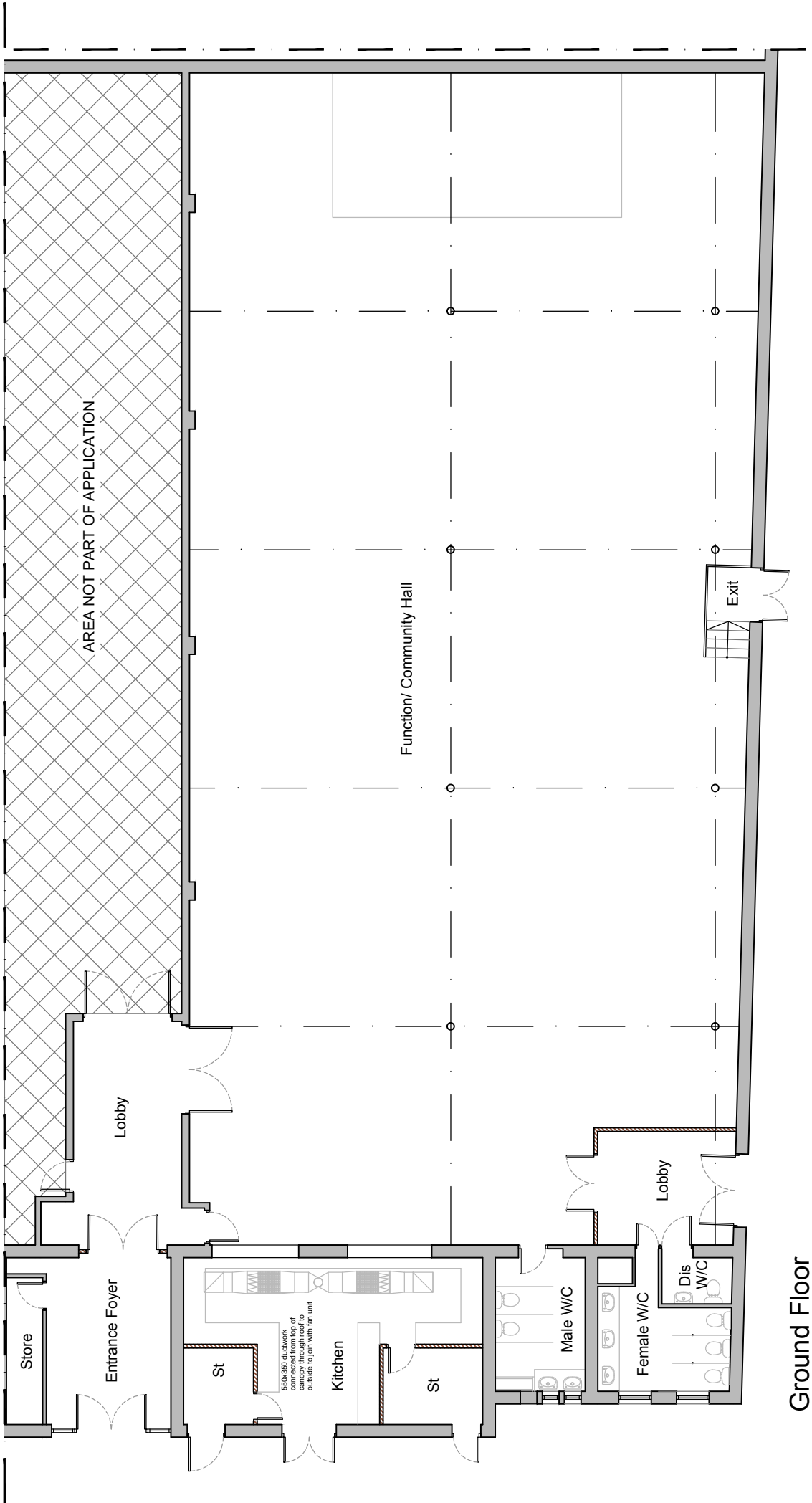
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Project Location:
St Pauls Mill
Barbara St
Bolton
BL3 6UQ

Drawing:

02 Existing Elevations

Scale:	Size:	Date:
1:100	A3	08/15



Ground Floor

NOTES:

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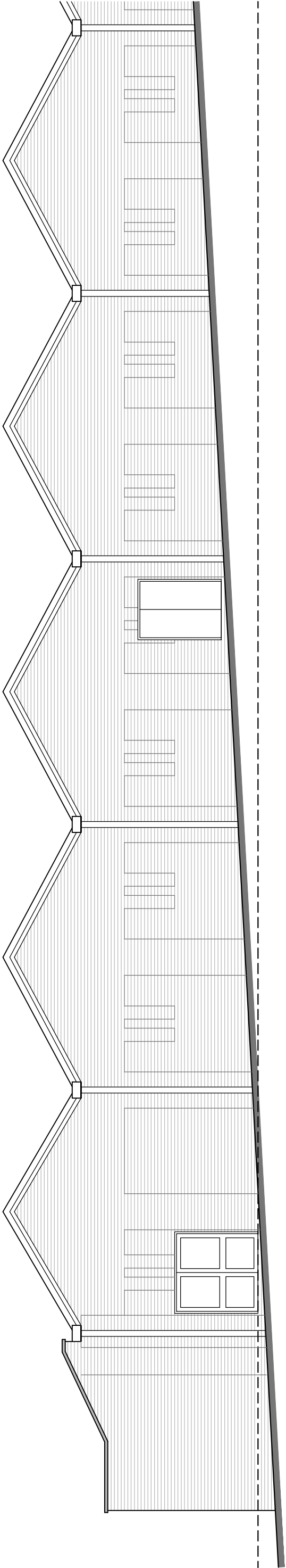
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Project Location:
St Pauls Mill
Barbara St
Bolton
BL3 6UQ

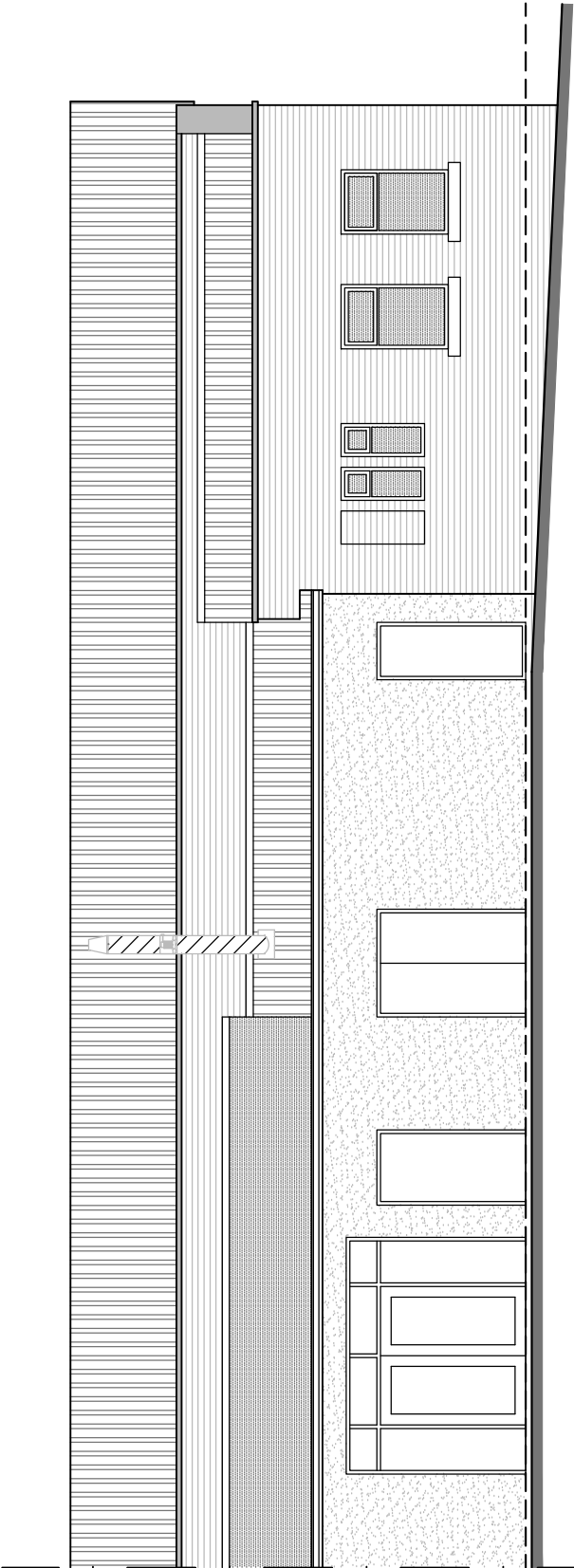
Drawing:

03 Proposed Plan

Scale: 1:150	Size: A3	Date: 08/15
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Front Elevation



Side Elevation

NOTES:

No scaling or dimensions other than those figured are to be taken from this drawing. All dimensions to be checked on site

This drawing is for use at the named location and for Planning Permission purposes only and as such should not be used at any other location or for any other purposes

Project Location:
St Pauls Mill
Barbara St
Bolton
BL3 6UQ

Drawing:

04 Proposed Elevations

Scale:	Size:	Date:
1:100	A3	08/15



NOTES:

No scaling or dimensions other than those figured are to be taken from this drawing. All dimensions to be checked on site

This drawing is for use at the named location and for Planning Permission purposes only and as such should not be used at any other location or for any other purposes

Project Location:

St Pauls Mill
Barbara St
Bolton
BL3 6UQ

Drawing:

05 Proposed Site

Scale:

1:500

Size:

A3

Date:

18/02

Site Plan

Application Area - 1,333 SqM/ 14,345 SqFt

Existing Building

Application Boundary

Land Under Client Ownership

Application number
95676/16



Development & Regeneration Dept
Development Management Section

Town Hall, Bolton, Lancashire, BL1 1RU
Telephone (01204) 333 333

Bolton
Council

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Date of Meeting: 30/06/2016

Application Reference: 95676/16

Type of Application: Conservation Area Consent
Registration Date: 25/01/2016
Decision Due By: 20/03/2016
Responsible Officer: Helen Williams

Location: ARMSTRONG AGGREGATES LTD, HORWICH RECYCLING CENTRE, CHORLEY NEW ROAD, HORWICH, BOLTON, BL6 5NJ

Proposal: DEMOLITION OF SIX BUILDINGS

Ward: Horwich and Blackrod

Applicant: Armstrong Aggregates Ltd
Agent : The Mineral Planning Group

Officers Report

Recommendation: Approve subject to conditions

Proposal

Permission is sought for the demolition of six buildings within the Horwich Locomotive Works Conservation Area. These buildings are as follows:

1. The long building to the north east of the site comprising the Riveting Tower & Boiler Shop Smithy, the Foundry and the Horwich Business Park building (as named within the Council's Conservation Area Management Plan for Horwich Locomotive Works and labelled buildings no.5 and no.9). The middle part of the building (labelled Horwich Business Park) is not classed as a significant building within the Conservation Area Management Plan and is therefore not given a specific building number.
2. The middle and south eastern part of the Small Workshops (as named within the Council's Conservation Area Management Plan for Horwich Locomotive Works and labelled building no.3). This building is to the south and west of the Riveting Tower and Boiler Shop Smithy.
3. The building to the south of those named in 1. and 2. above, comprising the Fitting and Machine Shop, Points & Crossings, Signal Shop and Welding Tube Shop (as named within the Council's Conservation Area Management Plan for Horwich Locomotive Works and labelled building no.7 and no.11).
4. The Smithy and Bolt Shop (as named within the Council's Conservation Area Management Plan for Horwich Locomotive Works and labelled building no.8), which is to the east of the site.
5. A building to the north of the Foundry (no.9) which is not named or labelled within the Council's Conservation Area Management Plan as it is not considered to be significant.
6. A building between the Fitting and Machine Shop, Points & Crossings, Signal Shop (no.7) and the Smithy and Bolton Shop (no.4) which is not named or labelled within the Council's Conservation Area Management Plan as it is not considered to be significant.

All of the six buildings have previously been granted planning permission for their demolition under

Part A of planning approval 91352/14 (the comprehensive redevelopment of Horwich Loco Works). The applicant is proposing the separate demolition of the six buildings within their ownership so they can, "...begin demolition, with immediate effect" and, "...begin the process of moving existing activities away from the Loco Works site".

The archaeological investigation and recording required (by condition 23 on 91352/14) prior to the demolition of the buildings on the site would only need cover the six buildings applied for in this application (owned by the applicant) rather than all the buildings approved for demolition, therefore meaning that the applicant could start their demolition sooner (and move their operations away from the site sooner).

Site Characteristics

The buildings subject to this planning application are located within Horwich Recycling Centre and within Horwich Locomotive Works Conservation Area. The principle of demolishing these buildings has been established through the approval of the comprehensive redevelopment of the Loco Works site (the Rivington Chase development) under planning approval 91352/14.

The buildings subject to the proposed demolition include:

Building no.3: Small Workshops

A low, single storey, narrow line of workshops, built in 1887. The building retains the original cast-iron windows. Some of its roof has been re-covered in metal sheeting.

Building no.5: Riveting Tower & Boiler Shop Smithy

The tall riveting tower, built in 1886, is a landmark on the site and is a triple-ridged building. The former boiler shops to the north west have previously been demolished, exposing the internal structure of the riveting tower, which is now at risk. The former boiler shop smithy adjacent to the south east is a double-ridged structure with slate roofs and ridge ventilation.

Building no.7: Fitting and Machine Shop, Points and Crossings and Signal Shop

A low, triple-ridged workshop built in 1887, which is relatively intact.

Building no.8: Smithy and Bolt Shop

A late 1880s building with double-ridged slate and glazed roof. The building was historically divided into two parts, with the larger part (identified by the truncated chimney stacks above the eaves) originally containing over forty hearths. The building is now used for recycling and is in a very poor condition.

Building no.9: Foundry

The foundry was built in 1886 and is a large-scale structure dominating the south and east of the works. It is visible from the adjacent residential streets to the north. The foundry was converted to mechanisation in the 1950s when it expanded northwards to take in the former forge. The roofs are partly clad in sheet metal or asbestos.

Building no.11: Welding Tube Shop

Built at the turn of the 20th century this building has a single span slated roof and 7-bay elevations with original windows.

Policy

National Planning Policy Framework (NPPF)

Core Strategy Policies: CG3 The Built Environment; CG4 Compatible Uses; M1 and M2 Horwich Loco

Works; OA1 Horwich and Blackrod.

Horwich Locomotive Works Conservation Area Management Plan
SPD Former Horwich Loco Works

Analysis

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- * impact on the significance of Horwich Locomotive Works Conservation Area
- * impact on archaeology
- * impact on public and workers' health

Impact on the Significance of Horwich Locomotive Works Conservation Area

Section 12 of the National Planning Policy Framework (NPPF) concerns conserving and enhancing the historic environment. Paragraph 132 states that, when considering the impact of a proposed development on the significance of a designated heritage asset, great weight should be given to the asset's conservation. The more important the asset, the greater the weight should be. Significance can be harmed or lost through alteration or destruction of the heritage asset or development within its setting. As heritage assets are irreplaceable, any harm or loss should require clear and convincing justification. Paragraph 133 continues, where a proposed development will lead to substantial harm to or total loss of significance of a designated heritage asset, local planning authorities should refuse consent, unless it can be demonstrated that the substantial harm or loss is necessary to achieve substantial public benefits that outweigh that harm or loss, or all of the following apply:

- * the nature of the heritage asset prevents all reasonable uses of the site; and
- * no viable use of the heritage asset itself can be found in the medium term through appropriate marketing that will enable its conservation; and
- * conservation by grant-funding or some form of charitable or public ownership is demonstrably not possible; and
- * the harm or loss is outweighed by the benefit of bringing the site back into use.

Core Strategy Policy CG3.4 states that the Council will conserve and enhance the heritage significance of heritage assets and heritage areas, recognising the importance of sites, areas and buildings of archaeological, historical, cultural and architectural interest and their settings. Policy OA1.9 states that the Council will conserve and enhance the character of the existing landscape and physical environment, especially the conservation areas at Horwich town centre, Horwich Loco Works and Wallsuches.

The six buildings proposed for demolition were included in the approved "Demolition and Retention

Plan" (1010-312 Rev. J) under planning approval 91352/14 (the "Rivington Chase" approval) and shown for demolition. The six buildings are within the applicant's ownership and they consider that a separate approval for these buildings' demolition would allow them to, "...begin demolition, with immediate effect" and, "...begin the process of moving existing activities away from the Loco Works site".

The loss of the six buildings, along with the other buildings shown for demolition on the approved demolition and retention plan, was thoroughly assessed during the determination of planning application 91352/14 (the application for the comprehensive redevelopment of the Loco Works site). It was concluded within the determination of this application that the substantial public benefits associated with the redevelopment of the Loco Works site (including the major contribution to housing provision, employment provision, substantial financial investment into Horwich, new jobs, and commercial and leisure spaces for the local community) would outweigh the considerable weight given to the harm caused to the significance of the conservation area by the loss of the buildings.

The loss of the six buildings subject to this application have therefore been previously accepted, as has the loss of significance of Horwich Loco Works Conservation Area.

Impact on Archaeology

Core Strategy Policy CG3.4 states that the Council will conserve and enhance the heritage significance of heritage assets and heritage areas, recognising the importance of sites, areas and buildings of archaeological, historical, cultural and architectural interest and their settings.

During the determination of planning application 91352/14 (for the comprehensive redevelopment of the Loco Works site) Greater Manchester Archaeological Advisory Service (GMAAS) advised that further information and assessment was necessary to understand the archaeological significance of the locomotive works buildings and any pre-existing industrial archaeology. Therefore a condition (condition 23) was attached to the planning approval to ensure that this information and assessment are submitted prior to the demolition of any building, except those shown for immediate demolition on the approved demolition and retention plan.

The applicant has agreed to undertake the work required by GMAAS on the six buildings in their ownership (proposed for demolition in this latest application) and therefore a condition requiring site investigation and recording of these six buildings is proposed to be attached to this approval if granted.

Impact on Public and Workers' Health

Policy CG4 of the Core Strategy states that the Council will ensure that new development is compatible with surrounding land uses and occupiers and does not generate unacceptable nuisance, odours, fumes, noise or light pollution, nor cause detrimental impacts upon water, ground or air quality.

Asbestos is known to be contained within the buildings on the former Loco Works site, as well as within the ground. Whilst the managing and disposal of asbestos is a matter for the Health and Safety Executive (HSE) rather than Planning, the applicant has confirmed that prior to demolition all buildings will be inspected for the presence of asbestos, and should any be identified, this would be removed and disposed of by an appropriate licensed contractor (as confirmed within application 91352/14).

Conclusion

As discussed above, the loss of the buildings (and the loss of significance of Horwich Loco Works Conservation Area) has already been accepted by the approval of planning application 91352/14. The

requirement for the applicant to only undertake the archaeological investigations and recordings for the buildings within their ownership would enable them to demolish the buildings sooner and begin to move their operations off the site. Reassurances have been given by the applicant regarding the safe demolition of the buildings, which would however be a matter for the HSE rather than Planning. Members are therefore recommended to approve this application subject to the suggested conditions.

Representation and Consultation Annex

Representations

Letters:- An objection has been received from Horwich Heritage, which raised the following concerns:

- * Conservation Area Consent has not been applied for, so technically the application is not valid (*officer's comment: the requirement for conservation area consent was abolished by the government on 1st October 2013, when it instead became the requirement for applicants to submit a full planning application when proposing the demolition of a building in a conservation area. The submitted application is therefore correct as conservation area consent now longer exists*);
- * Despite claims that the demolition has been approved in principle, justification still has to be provided, which it has not been;
- * If the Council is minded to approve the demolition an historic record of the buildings and their former uses (plans and photos) needs to be provided; this has not been submitted (*officer's comment: as discussed above, officers are recommending that the condition requiring the submission of a report on a programme of archaeological works (condition 23 of planning approval 91352/14) be replicated onto this latest approval, to ensure that a sufficient historic record of the buildings is made prior to demolition. The applicant has agreed to this and are to commission Liverpool Museum Field Archaeology Unit to carry out this work*).

Horwich Town Council:- Objected to the originally submitted proposal for the demolition of three buildings at their meeting of 18th February 2016 on the grounds of safety concerns with the removal of asbestos. Further comments from town councillors regarding the amended proposal to demolish six buildings will be reported directly at the Committee meeting.

Consultations

Advice was sought from the following consultees: The Council's Conservation Adviser and Drainage Officers.

Planning History

Planning application 91352/14 was approved in September 2015 for:

Part A - Full planning application for site access, demolition of some and change of use of core heritage buildings for A1 retail and D2 leisure (both up to 2,500 sqm) plus A2, A3, A4, A5, B1, C1 and D1 uses including car parking, diversion to public rights of way, land remodelling/site remediation, together with related associated works.

Part B - Outline application for residential development (C3) up to 1,700 dwellings, A1 retail (up to 2,500 sqm), employment zone of B1 and B2 uses and up to 2,700 sqm of ancillary D1 uses, children's play area, recreational open space and landscaping together with ecological mitigation areas, attenuation features and new cycle/pedestrian connections (means of access details only).

Conservation area consent application 82720/09 for the partial demolition of the former Smithy and Bolt Shop was withdrawn by the applicant in November 2009.

Application 82447/09 for the change of use of Horwich Business Park (Armstrongs) from B2 industrial to a biomass combined heat and power plant (CHP) was refused in November 2009.

A certificate of lawfulness for the proposed operation of a renewable energy facility at Horwich Business Park was refused in November 2008 (80869/08).

Recommendation: Approve subject to conditions

Recommended Conditions and/or Reasons

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason

Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. No demolition hereby approved shall commence unless and until a programme of archaeological works have been undertaken in accordance with a Written Scheme of Investigation (WSI) which is to be submitted to and approved in writing by the Local Planning Authority. The WSI shall cover the following:

1. A phased programme and methodology of site investigation and recording to include:

- archaeological building survey;
- (where merited) elevation by geophysical survey;
- (where merited) elevation by evaluation trenching;
- (where merited) open area excavation;
- (where merited) environmental sampling/assessment/analysis.

2. A programme for post investigation assessment to include:

- analysis of the site investigation records and finds;
- production of a final report on the significance of the heritage assets represented.

3. Provision for publication and dissemination of the analysis and report on the site investigation.

4. Provision for archive deposition of the report, finds and records of the site investigation.

5. Nomination of a competent person or persons/organisation to undertake the works set out within the approved WSI.

Reason

In order to record and advance understanding of the significance of any heritage assets at the site and to make a record and publicly accessible archive in accordance with policy CG3.4 of Bolton's Core Strategy and National Planning Policy Framework Section 12.

3. The development hereby permitted shall be carried out in complete accordance with the following approved plans:

200/4 - Dwg. 1. V2; "Demolition"; dated Feb '16 and received 15th April 2016

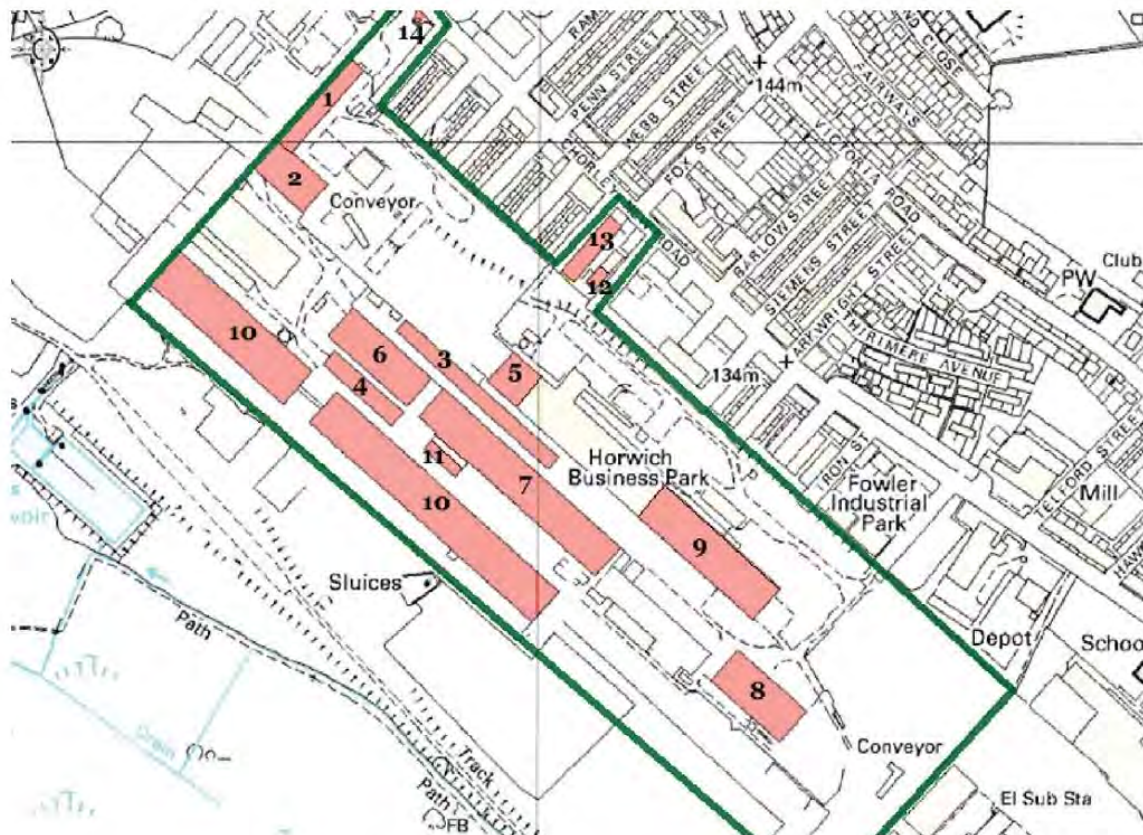
Reason

For the avoidance of doubt and in the interests of proper planning.

Site: Loco Works		Plan: Demolition	
Date: Feb' 16'	Drawn By: CH		
Ref: 200/4 - Dwg.1. V2			
<div>Key:</div> <div><div><div></div><div>To be Demolished</div></div><div><div></div><div>Currently being Demolished</div></div><div><div></div><div>Deliniation Between Buildings</div></div></div> <div>DRAFT</div>			
<div><div><div><div></div><div>The Mineral Planning Group</div><div>Fox Brow, Brow Lane,</div><div>Clayton, Bradford,</div><div>West Yorkshire , BD14 6PT</div><div>Tel: 01274 884599</div><div>Fax: 01274 884664</div><div>Mpgroup@mpg.yorks.com</div><div>www.mpg.yorks.com</div></div><div><div></div><div>The Mineral Planning Group 2015</div></div></div></div>			



Extract from the Council's Conservation Area Management Plan for Horwich Loco Works – Plan showing “significant buildings”



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Fig 2: Plan of the Conservation Area showing significant buildings (see section 3.1 for historic building names)

- | | | |
|---|---|--------------------------------------|
| 1. Rivington House | 6. Millwrights Shop & Pattern Makers | 10. Erecting & Repair Shop |
| 2. Stores | 7. Fitting & Machine Shop; Points & Crossings; Signal Shop | 11. Welding Tube Shop |
| 3. Small Workshops | 8. Smithy & Bolt Shop | 12. Cottage Hospital |
| 4. Heavy Machine Shop | 9. Foundry | 13. Dining Room, Gooch Street |
| 5. Riveting Tower & Boiler Shop Smithy | | 14. War Memorial |

Photographs of buildings subject to this application (taken from the Conservation Area Management Plan)

Building no. 3: Small Workshops



Building no. 5: Riveting Tower and Boiler Shop Smithy





Building no. 8: Smithy and Bolt Shop



Building no. 11: Welding Tube Shop



Application number 96250/16



Development & Regeneration Dept
Development Management Section

Town Hall, Bolton, Lancashire, BL1 1RU
Telephone (01204) 333 333

Bolton Council

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Date of Meeting: 30/06/2016

Application Reference: 96250/16

Type of Application: Advertisement Consent
Registration Date: 05/04/2016
Decision Due By: 30/05/2016
Responsible Officer: Kara Hamer

Location: 254 CHORLEY NEW ROAD, HORWICH, BOLTON, BL6 5NP

Proposal: RETENTION OF SIGN ON BUILDING

Ward: Horwich and Blackrod

Applicant: RPC Letting Agents

Agent :

Officers Report

Recommendation: Approve subject to conditions

Proposal

This application seeks to obtain retrospective advertisement consent to erect 1 no. fascia sign which would attach to the side elevation of the premises.

1 fascia sign (non illuminated) - 2.44m x 3.66m x 0.25m.

Site Characteristics

The application site lies to the south east of Horwich town centre to the southern side of Chorley New Road which is considered to be a main arterial route between Horwich and Bolton. The proposed advertisement site is at first floor level on the side gable elevation adjoining a single storey end terrace (no. 256) and is sited at the junction of Chorley New Road and Gooch Street. The single storey end terrace was previously a bathroom showroom and has since been vacant and fallen into disrepair. The premises operates as a Lettings Agent and forms part of a parade of shops and commercial premises along the main road frontage with residential and employment uses located in the surrounding area. To the rear the site faces the gables of a row of residential terraced properties on Gooch Street. The adjoining property at no. 252 is a sandwich shop.

Policy

National Planning Policy Framework

Core Strategy Policy CG3 The Built Environment; OA1 Horwich and Blackrod.

SPD General Design Principles

Analysis

The Town and Country Planning (Control of Advertisements) Regulations 2007 permit Local Planning Authorities to exercise control over advertisements in the interests of visual amenity and

highway safety.

The National Planning Policy Framework recognises that poorly placed advertisements can have a negative impact on the appearance of the built and natural environment. Control over outdoor advertisements should be efficient, effective and simple in concept and operation. Only those advertisements which will clearly have an appreciable impact on a building or on their surroundings should be subject to the local planning authority's detailed assessment. Advertisements should be subject to control only in the interests of amenity and public safety, taking account of cumulative impacts.

The main impact of the proposal is:-

* impact on the character and appearance of the area/impact on highway safety.

Impact on the character and appearance of the area

Bolton Core Strategy Policy CG3 seeks to require development to be compatible with the surrounding area.

The site is part of a corner plot sited at the junction of Chorley New Road and Gooch Street. The site forms part of a parade of shops and commercial premises along the main road frontage with residential and employment uses located in the surrounding area.

The fascia sign will attach to the side (east) gable elevation of the premises at first floor level. The sign comprises of blue text on a white background. There is advertisement consent (95994/16) for signage on the currently vacant premises (no. 256) which has approval (95391/15) for change of use from A1 retail to an A5 hot food take away (Domino's) and at the site visit the Officer noted that works are underway. The proposed fascia sign is simple in design and in use of colour and is consistent with the style of others in the near vicinity and as such is not considered to detract from the overall visual appearance of the site or the character and appearance of the area.

The Council's Highway Engineers have been consulted and have raised no objection.

Conclusion

It is therefore considered that the advertisement fascia sign complies with the aforementioned policies. It is recommended that advertisement consent be granted for the proposed sign.

Representation and Consultation Annex

Representations

Letters:- none.

Horwich Town Council:- raised objections at their meeting of 19/05/16 on the grounds of size of the signage and the sign being a distraction to drivers.

Elected Members:- no comments received.

Consultations

Advice was sought from the following consultees: The Council's Highways Management Engineers.

Planning History

87917/12 - Display of one banner on gable wall - Refused June 2015.

Recommendation: **Approve subject to conditions**

Recommended Conditions and/or Reasons

1. 1. No advertisement is to be displayed without the permission of the owner of the site or any other person with an interest in the site entitled to grant permission.
2. No advertisement shall be sited or displayed so as to—
 - a) endanger persons using any highway, railway, waterway, dock, harbour or aerodrome (civil or military);
 - (b) obscure, or hinder the ready interpretation of, any traffic sign, railway signal or aid to navigation by water or air; or
 - (c) hinder the operation of any device used for the purpose of security or surveillance or for measuring the speed of any vehicle.
3. Any advertisement displayed, and any site used for the display of advertisements, shall be maintained in a condition that does not impair the visual amenity of the site.
4. Any structure or hoarding erected or used principally for the purpose of displaying advertisements shall be maintained in a condition that does not endanger the public.
5. Where an advertisement is required under these Regulations to be removed, the site shall be left in a condition that does not endanger the public or impair visual amenity.

Reason

Required to be imposed pursuant to Regulation 14 of the Town and Country Planning (Control of Advertisements) (England) Regulations 2007.

2. The development hereby permitted shall be carried out in complete accordance with the following approved plans:

Sign detail (scanned to file 05/04/16)

Photo (scanned to file 05/04/16)

Site plan (scanned to file 05/04/16)

Reason

For the avoidance of doubt and in the interests of proper planning.

RPC Letting Agents

www.rpclettings.co.uk

**LANDLORDS
REQUIRED
Tenants Waiting
01204 699 277**

Overall Size Of Sign 3660mm Wide X 2440mm High
Consisting Of 3 x 1220 X 2440 Sheets of Aluminium Composit
fixed with screws into brown plastic plugs.
The tallest lettering is 350mm
The high²⁴t from ground level is approx.3.5 meters
although it situated above a ground floor unit.

Application number 96256/16



Development & Regeneration Dept
Development Management Section

Town Hall, Bolton, Lancashire, BL1 1RU
Telephone (01204) 333 333

Bolton Council

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Date of Meeting: 30/06/2016

Application Reference: 96256/16

Type of Application: Full Planning Application
Registration Date: 05/04/2016
Decision Due By: 04/07/2016
Responsible Officer: Helen Williams

Location: RUMWORTH SCHOOL, ARMADALE ROAD, BOLTON, BL3 4TP

Proposal: ERECTION OF EXTENSIONS TOGETHER WITH INTERNAL ALTERATIONS, NEW CLADDING TO EXISTING BUILDING, NEW CAR PARKING AREAS, NEW PLAYGROUND AREA, BIN STORE, RELOCATION OF EXISTING MUGA AND RELOCATION OF GARAGE AND STORE

Ward: Heaton and Lostock

Applicant: Bolton MBC Corporate Property Services
Agent : Good & Tillotson

Officers Report

Recommendation: Approve subject to conditions

Proposal

Permission is sought for the following alterations at Rumworth School:

- * Erection of a two storey extension to the north of the front part of the school to accommodate nine new classrooms (approximately 19 metres wide by 25 metres in length);
- * Erection of a part single, part two storey extension to the west of the front part of the school to accommodate a hall, small pool, changing rooms, kitchen and offices (approximately 32 metres in length by 25 metres in width);
- * Formation of a new car parking area to the front of the school (adjacent Armadale Road), compromising 35 spaces;
- * Formation of new playground area to the south west of the school building, which is also to be used a temporary parking area for mini buses;
- * A new relocated bin store between the new playground and the existing parking area to the side of the new western extension;
- * The relocation of the existing garage and store from the front of the site (adjacent the entrance on Armadale Road) to the southern corner of the proposed new playground;
- * The relocation of the existing MUGA (multi use games area) from the rear of the existing car park to the east of the proposed new playground;
- * Internal alterations to the existing school building to facilitate the rearrangement of rooms;
- * Re-cladding of the existing school building.

Site Characteristics

Rumworth School is a special school for 11 to 19 year olds. Ladybridge High School is attached to the

east and Bolton Music Centre is to the south. The school is accessed from Armadale Road to the north west of the site and there is a car park to the west of the school building.

There are currently six portable buildings sited on the car park area, which are providing temporary accommodation for the school whilst the school building is being repaired after storm/wind damage.

To the rear of the car park is playground and MUGA and beyond these are school fields.

To the north of the application site are the houses on Armadale Road and Crossford Drive, and to the west are the rears and rear gardens of the houses on Langside Drive.

Policy

National Planning Policy Framework (NPPF)

Core Strategy Policies: A1.1 Secondary Schools; P5 Transport and Accessibility; S1 Safe Bolton; CG1 Green Bolton; CG2 Sustainable Design and Construction; CG3 The Built Environment; CG4 Compatible Uses; OA4 West Bolton.

SPD General Design Principles

SPD Accessibility, Transport and Safety

Analysis

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- * impact on the character and appearance of the school building and the surrounding area
- * impact on the highway
- * impact on the amenity of neighbouring residents

Impact on the Character and Appearance of the School Building and the Surrounding Area

Policy CG3 of the Core Strategy states that the Council will conserve and enhance local distinctiveness, ensuring development has regard to the overall built character and landscape quality of the area, and will require development to be compatible with the surrounding area, in terms of scale, massing, grain, form, architecture, street enclosure, local materials and landscape treatment. Policy OA4 of the Core Strategy states that the Council will conserve and enhance the character of the existing physical environment, and will ensure that development in West Bolton has particular regard to the overall density, plot sizes, massing and materials of the surroundings – these features should be retained where possible.

The proposed extensions will significantly increase the footprint of the school, providing much

improved accommodation. The extensions, proposed at the front of the school and facing Armadale Road, would increase the school's visual presence when viewed from the main road. The extensions are to be contemporary in design, with the differing roof designs helping to break up the massing of the extended building. The existing school building is also proposed to be re-clad.

It is considered that the proposed school extensions are of good design, compliant with Policies CG3 and OA4 of the Core Strategy.

The proposed new car parking area, new playground and relocated MUGA will increase the hard surfaced area of the site. A landscape condition is therefore suggested to ensure that the remaining green spaces around the school are sufficiently treated.

It is therefore considered that the proposal complies with Policies CG3 and OA4 of the Core Strategy.

Impact on the Highway

Policy P5 of the Core Strategy states that the Council will ensure that developments take into account [amongst other things] servicing arrangements and parking, in accordance with the parking standards set out in Appendix 3. Policy S1.2 states that the Council will promote road safety in the design of new development.

To accommodate the proposed expansion of the school, a new car park comprising 35 parking spaces (including two disabled spaces) is proposed to the front of the site, along with 28 spaces within the existing car park area. The proposed new playground to the rear of the car park is also proposed as a temporary parking area for 14 school mini buses.

Vehicular access to the school is proposed to continue to be off Armadale Road, in the form of a one-way system. Following comments from the Council's Highways Engineers the applicant has illustrated (by the use of swept paths) that refuse vehicles would be able to access the area by the proposed bin store.

Highways Engineers have raised no objection to the proposed development and it is considered that the proposal would not jeopardise highway safety, compliant with Policies P5 and S1.2 of the Core Strategy.

Impact on the Amenity of Neighbouring Residents

Policy CG4 of the Core Strategy states that the Council will ensure that new development is compatible with surrounding land uses and occupiers, protecting amenity, privacy, safety and security, and does not generate unacceptable nuisance, odours, fumes, noise or light pollution. Section 3 of SPD General Design Principles sets out the Council's recommended interface distances between dwellings.

Proposed extensions

The part of the proposed extensions that is closest to neighbouring dwellings is the northernmost corner of the proposed two storey extension. This will be approximately 30 metres away from the windows at the fronts of 157 and 159 Armadale Road, which well exceeds the recommended interface distances prescribed within SPD General Design Principles.

The side of the part single, part two storey extension containing the hall will be some 60 metres away from the rears of the dwellings on Langside Drive, far exceeding the recommended interface distances within the SPD.

Proposed relocation of garage and store

The pebbledashed, dual pitched roof garage and store that is currently sited at the entrance to the school is proposed to be relocated to the southern corner of the proposed new playground/temporary mini bus parking area. This will be approximately 40 metres away from the rear of 5 Langside Drive, which far exceeds the recommended interface distance of 13.5 metres, as prescribed within the SPD.

Potential increase in noise and disturbance

The proposed extensions will facilitate additional pupils at the school. There will inevitably be additional traffic and noise generated at school opening and finishing times and during break times, however it is not considered that this would unduly harm the amenity of neighbouring residents given the hours of the day these would occur and the distances between the nearest neighbouring dwellings and the school.

For these reasons it is considered that the proposed development would not unduly harm the amenity of neighbouring residents, compliant with Policy CG4 of Bolton's Core Strategy.

Conclusion

For the reasons discussed above, it is considered that the proposed development would be compatible with the character and appearance of the area, would not jeopardise highway safety and would not unduly harm the amenity of neighbouring residents, compliant with policy. Members are therefore recommended to approve this application.

Representation and Consultation Annex

Representations

Letters:- One objection has been received from a neighbouring resident at Langside Drive, which raises the following concerns:

- * How is the swimming pool to be heated? Are there going to be externally mounted noisy heat pumps and where will these be?;
- * Where are the water treatment chemicals for the swimming pool going to be stored?;
- * What is the maximum permissible noise levels for the ventilation fans mounted on the roof? *(officer's comment to concerns above: a condition requesting further details of any plant for the pool and hall is suggested);*
- * Will the car parking areas be illuminated and will the hours of lighting be restricted? *(Officer's comment: an external lighting condition for the car park is suggested should any lighting be proposed);*
- * How will surface water run off be managed from the new car park area? Recent changes at the site have caused flooding within my garden *(Officer's comment: a condition requesting further details of how the car park and playground will be surfaced and drained is suggested);*
- * The trees planted around the perimeter of the site should be preserved as they provide a vital habitat for wildlife *(officer's comment: the proposed plans show that a number of trees will be lost as a result of the new playground, however a landscaping condition is suggested to replace any trees lost);*
- * The submission does not provide sufficient information with regard to the position of the garage in relation to their property *(officer's comment: the garage will be approximately 40 metres away from the rear of their property, which far exceeds the recommended interface distance (13.5 metres) within SPD General Design Principles);*
- * Will vehicles bring supplies to the new kitchen? Noise from reversing delivery vehicles, tail lifts and trolleys will be unreasonable in the early morning;
- * Noise and disturbance to neighbours from bin lorries due to the proposed location of the bin store. This could be avoided if the bins were left in their current location at the front of the site *(officer's comment to the two concerns about noise: the potential impact on the amenity of neighbouring residents is considered in the analysis above);*
- * The radii at the entrance and exit from Armadale Road are too tight for large vehicles such as bin lorries and delivery vehicles. This will result in vehicles having to reverse through the exit to gain access to the rear car park *(officer's comment: an amended plan has been submitted to show that refuse vehicles can enter and leave the site).*

Consultations

Advice was sought from the following consultees: Highways Engineers, Pollution Control Officers, Landscape Officers and Asset Management Officers (Children's Services).

Planning History

Application 96380/16 for the siting of six portable cabins to provide temporary classroom space following storm damage to the school was under determination at the time of writing this officer's report.

A single storey extension to the music room was granted permission in March 2009 (81504/09).

A non-illuminated sign was approved in October 2008 (80704/08).

Permission was granted in September 2000 for the erection of a two storey extension at Deane School, new roofs and the refurbishment of elevations to the school buildings (57483/00).

Recommendation: Approve subject to conditions

Recommended Conditions and/or Reasons

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason

Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. Prior to the commencement of the development hereby approved details of an "In", "Out", "No Exit" and "No Entry" signing scheme within the site shall be submitted to and approved by the Local Planning Authority. The approved signing scheme shall be implemented in full before the development hereby approved is first occupied or brought into use and retained thereafter.

Reason

To safeguard highway safety on Armadale Road and to comply with policy S1.2 of Bolton's Core Strategy.

3. Before the development hereby approved is first brought into use a scheme shall be submitted to and approved in writing with the Local Planning Authority for external lighting/floodlighting. The lighting shall be designed to an illumination value of 5 lux at the nearest residential property. The approved scheme shall be implemented in full before the development is first brought into use and retained thereafter.

Reason

To safeguard the character and appearance of the locality and to prevent light pollution and in order to comply with policy CG4 of Bolton's Core Strategy.

4. Prior to the development hereby approved/permitted being first brought into use a visibility splay measuring 2.4 metres by 43 metres shall be provided at the junction of the school exit with Armadale Road, and subsequently remain free of all obstructions between the height of 1.05 metres and 2 metres (as measured above carriageway level).

Reason

To ensure traffic leaving the site has adequate visibility onto the highway and to comply with policy S.1 of Bolton's Core Strategy.

5. Before the approved/permitted development is first brought into use no less than 35 car parking spaces with minimum dimensions of 2.5 metres by 5 metres shall be marked out and provided within the proposed new car parking area, in accordance with drawing ref: 1586 05 Rev. J. Such spaces shall be made available for the parking of cars at all times the premises are in use.

Reason

To ensure that adequate provision is made for vehicles to be left clear of the highway and to comply with policy P5 and Appendix 3 of Bolton's Core Strategy.

6. Before the approved/permitted development is first brought into use no less than 28 car parking spaces with minimum dimensions of 2.4 metres by 4.8 metres shall be marked out and provided within the existing car parking area, in accordance with drawing ref: 1586 05 Rev. J. Such spaces shall be made available for the parking of cars at all times the premises are in use.

Reason

To ensure that adequate provision is made for vehicles to be left clear of the highway and to comply with policy P5 and Appendix 3 of Bolton's Core Strategy.

7. Prior to the development hereby approved being first occupied or brought into use, a scheme detailing how parts of the site to be used by vehicles (including the playground) are to be laid out, constructed, surfaced and drained shall be submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be implemented in full and thereafter made available for the use of vehicles at all times the development is in use.

Reason

To encourage drivers to make use of the parking and circulation area(s) provided and to ensure adequate drainage of these areas, compliant with policies P5 and CG1.5 of the Core Strategy.

8. Trees and shrubs shall be planted on the site in accordance with a landscape scheme to be submitted and approved in writing by the Local Planning Authority prior to the development being first brought into use. The approved scheme shall be implemented in full and carried out within 6 months of the occupation of any of the buildings or the completion of the development, whichever is the sooner, or in accordance with phasing details included as part of the scheme and subsequently approved by the Local Planning Authority. Any trees and shrubs that die or are removed within five years of planting shall be replaced in the next available planting season with others of similar size and species.

Reason

To reflect and soften the setting of the development within the landscape and to comply with policy CG3 of Bolton's Core Strategy.

9. Before the development is first brought into use a detailed scheme shall be submitted to and approved by the Local Planning Authority showing the design and size of the proposed bin store. The approved scheme shall be implemented in full prior to first occupation and retained thereafter.

Reason

As only the location of the bin store has been submitted with the application, and to comply with policy CG3 of Bolton's Core Strategy.

10. Prior to the installation of any plant or ventilation in association with the pool and the hall full details of the plant and ventilation (including proposed rating levels) shall be submitted to and approved in writing by the local planning authority before it is first installed. The plant shall be installed fully in accordance with the approved details and retained thereafter.

Reason

To minimise the impact of noise on the general and residential amenity in the area and to comply with policy CG4 of Bolton's Core Strategy.

11. The development hereby permitted shall be carried out in complete accordance with the following approved plans:

1586 05 Rev. J; "Site Scheme Plan on Topographical Survey"; dated 24.05.16
1586 06 Rev. K; "Proposed Scheme Plan Ground Floor"; dated October 2015 (original date)
1586 07 Rev. J; "Proposed Scheme Plan First Floor"; dated October 2015 (original date)
1586 16; "Proposed Elevations"; dated 01.04.16

Reason

For the avoidance of doubt and in the interests of proper planning.

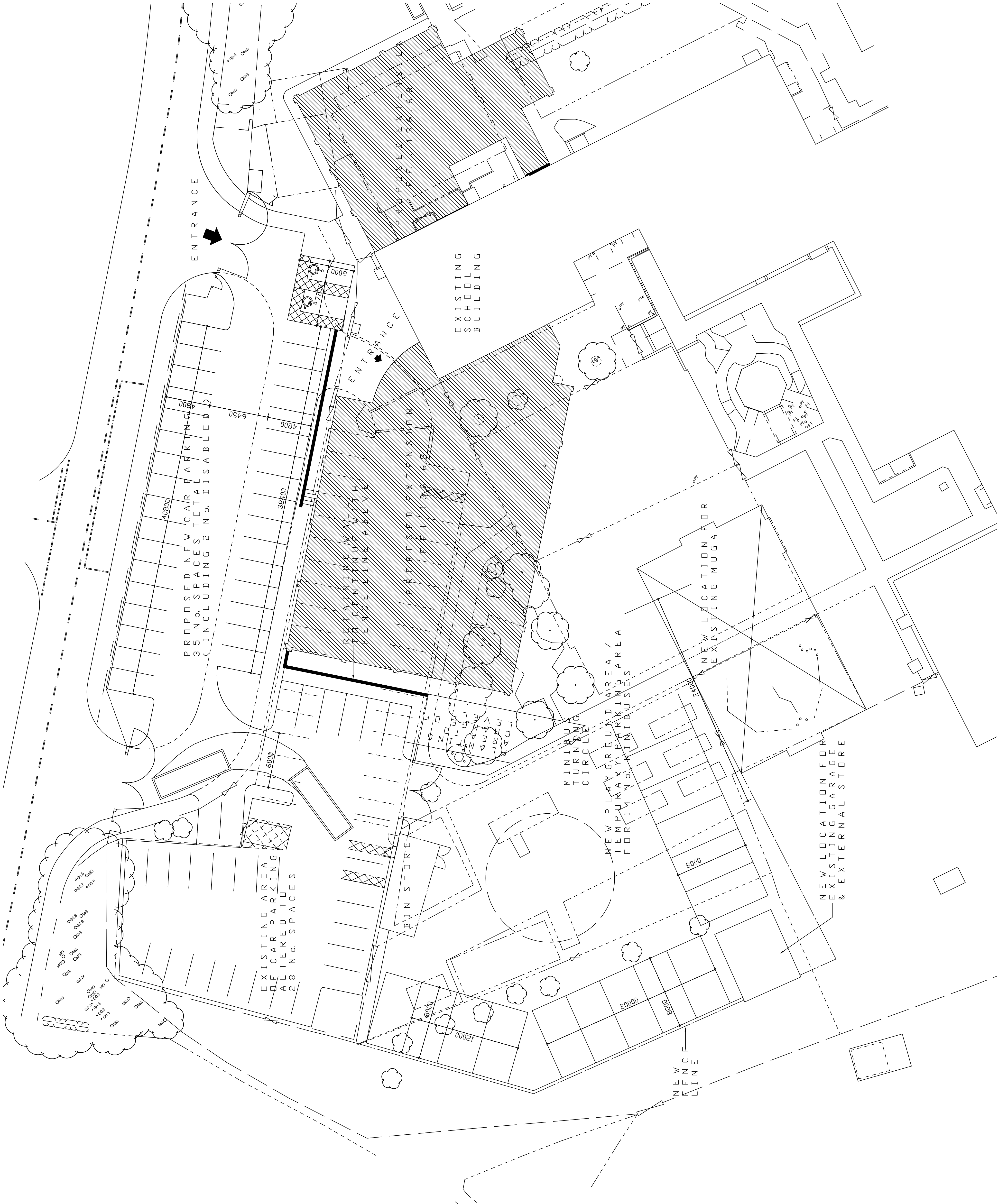
NOTES:
Copies of all documents prepared by the architect and any works executed from these documents and drawings, shall unless otherwise agreed, remain the property of the architect and must not be reproduced by, lent or disclosed to, a third party without the written consent of Good and Tillotson.

DO NOT SCALE OFF THIS DRAWING.
All dimensions to be checked on site and any discrepancies to be referred to the architect before proceeding.

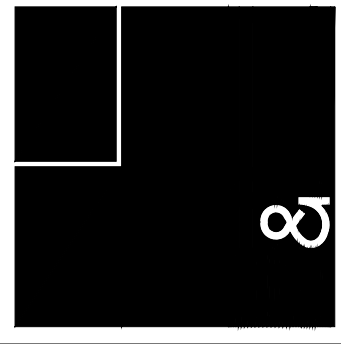
ALL LEVELS TO BE CHECKED ON SITE.
All discrepancies between information shown on the drawings and the information in the specification to be referred to the architect before proceeding.

All component sizes and references to be checked prior to the ordering of materials.
Positions and designation (i.e. combined, surface water or foul) of existing drain runs on site are to be checked / confirmed by the building contractor prior to commencement of any drainage works. Any existing drains, to which additional connections are to be made, are also to be checked / confirmed by the building contractor. All connections are to be made in accordance with the type of effluent being connected.

All relevant boundary positions are to be checked prior to proceeding.



J	Bin store relocated and Bin Lorry Tracking shown	M.R.P.	24.05.16
H	External steps to front of building repositioned	M.D.J.	04.04.16
G	Existing MUGA relocated	M.D.J.	11.02.16
F	Car parking and site layout amended	M.D.J.	05.02.16
E	Building areas reduced in line with G.A.	P.J.S.	17.12.15
D	Drawing updated following meeting with the client on 10.12.2015	MIL	15.12.15
C	Building areas reduced in line with G.A. 1 no. site entrance omitted	P.J.S.	
B	Drawing updated (Kitchen area) following meeting with the client on 25.11.2015	MIL	01.12.15
A	Drawing updated following meeting with client on 12.11.2015	MIL	17.11.15
Rev	Revision	Incl.	Date



Good & Tillotson Chartered Architects
2 The Studios,
35-37 Chorley Old Road,
Bolton,
BL1 4LU
Tel: 01204 497960
Fax: 01204 497776
Email: info@goodandtillotson.co.uk
Web: www.goodandtillotson.co.uk

Client: Bolton MBC


Project: Proposed Extension and Alterations at Rumworth School
Deane
Bolton

Drawing: Site Scheme Plan on Topographical Survey

Project No: **1586**

Drawing No: **05**

Drawing Revision: **J**

RIBA 
Chartered Practice

Date: October 2015
Drawn: Proposed by G&T
Scale: 1:200 @ A1

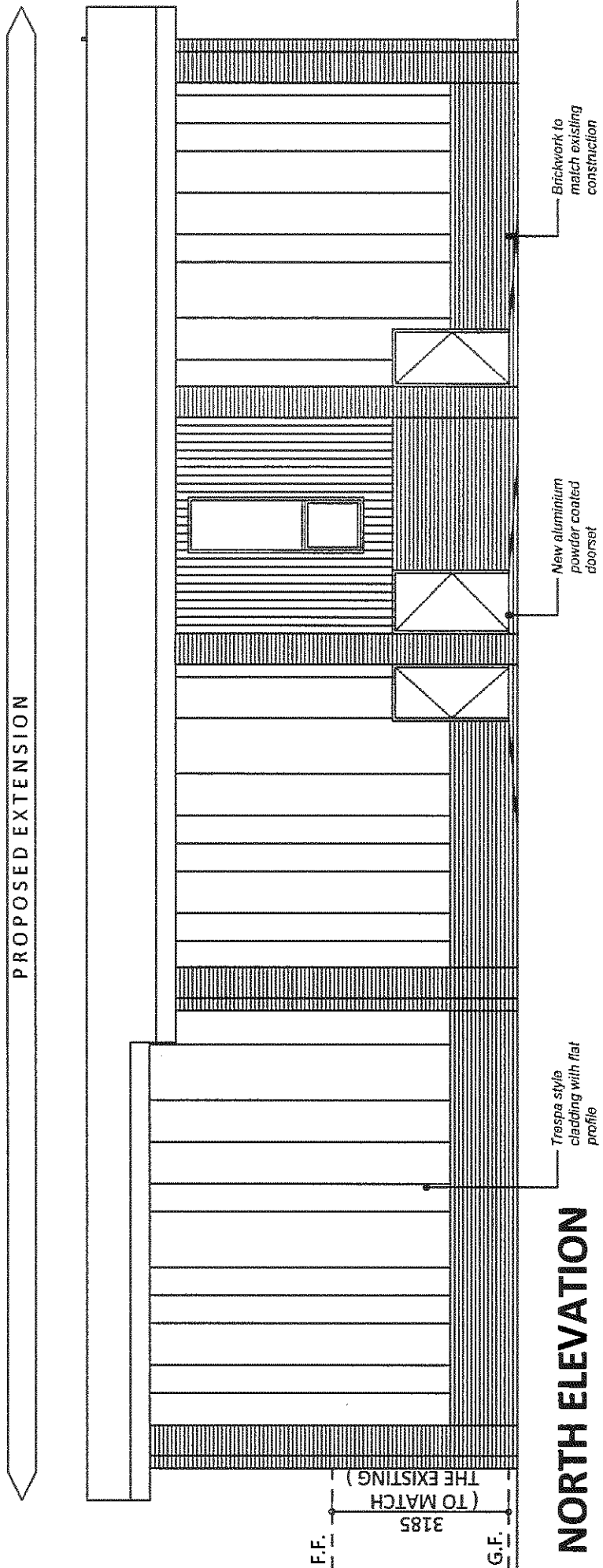
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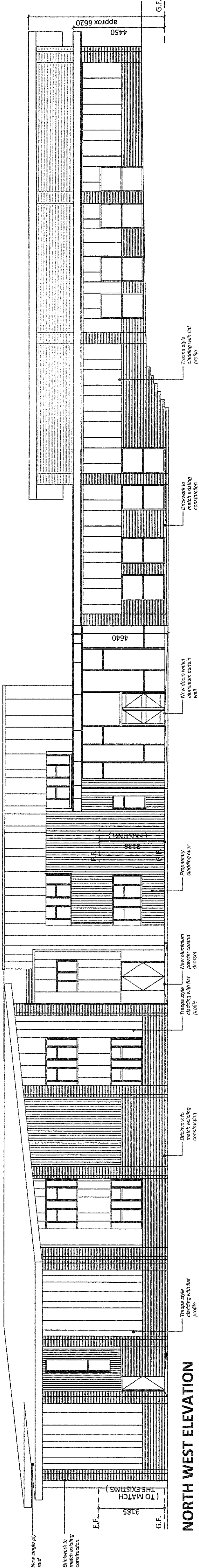
All component sizes and references to be checked prior to the ordering of materials.
Positions and designation (i.e. combined, surface water or foul) of existing drain runs on site are to be checked / confirmed by the building contractor prior to commencement of any drainage works. The contractor shall be responsible for ensuring that any new drainage is installed in accordance with the relevant standards and is suitable to accommodate the type of effluent being connected.

All relevant boundary positions are to be checked prior to proceeding.

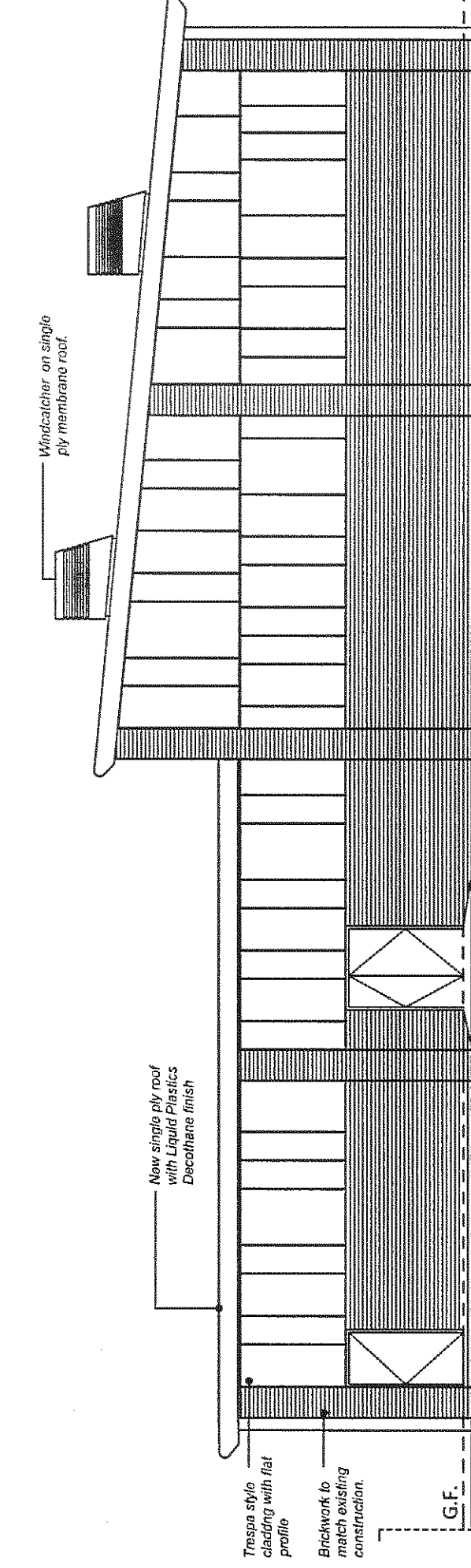


EXISTING WITH NEW FINISHES

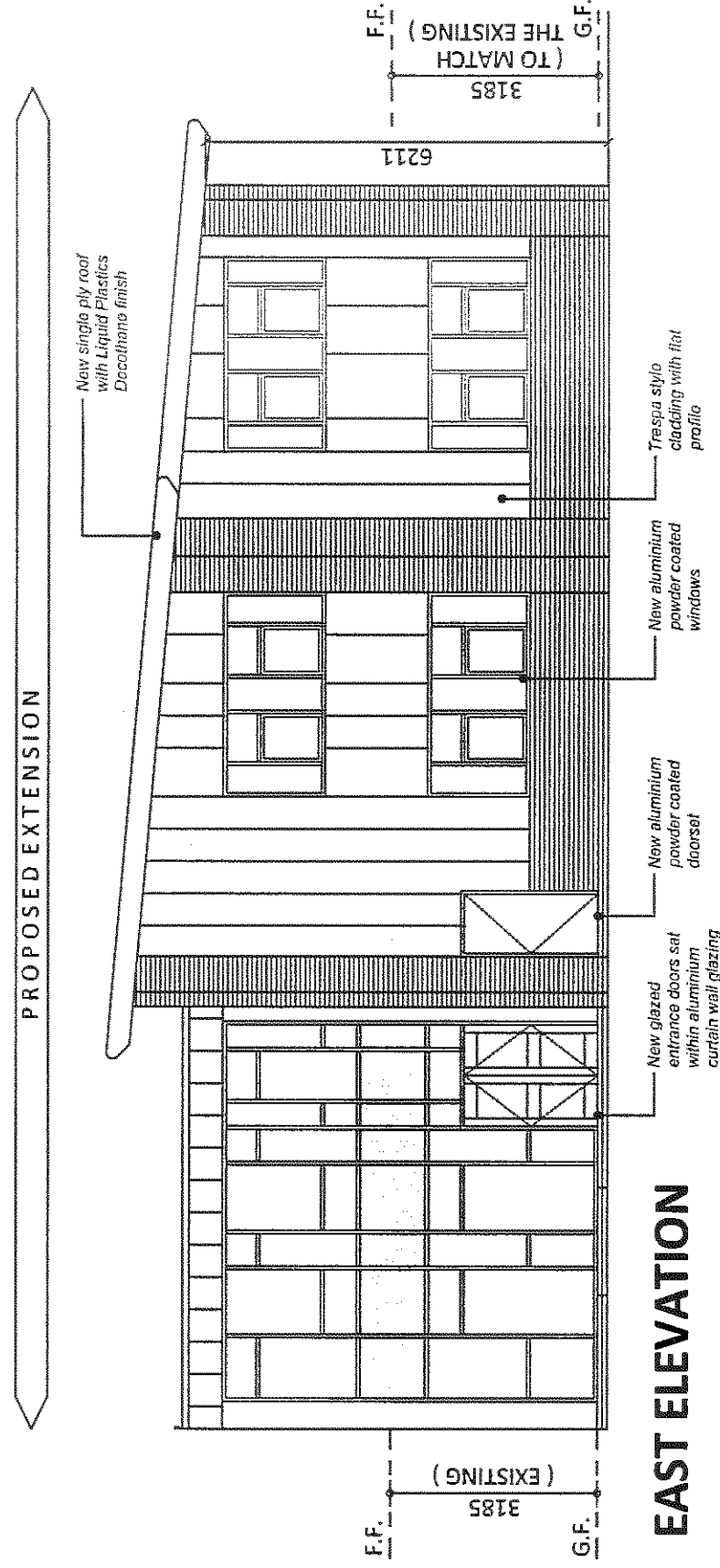
PROPOSED EXTENSION



NORTH WEST ELEVATION



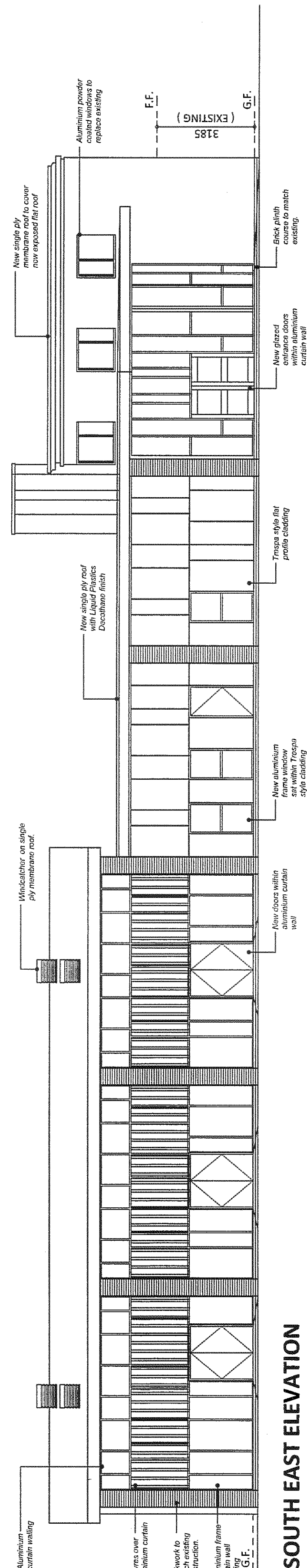
SOUTH WEST ELEVATION



EAST ELEVATION

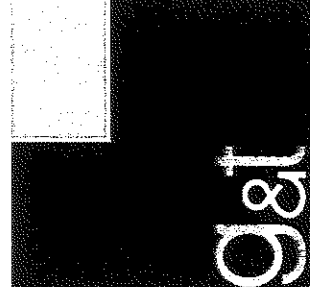
EXISTING WITH NEW FINISHES

PROPOSED EXTENSION



SOUTH EAST ELEVATION

05 APR 2015
96256/16



Good & Tiltson Chartered Architects
2 The Studios,
318 Chorley Old Road,
Bolton,
BL4 4UJ
Tel: 01204 497700
Fax: 01204 497778
Email: info@goodandtiltson.co.uk
Web: www.goodandtiltson.co.uk

Client

Project:
Proposed Extension and Alterations
Rumworth School
Deane
Bolton

Drawing:
Proposed Elevations
Planning

Client:
RIBA
Chartered Practice

Date:
01/04/16
Drawn:
MP
Scale:
1:100 @ A1
Drawing Revisions:
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Rev

Revision

Inl

Date

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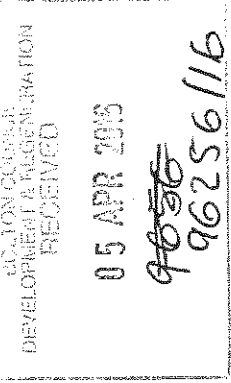
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All discrepancies between information shown on the drawings and the information in the specification to be referred to the architect prior to proceeding.

All component sizes and references to be checked prior to ordering of materials.


Positions and designation (i.e. combined, surface water or foul) of existing drain runs on site are to be checked / confirmed by the existing contractor prior to commencement of any drainage works. Any existing drains, to which additional connections are to be made, are also to be checked as necessary to ensure that they are suitable to accommodate additional capacity and the type of effluent being connected.

All relevant boundary positions to be checked prior to proceeding.



Rev.	Revision	Inl.	Date
E	Drawing updated following meeting with the client on 10.12.2015	MJL	15.12.15
D	Amended to suit M&E requirements	P.J.S.	08.12.15
C	Floor area reduced	P.J.S.	03.12.15
B	Drawing updated Kitchen area following meeting with the client on 28.11.2015	MJL	30.11.15
A	Drawing updated following meeting with the client on 12.11.2015	MJL	17.11.15

Good & Tilletson Chartered Architects
2 The Studios,
318 Chorley Old Road,
Bolton,
BL1 4JU
Tel : 01204 497700
Fax : 01204 497776
E-mail : info@goodandtilletson.co.uk
Web : www.goodandtilletson.co.uk

Client :	Bolton MBC	
Project :	Proposed Extension and Alterations at : Runwirth School Deane Bolton	Drawing : PROPOSED SCHEME PLAN GROUND FLOOR PLANNING DRAWING
	RIBA  Chartered Practice	Date : October 2015 Drawn : proposed by G&T Scale : 1 : 100 @ A1
Project No. :	1586	Drawing Number : 06 Drawing Revision : K

K	Drawing updated following client comments on 28.01.16	M.D.J.	29.01.16
J	Drawing updated following client comments on 20.01.16	K.C.	27.01.16
H	Drawing updated following client comments on 04.01.16	K.C.	05.01.16
G	Drawing updated following meeting with the client on 22.12.2015	M.J.L / P.J.S.	23.12.15
F	Floor area reduced	P.J.S.	17.12.15

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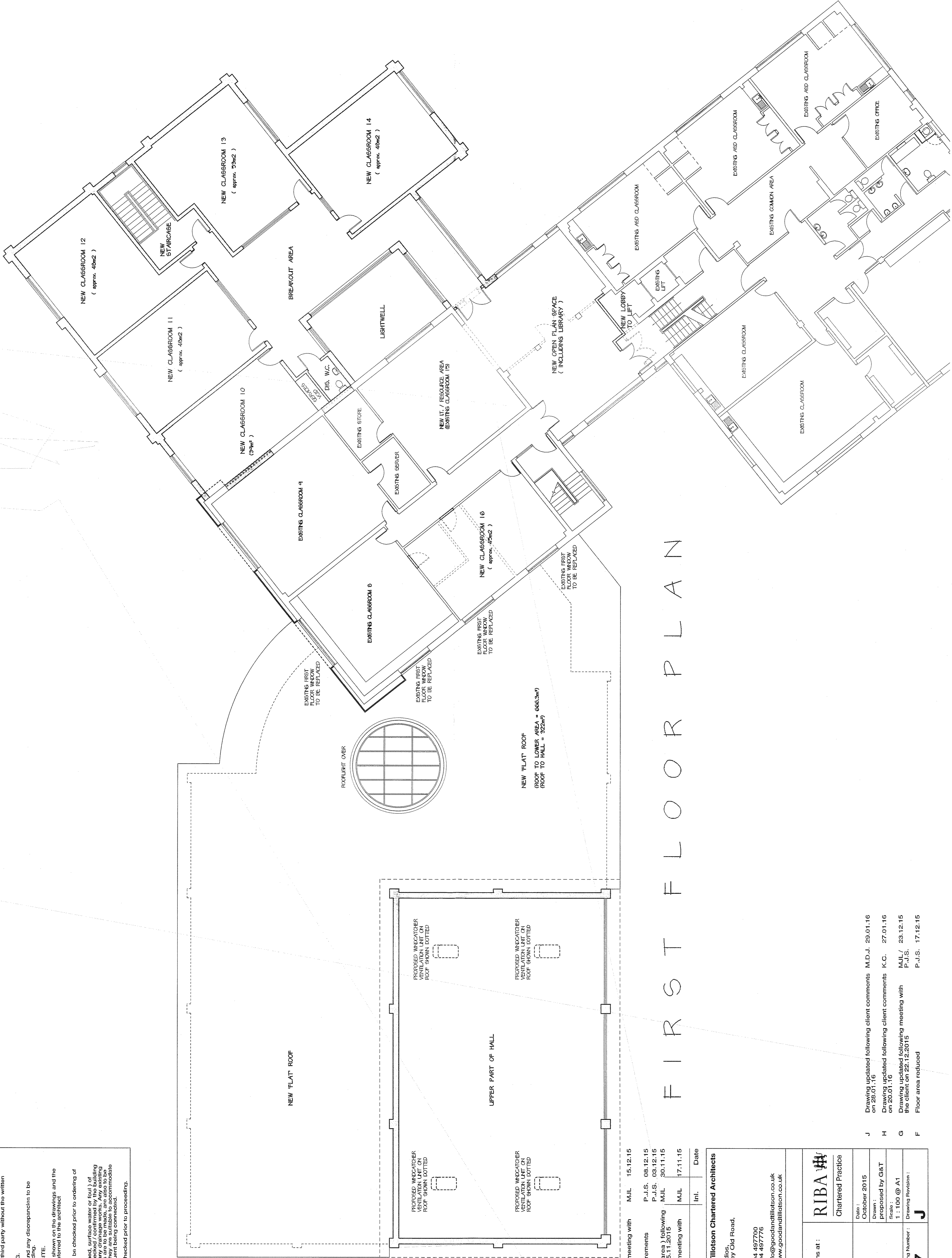
ALL LEVELS TO BE CHECKED ON SITE.

All discrepancies between information shown on the drawings and the information on site to be referred to the architect prior to proceeding.

All component sizes and references to be checked prior to ordering of materials.

Positions and designation (i.e. combined, surface water or foul) of existing drain runs on site are to be checked / confirmed by the building contractor prior to commencement of any drainage works. Any existing drains to be removed or replaced shall be checked and confirmed by the building contractor as necessary to ensure that they are suitable to accommodate additional capacity and the type of effluent being connected.

All relevant boundary positions to be checked prior to proceeding.



FIRST FLOOR PLAN

Rev.	Revision	Inl.	Date
E	Drawing updated following meeting with the client on 10.12.2015	MJL	15.12.15
D	Amended to suit M&E requirements	P.J.S.	08.12.15
C	Floor area reduced	P.J.S.	03.12.15
B	Drawing updated (Kitchen area) following meeting with the client on 25.11.2015	MJL	30.11.15
A	Drawing updated following meeting with the client on 12.11.2015	MJL	17.11.15



Good & Tillotson Chartered Architects
2 The Studios,
219 Churchway Old Road,
Bolton,
BL1 4JU
Tel : 01204 497709
Fax : 01204 497776
E-mail : info@goodandtillotson.co.uk
Web : www.goodandtillotson.co.uk

Client : Bolton MBC	
Project : Proposed Extension and Alterations at : Bolton North School Bolton	RIBA Chartered Practice
Drawing : PROPOSED SCHEME PLAN FIRST FLOOR PLANNING DRAWING	Date : October 2015 Drawn : proposed by G&T Scale : 1 : 100 @ A1
Project No. : 1586	Drawing Number : 07 Drawing Revision : J

J	Drawing updated following client comments on 28.01.16	M.D.J.	29.01.16
H	Drawing updated following client comments on 20.01.16	K.C.	27.01.16
G	Drawing updated following meeting with the client on 22.12.2015	MJL / P.J.S.	23.12.15
F	Floor area reduced	P.J.S.	17.12.15

RECEIVED
05 APR 2015
96256/16

Application number 96270/16



Development & Regeneration Dept
Development Management Section

Town Hall, Bolton, Lancashire, BL1 1RU
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Date of Meeting: 30/06/2016

Application Reference: 96270/16

Type of Application: Full Planning Application
Registration Date: 18/04/2016
Decision Due By: 12/06/2016
Responsible Officer: Kara Hamer

Location: LAND AT HETLANDS, MARKLAND HILL, BOLTON, BL1 5AF

Proposal: RETENTION AND ADAPTATION OF 2.4M HIGH BOUNDARY FENCE FOR A TEMPORARY PERIOD OF TWO YEARS.

Ward: Heaton and Lostock

Applicant: ICS Construction (Bolton) Limited
Agent : Frank Whittaker Town Planning Consultants

Officers Report

Recommendation: Approve subject to conditions

Proposal

Temporary permission is sought for a two year period for the retention and alteration of 2.4 m high boundary fencing (security hoarding).

The Applicant submits that the boundary fencing exceeds height restrictions in the General Permitted Development (England) Order 2015 by virtue of the site being in pre-construction stage and permission is therefore required. The Applicant submits that the boundary fence is appropriate and preferable to 'general' contractors fencing for the duration of the construction period.

Boundary security fencing to a construction site is Permitted Development once planning consent for development is granted. It is noted that Schedule 2, Part 2, Class A of the General Permitted Development (England) Order 2015 provides deemed consent for the erection of fencing – generally restricted to 2m high or 1m high where it abuts a public highway used by vehicular traffic.

Site Characteristics

This is a previously developed site within the residential area, which is bound by approx. 1.2m high stone wall to the boundary with Markland Hill to the west. To the north the site is bound by Springlawn, to the east the site is bound by residential properties accessed via Clarebank and to the south is Christ Church.

There are a number of TPO trees to the west, east and north boundaries.

Policy

National Planning Policy Framework (NPPF)

Core Strategy Policies: CG1 Cleaner Greener Bolton; CG3 The Built Environment; CG4 Compatible

Uses; OA4 West Bolton.

SPD General Design Principles 2015

Analysis

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- * impact on the design and character of the area and residential amenity
- * impact on protected trees

Impact on the Design and Character of the Area and Residential Amenity

The NPPF in paragraph 56 states that The Government attaches great importance to the design of the built environment. Good design is a key aspect of sustainable development, is indivisible from good planning, and should contribute positively to making places better for people.

Core Strategy policy CG3 seeks to ensure that development proposals contribute to good urban design, conserve and enhance local distinctiveness and have regard to the overall built character and landscape quality of the area. Furthermore, development should be compatible with the surrounding area, in terms of scale, massing, grain, form, architecture, street enclosure, local materials and landscape treatment.

Core Strategy policy CG4 seeks to ensure that development is compatible with surrounding land uses and occupiers, protecting amenity, privacy, safety and security, and does not generate unacceptable nuisance, odours, fumes, noise or light pollution.

Original submitted plans indicated that the proposed timber fencing to the Markland Hill frontage would be stepped in line with the gradient of the road and would comprise 3.660m sheathing boards, cut to the horizontal with horizontal planted boards to introduce relief detail and timber plants to simulate vertical posts. Amended plans have been submitted with revisions to the relief detail to reduce water ingress which could result in water damage. It is considered that the revised relief detail is simpler in design and is wholly acceptable. The fence is sited above the existing approx. 1.2m high stone wall and will be painted 'Dark Forest Green'.

The proposed timber fencing to the north, east and south boundaries (non main road frontages) will be retained as existing and painted 'Dark Forest Green'.

The applicant submits that the fencing is to protect the site during preparatory and development phases of a future proposed residential development after which the fencing will be replaced by a permanent boundary treatment which will be agreed and approved by the Local Planning Authority.

It is considered that the proposed temporary fencing will not cause undue harm to the amenity of nearby residents nor will generate unacceptable nuisance. The Applicant has worked with neighbouring residents and the LPA in proposing alterations to soften the appearance of the temporary boundary fencing against the backdrop of the cleared site and mature trees and vegetation.

Officers advise that the siting of the boundary fencing is granted two year temporary permission for the pre-construction stage of the future development of the site, with Conditions included in an approving Decision Notice to paint the fence and complete alterations within 60 days of approval being granted.

Subject to Conditions, the proposal is considered to comply with Core Strategy policy CG3 and CG4 of the Core Strategy.

Impact on Protected Trees

The NPPF states in paragraph 109 that the planning system should contribute to and enhance the natural and local environment by protecting and enhancing valued landscapes, geological conservation interests and soils.

Core Strategy Policy CG1.2 safeguards and enhances biodiversity in the borough by protecting sites of urban biodiversity including trees, woodland and hedgerows from adverse development, and improving the quality and inter connectivity of wildlife corridors and habitats.

The Council's Trees and Woodland Officer has raised no objections, and comments;

The location plan (P1712/16/01) for this fence is illustrated as being the line on which the existing board fencing is currently on; and that this does not require the removal or pruning of trees along this line. The vertical posts would not appear to be at the specified 3.66m centres (Ref: Drawing P1712/16/02) and I would therefore consider that to take the fence down and re-centre the posts may create damage within the root protection zones of the trees along this line. Therefore I would suggest that the fence is to be retained and that this is undertaken using the posts that are currently in position, with the height being reduced accordingly. As the fences on the other three sides of the site are already in place, it would seem appropriate that they are not moved/repositioned so to avoid any potential damage and disturbance within the root protection zones (RPZs) of the trees.

It is therefore considered that the proposal accords with Core Strategy Policy CG1.2.

Conclusion

It is considered that subject to Conditions, the proposal complies with aforementioned local and national planning policy as detailed above and is therefore recommended for approval.

Representation and Consultation Annex

Representations

Letters:- 8 letters have been received objecting to the proposal on the grounds of;

- The fence at its current height is an eyesore. In whatever colour the tall fence is visible from the church's Garden of Remembrance;
- The height at 2.4m is oppressive;
- We remind the authority that the entrance to Springlawns was the scene of a serious sexual assault relatively recently and is close to the Victoria Inn (Public Inn);
- The shade of Green suggested may be too dark;
- The Parochial Church Council of Christ Church Heaton object to this application. The fence has been erected without our consent or approval and partly on our land. The applicant has entered and removed shrubs and vegetation without consent. Similarly postholes have been dug next to the columbarium wall where ashes are interred. The dimensions of the fence are excessively high; the materials used of poor quality, unsightly and jarring of the surroundings, especially when viewed from the abutting Garden of Remembrance and columbarium wall by parishioners and relatives paying their respects. The fence detracts from the local amenity of Markland Hill. The detailed statement highlights problems of trespass at 3.2 (a) but these were exacerbated by the applicant's own tree felling at 3.1. and are now being used in support of this application. Although not the subject of this fencing application the plan PROPOSED_SITE_LAYOUT-563898.pdf shows the vehicular route of access for the proposed residential development. The access encroaches onto PCC land without agreement. The fence has been incorrectly sited to facilitate this vehicular access route.

Officer Comment - Christ Church and the Applicant have been advised by the LPA that the issues raised by the Church Council are civil matters and not material planning considerations.

Consultations

Advice was sought from the following consultees: The Council's Tree and Woodland Officers.

Planning History

Various tree applications.

Recommendation: **Approve subject to conditions**

Recommended Conditions and/or Reasons

1. This permission shall be for a temporary period expiring on 29 June 2018 when the site boundary fencing hereby approved shall be removed and the land reinstated in accordance with a scheme to be approved in writing by the Local Planning Authority before the expiry of this permission. The approved scheme shall be implemented in full and retained thereafter.

Reason

The applicant has only applied for temporary permission and to comply with policies CG3 of Bolton's Core Strategy.

2. Within 60 days of this Decision Notice being issued, the painting and alteration works to the hereby approved site boundary fencing shall be completed, in accordance with the approved details:
 - Specification detail, Drwg. No. P1712/16/02, April 2016 (scanned to file 08/04/16); and
 - Colour Sample, Dark Forest Green, P1712/16/05 (scanned to file 08/04/16).

The approved fence shall be retained until the expiry of the temporary consent.

Reason

To comply with policy CG3 of Bolton's Core Strategy.

3. The development hereby permitted shall be carried out in complete accordance with the following approved plans:

Location Plan (scanned to file 18/04/16)

Specification detail, Drwg. No. P1712/16/02, April 2016 (scanned to file 14/06/16)



Colour Sample, Dark Forest Green, P1712/16/05 (scanned to file 08/04/16)

Reason

For the avoidance of doubt and in the interests of proper planning.



Location and Site : P1712/16/01

-  **Markland Hill Frontage** — Existing Fence to be retained and adapted in accordance with Specification P1712/16/02 and Statement
-  **Non-Frontage Fencing** — To be retained without adaptation but spray stained (Dark Forest Green – see colour sample)

Revised : Red Edge



Specification :-


- 1 Retain Sheathing Board Fence
- 2 100x25mm horizontal edging
- 3 50x25mm Vertical plant (board ends)
- 4 Repeat in uniform groups of boards
- 5 Paint/stain all external faces of fence "Forest Green"

Footpath gradient to Markland Hill

PROPOSAL :- Retention / Adaptation of Timber Fencing
SITE Hetlands : Markland Hill Bolton

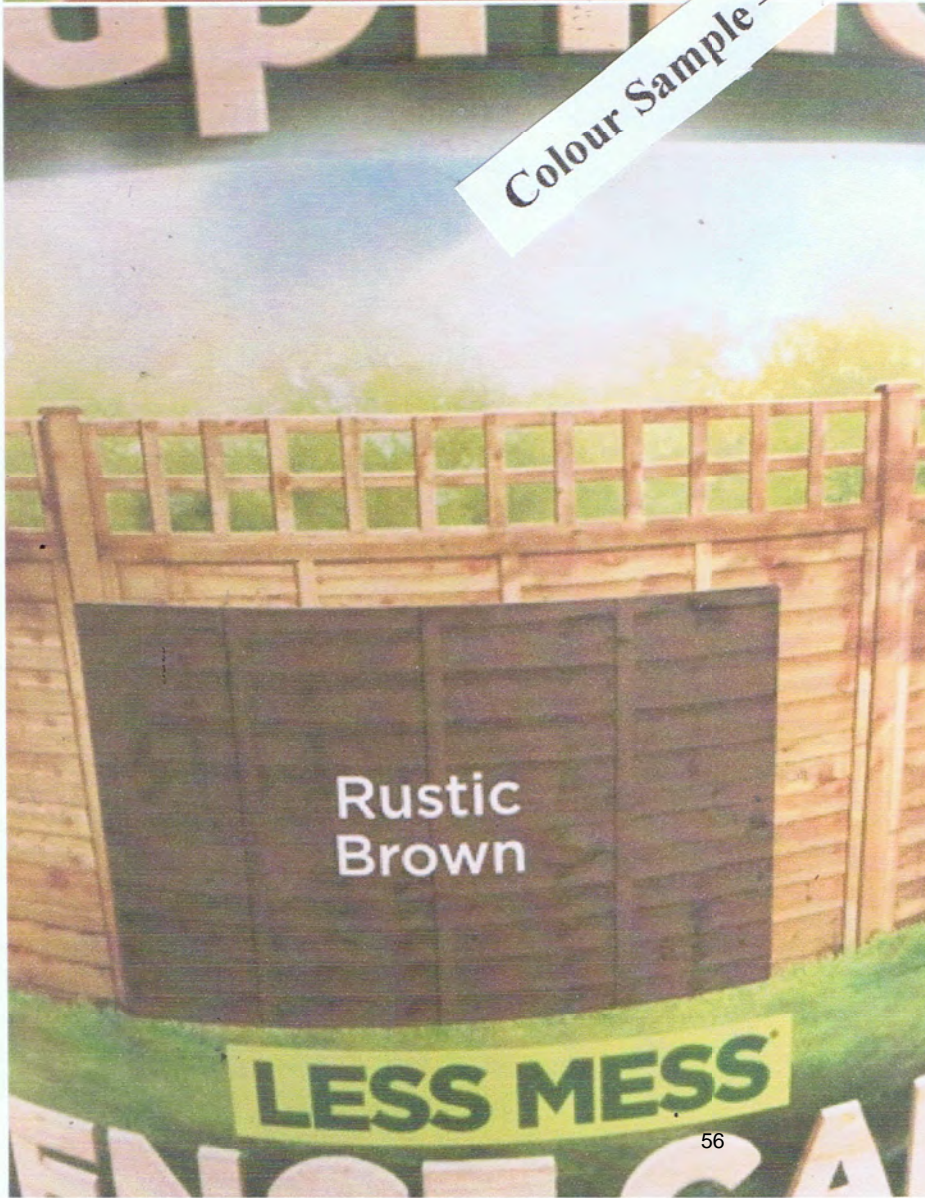
Scale 1:50
 Date April 2016

Frank Whittaker
 PLANNING CONSULTANTS



Forest
Green

Colour Sample – Dark Forest Green P1712/16/05



Rustic
Brown

LESS MESS

Application number 96323/16



Development & Regeneration Dept
Development Management Section

Town Hall, Bolton, Lancashire, BL1 1RU
Telephone (01204) 333 333

Bolton Council

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Date of Meeting: 30/06/2016

Application Reference: 96323/16

Type of Application: Full Planning Application
Registration Date: 13/04/2016
Decision Due By: 07/06/2016
Responsible Officer: Helen Williams

**Location: JUNCTION OF MAKINSON ROAD AND MATCHMOOR LANE,
HORWICH, BOLTON**

Proposal: ERECTION OF GATEHOUSE TO PILKINGTON QUARRY.

Ward: Horwich North East

Applicant: Armstrong Aggregates Ltd.

Agent : The Mineral Planning Group Ltd.

Officers Report

Recommendation: Approve subject to conditions

Proposal

Permission is sought for the erection of a gatehouse to Pilkington Quarry to the north of the junction of Makinson Road with Matchmoor Lane. The building is to be single storey and 9.5 metres in length by 5.9 metres in width. It is to be constructed from ashlar stone with a natural slate pitched roof. The building is to partly sit on the existing water pumping station and the ground to the north western side is to be built up underneath the proposed gatehouse, so that the building is level with Makinson Road at its rear (north eastern part) as Makinson Road rises steeply to the north east.

The gatehouse will comprise a two person control room, visitor seating, store, brew room and toilet. A four space car park is proposed to the north east of the building, which will be accessed off Makinson Road.

The applicant has stated that the purpose of the proposed gatehouse is to provide security for both Pilkington Quarry and the residents of Makinson Road, as the area is prone to both fly-tipping and anti-social activities. They state that there is also a risk of theft and damage to quarry equipment and materials.

When this application was submitted the plans showed a cantilever gate at the entrance to Makinson Road. This gate has now been deleted from the plans following concern from a neighbouring resident about access to their property.

Site Characteristics

The application site is land to the north of the junction of Makinson Road with Matchmoor Lane. Makinson Road is an unadopted highway and has been surfaced in concrete by the applicant (to support heavy vehicles). The road leads up to Pilkington Quarry.

Public Right of Way HOR057 runs the whole length of Makinson Road. 1 and 2 Grundy Cottages are sited to the north east of the application site, approximately 80 metres further up Makinson Road.

The application site rises steeply uphill to the north east. There is a small stone built 'plinth-like' structure to the front of the site which is a water pumping station for the quarry.

The application site is located within the Green Belt.

Policy

Planning Policy Framework (NPPF)

Core Strategy Policies: P4.3 Infrastructure for Mineral Exploitation; P5 Transport and Accessibility; S1 Safe Bolton; CG1 Green Bolton; CG3 The Built Environment; CG4 Compatible Uses; OA1 Horwich and Blackrod.

Allocation Plans Policies: P8AP Public Rights of Way; CG7AP Green Belt.

Analysis

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- * impact on the purposes and openness of the Green Belt
- * impact on the character and appearance of the area
- * impact on the highway
- * impact on the public right of way
- * impact on a culverted watercourse

Impact on the Purposes and Openness of the Green Belt

Section 9 of the National Planning Policy Framework (NPPF) concerns protecting Green Belt land. The Government attaches great importance to Green Belts. The fundamental aim of Green Belt policy is to prevent urban sprawl by keeping land permanently open; the essential characteristics of Green Belts are their openness and their permanence.

Paragraph 89 of the NPPF states a local planning authority should regard the construction of new buildings as inappropriate in Green Belt. The paragraph however lists exceptions to this, which include buildings for agriculture and forestry. Paragraph 89 does not include buildings that are associated with mineral extraction, however paragraph 90 states that mineral extraction is not inappropriate within the Green Belt. As a building associated with mineral extraction (which this application is for) is not specifically listed within the exceptions to paragraph 89, the proposed development must therefore be considered to be inappropriate development.

Paragraph 87 of the NPPF states that inappropriate development is, by definition, harmful to the Green Belt and should be approved except in very special circumstances. Paragraph 88 continues that when considering any planning application, local planning authorities should ensure that substantial weight is given to any harm to the Green Belt. Very special circumstances will not exist unless the potential harm to the Green Belt by reason of inappropriateness, and any other harm, is clearly outweighed by other considerations.

Allocations Plan Policy CG7AP concerns Green Belt and reflects the guidance contained within the NPPF.

The applicant has submitted the following as very special circumstances in this case:

- * The proposed gatehouse does not conflict with any of the five purposes of the Green Belt (paragraph 80 of the NPPF);
- * Mineral extraction is not an inappropriate use within the Green Belt (paragraph 90 of the NPPF);
- * The security afforded by a gatehouse is essential for effective mineral extraction at Pilkington Quarry (the extension area) as the site is subject to fly-tipping and anti-social behaviour;
- * The applicant cannot continue to run the currently unmitigated risk of damage to plant and equipment and the site's safety boundaries (fences and walls);
- * There are drug related activities on the land.

Officers would also note that Schedule 2, Part 17, Class K of the Town and Country Planning (General Permitted Development) (England) Order 2015 allows for the provision of structures in connection with mineral operations without the need for planning permission, however the proposed gatehouse does not fit neatly within the criteria.

The proposed building is to be relatively small in scale, measuring 9.5 metres in length by 5.9 metres in width and at single storey in height. The building will inevitably effect the openness of the Green Belt in this location given that the site is currently open (apart from the low rise water pumping station) however it is considered this the harm on the openness of the Green Belt in this location is limited given the proposed scale of the building. The proposed car park will be sited behind an existing stone wall.

Officers consider that the proposed use of the building (as a security gatehouse for the quarry) would not harm the purposes of the Green Belt.

It is considered that the very special circumstances put forward by the applicant (which justify the need for the development) outweigh the (considered) limited harm that would be caused to the openness of the Green Belt in this location.

It is however suggested that a condition be attached to any approval to remove the gatehouse and reinstate the land back to a field once the restoration of Pilkington Quarry has been completed (by 31st December 2027).

It is considered that whilst the proposed gatehouse may constitute inappropriate development in the Green Belt, the very special circumstances put forward by the applicant clearly outweigh the harm to the openness of the Green Belt in this location, compliant with Section 9 of the NPPF and Allocations Plan Policy CG7AP.

Impact on the Character and Appearance of the Area

Policy CG3 of the Core Strategy states that the Council will conserve and enhance local

distinctiveness, ensuring development has regard to the overall built character and landscape quality of the area, and will require development to be compatible with the surrounding area, in terms of scale, massing, grain, form, architecture, street enclosure, local materials and landscape treatment. Policy OA1 of the Core Strategy states that the Council will conserve and enhance the character of the existing landscape and physical environment, and will ensure that development in Horwich and Blackrod respects street patterns, the grain and the form of predominant architectural styles and where possible makes sympathetic use of locally distinctive materials such as stone.

The proposed gatehouse is to be relatively small in scale, which befits its purpose as a security 'lodge'. The front of the building (its south western elevation) will be elevated from the road, with this part of the building being sited on top of the existing low level water pumping station.

It is considered that the proposed building is of good design, being constructed of ashlar stone (which is a local material) and natural slate. The building will provide an attractive entrance to Pilkington Quarry whilst also being a functional building.

The proposed car parking area is also kept limited in scale.

It is considered that the proposed development would conserve local distinctiveness and respect the local rural vernacular and locally sourced materials, compliant with Policies CG3 and OA1 of the Core Strategy.

Impact on the Highway

Policy P5 of the Core Strategy states that the Council will ensure that developments take into account [amongst other things] freight movement, servicing arrangements and parking. Policy S1.2 states that the Council will promote road safety in the design of new development.

Following comments from the Council's Highways Engineers the applicant has amended their plans so that a corner of the proposed gatehouse no longer encroaches onto the highway.

The Council's Highways Engineers have raised no objection to the proposed development and have requested a couple of conditions concerning the laying out of the car park.

It is therefore considered that the proposed development would not jeopardise highway safety, compliant with Policies P5 and S1.2 of the Core Strategy.

Impact on the Public Right of Way

Policy P8AP of the Allocations Plan states that the Council will permit development proposals affecting public rights of way, provided that the integrity of the right of way is retained.

Public Right of Way HOR057 runs the whole length of Makinson Road.

The proposed development will not affect the integrity of the public right of way, especially as the applicant has removed the originally proposed cantilever gate. It is therefore considered that the proposal complies with Policy P8AP of Bolton's Allocations Plan.

The Council's Public Rights of Way Officer has suggested that the applicant defines a footway of no less than 1.5 metres wide on the western side of the highway (Makinson Road) to encourage pedestrians to use this part of the road and therefore away from vehicular activity. The proposal will however not result in any increase in traffic movement to the quarry, as this is restricted by condition under the original planning application for Pilkington Quarry. Officers therefore consider it would be unnecessary for the applicant to provide such markings or physical kerbs for this particular proposal.

Impact on a Culverted Watercourse

Policy CG1.5 states that the Council will reduce the risk of flooding in Bolton and other areas downstream by minimising water run-off from new development and ensuring a sequential approach is followed, concentrating new development in areas of lowest flood risk.

The Council's Drainage Officers have commented that there is potentially a culverted watercourse situated within the application site and that this pipeline may still be receiving surface water flows. Officers state that if this pipeline is found to be present in the location where the gatehouse is proposed then the applicant should provide a minimum of a 3 metre easement either side of the pipe between any proposed structure and the pipe. If the culverted watercourse requires diverting then the applicant would require consent from the Council under Section 23 of the Land Drainage Act 1991.

A condition alerting the applicant to this potential requirement is therefore suggested.

Conclusion

The very special circumstances put forward by the applicant are considered to outweigh the harm to the openness of the Green Belt in this location. The building would be compatible with its surroundings and no harm has been found to matters such as highway safety or the nearby public right of way.

Members are therefore advised to approve the application subject to the recommended conditions.

Representation and Consultation Annex

Representations

Letters:- Two letters of objection have been received from a resident of Makinson Road and a nearby resident. These letters raise the following concerns:

- * Any vehicle or vehicles being stopped for checks at the gatehouse would block the entrance/exit of Matchmoor Lane and the junction on Georges Lane, which would severely restrict the flow of traffic currently using this junction, and access to residents of Grundy Cottages;
- * It would be better for the gatehouse to be sited further up Makinson Road past Grundy Cottages;
- * Want reassurance that the resident on Makinson Road will maintain full access to their property once the gatehouse has been erected (*officer's comment: the applicant has removed the originally proposed cantilever gate from the proposal*);
- * Want reassurance that the public right of way will still be in place (*officer's comment: the public right of way is to remain and will not be restricted*);
- * Want speed bumps on Makinson Road as the trucks go fast up and especially down the road. Concern about children playing in their garden and sheep and horses on the road;
- * Erosion of Green Belt and disappearance of moorland as a result of the quarrying; Dust, noise and smell of traffic and landfill is horrible (*officer's comment: these concerns are not material to the submitted application as they concern existing quarry operations*).

Horwich Town Council:- objected to the proposal at their meeting of 19th May 2016 due to concerns about access to nearby residential properties (*officer's comment; the application has removed the originally proposed cantilever gate from the proposal*).

Consultations

Advice was sought from the following consultees: Highways Engineers, Public Rights of Way Officer, Drainage Officers and United Utilities.

Planning History

A water pumping station for the quarries (located to the south of the site) was approved in June 1995 (46640/95).

Recommendation: **Approve subject to conditions**

Recommended Conditions and/or Reasons

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason

Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. This permission shall be for a temporary period expiring on **31st December 2027** when the gatehouse hereby approved shall be discontinued and the building full removed and the land reinstated in accordance with a scheme to be approved in writing by the Local Planning Authority before the expiry of this permission. The approved scheme shall be implemented in full and retained thereafter.

Reason

The building is only required during the life of the quarry and its retention after this period would

constitute inappropriate development in the Green Belt. To comply with policy CG7AP of Bolton's Allocations Plan.

3. Prior to the commencement of development details of the exact location of the culverted watercourse must be submitted to the local planning authority. If the culverted watercourse requires diverting (if a 3 metre easement cannot be achieved) details of this work will need to be submitted to and approved in writing by the local planning authority. Any diversion of the watercourse should be implemented in full accordance with the approved details prior to the commencement of development.

Reason

The pipeline should remain in situ as it is still receiving surface water flows, and to comply with policy CG1.5 of Bolton's Core Strategy.

4. Prior to the commencement of development the details (samples if required) of the type and colour of materials to be used for the external walls and roof to be agreed with the Local Planning Authority. The approved materials shall be implemented in full thereafter.

Reason

To ensure the development reflects local distinctiveness and to comply with policy CG3 of Bolton's Core Strategy.

5. Prior to the development hereby approved/permitted being first occupied or brought into use the means of vehicular access to the site from Makinson Road shall be constructed in accordance with the drawing ref 3425-16-02 Rev. C.

Reason

In the interests of highway safety and in order to comply with policies S1 and P5 of Bolton's Core Strategy.

6. Before the approved/permitted development is first brought into use no less than 4 car parking spaces shall be provided within the curtilage of the site, in accordance with drawing ref: 3425-16-02 Rev. C. Such spaces shall be made available for the parking of cars at all times the premises are in use.

Reason

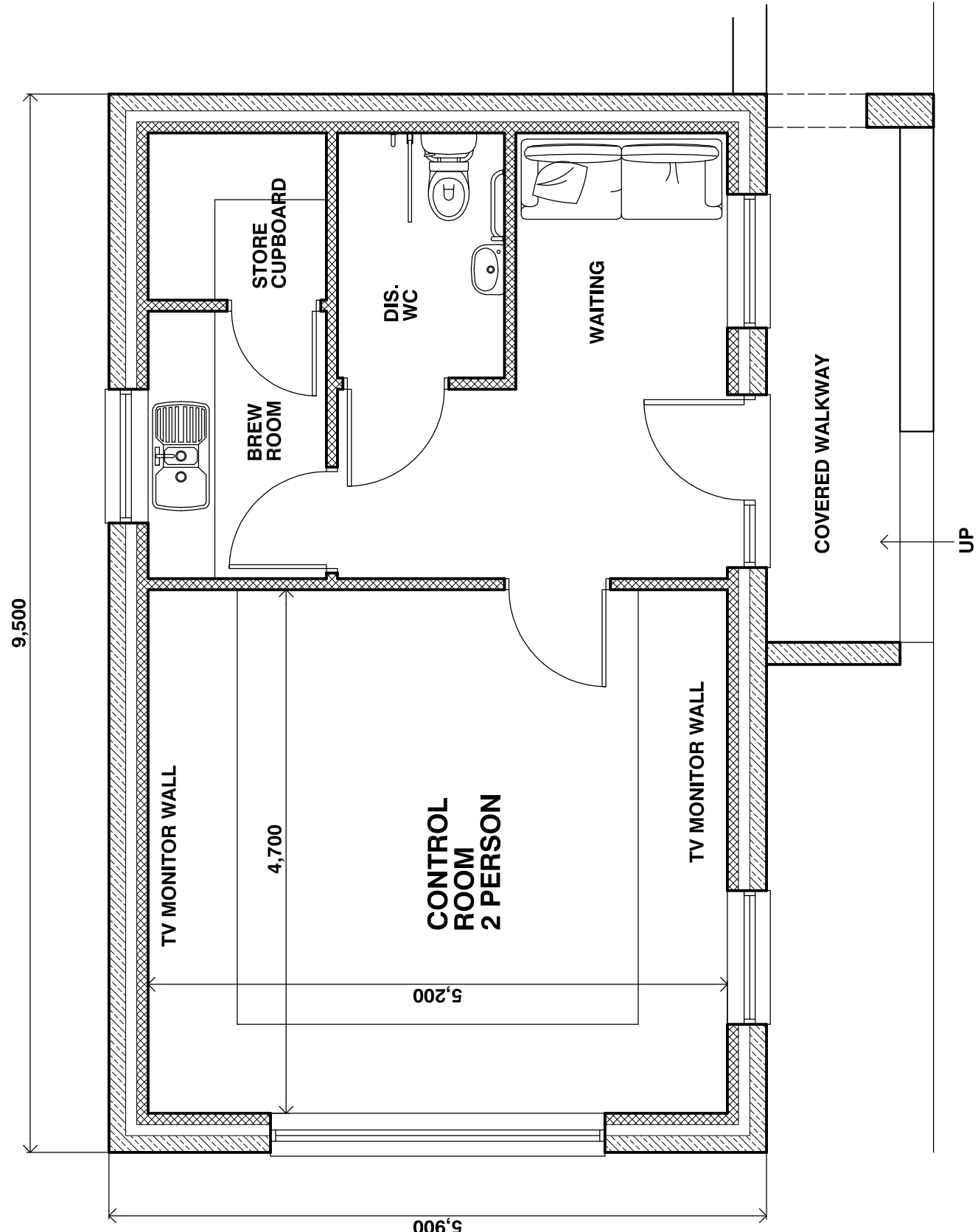
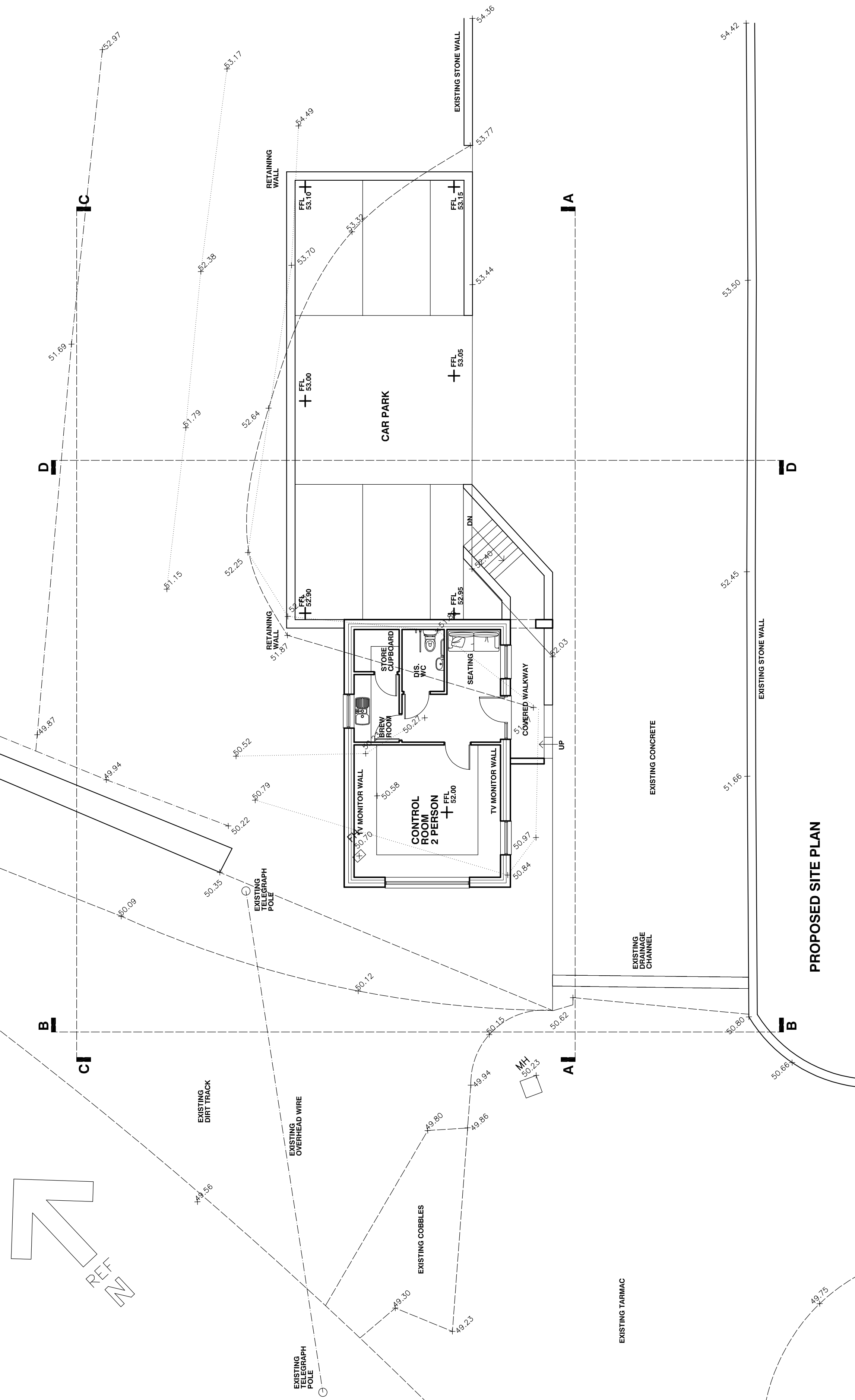
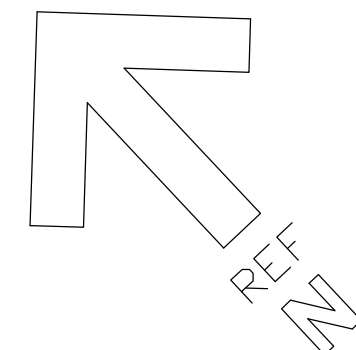
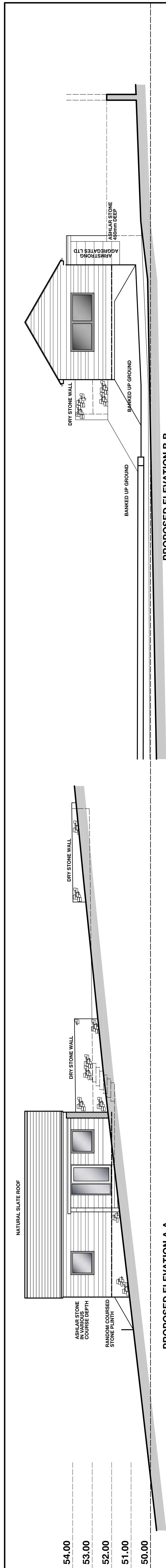
To ensure that adequate provision is made for vehicles to be left clear of the highway and to comply with policy P5 and Appendix 3 of Bolton's Core Strategy.

7. The development hereby permitted shall be carried out in complete accordance with the following approved plans:

3425-16-02 Rev. C; "Proposed Plan & Elevations"; dated 20.05.16

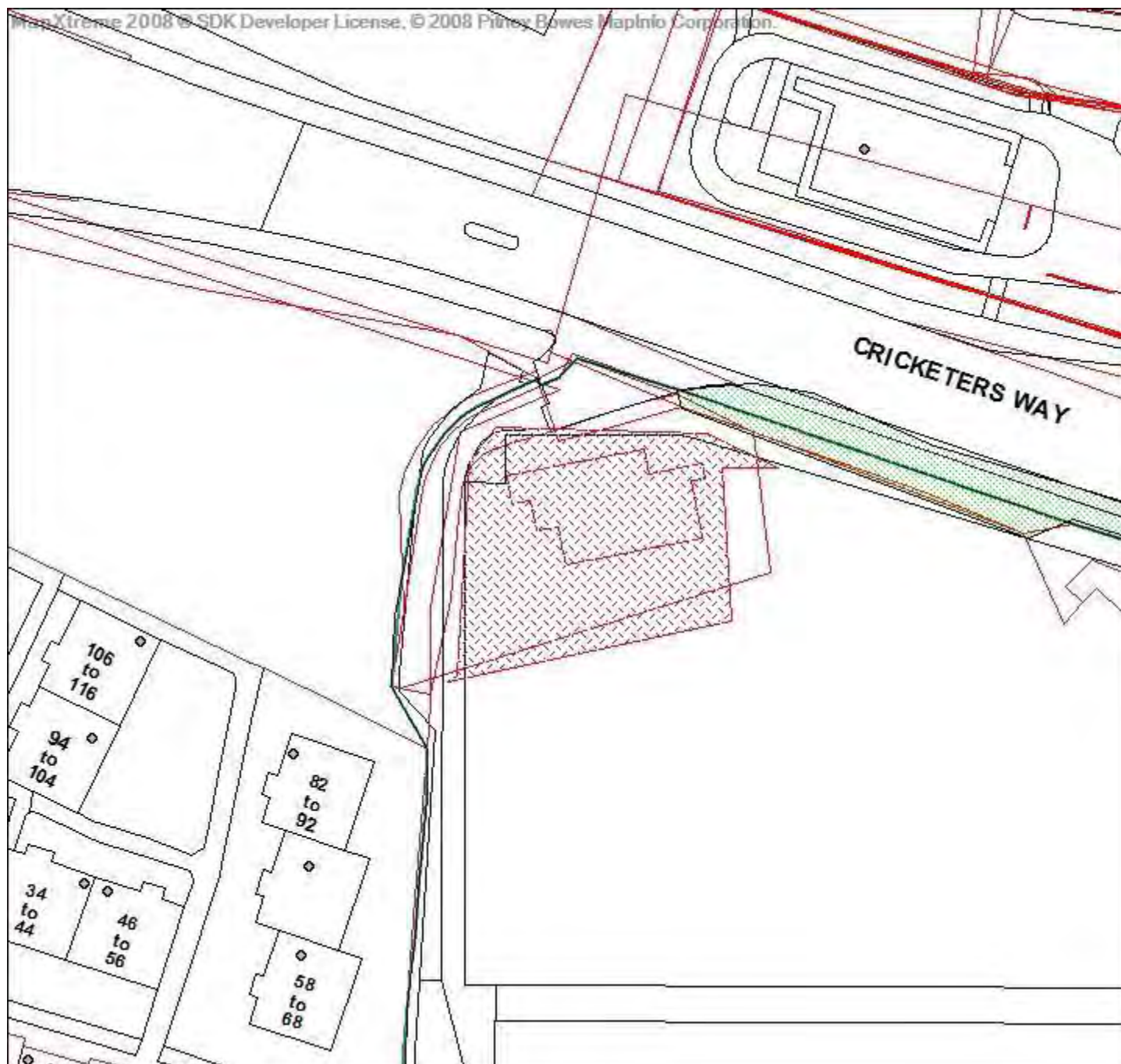
Reason

For the avoidance of doubt and in the interests of proper planning.



Client	ARMSTRONG AGGREGATES PROPOSED GATEHOUSE MATCHMOOR LANE HORWICH BOLTON		C 20.05.16 BUILDING RELOCATED NORTH EAST DW B 06.04.16 ACCESS FOR PUBLIC RIGHT OF WAY NOTED DW A 09.03.16 BUILDING INCREASED IN SIZE EXACTLY ANTICIPATED
Project	PROPOSED PLAN & ELEVATIONS 3425-16-02 C 1:100 @ A1	Drawing No. 01 FEB 2016 Drawn By Date	Peter Dickinson R.I.B.A. 169 Appleby Lane North Appleby Bridge Wigan, Lancs Tel. No. 01257 252203 Website www.peterdickinson.co.uk E-mail admin@peterdickinson.co.uk

Application number 96332/16



Development & Regeneration Dept
Development Management Section

Town Hall, Bolton, Lancashire, BL1 1RU
Telephone (01204) 333 333

Bolton Council

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Date of Meeting: 30/06/2016

Application Reference: 96332/16

Type of Application: Full Planning Application

Registration Date: 03/05/2016

Decision Due By: 27/06/2016

**Responsible
Officer: Alex Allen**

**Location: SAINSBURYS SUPERMARKETS LTD, CRICKETERS WAY,
WESTHOUGHTON, BOLTON, BL5 3ZS**

**Proposal: SITING OF A VETS 4 PETS CONCESSION POD ON PART OF CAR
PARK**

Ward: Westhoughton South

Applicant: Sainsburys

Agent : CHQ Partnership Ltd

Officers Report

Recommendation: Approve subject to conditions

Proposal

The applicant proposes the erection of a veterinary surgery which would comprise 3 consulting rooms, an operating theatre, reception and waiting area, pharmacy and staff facilities. The new building would take the form of a 'pod' which is a flat roofed building faced with Kingspan composite panels. The new building would be c 15.7 metres in length and 10.5 metres wide/deep.

The pod would be located in the north west corner of the Sainsburys supermarket site. The development would require the reorganisation of the existing car park with the loss of 26 car parking spaces. The development would make provision for new road layout and 5 spaces, therefore a net loss of 19 spaces.

The proposed hours of use of the surgery would be between 0830 hrs to 1900 hrs Monday to Saturdays and 1000 hrs to 1600 on Sundays.

Site Characteristics

The application site is located within the north west corner of the Sainsburys supermarket site. The site is currently used for car parking and vehicular circulation in conjunction with the established retail use of the site. Cricketers Way is to the north some 12 metres away with the closest residential properties on the Welland (No's 82-92) occupying land some 33 metres away to the south west of the site.

Policy

National Planning Policy Framework

Core Strategy policies P5 Accessibility, S1 Safe, CG3 the Built Environment, CG4 Compatible Uses and

OA3 Westhoughton.

Supplementary Planning Documents General Design Principles and Accessibility, Transport and Road Safety.

Analysis

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- * principle of development;
- * impact on the character and appearance of the area
- * impact on adjoining uses
- * impact on parking / highway safety

Principle of development

Guidance contained within NPPF states that the purpose of the planning system is to contribute to the achievement of sustainable development. There are three dimensions to sustainable development, economic, social and environmental. The economic role is to contribute to a strong, responsive and competitive economy by ensuring sufficient land of the right type is available in the right places to support growth and innovation.

The proposal would assist in creating a new business in the local area providing jobs for 4 full time employees and 5 part time and staff driving economic development. It is noted that a number of objectors are concerned about the impact of the proposal on existing veterinary practices in the Westhoughton area. However, the impact of a proposed new business on existing businesses is not a material planning consideration. The planning system does not operate to protect existing business interests.

The proposal would comply with guidance contained within NPPF to secure economic growth.

Impact on the character and appearance of the area

Core Strategy policies CG3 and OA3 seek to ensure that new development proposals conserve and enhance local distinctiveness/the local environment.

The proposed building would be constructed on an existing car park area which is indistinct in its character. The proposed building is modest in scale and whilst relatively utilitarian in its character / build (i.e. standard Kingspan construction) it would be viewed in the context of the large Sainsburys store to the south. The building would also be set back from Cricketers Way to the north and is screened by existing landscaping.

The proposal would be in character with the surrounding area and complies with Core Strategy policies CG3 and OA3.

Impact on adjoining uses

Core Strategy policy CG4 seeks to ensure that new development is compatible with surrounding land uses and occupiers, protecting amenity, privacy, safety and security.

The application would bring a new use to an existing large car park, providing additional pedestrian movement and in turn enhanced natural surveillance over the wider area.

It is considered that the building would be sufficiently insulated so as not to result in any harm to nearby residents living condition. This would also be limited by the proposed opening hours of the practice and also the distance of approximately 33 metres between the proposed building and the closest residential properties on the Hoskers to the south west of the site.

In addition, the proposed surgery is some 33 metres away from the front elevation of the supermarket. It is not uncommon to have veterinary practices in relative close proximity to food outlets e.g. Pets at Home and Asda on the Middlebrook development. It is not considered that the relationship between the two uses is inappropriate or likely to result in food hygiene health and safety issues.

It is considered that the proposal complies with Core Strategy policy CG4.

Impact on parking / highway safety

Core Strategy policy P5.1 and P5.5 seeks to ensure that new development proposals make provision for adequate parking and accessibility by different types of transport including pedestrians, cyclists.

The proposal would result in the net loss of 19 spaces from the existing car parking. The overall floorspace provided within the proposed development would amount to 164 sq.m. The surgery use would be a D1 use which would require potentially a maximum of 9 car parking spaces within the overall car park. Whilst the retail use would require a maximum total of 185 car parking spaces. It is noted that the existing supermarket over provides for car parking.

The Council's Highways Engineers have assessed the proposal and consider that they have no objections.

It is considered that the proposal combined with the use of the existing supermarket would provide sufficient car parking to meet the requirements of both businesses and complies with policy.

Conclusion

The proposal would result in a modest change to both the character and appearance of the area and to the level of car parking within the existing supermarket car park without compromising local residential amenity. Issues of animal welfare or the impact of the proposals on existing veterinary practices are not material planning considerations. The proposal complies with policy and is recommended for approval subject to conditions.

Representation and Consultation Annex

Representations

Letters:- three objection letters have been received, one letter from the Director of an existing veterinary practice in Westhoughton and two other letters. The letters raise the following concerns:

- There are two existing independent vets practices in Westhoughton. The provision of an additional practice will have an adverse impact on vet services in Westhoughton undermining viability. The applicant will use locum vets and graduate vets with less experience;
- The building type raises concerns over temperature control, storage of dead bodies need to be stored prior to disposal;
- Wrong to site a vets practice close to a food retailer;
- Loss of 32 spaces not 13;
- There is a shortage of car parking spaces at peak times;
- Vehicle movements make transporting sick animals unsafe.

Petitions:- no petitions received.

Town Council:- raise objection due to the proposal resulting in a loss of 13 car parking spaces, noise and disturbance to nearby flats (The Welland), proximity to food store, will attract youths to the car park resulting in anti social behaviour, potential health and safety issues in the portacabin.

Elected Members:- no comments received.

Consultations

Advice was sought from the following consultees: Westhoughton Town Council, the Council's Highways Engineers and Pollution Control officers.

Planning History

Planning permission was granted in July 2008 (Ref: 78000/07) for the erection of a retail store, the provision of car parking with servicing and access.

Recommendation: **Approve subject to conditions**

Recommended Conditions and/or Reasons

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason

Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. Prior to the commencement of development the details (samples if required) of the type and colour of materials to be used for the external walls and roof to be agreed with the Local Planning Authority. The approved materials shall be implemented in full thereafter.

Reason

To ensure the development reflects local distinctiveness and to comply with policy CG3 of Bolton's Core Strategy.

3. The premises subject of this consent shall not be open for trade outside the following hours:-

0830 to 1900 Mondays to Saturday;
1000 to 1600 hrs on Sundays.

Reason

To safeguard the living conditions of residents and the amenity and character of the area with regard to noise and/or disturbance.

4. The premises shall be used for a veterinary surgery and for no other purpose (including any other purposes in Class D1 of the Schedule to the Town and Country Planning (Use Classes) Order 2015, or in any provision equivalent to that Class in any statutory instrument revoking and re-enacting that Order).

Reason

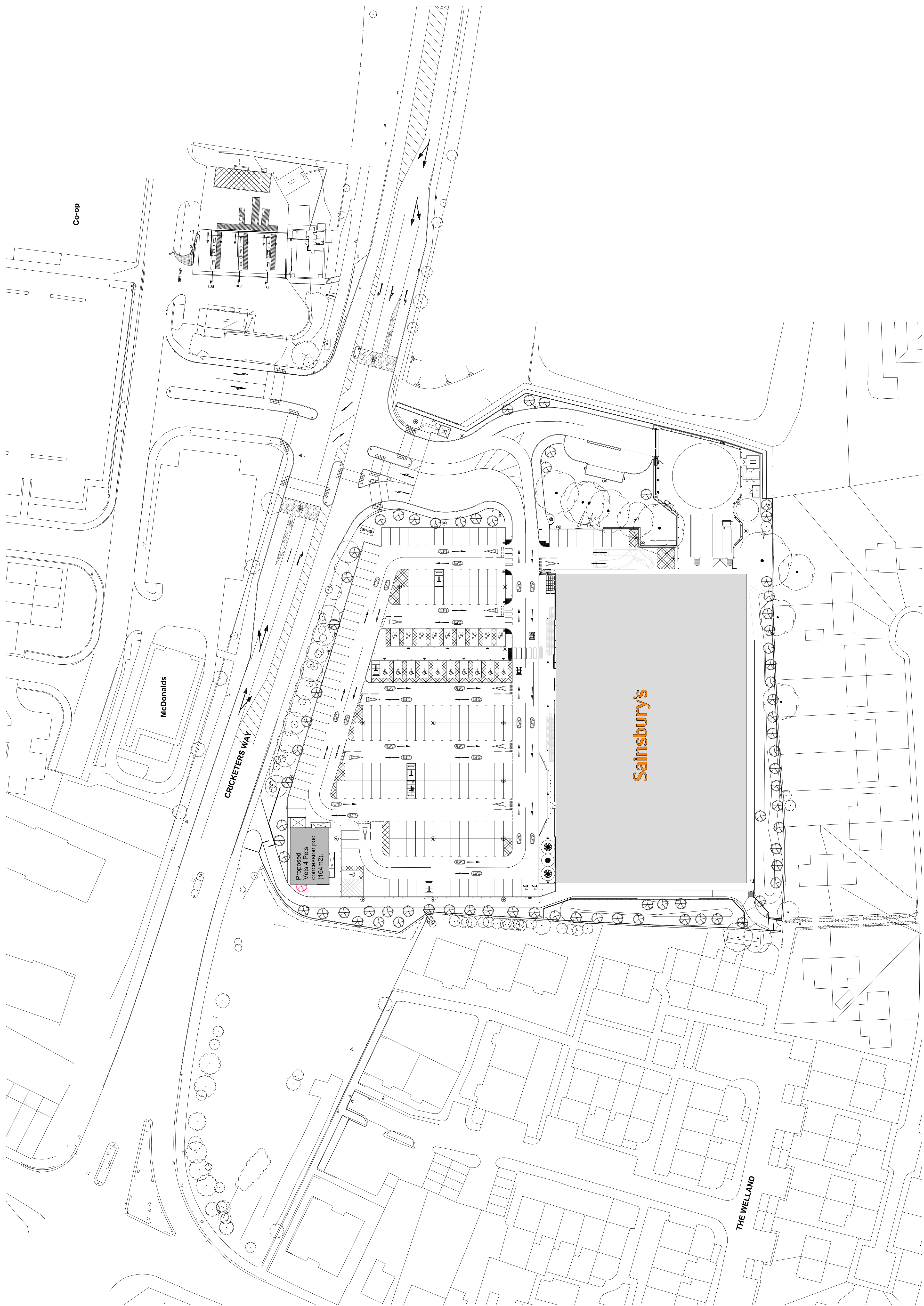
For the avoidance of doubt as to what is permitted.

5. The development hereby permitted shall be carried out in complete accordance with the following approved plans:

Drawing No. CHQ.15.11.720 - PL03 - Proposed Site Plan dated January 2016;
Drawing No. CHQ.15.11.720 - PL05 Rev A - Proposed Elevations dated January 2016;
Drawing No. CHQ.15.11.720 - PL06 Rev A - Existing and Proposed Detail Site Plan dated January 2016;

Reason

For the avoidance of doubt and in the interests of proper planning.



SAINSBURY'S EXISTING CAR PARKING	
Standard Car Parking at 2.5 x 4.8m	221
Disabled Car Parking Spaces	10
Parent & Child Car Parking Spaces	9
Trailer Bays	24
Recycling Bays	Y
SAINSBURY'S PROPOSED CAR PARKING	
Standard Car Parking at 2.5 x 4.8m	209
Disabled Car Parking Spaces	11
Parent & Child Car Parking Spaces	9
Trailer Bays	23
Recycling Bays	Y

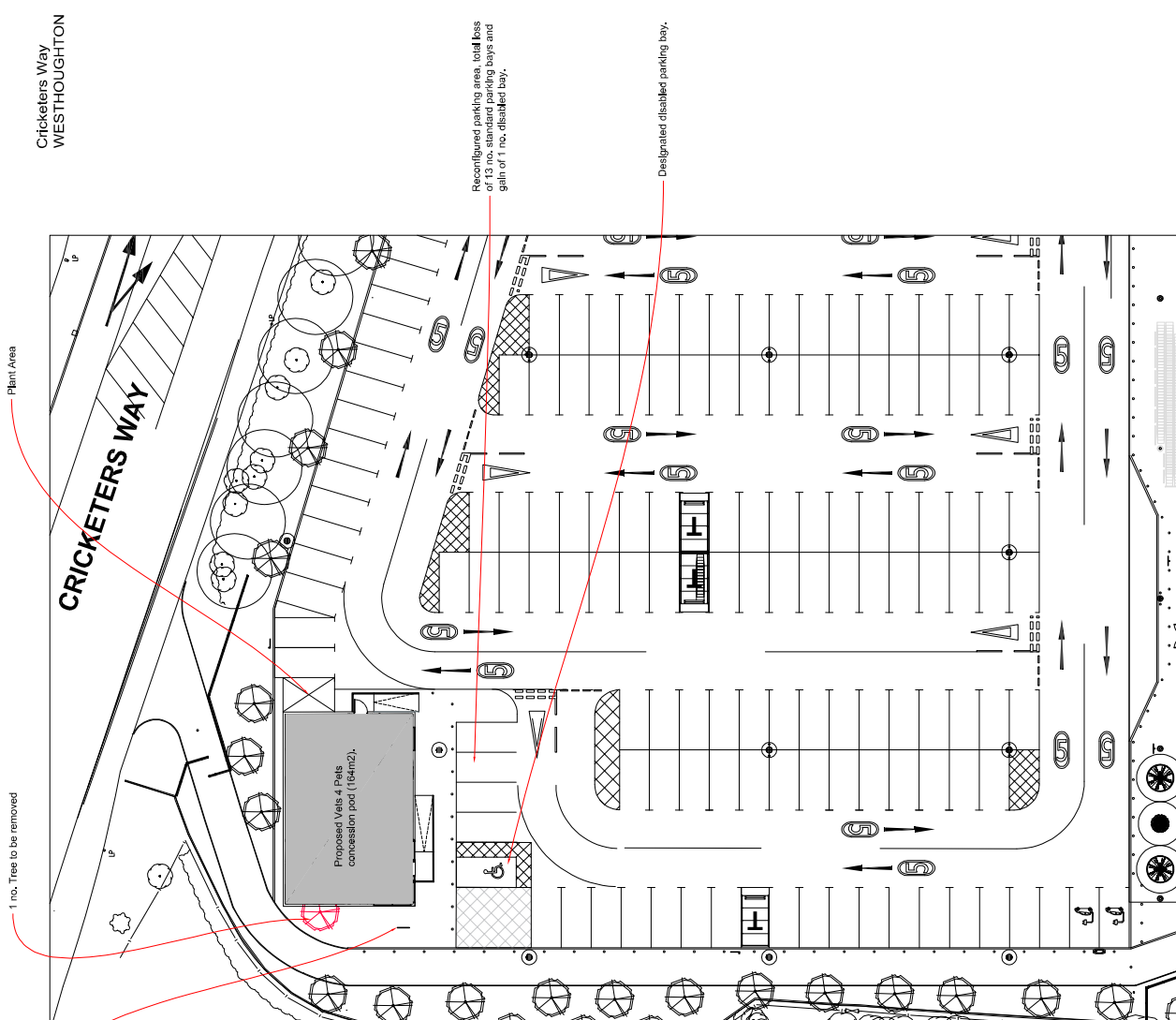
REV	DESCRIPTION	DATE	INITIALS	REV DESCRIPTION	DATE	INITIALS
-	-	-	-	-	-	-
<p>All drawings are subject to full site survey. All dimensions are to be checked on site.</p> <p style="text-align: right;">PLANNING</p>						

CHQ ARCHITECTS

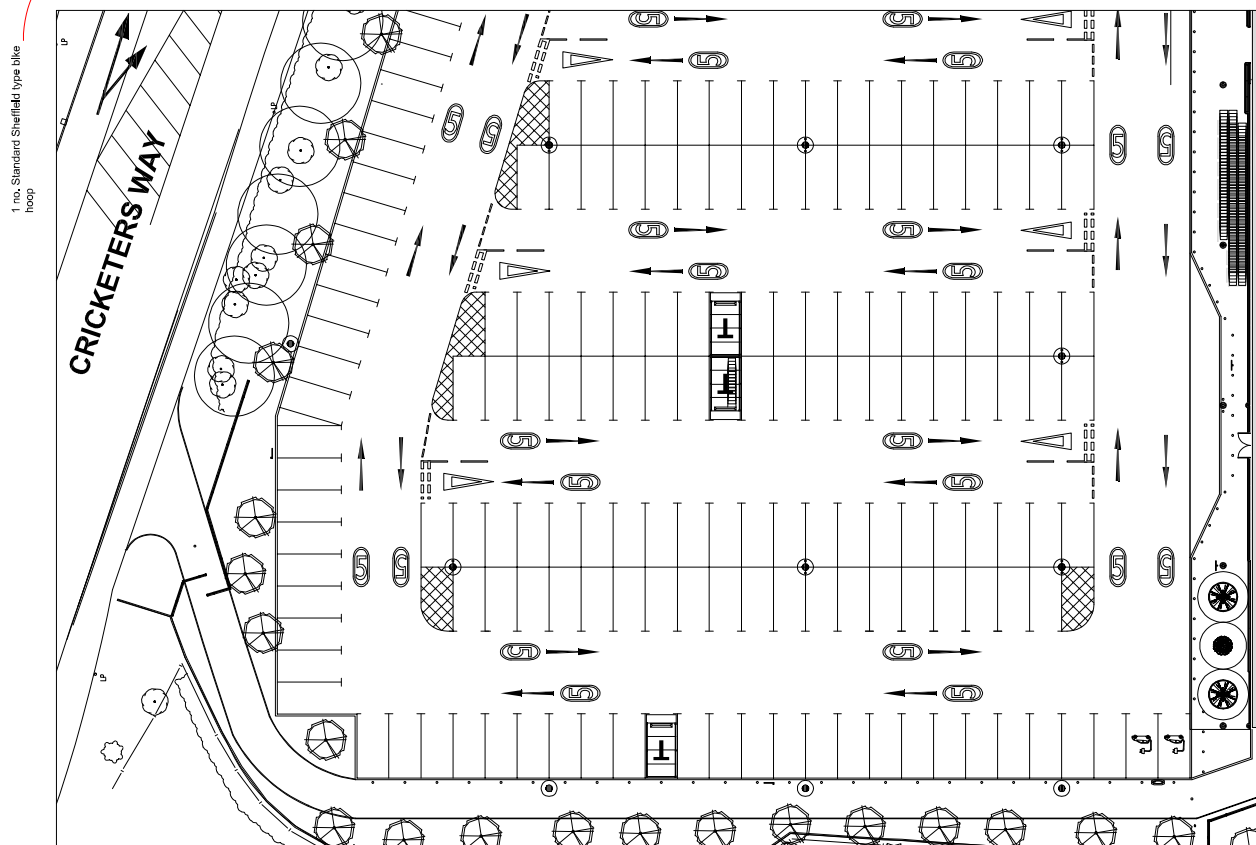
CHQ Architects Ltd.
The Maltings, 44 Whitehorse Street
Bathwick, Bath BA2 4BN
Tel: 01225 317 600
Email: design@chq-architects.co.uk
www.chq-architects.co.uk

PROJECT
Cricketers Way,
Westoughton,
Bolton
BLS 3ZS

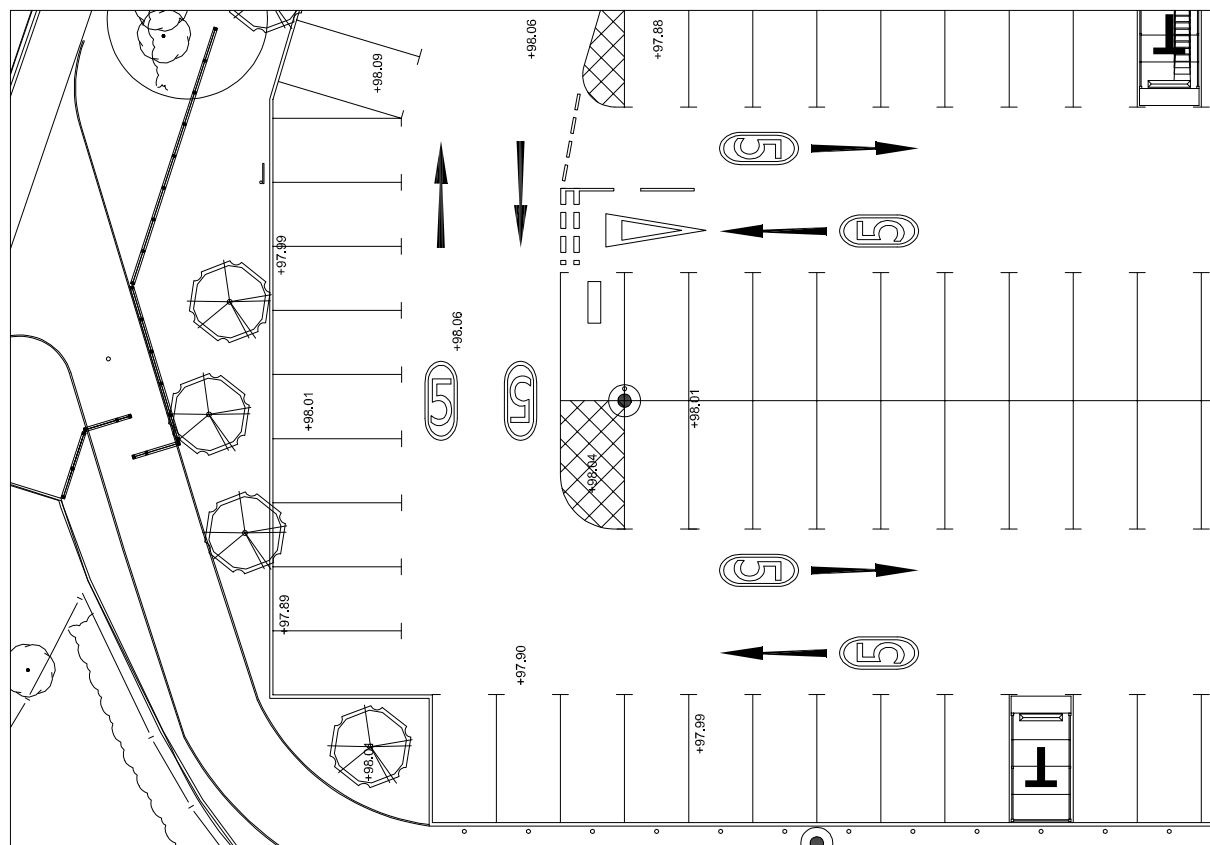
DATE	January 2016	DRAWN	HS
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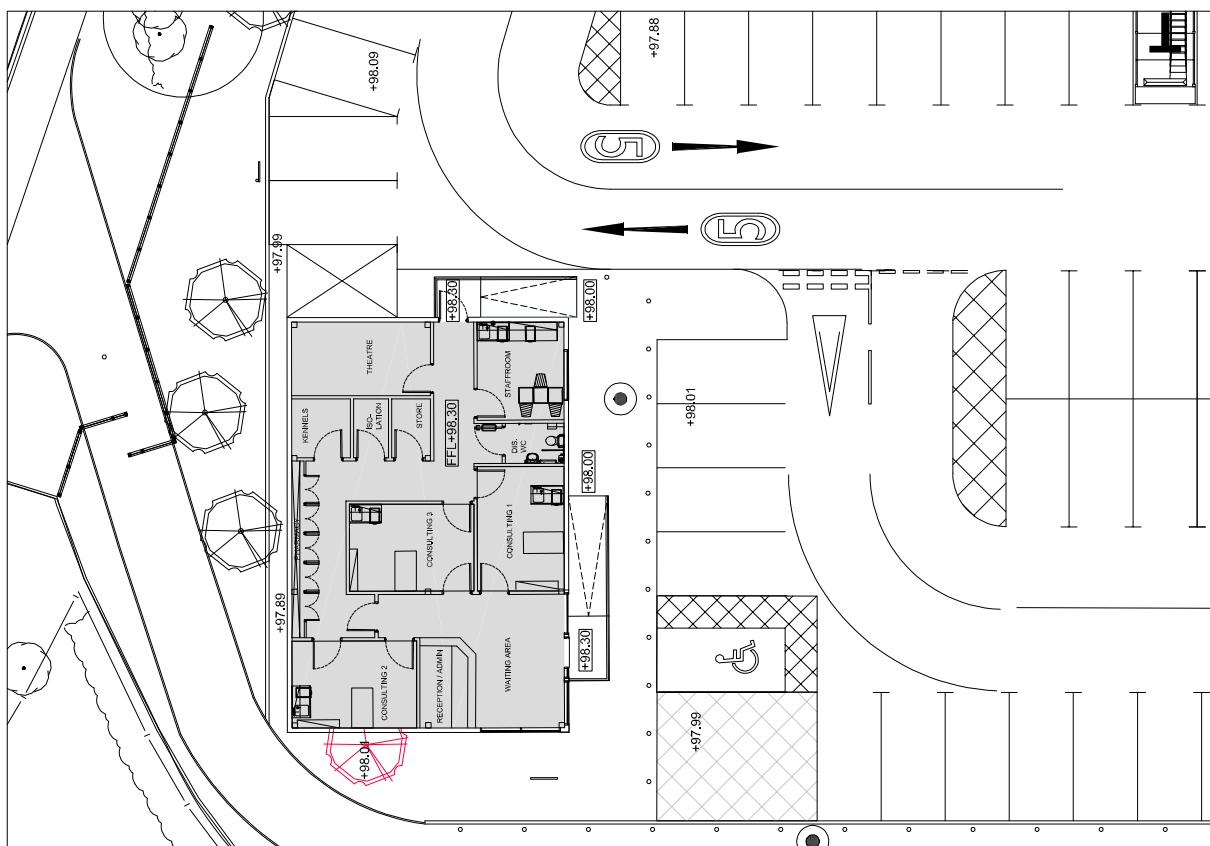
**Proposed Detail Site Plan
1:200 @ A1**



Existing Detail Site Plan
1:200 @ A1



**Existing Detail Site Plan
1:100 @ A1**



**Proposed Detail Site Plan
1:100 @ A1**

Application number 96369/16



Development & Regeneration Dept
Development Management Section

Town Hall, Bolton, Lancashire, BL1 1RU
Telephone (01204) 333 333

Bolton Council

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Date of Meeting: 30/06/2016

Application Reference: 96369/16

Type of Application: Full Planning Application
Registration Date: 13/04/2016
Decision Due By: 07/06/2016
Responsible Officer: Helen Williams

**Location: JUNCTION OF GEORGES LANE AND CHORLEY OLD ROAD,
HORWICH, BOLTON**

Proposal: IMPROVEMENTS TO VISIBILITY SPLAY AT ROAD JUNCTION

Ward: Horwich North East

Applicant: Armstrong Aggregates Ltd.

Agent : The Mineral Planning Group Ltd.

Officers Report

Recommendation: Approve subject to conditions

Proposal

Permission is sought for the widening of the bottom of Georges Lane, at its junction with Chorley Old Road, by improving the visibility splay at the western corner. This would involve widening the width of the carriageway at the junction and constructing a new footpath (pavement) into the wooded area to the west (Tup Row Wood). The existing dry stone wall at the junction would be rebuilt to follow the new back of footway curve at this location.

The applicant has stated that the proposed junction improvement is required (in association with Montcliffe and Pilkington Quarries) to:

- * Improve road safety for users of the junction of Georges Lane with Chorley Old Road by improving visibility for drivers exiting Georges Lane. At present visibility to the right (Horwich town centre direction) is limited by a lack of an appropriate splay and is largely obstructed by a number of tall trees. These features also hinder visibility of vehicles that are exiting Georges Lane to users on Chorley Old Road;
- * Avoid large vehicles having to use the opposite/wrong side of Georges Lane when turning left into road off Chorley Old Road from the Horwich town centre direction;
- * Significantly reduce the potential for accidents, as manoeuvring around the junction would be made easier and safer for all users;
- * Conform with HGV requirements for junctions. The existing junction has never conformed with this and the quarry operator has wanted to improve this for a while;
- * Reduce congestion by improving waiting times for vehicles emerging on to Chorley Old Road. This will in turn reduce journey times, delays and frustration of road users;
- * Support economic growth. The junction improvement would allow adequately sized crushers and screens to be brought to the quarries by low-loader and articulated lorries, in order to facilitate the proper working of minerals. At present the most effective means of working minerals in the

quarries are partially compromised due to want of better-suited plant and equipment, which cannot always readily be transported to and from the site.

The proposed junction improvement will result in the loss of two protected trees (a Sycamore and a Beech).

Site Characteristics

The application site measures 0.04 hectares and comprises the western corner of the junction of Georges Lane with Chorley Old Road. The site contains the south eastern corner of Tup Row Wood, which is a protected woodland (Tree Protection Order Bolton (Georges Lane, Horwich) 1985. The woodland area is bounded by a dry stone wall, which is sited to the back of the pavement.

The site is located at the south eastern corner of Wallsuches Conservation Area and is also within the Green Belt (edge of the Green Belt).

Policy

National Planning Policy Guidance (NPPF)

Core Strategy Policies: P5 Transport and Accessibility; S1.2 Road Safety; CG1.1 Rural Biodiversity; CG3 The Built Environment; OA1 Horwich and Blackrod.

SPD Accessibility, Transport and Safety

Wallsuches Conservation Area Appraisal

Analysis

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- * impact on protected trees
- * impact on the character and appearance of Wallsuches Conservation Area
- * impact on biodiversity
- * impact on the highway

Impact on Protected Trees

Policy CG1.1 of the Core Strategy states that the Council will safeguard and enhance the rural areas of the borough from development that would adversely affect its biodiversity including trees, woodland and hedgerows.

The application site comprises the south eastern corner of Tup Row Wood, which is protected

woodland under Tree Protection Order Bolton (Georges Lane, Horwich) 1985. The woodland comprises mainly of Sycamore and Beech trees.

The proposed junction improvement involves the widening of the width of the carriageway at the bottom of Georges Lane and the construction of a new footpath (pavement) into the wood. The existing dry stone wall at the junction would be rebuilt to follow the new back of footway curve at this location.

The proposal would result in the loss of two trees, one Sycamore tree and one Beech tree, the two nearest trees to the western junction at the bottom of Georges Lane. The Council's Tree Officer has commented that the Beech tree to be felled is poorly formed.

The Tree Officer has also commented that the proposed junction improvement will also result in the requirement for works within the root protection zone of an adjacent Ash tree on Georges Lane. The Tree Officer has therefore recommended that any excavation for the new pavement within the root protection zone of the Ash tree should be undertaken with extreme care so not to damage the roots. They also recommend that the new kerb line for the road should be hand dug to reduce the impact on the root system of the Ash tree.

The Council's Tree Officer has not raised a formal objection to the proposed loss of the Sycamore and Beech tree.

It is therefore to be determined whether the loss of the two trees would have a detrimental affect on the character and appearance of Wallsuches Conservation Area and the biodiversity of the woodland. These impacts are assessed below.

Impact on the Character and Appearance of Wallsuches Conservation Area

Policy CG3 of the Core Strategy states that the Council will conserve and enhance local distinctiveness, ensuring development has regard to the overall built character and landscape quality of the area, and will require development to be compatible with the surrounding area, in terms of scale, massing, grain, form, architecture, street enclosure, local materials and landscape treatment. Policy CG3.4 states that the Council will conserve and enhance the heritage significance of heritage assets and heritage areas, recognising the importance of sites, areas and buildings of archaeological, historic, cultural and architectural interest and their settings. Policy OA1 of the Core Strategy states that the Council will conserve and enhance the character of the existing landscape and physical environment, especially the conservation areas at Horwich town centre, Horwich Loco Works and Wallsuches and will ensure that development in Horwich and Blackrod respects street patterns, the grain and the form of predominant architectural styles and where possible makes sympathetic use of locally distinctive materials such as stone.

The application site is located within Wallsuches Conservation Area.

The proposal would result in the loss of the two trees nearest to the western corner of the junction of Georges Lane with Chorley Old Road, the resiting of the dry stone wall, the widening of the carriage way at the junction, and the resiting of the pavement.

The two trees to be felled are a Sycamore (the tree nearest to the junction) and a Beech. The trees occupy the south eastern tip of Tup Row Wood, a triangular shaped woodland that extends up to Wallsuches (the road). The woodland comprises mainly of Sycamore and Beech trees.

The Beech tree is poorly formed (confirmed by the Council's Tree Officer) and has not grown particularly tall. The Sycamore tree is smaller in height than the neighbouring trees and has had

lower branches removed, presumably so that they do not overhang the highway. It is not considered that the loss of these two trees, which are smaller than the neighbouring trees and are at the far corner of the woodland, would have a detrimental impact on the character and appearance of the conservation area.

Whilst the carriageway at the bottom of Georges Lane would be wider, it is again not considered that this would harm the character and appearance of the conservation area given that the junction adjoins a wide main road. The rebuilding of the dry stone wall in its new location will ensure that this important boundary to the woodland is retained.

It is therefore considered that the proposed development would not detrimentally harm the character and appearance of Wallsuches Conservation Area, compliant with Policies CG3 and OA1 of Bolton's Core Strategy.

Impact on Biodiversity

Policy CG1.1 of the Core Strategy states that the Council will safeguard and enhance the rural areas of the borough from development that would adversely affect its biodiversity.

Tup Row Wood is within the West Pennine Moors management area. As a broadleaf woodland it is a Bolton, Greater Manchester and UK Biodiversity Action Plan habitat and a Section 41 habitat of the Natural Environment and Rural Communities Act 2006 for the purpose of conserving UK biodiversity.

The Council's Wildlife Liaison Officer (Greenspace section) has commented that the proposed tree loss is minimal and that the applicant has sufficiently justified why the trees need to be lost. They raise no objection.

It is therefore considered that the proposal would not harm biodiversity, compliant with Policy CG1.1 of the Core Strategy.

Impact on the Highway

Policy P5 of the Core Strategy states that the Council will ensure that developments take into account [amongst other things] accessibility by different types of transport and servicing arrangements. Policy S1.2 states that the Council will promote road safety in the design of new development.

The applicant has stated within their submission that the proposed junction improvement is required as the junction currently does not conform with HGV requirements for junctions. HGVs travel to and from the applicant's quarries at Montcliffe and Pilkington Quarries, which are accessed from Georges Lane. The applicant has referred to large vehicles having to use the opposite/wrong side of Georges Lane when they turn left into road from the Horwich town centre direction. The applicant believes that the proposal would improve road safety for all users of the junction, increase visibility towards the Horwich town centre direction, reduce the potential for accidents and reduce congestion by improving waiting times for vehicles emerging onto Chorley Old Road.

To support their proposal the applicant has submitted a statement detailing the alternative improvements to the junction they considered before proposing the application proposal. These were as follows:

- * A larger splay at the western corner of the junction - this was discredited on the grounds it would cause a noticeable reduction in trees in the area.
- * A smaller splay at the western corner of the junction - this was discredited as it would not effectively serve the purpose of markedly improving safety.
- * Not creating a splay at all - this was not seen as a viable option as it would result in the

continuation of the on-going problems at this junction.

- * Removal of just trees with no highway re-alignment - this was discredited as it would only solve half of the existing problem as it would not assist manoeuvrability.
- * Clearing and realigning land on the opposite side of the junction (eastern corner) - this was discounted as it would not resolve the current (and overriding) issue of vehicles approaching from Horwich (along Chorley Old Road) having difficulties negotiating the corner to access Georges Lane.

The applicant have confirmed that their transport consultants have advised that the application proposal proposes the minimum workable radius at the western corner of the junction.

Third parties have questioned why traffic lights could not instead be installed on Chorley Old Road, to allow vehicles to turn left into Georges Lane without oncoming vehicles. The Council's Highways Engineers have confirmed that there is no justification in terms of vehicle numbers/accident records for the Council to ask the applicant to signalise this junction and there would be (unjustifiable) financial implications for the applicant (approximately £140-180,000). Engineers have also confirmed that this suggestion may not be achievable from an engineering perspective owing to the nature of Chorley Old Road at the location (forward visibility from the bend to potential signal heads).

The Council's Highways Engineers have raised no objection to the proposed junction improvement and agree that the proposal would improve the ability for large HGV tipper-vehicles to turn at that location without encroachment onto the other side of road and would improve the visibility onto Chorley Old Road from the junction.

Engineers advise that the applicant would possibly need to enter into a legal S278 Agreement with the Highways Authority in order to under the implementation and supervision of work on the highway. Should the application be approved, the applicant will be made of aware of this through an informative note on the decision notice.

Third parties have raised concerns that the proposed junction improvement will facilitate an increase in vehicle movements to the quarries and using Georges Lane. The number of heavy vehicles that can visit the quarries is restricted by planning conditions on the original quarry planning applications to 120 movements a day (combined). The proposed junction improvement will not affect the applicant's requirement to conform to these conditions.

The proposed junction improvements will make it easier for articulated lorries to turn into Georges Lane, however the applicant has stated that they only expect such vehicles to visit the quarries approximately 15 times a year.

It is considered that the applicant has provided sufficient justification as to why the proposed works are required and it is considered that the proposal would improve highway safety, compliant with Policies P5 and S1.2 of the Core Strategy.

Conclusion

The proposed widening of Georges Lane at its junction with Chorley Old Road will result in the loss of two protected trees, however it is considered that the proposal would not detrimentally harm the character and appearance of the area (which is within Wallsuches Conservation Area) or the biodiversity of the woodland. It is considered that the proposed development would improve highway safety at the junction and that there are no suitable alternatives to be made at the junction.

Members are therefore recommended to approve this application subject to the suggested conditions.

Representation and Consultation Annex

Representations

Letters:- 38 letters of objection have been received. These raise the following concerns:

- * Loss of protected trees; Tup Row Woods are an important group of trees; Mature trees will be lost forever;
- * Impact on the character and appearance of Wallsuches Conservation Area;
- * Loss of plant biodiversity; Impact on the environment;
- * Impact on wildlife; Birds, owls and bats that are supported in the trees;
- * The trees help to muffle the sounds from the trucks;
- * The existing sightlines/visibility splays are adequate; No need for the proposed improvements;
- * There is not a problem with accidents at this junction;
- * The applicant had stated that the existing junction was appropriate within their original permissions for the quarries; Why is it inappropriate now?;
- * Highway safety concerns; Large vehicles need no further encouragement to come and go any faster than they already do;
- * Increase in traffic (*officer's comment: There is no proposal to increase vehicles movements to and from the quarries. Planning approval for both Montcliffe and Pilkington Quarries restricts the number of heavy vehicles visiting the quarries to 120 a day (combined);*
- * It appears that much larger vehicles (articulated lorries) are to be used following the proposal, as the submitted plan illustrates articulated lorries; There will be an increase in articulated lorries using Georges Lane;
- * The proposal is totally unnecessary given that the applicant has stated that articulated vehicles only visit the quarries approximately 15 times a year;
- * Pedestrian safety; pedestrians trying to cross the road in this location as the junction will be wider; Could a traffic island be introduced on Georges Lane? (*officer's comment: Highways Engineers have confirmed that it would be hard to accommodate a 2 metre pedestrian refuge at this location without it causing conflict with accessing/egressing HGVs. The swept-path analysis for a large HGV at the junction shows the tracking of the chassis covering the centre line of the carriageway);*
- * Could traffic lights not be installed instead on Chorley Old Road, to allow vehicles to turn left into Georges Lane without oncoming vehicles? (*officer's comment: Highways Engineers have confirmed that there is no justification in terms of vehicle numbers/accident records for the Council to ask the applicant to signalise this junction and there would be financial implications (approximately £140-180k). They also confirm that the suggestion may not be achievable from an engineering perspective owing to the nature of Chorley Old Road at the location (forward visibility from the bend to potential signal heads);*
- * Safety of cyclists and horse riders;
- * Loss of the traffic island (*officer's comment: it is not proposed to remove the traffic island on Chorley Old Road);*
- * Damage to Georges Lane owing to an increase in heavy vehicles; there is an urgent requirement for weight restrictions being imposed;
- * Increase in road noise and pollution (*officer's comment: as explained above, there is no proposed increase in vehicles to the quarries);*
- * Increase in surface water flooding;
- * At the last bend before the junction of Georges Lane there is a fork to a disused section of road connecting to Chorley Old Road. Why not reinstate this and avoid destroying part of Wallsuches Conservation Area?;
- * Concerns that the proposed junction improvements are to facilitate fracking (*officer's comment: there are no planning proposals for fracking in the area);*
- * Concerns that Armstrongs are wanting to improve the junction to facilitate a major scheme

(officer's comment: no other proposals are before the Council);

- * No specific employment figures are provided so it is impossible to consider the possible economic benefits of the proposals;
- * Loss of view from the Jolly Crofter's beer garden and neighbouring dwellings *(officer's comment: this is not a material planning consideration).*

Horwich Town Council:- raised an objection to the proposal at their meeting of 19th May 2016 on the grounds of loss of trees and the impact on the conservation area. Councillors queries why Armstrongs are asking for this now rather than in 2011 when the application for quarry operation was made.

Elected Members:- Cllr. Silvester has written in objection to the proposal for the following reasons:

- * An area of Tup Row Wood would be removed, which supports a variety of bird, mammal and insect life;
- * The widening would be a hazard for pedestrians crossing over an enlarged junction with the HGV movements that will occur;
- * The transport report submitted in 2011 states that the junction provides good visibility in both directions for all turning vehicles, therefore Cllr Silvester questions why the junction widening is needed;
- * The applicant has stated that there should be no adverse effect or noticeable impact on the highway at the Georges Lane junction from the quarry lifetime extension.

Chris Green MP has objected to the proposal as he believes that the number of dumper trucks travelling through the local area will be increased should the application be approved, thus leading to added congestion during the day, combined with the loss of trees and greenspace.

Consultations

Advice was sought from the following consultees: Highways Engineers, Tree Officers and Wildlife Liaison Officers.

Planning History

None concerning the application site.

Recommendation: Approve subject to conditions

Recommended Conditions and/or Reasons

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason

Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. No demolition, development or stripping of soil shall be started until:

1. The trees within or overhanging the site which are to be retained and are subject of a Tree Preservation Order (TPO) have been surrounded by fences of a type to be agreed in writing with the Local Planning Authority prior to such works commencing.
2. The approved fencing shall remain in the agreed location (in accordance with BS 5837:2012) until the development is completed or unless otherwise agreed in writing with the Local Planning Authority and there shall be no work, including the storage of materials, or placing of site cabins, within the

fenced area(s).

3. No development shall be started until a minimum of 14 days written notice has been given to the Local Planning Authority confirming the approved protective fencing has been erected.

Reason

To protect the health and appearance of the trees, and to comply with policies CG1.1 and CG3 of Bolton's Core Strategy.

3. Prior to the commencement of development details of the location, height and finished appearance of the relocated dry stone wall hereby approved shall be submitted to and approved in writing by the Local Planning Authority. The wall shall then be constructed in full accordance with the approved details within 3 months of the junction improvements having been completed, and retained thereafter.

Reason

To safeguard the character and appearance of the conservation area and to safeguard the health of the remaining trees, and to comply with policies CG1.1 and CG3 of Bolton's Core Strategy.

4. The highway widening and footway improvements hereby approved at the junction of Georges Lane/Chorley Old Road shall be constructed in accordance with drawing ref: 200/x-2.

Reason

In the interests of highway safety and in order to comply with policies S1 and P5 of Bolton's Core Strategy.

5. The new kerb line for the carriageway shall be hand dug.

Reason

To reduce the impact on the root system of the Ash tree and to comply with policy CG1.1 of Bolton's Core Strategy.

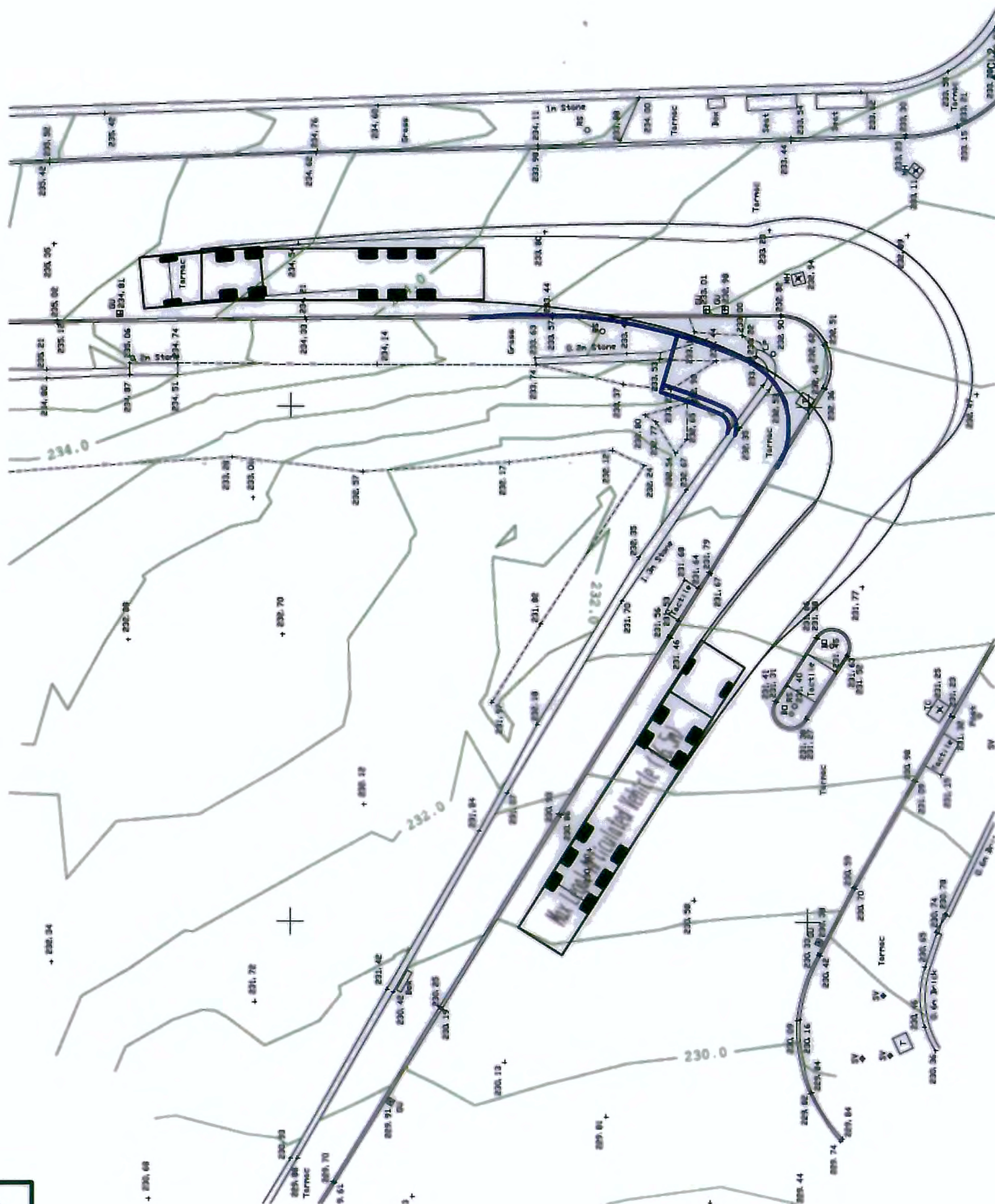
6. The development hereby permitted shall be carried out in complete accordance with the following approved plans:


200/x-2; "Visibility Splay"; dated March 2016

200/x - Tree 1; dated Apr' 16

Reason

For the avoidance of doubt and in the interests of proper planning.



Site: Makinson Road Gatehouse and George's Lane Visibility Splay	
Drawing: Visibility Splay	
Client: Armstrongs Aggregates Ltd.	
Date: March 2016	Client Ref: 200/X
Drawn by: CH	Scale: 1:5000 @ A3
KEY — Proposed Kerb Realignment	
Drawing No: 200/x - 2	
The Mineral Planning Group Fox Brow, Brow Lane Clayton, West Yorkshire BD14 6PT Tel: 01274 884599 Fax: 01274 884664 Email: headoffice@mpgyorks.co.uk www.mpgyorks.co.uk  © The Mineral Planning Group 2015	

The Mineral Planning Group
 Fox Brow, Brow Lane
 Clayton, West Yorkshire
 BD14 6PT
 Tel: 01274 884599
 Fax: 01274 884664
 Email: headoffice@mpgyorks.co.uk
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Application number
96401/16



Development & Regeneration Dept
Development Management Section

Town Hall, Bolton, Lancashire, BL1 1RU
Telephone (01204) 333 333

Bolton
Council

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Date of Meeting: 30/06/2016

Application Reference: 96401/16

Type of Application: Full Planning Application
Registration Date: 22/04/2016
Decision Due By: 16/06/2016
Responsible Officer: Helen Williams

Location: 379A CHORLEY NEW ROAD, HORWICH, BOLTON, BL6 6DT

Proposal: ERECTION OF FRONT DORMER (RESUBMISSION OF 94597/16)

Ward: Horwich North East

Applicant: c/o Neil Pike Architects

Agent : Neil Pike Architects

Officers Report

Recommendation: Refuse

Background

This application was deferred at the last Committee meeting for further details regarding the applicant's current fostering situation. The applicant has responded as follows:

"Our house currently has 2 children age 12 and 15, we currently don't have any foster children or foster babies with us at the minute due to our daughter having to move into the spare room, which is usually used for the foster children. Our daughter's old room is in the attic however the head height and size is no longer adequate for her as she can barely stand up straight in most parts of the room. The attic room is not allowed to be used for foster children as they have to be on the same floor as ourselves. The house isn't big enough for us to foster at the moment due to our daughter having to use the spare room now.

We currently have no foster children living with us due to the issue with the attic room, we usually foster 1-2 children at a time . We have 2 of our own children who also live with us."

Officers are still recommending that the application be refused, for the reasons discussed below.

Proposal

Councillor Kellett has requested that this application be heard before Planning Committee.

This application is a resubmission of planning application 94597/15, which was refused under delegated powers in January 2016 for the following reason:

"The proposed front dormer window would, by virtue of its design, size and siting, be detrimental to the character and appearance of the host dwelling and the street scene and is contrary to Policies CG3 and OA1 of Bolton's Core Strategy and Supplementary Planning Document - "House Extensions"."

The proposal was refused as SPD House Extension states in paragraph 5.11 that, in areas where dormers on the front of properties are absent, applications for new dormers on elevations which face a highway or on other prominent elevations are unlikely to be considered acceptable.

The proposed plans do not differ from those refused, however the applicant has submitted the following information [as summarised by the case officer] to support their proposal:

- * Their reason for the front dormer is that they foster and that there is a lack of space for their own children.
- * There is not much head height on the second floor for their children, owing to their age and height.
- * The lack of space is affecting their position to continue fostering for Bolton as all foster children have to be on the same floor as the foster parents (meaning their children have bedrooms on the second floor).
- * Their neighbours have raised no objection.
- * They do not want to move house as their children are happy and settled in school.
- * They do not overlook anyone.
- * A dormer at the front will not have a negative effect on the road.

Site Characteristics

The application property is a newly built four bedroom dwelling on the end of a row of four terraced dwellings. The dwelling has a single storey front element and an existing rear dormer window and front roof lights to accommodate use of the loft space.

On this side of Chorley New Road are rows of similarly designed terraced dwellings. None of the dwellings in the vicinity have front dormers.

The dwellings on this side of the road are set back from the main road on a secondary access road. Chorley New Road is a main thoroughfare to Horwich Town Centre.

Policy

National Planning Policy Framework (NPPF)

Core Strategies Policies: CG3 The Built Environment; CG4 Compatible Uses; OA1 Horwich and Blackrod.

SPD House Extensions
SPD General Design Principles

Analysis

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development

Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- * impact on the character and appearance of the dwelling and the surrounding area
- * impact on the amenity of neighbouring residents

Impact on the Character and Appearance of the Dwelling and the Surrounding Area

Policy CG3 of the Core Strategy states that the Council will conserve and enhance local distinctiveness, ensuring development has regard to the overall built character and landscape quality of the area, and will require development to be compatible with the surrounding area, in terms of scale, massing, grain, form, architecture, street enclosure, local materials and landscape treatment. Policy OA1 of the Core Strategy states that the Council will conserve and enhance the character of the existing landscape and physical environment, and will ensure that development in Horwich and Blackrod respects street patterns, the grain and the form of predominant architectural styles and where possible makes sympathetic use of locally distinctive materials such as stone.

SPD House Extensions provides general advice on house extensions and offers guidance relating to the effect of extensions on the appearance of the dwelling itself and the effect of extensions on the street scene and the character and appearance of the area.

Paragraph 5.11 of the SPD states that, in areas where dormers on the front of properties are absent, applications for new dormers on elevations which face a highway or on other prominent elevations are unlikely to be considered acceptable. Exceptions to this approach may however be made where it is considered that the proposed dormer will not affect the appearance of the dwelling or the character of the area as a whole. This may be the case where there is a variety or mix in the scale and appearance of nearby properties, or where the proposed dormer is appropriate in scale and has been carefully designed to appear as a feature of the original dwelling.

The dwelling has an existing rear dormer and roof lights within the front roof plane, to accommodate living space within the roof.

There are no dormers on the front of any other of the houses on this stretch of Chorley New Road. It could not be argued that there are a variety or mix of dwellings in the area, given that the dwelling is sited within a row of similarly designed dwellings.

The scale of the proposed dormer is large, in that it will cover almost the majority of the front roof plane (which is a lot more than the one quarter advised within paragraph 5.10 of the SPD where front dormers are acceptable). It is therefore considered that the proposed dormer would harm the character and appearance of the dwelling and the street scene, contrary to Policies CG3 and OA1 of the Core Strategy.

Impact on the Amenity of Neighbouring Residents

Policy CG4 of the Core Strategy states that the Council will ensure that new development is compatible with surrounding land uses and occupiers, protecting amenity, privacy, safety and security.

SPD House Extensions provides general advice on house extensions and offers guidance relating to the impact of extensions on neighbouring properties, particularly in relation to natural light, privacy and overlooking.

It is not considered that the proposed dormer would unduly harm the amenity of any neighbouring resident as it will not overlook any neighbouring dwelling. It is therefore considered that the proposal

complies with Policy CG4 of the Core Strategy.

Other Considerations

The applicant has submitted a statement explaining why they require the proposed front dormer window. This has been summarised within the proposal section of this report.

Officers do not consider that the applicant's reasons for requiring the front dormer outweigh the harm that would be caused to the street scene in this instance, nor do they justify a departure from the Council's policy guidance on house extensions.

Conclusion

For the reasons discussed above it is considered that the proposed front dormer window would harm the character and appearance of the host dwelling and the street scene, contrary to Policies CG3 and OA1 of the Core Strategy and the guidance contained within SPD House Extensions. Members are therefore recommended to refuse this application.

Representation and Consultation Annex

Representations

Horwich Town Council:- raised no objection at their meeting of 19th May 2016.

Elected Members:- Councillor Kellett has requested that the final determination of this application be made by the Planning Committee.

Consultations

Advice was sought from the following consultees: None.

Planning History

Planning application 94597/15 for the erection of a front dormer was refused under delegated powers in January 2016 for the following reason:

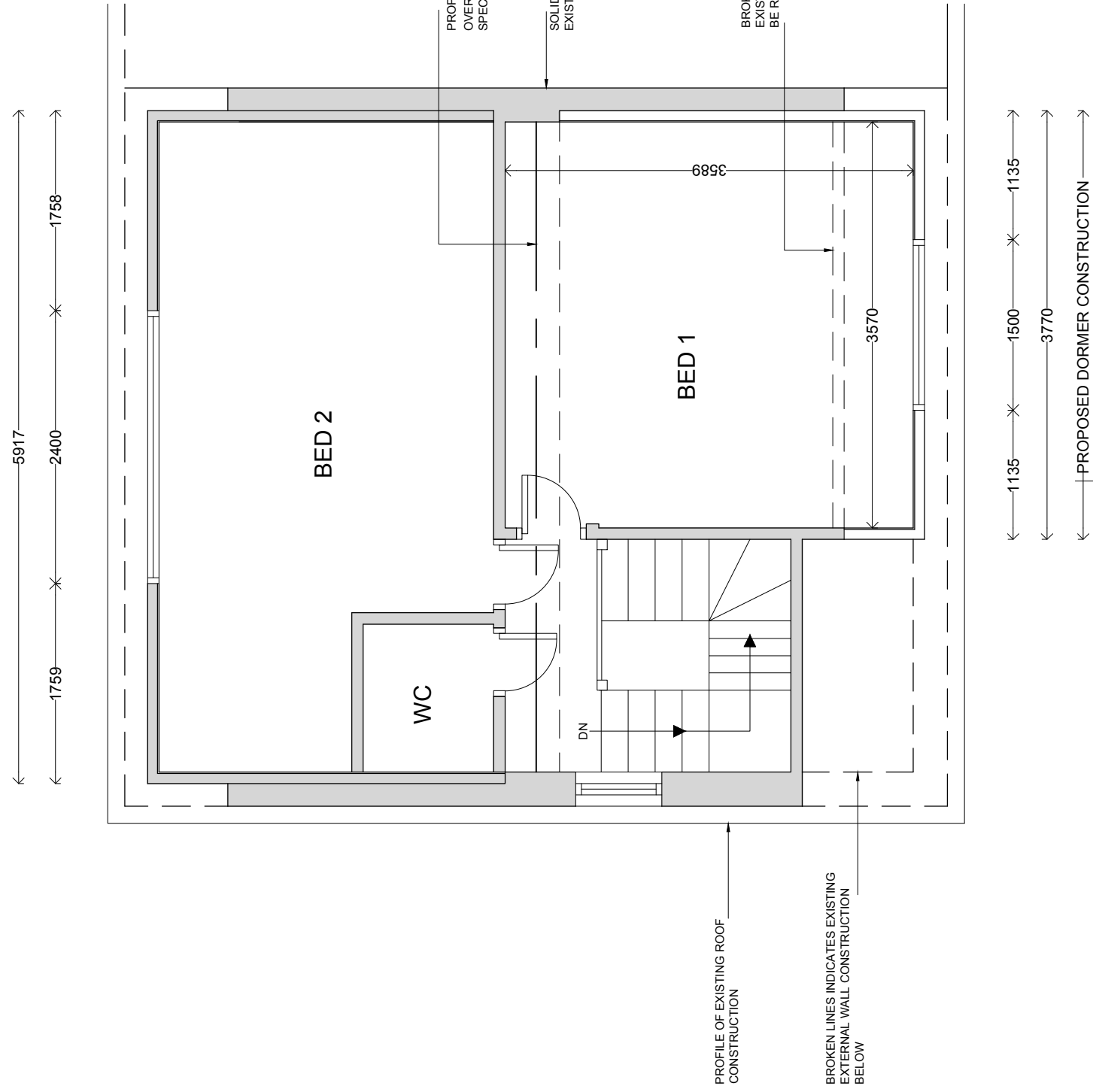
"The proposed front dormer window would, by virtue of its design, size and siting, be detrimental to the character and appearance of the host dwelling and the street scene and is contrary to Policies CG3 and OA1 of Bolton's Core Strategy and Supplementary Planning Document - "House Extensions"."

Recommendation: Refuse

Recommended Conditions and/or Reasons

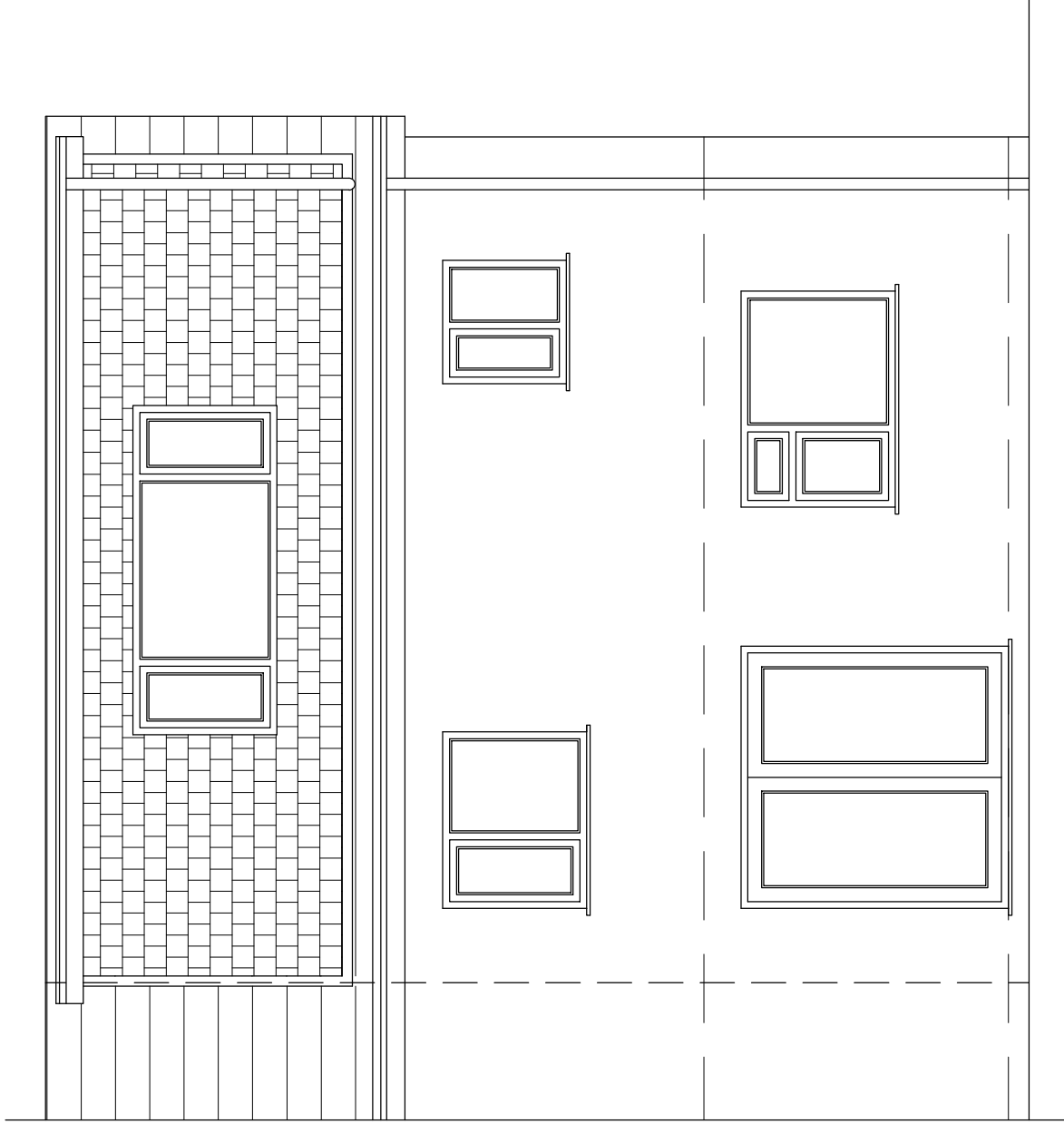
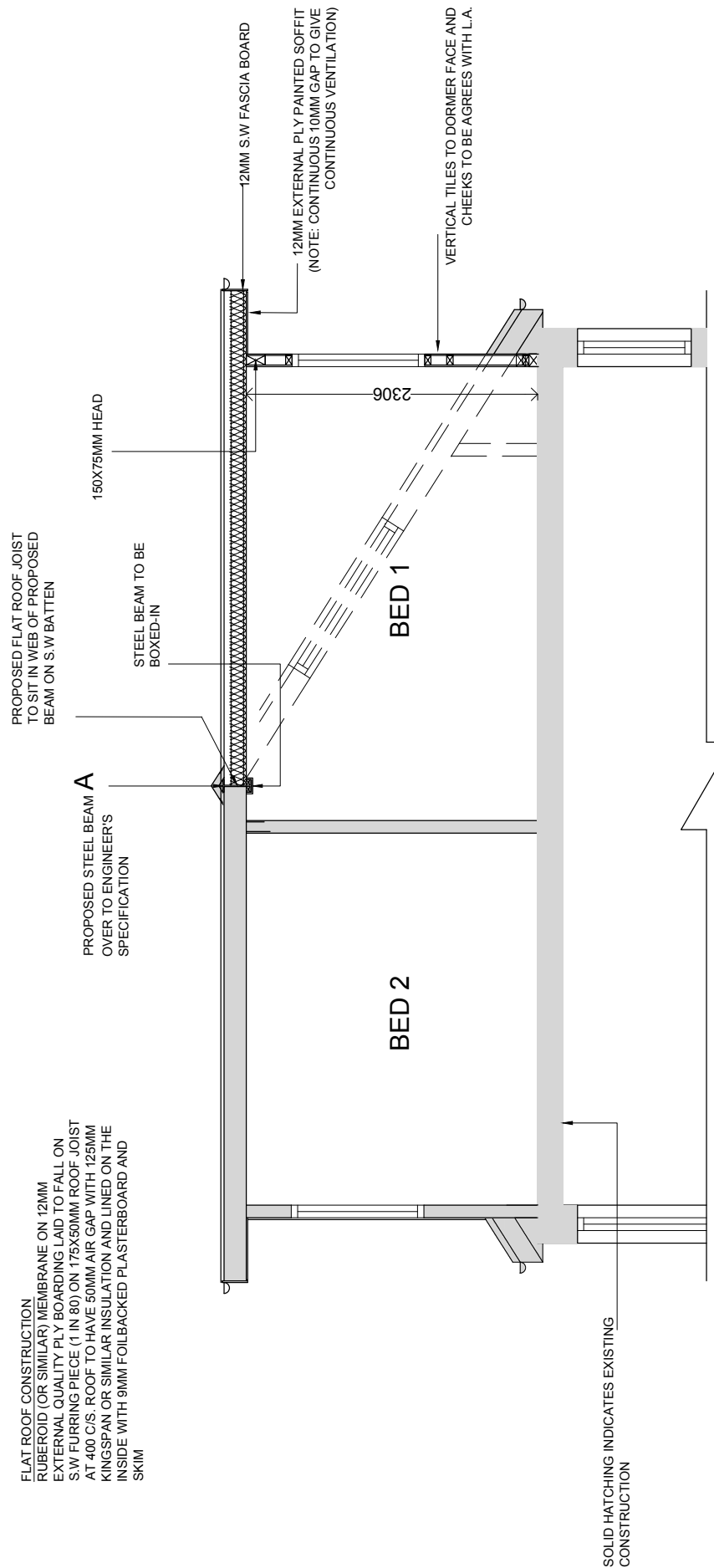
1. The proposed front dormer window would, by virtue of its design, size and siting, be detrimental to the character and appearance of the host dwelling and the street scene and is contrary to Policies CG3 and OA1 of Bolton's Core Strategy and Supplementary Planning Document - "House Extensions".

B

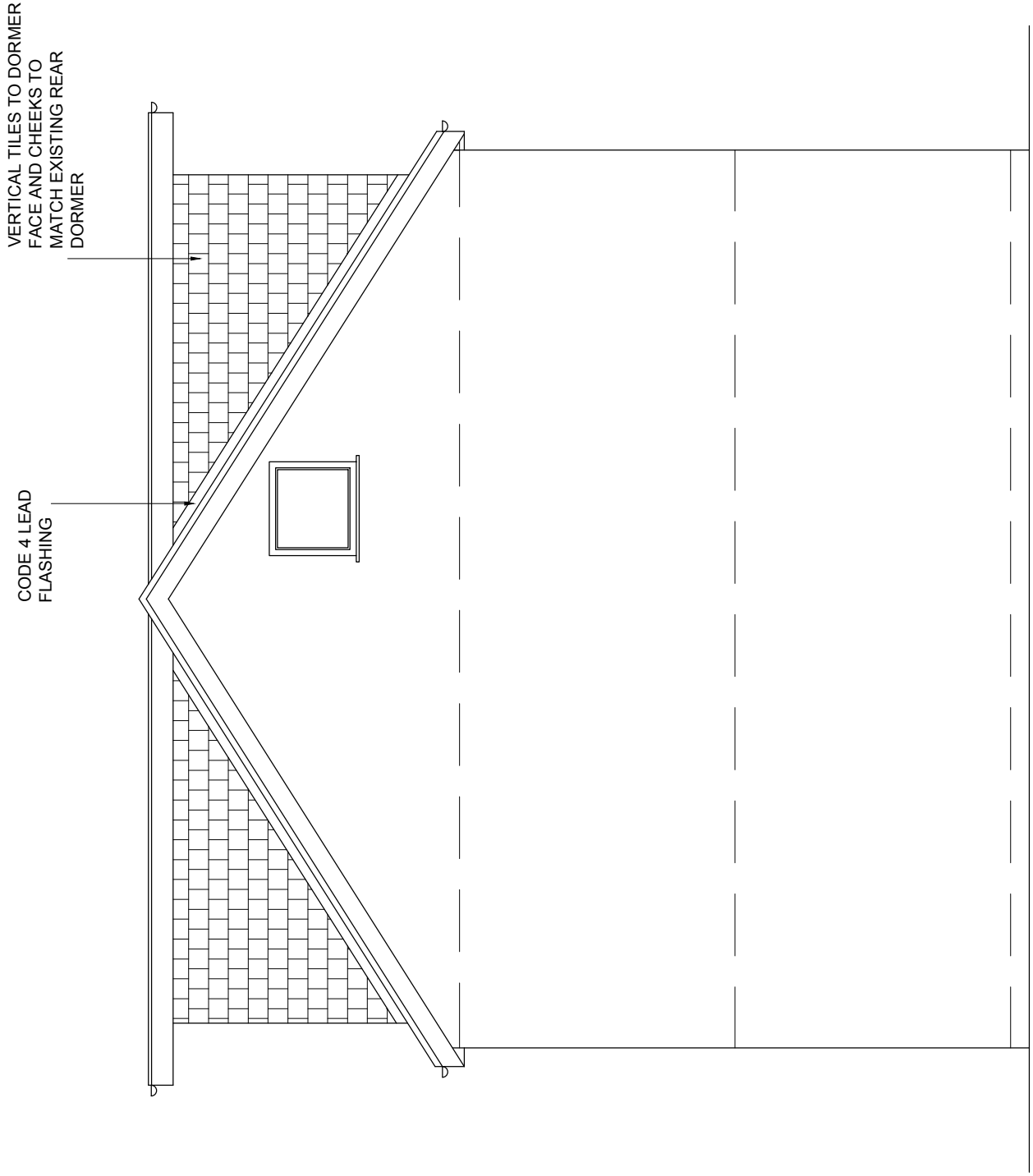


SECOND FLOOR PLAN

SECTION B-B

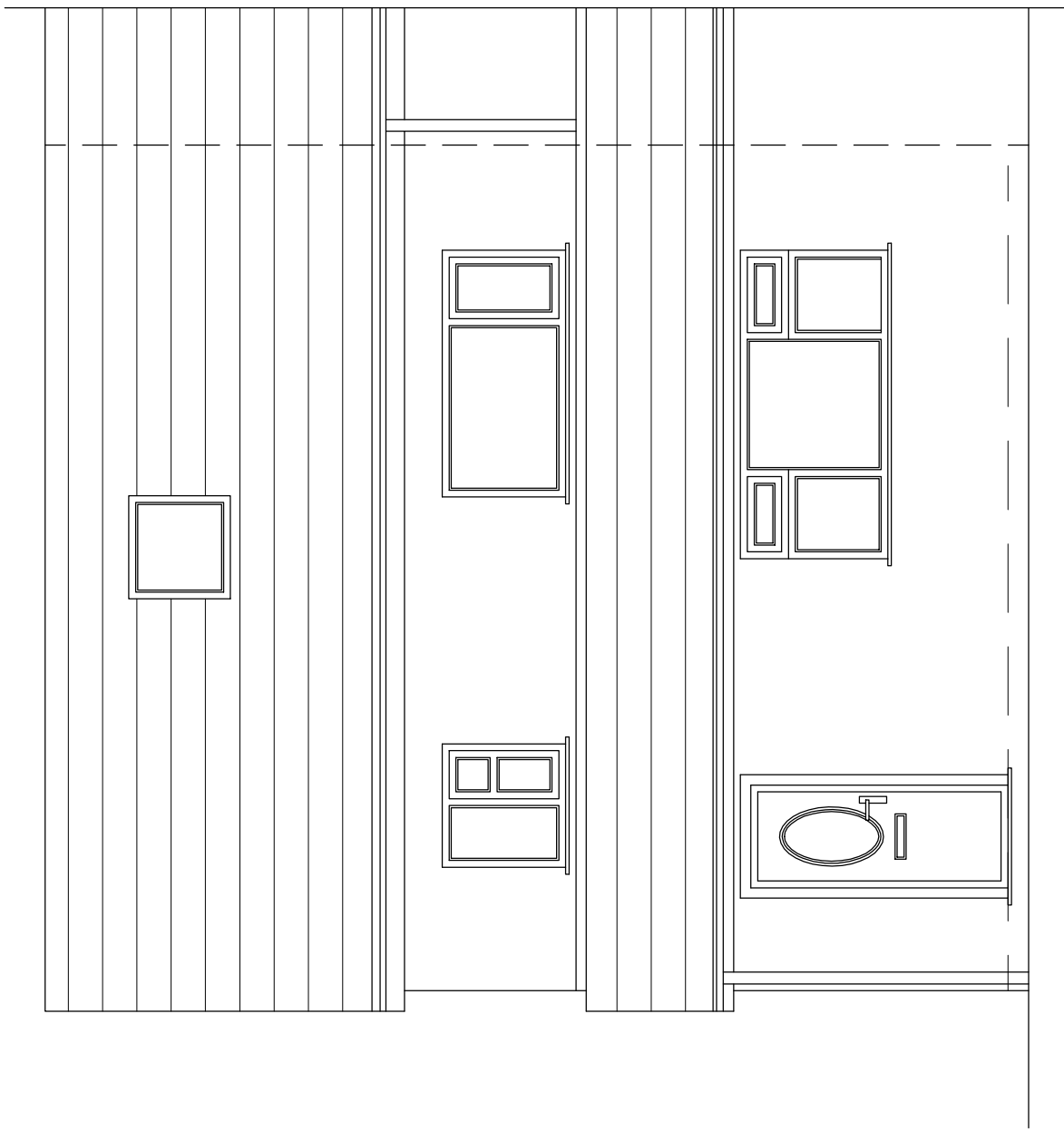


REAR ELEVATION

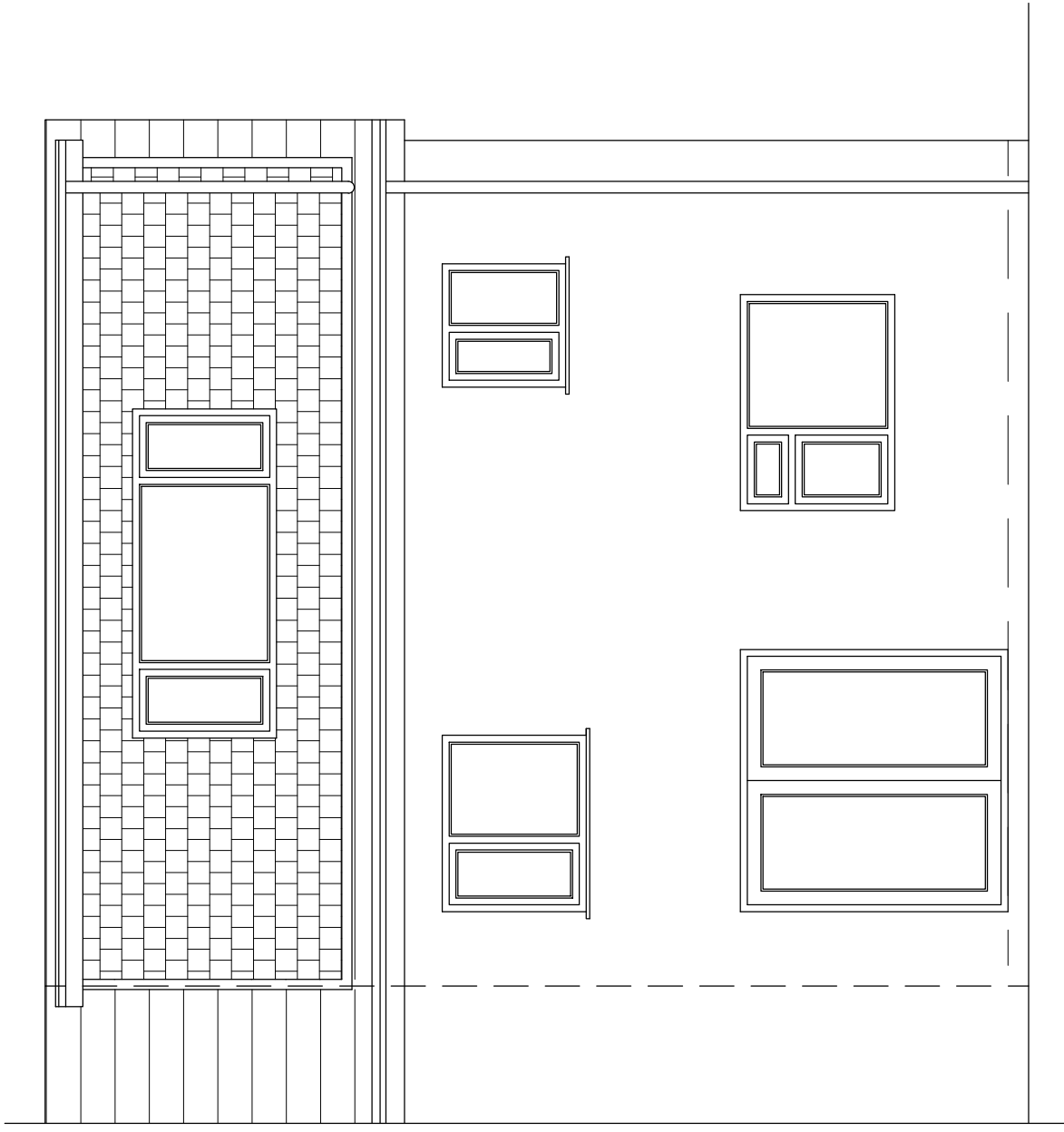


SIDE ELEVATION

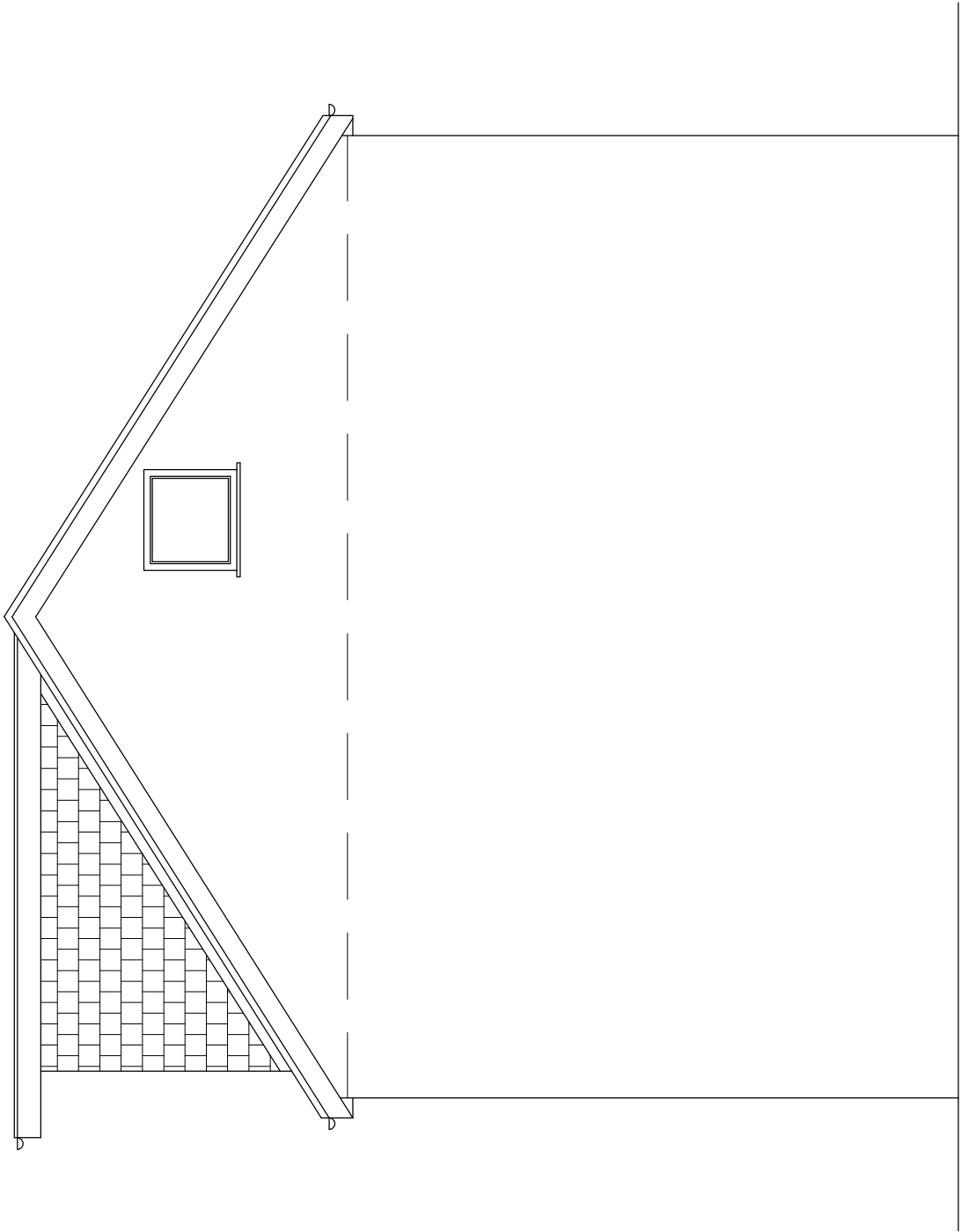
REV	DESCRIPTION
A	EXISTING REAR DORMER ADDED 18.05.16 R.S.P.
PROPOSED FLOOR PLAN, ELEVATIONS & SECTION	
PROPOSED FRONT DORMER	
379A CHORLEY NEW ROAD, BOLTON, BL6 6DT	
CLIENT:	CHADWICK
DWG NO:	K712/01
SCALE:	1/50 @ A1
DRAWN BY:	R.S.P.
DATE:	08/06/15
REV:	A
MICHIGAN HOUSE 17-19 CHORLEY NEW ROAD, BOLTON, BL1 4QR	
TEL: 01204 392233 FAX: 01204 528505	
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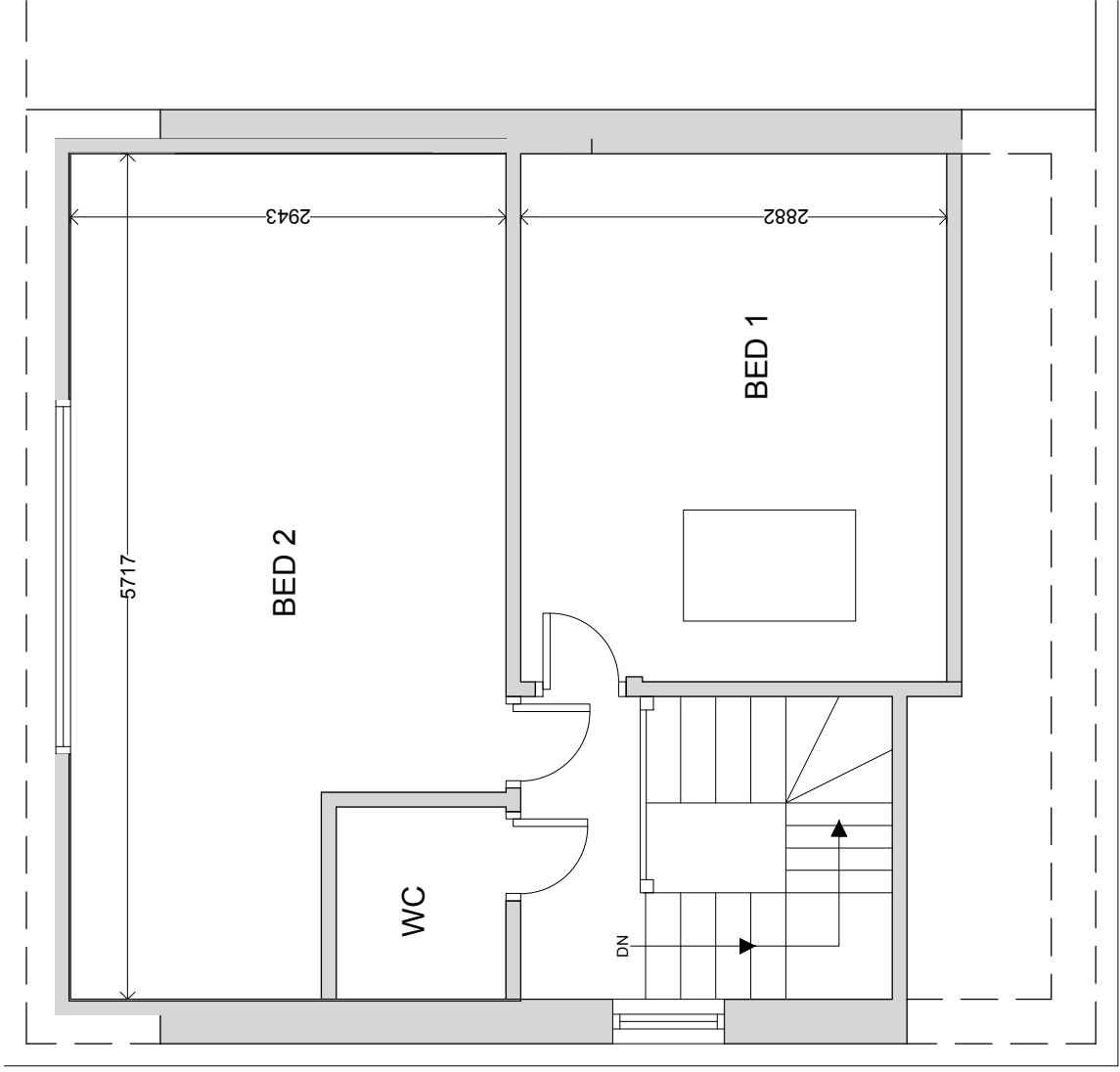
FRONT ELEVATION



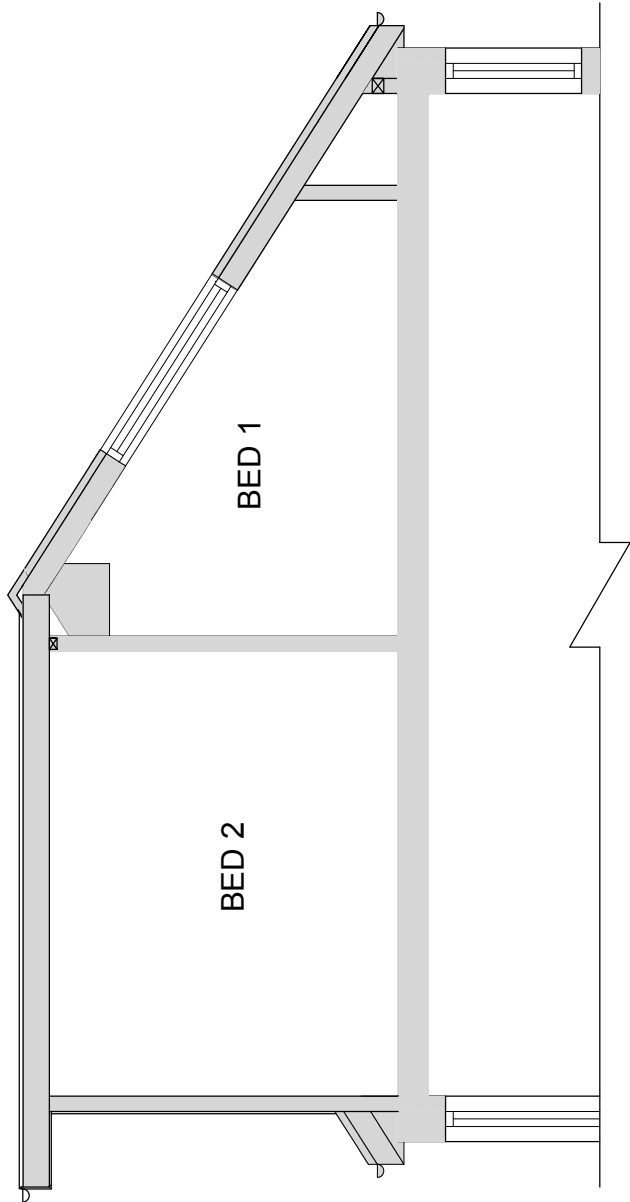
REAR ELEVATION



SIDE ELEVATION



SECOND FLOOR PLAN




SECTION B-B

REV	DESCRIPTION
A	EXISTING REAR DORMER ADDED 18.05.16 R.S.P.

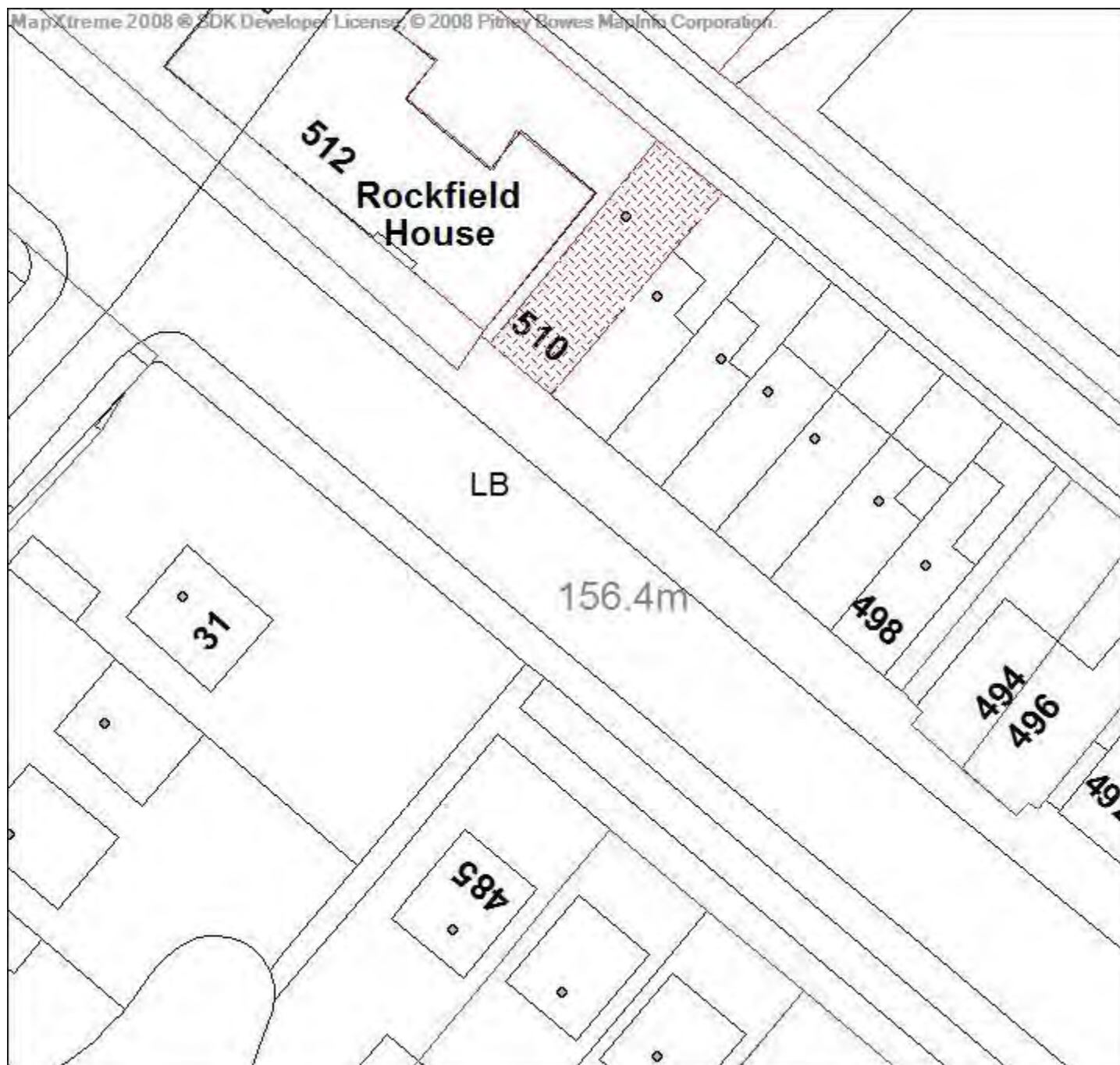
EXISTING FLOOR PLAN, ELEVATIONS & SECTION

PROPOSED FRONT DORMER
379A CHORLEY NEW ROAD,
BOLTON, BL6 6DT

CLIENT:	CHADWICK	 neil pike architects
DWG NO:	K712/01	
SCALE:	1/50 @ A1	
DRAWN BY:	R.S.P.	
DATE:	08/06/15	
REV:	A	

MICHIGAN HOUSE 17-19 CHORLEY NEW ROAD, BOLTON, BL1 4QR
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Application number
96440/16



Development & Regeneration Dept
Development Management Section

Town Hall, Bolton, Lancashire, BL1 1RU
Telephone (01204) 333 333

Bolton
Council

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Date of Meeting: 30/06/2016

Application Reference: 96440/16

Type of Application: Full Planning Application
Registration Date: 06/05/2016
Decision Due By: 30/06/2016
Responsible Officer: Paul Bridge

Location: 510 DARWEN ROAD, BROMLEY CROSS, BOLTON, BL7 9DX

Proposal: RETENTION OF NON REFLECTIVE EXTRACTION FLUE

Ward: Bromley Cross

Applicant: Mr Miah
Agent : PPY Design Ltd

Officers Report

Recommendation: Approve subject to conditions

Proposal

Consent is sought for the retention of a non-reflective extraction flue.

The extraction flue has been erected so that it runs internally through the building and protrudes through the rear roof slope and above the ridge line by approximately 600mm. The flue would be painted matt brown.

The premises are currently operating as a restaurant by way of a two year temporary consent. The temporary consent was permitted development by virtue of the Town and Country Planning (General Permitted Development) Order, Class D. Class D permits (amongst other things) the change of use a building and any land within its curtilage from a use falling within Class A1 (shops) to a flexible use falling within Class A3 (restaurants and cafes) for a single continuous period of up to 2 years beginning on the date the building and any land within its curtilage can also be used for the flexible use or on the date given in the notice. In this case the temporary period commenced on the 1st March 2016 and expires on the 1st March 2018.

Site Characteristics

The application site is an attractive, stone building fronting Darwen Road, Bromley Cross currently used as a restaurant. This section of Darwen Road is predominantly residential in character with commercial uses interspersed. Directly adjoining the application site is a residential dwelling at No.508 Darwen Road whilst adjacent to the site at 512 Darwen Road is Rockfield House (offices). To the rear of the site is a Back Darwen Road, beyond which is a bowling green.

Policy

National Planning Policy Framework 2012

Core Strategy: CG3 - The Built Environment, CG4 - Compatible Uses, OA5 North Bolton.

Analysis

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- * impact on the character and appearance of the area
- * impact on residential amenity

Impact on the Character and Appearance of the Area

Policy CG3 of the Core Strategy states that the Council will conserve and enhance local distinctiveness, ensuring development has regard to the overall built character and landscape quality of the area, and will require development to be compatible with the surrounding area, in terms of scale, massing, grain, form, architecture, street enclosure, local materials and landscape treatment. Policy RA2 of the Core Strategy relates specifically to developments in Farnworth and states that the Council will conserve and enhance the distinctive character of the existing physical and natural environment, will respect and strengthen the traditional grid-iron pattern and the street-scaping of existing houses where it is compatible with good urban design, and will require development to provide adequate privacy and amenity space.

The application involves the retention of an extractor flue to roof of the property. Due to its siting the flue is only visible from limited number of vantage points along both Darwen Road and Back Darwen Road. The flue is not large in scale and it is considered that the flue does not sufficiently affect the appearance of the existing building to warrant a refusal of the application on amenity grounds. The flue would be colour treated matt brown to soften its appearance and a condition has been attached to ensure this is carried out and maintained. In addition, the flue does not appear as an incongruous or alien feature within the street scene and is considered to be in accordance with policy CG3 of the Core Strategy.

Impact on Residential Amenity

Policy CG4 aims to ensure that new development is compatible with surrounding land uses and occupiers and protects amenity. Development should not generate unacceptable nuisance, odours, fumes, noise.

The area is predominantly residential in nature with residential terraced dwellings immediately adjacent to the site and office at No.512 Darwen Road.

As stated earlier in this report the premise is currently operating lawfully as a restaurant and therefore the restaurant requires an adequate extraction systems to help with the dispersal of odours.

Advances in technology have resulted in a situation where a modern fume extraction system (capable of eliminating almost all fumes associated with the cooking process) can be installed into almost any building. Whilst some information has been submitted in terms of the siting, this is limited in terms of technical detail and does not include the required level of details and specifications. The Councils Environmental Health Officers have been consulted and have no objections to the development subject to the attachment of a condition requiring full details of the fume extraction system to be submitted to and approved in writing by the Local Planning Authority. It is considered that the extraction unit would not result in neighbouring residents experiencing an unacceptable reduction in amenity by virtue of smells and odours.

As such, it is considered that the flue does not result in an unacceptable impact upon the living conditions of nearby residents. The proposal is therefore considered to be in accordance with Core Strategy Policy CG4.

Conclusion

The development does not result in an unacceptable impact on the visual amenity of the area, nor upon the living conditions of nearby residents in accordance with Core Strategy Policy CG4.

The application is recommended for approval.

Representation and Consultation Annex

Representations

Letters:-

Six letters of objection have been received, which raise the following concerns:

- Out of character with the area,
- Odours.
- The above issues have been addressed in the appraisal.
- The use of the premises as a restaurant in this location is unacceptable. *Response:- This application is for the retention of a extraction flue only and any issues in relation to the temporary lawful use of the premises as a restaurant cannot be considered in the determination of this application.*
- The premise is operating a takeaway service. *Response:- The alleged operation of a takeaway service is a matter for planning enforcement.*

Consultations

Advice was sought from the following consultees: Highways, Environmental Health

Planning History

92686/14 – Notification of Temporary Change of Use (Class D) from A1 retail to A3 Restaurant.

94183/15 - Notification of Temporary Change of Use (Class D) from A1 retail to A3 Restaurant.

96441/16 – Retention of externally illuminated fascia sign. (Decision Pending)

Recommendation: Approve subject to conditions

Recommended Conditions and/or Reasons

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason

Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. The development hereby permitted shall be carried out in complete accordance with the following approved plans:

PM6-01 Existing plans and elevations, dated April 2016.
PM6-00 Location Plan, dated April 2016
PM6-02 Block Plan, dated April 2016

Reason

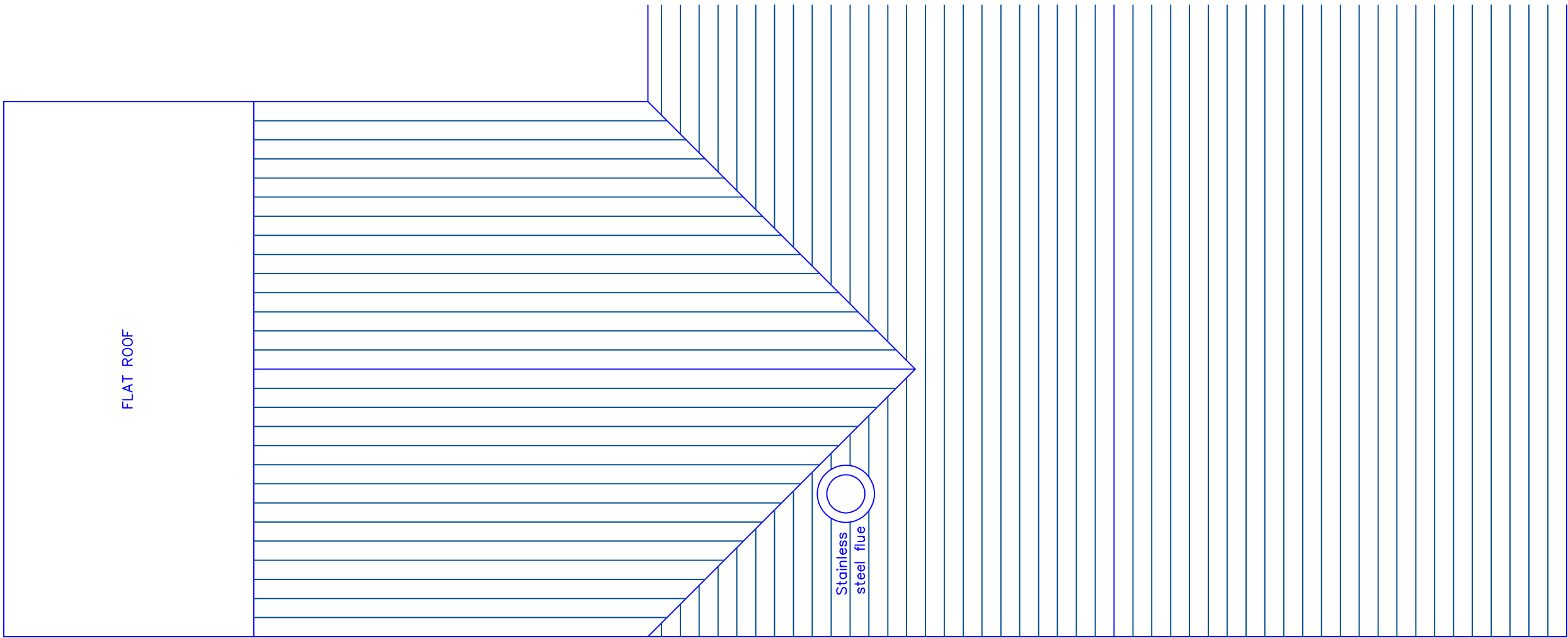
For the avoidance of doubt and in the interests of proper planning.

3. Within one month of the date of this permission full details of the air extraction equipment, including measures to alleviate noise, vibration, fumes and odours (and incorporating active carbon filters, silencers and anti-vibration mountings as necessary), shall be submitted to and approved in writing by the local planning authority. All equipment shall be then maintained/upgraded in accordance with these approved details and shall thereafter be operated, maintained and serviced in accordance with those approved details.

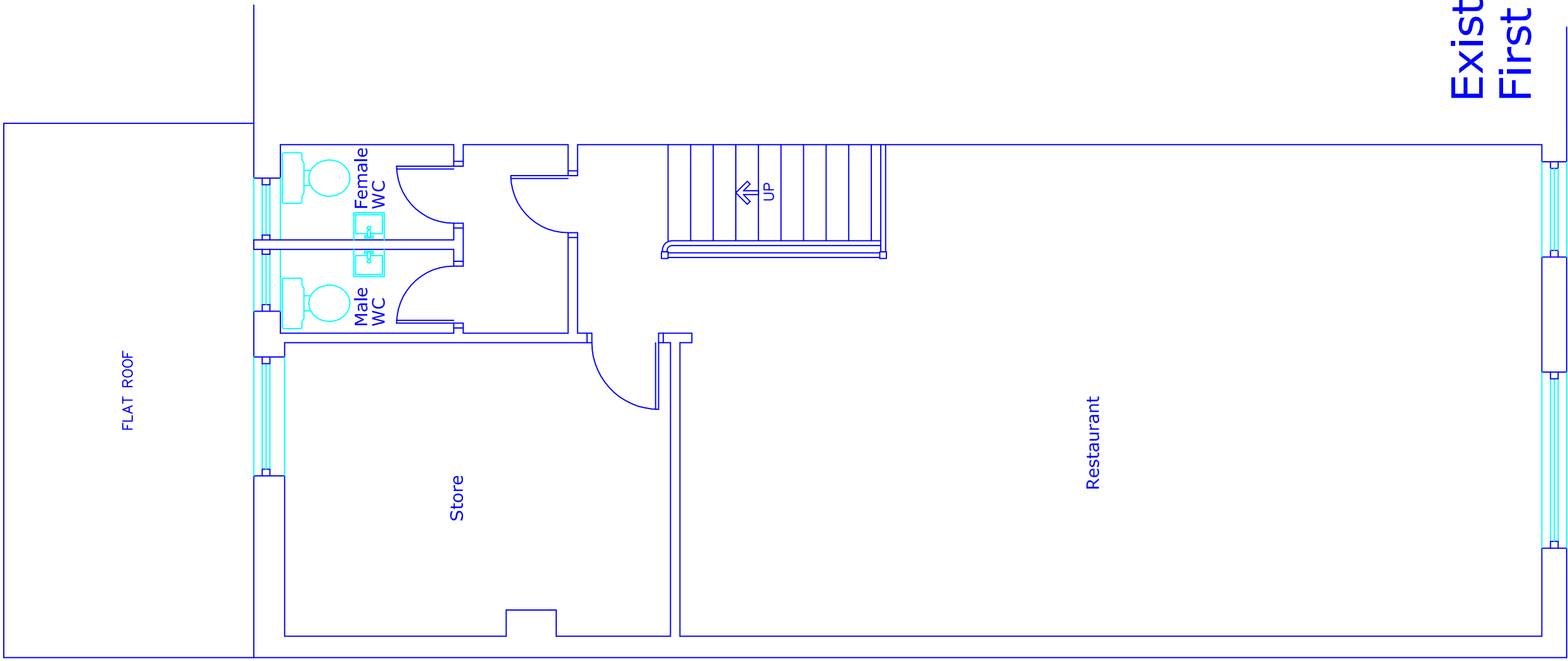
Reason:- In the interests of residential amenity.

4. Within one month of the date of this decision the approved flue shall be coloured treated matt brown and retain thereafter.

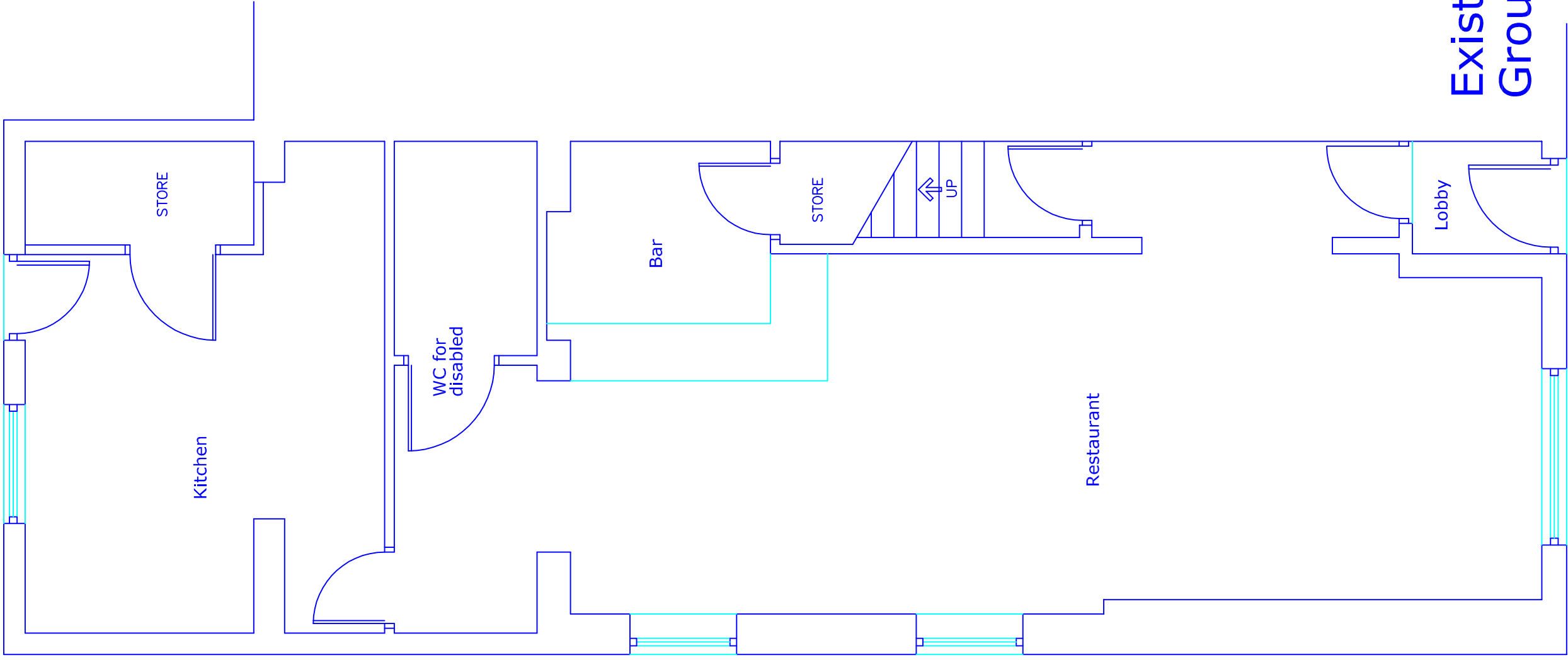
Reason:- In the interests of visual amenity.



Existing Plan
Roof Level



Existing Plan
First Floor

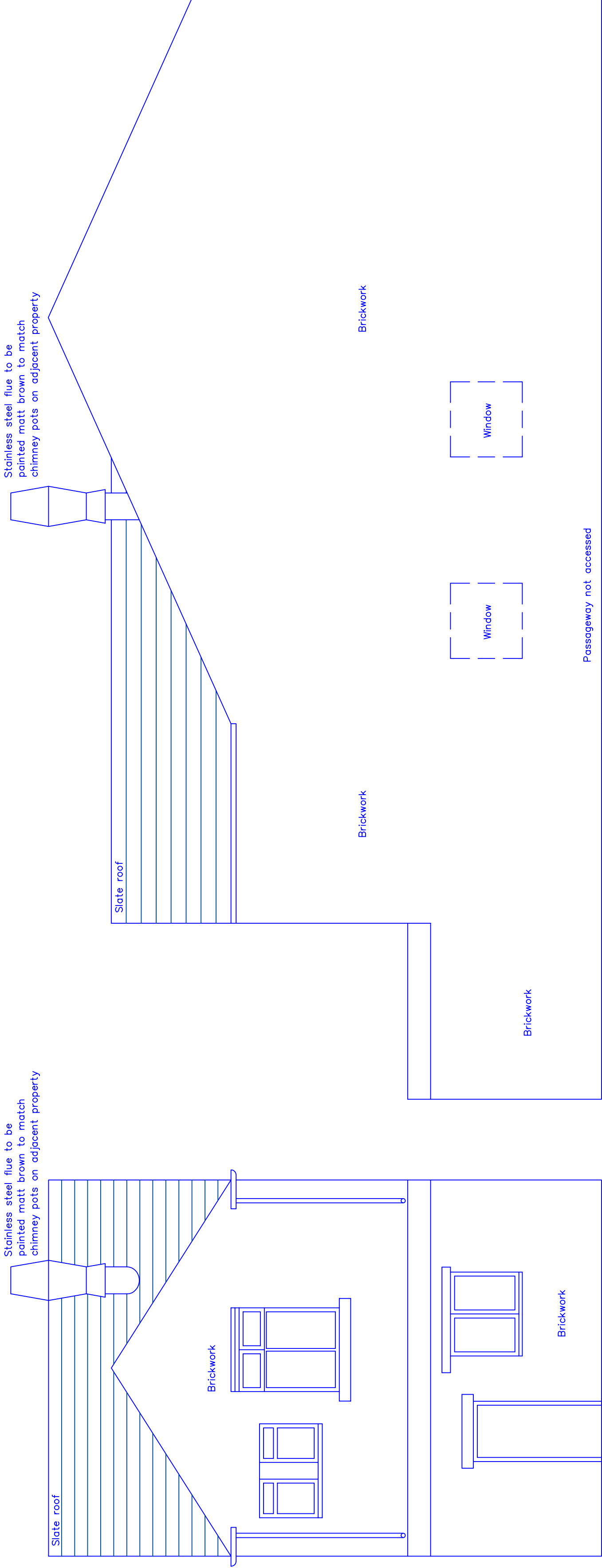


Existing Plan
Ground Floor

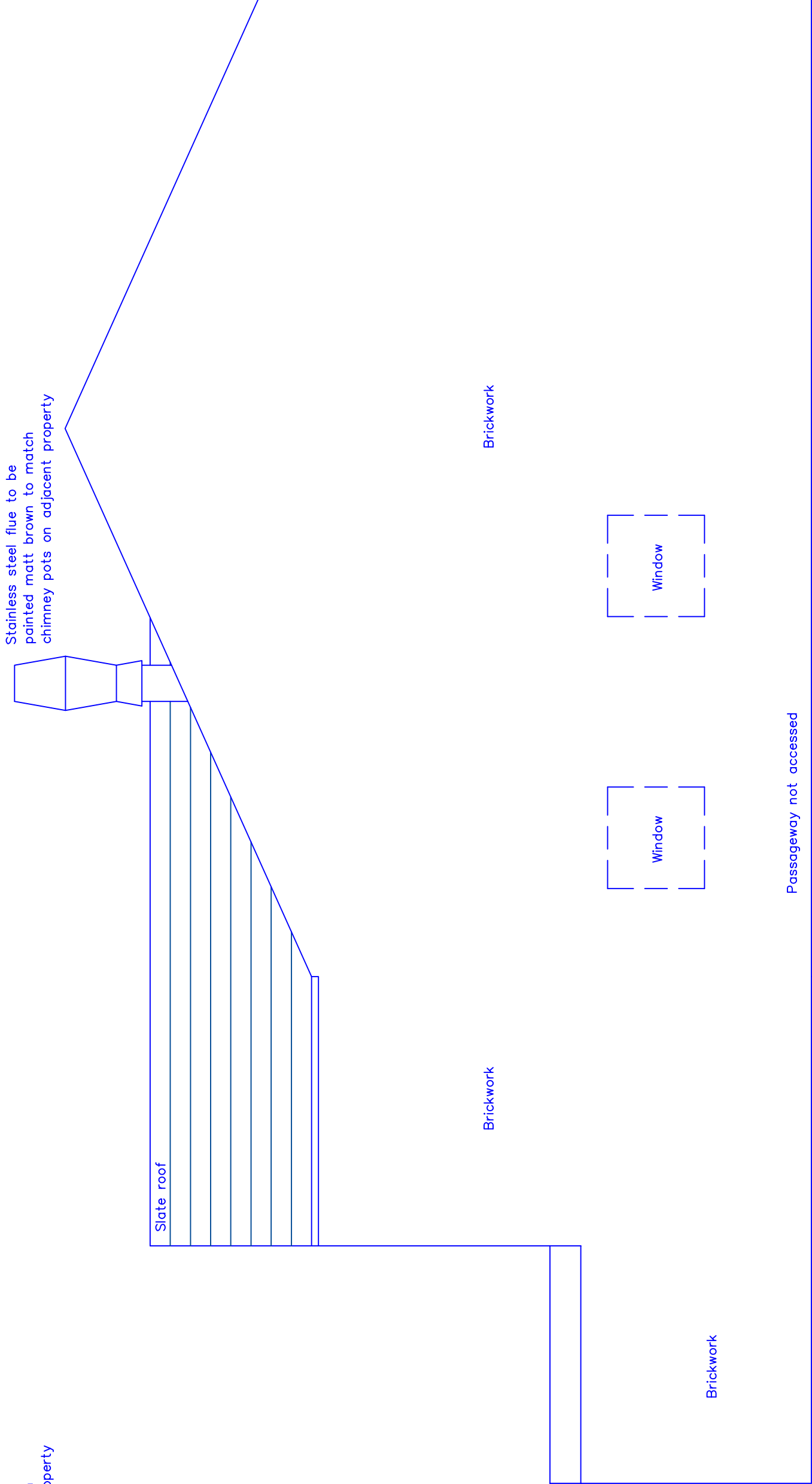
Stainless steel flue to be
painted matt brown to match
chimney pots on adjacent property

Stainless steel flue to be
painted matt brown to match
chimney pots on adjacent property

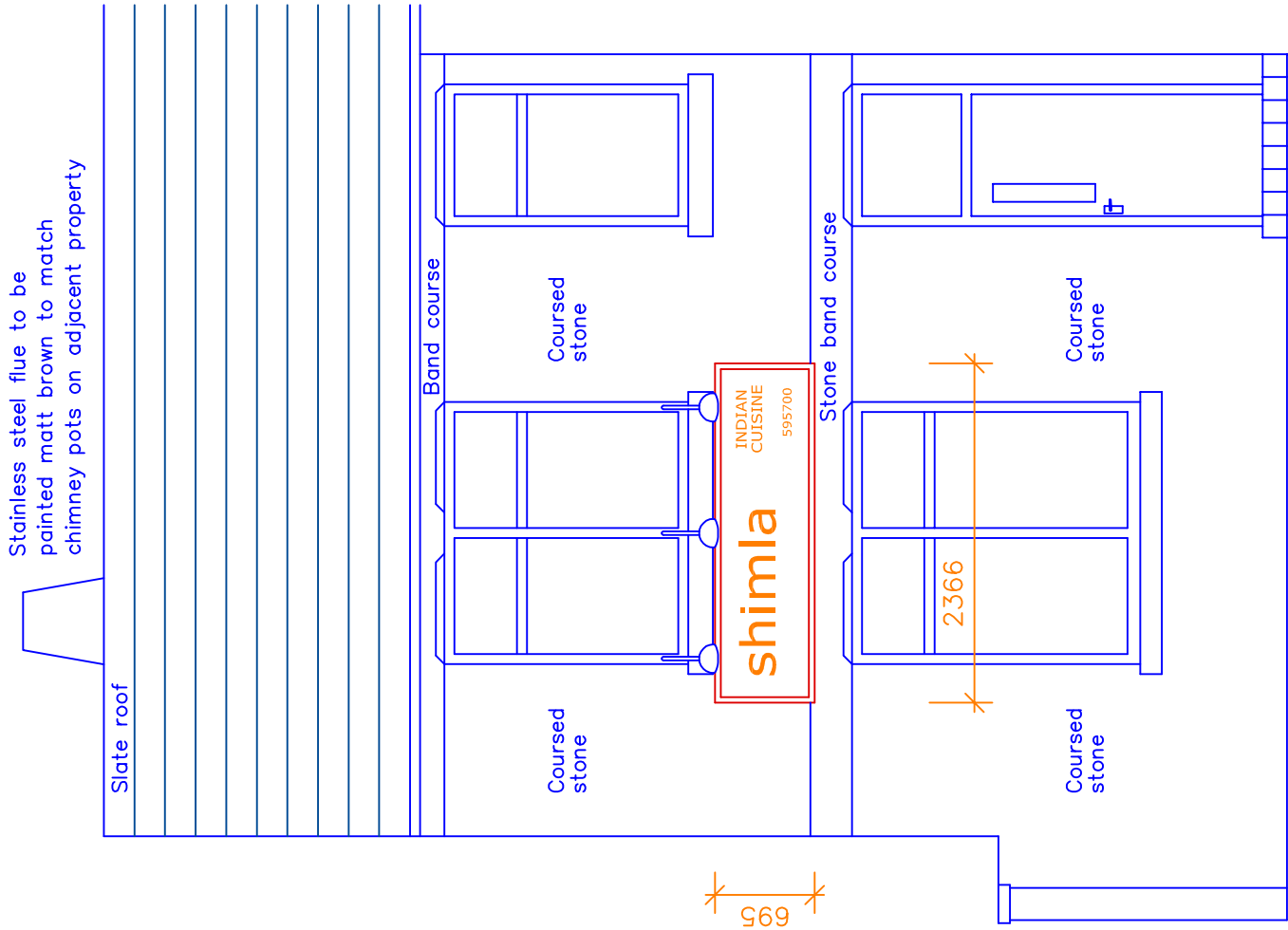
Stainless steel flue to be
painted matt brown to match
chimney pots on adjacent property



Extg Rear Elevation



Extg Side Elevation



Extg Front Elevation

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0123 m

510 Darwen Road
Bromley Cross
Bolton
BL7 9DX

scale 1:50 @ A1
April 2016

PM6-01

Existing Plans
& Elevations

PPY design Ltd

2 Helmshore Road, Holcombe Village
Ramsbottom, Lancashire BL8 4PA

Gary Dearden BAK(hons) DipArch RIBA
gary@ppy-design.co.uk www.ppy-design.co.uk
T. 01706 622174 M. 07712 669482

Application number 96444/16



Development & Regeneration Dept
Development Management Section

Town Hall, Bolton, Lancashire, BL1 1RU
Telephone (01204) 333 333

Bolton Council

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Date of Meeting: 30/06/2016

Application Reference: 96444/16

Type of Application: Full Planning Application
Registration Date: 29/04/2016
Decision Due By: 23/06/2016
Responsible Officer: Jeanette Isherwood

Location: 7A BOLTON ROAD, FARNWORTH, BOLTON, BL4 7JN

Proposal: ERECTION OF FIRST FLOOR EXTENSION TO SIDE AND DORMER TO REAR.

Ward: Farnworth

Applicant: Mr Varsani
Agent : Mr Patel

Officers Report

Recommendation: Approve subject to conditions

Proposal

Planning permission is sought for the erection of a first floor extension and rear dormer.

The proposed first floor extension would be 1.9 metres wide and extend the length of the building above the existing single storey element.

The proposed rear dormer would be double in width with the cheeks finished with vertical hung tiles. The proposed roof space conversion would incorporate two bedrooms, an ensuite and a further bathroom which would increase the existing residential accommodation to a three bedroomed unit.

The proposed materials for the extension would be render and tile.

Site Characteristics

The application site is located at the end of a small row of residential terraced dwellings on Bolton Road. The ground floor of the property is in commercial use as a newsagents/convenience store whilst the first floor incorporates a two-bedroomed residential flat. Bolton Road is main road which is characterised by a mix of residential and commercial uses.

The application premises is a two storey brick rendered building comprising a retail unit with ancillary living accommodation at first floor level. To the rear of the property is a railway embankment beyond which is a railway line. To the west of the site is small triangular shaped piece of grass land that houses an advertisement hoarding. Directly opposite the site are residential dwellings and a car park for a commercial use. The adjacent residential dwelling at 7b has a two storey outrigger which projects along the common boundary of the application site and has a window in the side elevation at ground floor level.

To the side is a car port/canopy with existing access off Bolton Road.

Policy

National Planning Policy Framework (2012)

Core Strategy policies:- P5 Accessibility, S1 Safe Bolton; CG1 Cleaner and Greener, CG3 The Built Environment, CG4 Compatible Uses, RA2 Farnworth.

SPD House Extensions

Analysis

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- * impact on the character and appearance of the area
- * impact on residential amenity
- * impact on parking and highway safety

Impact on the Character and Appearance of the Area

Policy CG3 of the Council's Core Strategy seeks to ensure that new development conserves and enhances local distinctiveness, having regard to the overall built character of the area and landscape quality of the area. In addition, Policy CG3 also aims to ensure that all new development is designed in a manner which is accessible to all.

The proposed first floor side extension would be visible within the street scene. It would be constructed from brick with a cream rendered finish and grey roof tiles. Given the size and design of the proposed extension, it is considered that the proposed first floor side extension would constitute a limited extension and would not result in a disproportionate addition over and above the size of the original building. In respect of the rear dormer, although large in size it would not be visible from within the street scene and is not directly overlooked to the rear. Its design is considered to be acceptable. A condition has been attached to ensure that materials are similar in appearance to the existing building.

It is considered therefore that the proposals would not have an unacceptable appearance on the character of the building or wider surrounding area in accordance with Policy CG3 of the Core Strategy.

Impact on Residential Amenity

Core Strategy policy CG4 seeks to ensure that new development is compatible with surrounding land uses and occupiers, protecting amenity, privacy, safety and security.

This first floor element of the proposal would run flush with both the main front and rear elevations of the building and would therefore not be visible from within the adjoining residential dwelling at No.7b Bolton Road. In addition there are no residential dwellings to the rear of the site.

The proposed dormer is not visible from the main road and is not directly overlooked by residential properties.

Notwithstanding the above objection the proposal due to its size and siting is not thought to restrict light to any neighbouring properties.

Given the above, the nature of the intended use of the extension and the position of the proposed extensions, it is considered the proposals would not result in any unacceptable impact on the amenity of neighbouring premises. The proposed development is in accordance with policy CG4 of the Core Strategy and SPD 'House Extensions'.

Impact on Parking and Highway Safety

Core Strategy policy S1 seeks to promote road safety in the design of new development. Core Strategy policy P5 seeks to ensure that development is accessible by different types of transport, prioritising pedestrians, cyclists and public transport over the motorised vehicle.

The site has existing parking to the side and the proposal would not result in the loss of the existing car parking spaces (under the canopy). Highways Officers were consulted on a previous larger scheme and their comments remain relevant to this proposal. It is considered that there is sufficient parking on site and it is not considered there would be a materially significant increase in the number of staff/visitors as the proposal would be for residential accommodation. Consequently, it is not considered that the proposal would have any adverse impact upon highway safety. Therefore, it is considered that the proposal would not cause any significant harm to highway safety, subject to conditions and is in accordance with the NPPF and policy P5 of the Core Strategy.

Conclusion

For the reasons discussed above the proposal is considered to be compliant with Core Strategy Policies P5, S1, CG3, CG4, RA2 Farnworth and SPD - House Extensions.

Members are requested to approve the proposal subject to conditions

Representation and Consultation Annex

Representations

Letters:- One objection has been received, raising the following issues:-

- the extension would block or restrict light to my property and it is within the deeds of the property to reserve the free and uninterrupted access of light and air at all times and hereafter and in the same manner as now exists to the windows or openings in the buildings belonging to me
- the planning application would clearly restrict the light and if granted would legally be in breach of my deed.
- The dormer would not fit the profile of any of the houses on Bolton Road and would stand out

and affect the area.

Consultations

Advice was sought from the following consultees: Highways

Planning History

Permission granted for a two storey side and rear extension, single storey rear porch with balcony above under ref 93892/15

Application 62134/02 for the retention of an internally illuminated sign was refused in 2002.

Permission was granted in 1982 for the retention of a new shop front (19245/82).

Recommendation: Approve subject to conditions

Recommended Conditions and/or Reasons

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason

Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. Before the approved development is first brought into use no less than 2 car parking spaces with minimum dimensions of 2.5 metres by 5.5 metres shall be provided within the curtilage of the site, in accordance with a scheme to be submitted to and approved by the Local planning Authority. Such spaces shall be made available for the parking of cars at all times the premises are in use.

Reason

To ensure that adequate provision is made for vehicles to be left clear of the highway

3. Prior to the development hereby approved being first occupied or brought into use, a scheme detailing how parts of the site to be used by vehicles are to be laid out, constructed, surfaced, drained and lit shall be submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be implemented in full and thereafter made available for the use of vehicles at all times the development is in use.

Reason

To encourage drivers to make use of the parking and circulation area(s) provided.

4. Prior to the development hereby approved being first occupied or brought into use a scheme shall be submitted to and approved in writing by the Local Planning Authority for the turning of vehicles within the curtilage of the site [to enable vehicles to enter and leave the site in forward gear]. The approved scheme shall be implemented in full before the development is first brought into use and retained thereafter and not to be used for any purpose except the turning of vehicles.

Reason

In the interests of highway safety.

5. The development hereby permitted shall be carried out in complete accordance with the following approved plans:

Proposed Floor Plans & Elevations - Dwg No. 01 Rev U - dated April 2016

Reason

For the avoidance of doubt and in the interests of proper planning.

6. The external surfaces of the extension hereby permitted shall be of a similar colour, texture and size of those of the existing building, and shall be retained thereafter.

Reason

To ensure the development visually reflects the existing building.

7. The dormer front(s) and cheeks shall be clad in materials similar to the existing roof.

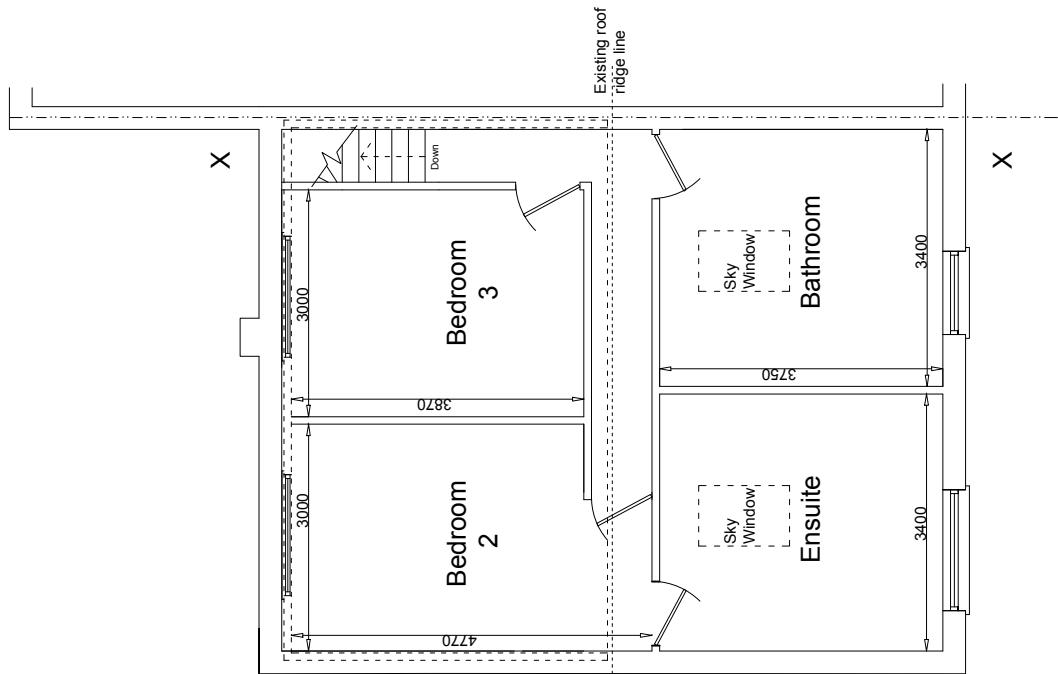
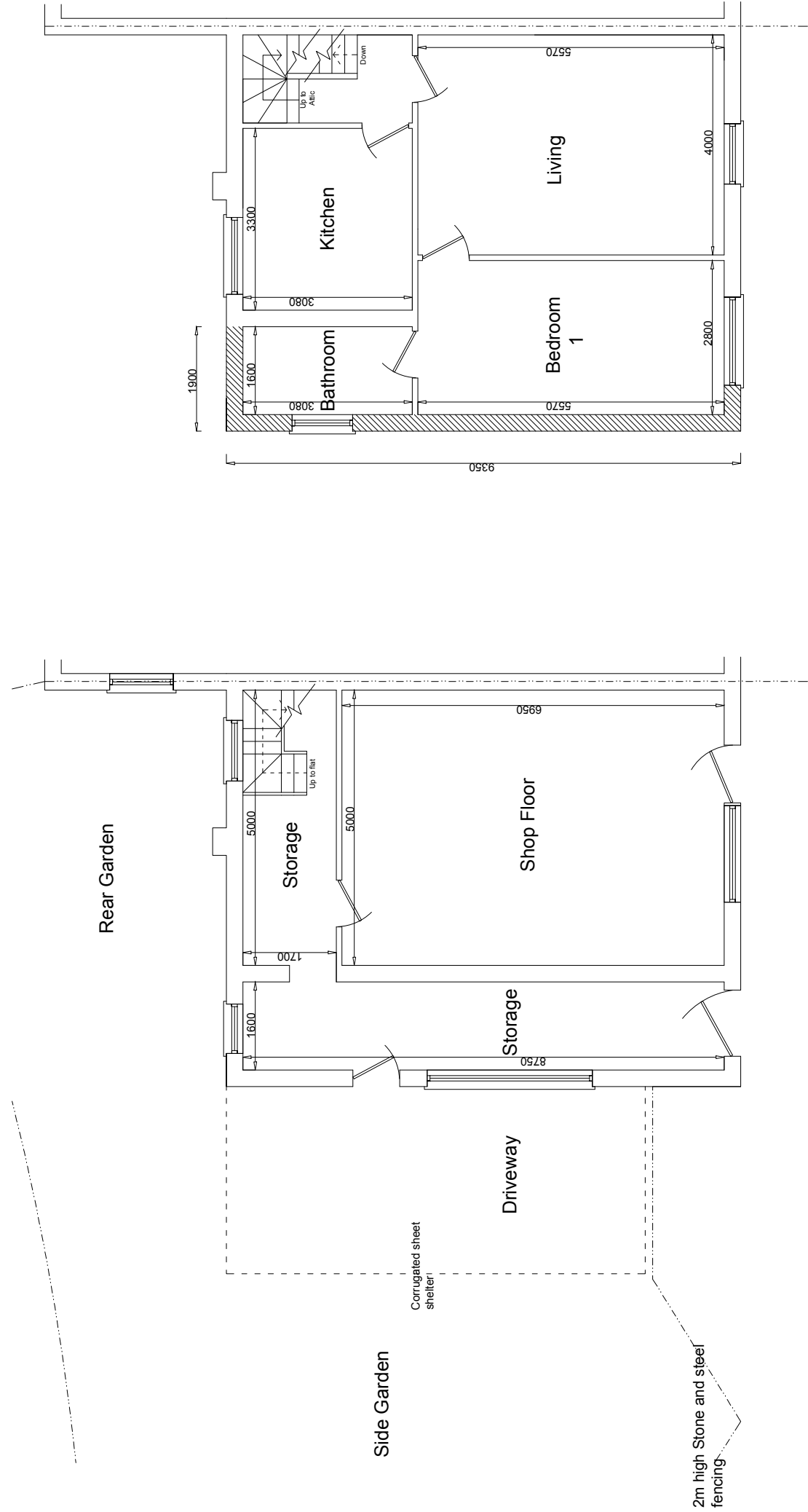
Reason

To ensure the development safeguards the visual appearance of the building.

7A Bolton Road, Farnworth, Bolton

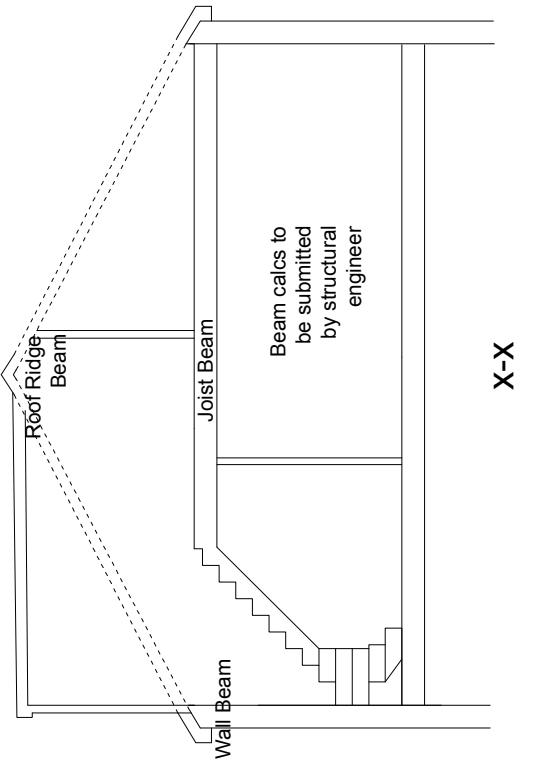
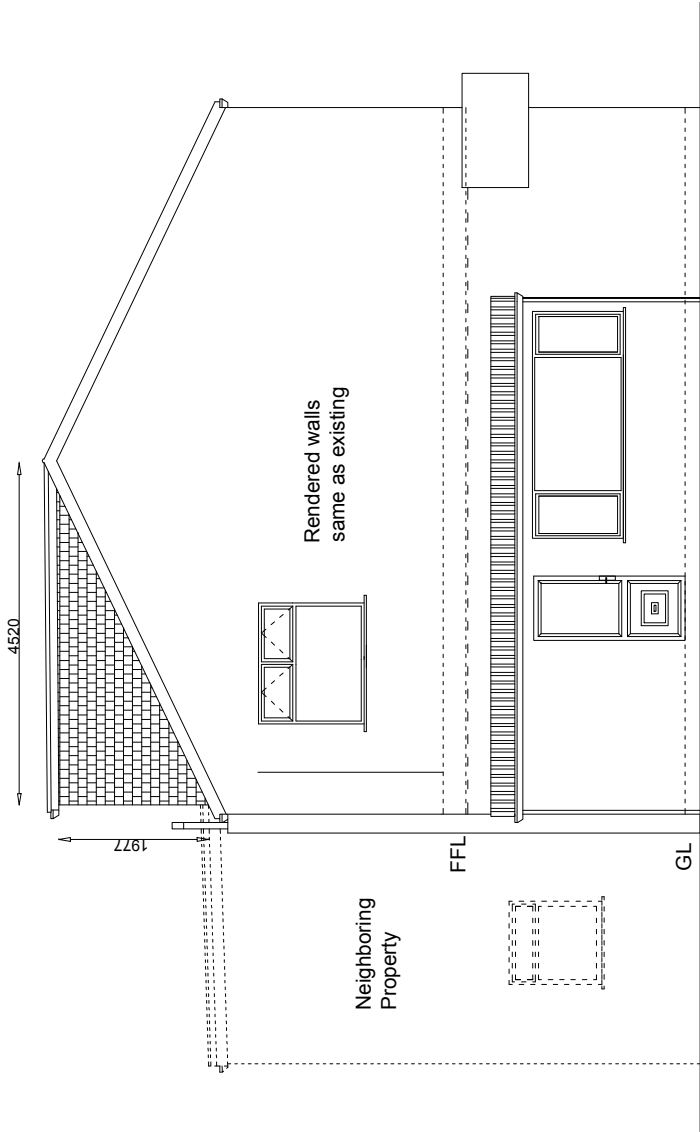
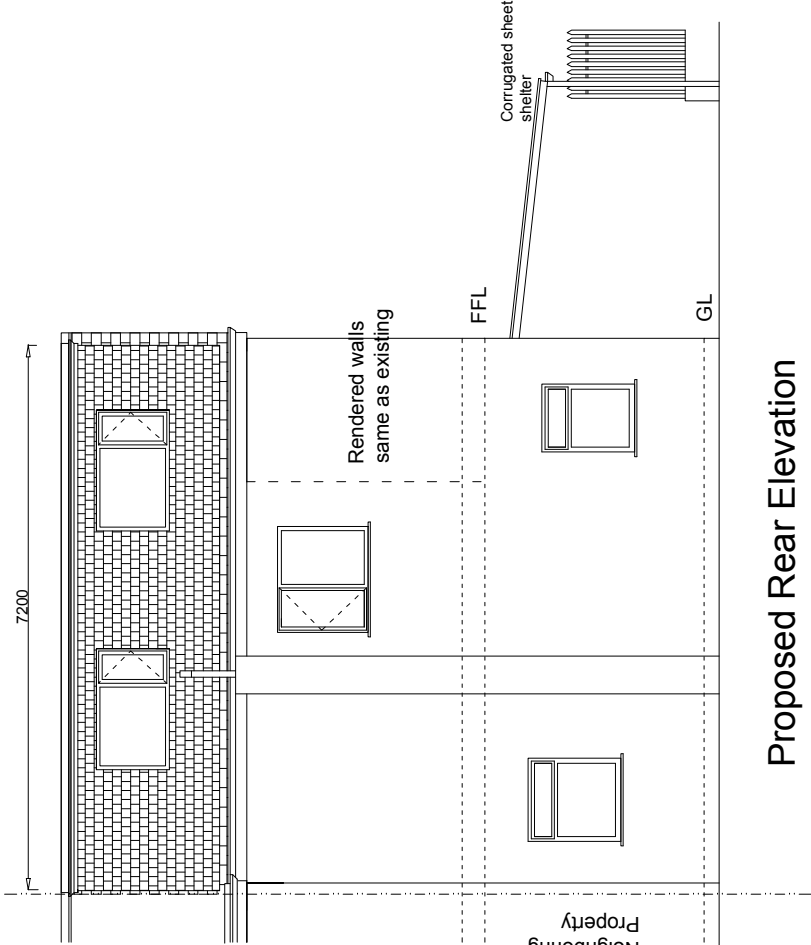
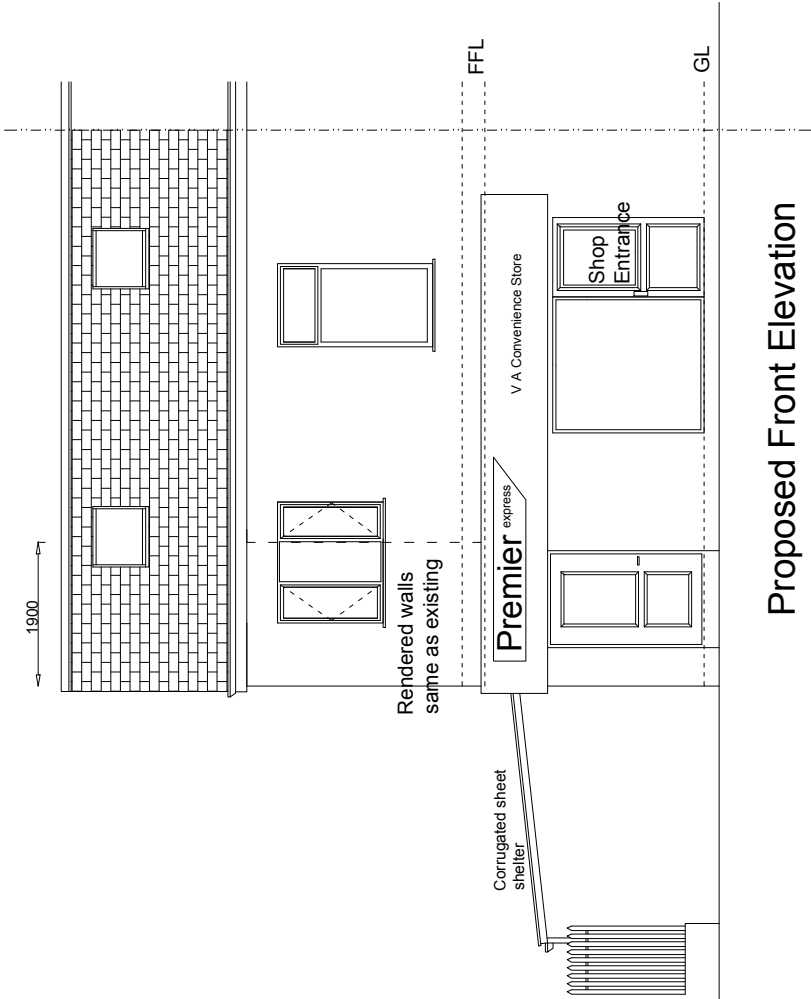
NOTES

1. DO NOT SCALE FROM DRAWING.
2. ALL NEW DOORS AND WINDOWS TO HAVE CATNIT LINGTELS OVER UNLESS SPECIFIED OTHERWISE WITH 150mm MIN NEW DRAIN BEARING. FOR EXTERNAL WALLS, WEEPHOLES TO BE PROVIDED AT NOT MORE THAN 450mm C/C.
3. ALL NEW CEILINGS 12.5mm FIREBOARD AND 5mm SKIM.
4. VENTILATION TO COMPLY WITH BUILDING RECS AND BS 5250 MINIMUM 1/20 OF FLOOR AREA. PROVIDE MECHANICAL EXTRACT FAN. CEILING OR WALL FIXINGS-DISCHARGE TO OUTSIDE VIA 100mm DIA PVC OR FLEXIBLE DUCTS. - BATHROOM FANS MIN 15 L/S DISCHARGE RATE (INTERNAL BATHROOM FAN WIRED TO LIGHT WITH 20 MINUTE OVER RUN). - KITCHEN FANS MIN 60 L/S DISCHARGE RATE (OR ALTERNATIVE IN COOKER HOOD MIN 30 L/S/ AIR CHANGE RATE).
5. PROVIDE BACKGROUND VENTILATION AS FOLLOWS:- ALL HABITABLE ROOMS - MIN 8000mm² TRICKLE VENT. - KITCHENS - MIN 4000mm² TRICKLE VENT.
6. WINDOWS TO BE DOUBLE GLAZED WITH PINKINGTON K GLASS AND 16mm AIR GAP. OPENABLE AREA OF WINDOW TO BE 1/20TH OF FLOOR AREA, WITH 0.33M² OPENABLE AREA AND A MINIMUM OF 450mm IN EITHER DIRECTION. HEIGHT OF CILL NOT MORE THAN 1100mm ABOVE FLOOR LEVEL AND NOT BELOW 800mm.
7. ABOVE FLOOR LEVEL. ALL WINDOWS TO BE ARGON FILLED AND HAVE A U-VALUE OF 1.6 W/m²K.
7. SAFETY GLASS TO BE PROVIDED. GLAZED DOORS AND SURROUNDS IN CRITICAL LOCATIONS TO COMPLY WITH BS 6206 AND BS 6200 AND PART N AND N1 OF BUILDING REGULATION.
8. DRAINAGE AND PLUMBING GENERALLY TO COMPLY WITH CURRENT BUILDING RECS. C.P., LOCAL WATERBOARD AND THE FOLLOWING:- BS 830 BUILDING DRAINAGE. - BS 5372 SANITARY PIPEWORK. - BS 6465 PART 1 SANITARY APPLIANCES.
9. ALL NEW DRAINAGE TO BE IN 100mm PVC ENCASED IN 150mm CONCRETE FLEXIBLY JOINTED AND LAID TO BS 8301 WITH 150mm PEA SHINGLE. PROVIDE R.C. LINGTELS (2 NOS BOURNCRETE LINTEL TYPES R15A) OVER WHERE DRAINAGE PASSES THROUGH WALLS. FALL ON ALL DRAINAGE NOT TO EXCEED 1:40 AND NOT LESS THAN 1:70.
10. PROVIDE RODDING EYE TO ALL WASTE PIPES WHERE CHANGE IN DIRECTION OCCURS.



Proposed Ground Floor Plan

Proposed First Floor Plan



Proposed Front Elevation

RAFTER ROOF CONSTRUCTION (C24, @ 300 C/C) ON HEAVY DUTY JOISTS HANGERS, 150 X 50 X 50 TANGULISED C24, @ 200 C/C) ON PLASTERBOARD AND SKIN. FIRING PIECES WITH FALL 1:40. INSULATION (BETWEEN RAFTER TO BE CELOTEX GA4100 100mm THICK AND 50MM UNDER THE RAFTER CELOTEX GA4000 190MM WATERPROOF PLYWOOD DECKING, 3 LAYERS OF FELLOE ASPHALT ROOF COVERING, 20 MM FASCIA BOARD, 1000 X 30 X 5 MILD STEEL STRAPS SCREWED TO THE UNDERSIDE OF JOISTS AND WALL AT 1800 MM C/C.

DORMER WALL CONSTRUCTION: TO BE CONSTRUCTED WITH 150 x 50 TREATED TIMBER STUD WORK WITH 12.5mm FIBRE BACK PLASTERBOARD SKIN COATED INTERNALLY, AND 13MM EXTERIOR GRADE PLYWOOD SCREWED TO STUDWORK AND BREATHABLE FELT WITH BATTENS FOR VERTICAL TILE HUNG WITH MATCHING TILES. INSULATION TO BE 100mm CELOTEX TUFFR GA3 100 BOARDS. TO DORMER FACE. PROVIDE AND FIX CODE 4 LEAD FLASHING AND SOKERS AT JUNCTIONS WITH MAIN ROOF AND APONS. DORMER SLOPE TO MATCH EXISTING. WITHIN 1M OF THE BOUNDARY THE EXTERNAL FACE OF THE PLYWOOD TO BE COVERED WITH 6mm MASTERBOARD.

LOFT FLOOR CONSTRUCTION:- FLOOR JOISTS 200X50 @300/c C24, TIMBER FLOOR BOARDS WITH MINIMUM MASS PER UNIT AREA 15kg/m². CEILING TO BE 12.5mm PLASTERBOARD AND SKIM COAT PLASTER. CAVITY TO BE FILLED WITH 100mm MINERAL WOOL WITH MINIMUM DENSITY OF 10 kg/m³

Proposed Rear Elevation

SLOPING ROOF IN LOFT - PROVIDE 50mm CROSS VENTILATION CAP TO ROOF. BETWEEN INSULATION AND ROOFING FELT. ROOF INSULATION TO BE TWO LAYERED SYSTEM WITH ONE CELOTEX 100mm X3100 BOARDS BETWEEN RAFTERS AND ONE 50mm G30305 BOARD ON THE BOTTOM OF THE RAFTERS. WHOLE ROOF TO BE VENTED WITH CONTINUOUS PROPRIETARY VENTS AT GUTTER LEVEL AND RIDGE LEVEL OR USE BREATHABLE FELT.

- NOTES ON LOFT
1. ALL CEILINGS TO BE 12.5mm PLASTERBOARD AND 5mm SKIM.
2. ALL INTERNAL STUD PARTITIONS ARE TO BE 100X 50mm SAWN TIMBER @ 400mm c/c & HORIZONTALLY NOGGED @ 600mm c/c. TO BE LINED WITH 15mm SOUNDABLOC PLASTERBOARD & 5mm SKIM ON BOTH SIDES. ALL PARTITIONS TO HAVE 100mm FIBREGLASS INSULATION.
3. ALL DOORS TO BE 1/2 HOUR FIRE RESISTANCE.
4. GLOOR STOPS TO BE 25mm X 35mm FIXED WITH SCREWS AT 230c/c AND GLUED.

5. ALL STEELWORK TO HAVE ½ HOUR FIRE PROTECTION COMPRISING OF ONE LAYER 12.5mm PLASTERBOARD AND 5mm SKIM COAT OR FIRE PROOF PAINT.

Proposed Gable Elevation

12. DORMER WALL TO BE CONSTRUCTED WITH 150 x 30 TREATED TIMBER STUDWORK WITH 12.5mm PLASTERBOARD AND SKIM COAT INTERNALLY, AND 18MM EXTERIOR GRADE PLYWOOD SCREWED TO STUDWORK AND BRAMBLEHART FELT WITH BATTENS FOR VERTICAL LINE HUNG WITH MATCHING TILES. INSULATION TO BE 100mm CELOTEX EXTRA RMX100 BOARDS. TO DORMER FACE. PROVIDE WITH MAIN ROOF AND APRONS, ABUTMENTS ETC. TILES TO MATCH EXISTING PLYWOOD TO BE CONVEYED TO THE STREET AND RE-USE.

13. FLOORBOARDS TO BE 21mm T & G BOARDS AND PROVIDE 100mm ROCKWOOL FLEX SLAB INSULATION BETWEEN JOISTS. SKIRTING AND ARCHITRAVE TO BE SOFTWOOD OGEE.

14. FASCIA BOARD TO BE 20mm THICK, AND PROVIDE ALL RAINWATER CUTTERS AND DOWN PIPES.

11. NEW STAIRCASE TO HAVE GOING (min) 225mm AND RISERS (max) 220mm AND MAX PITCH 42 DEGREES. HEADROOM TO BE 2000mm MIN. PROVIDE ALL TRIMMERS AND HANDRAILS ETC. WINDERS TO HAVE MIN OF 50mm BEARING ON NEWEL POST, HAND RAIL TO BE 900mm ABOVE FLOOR LEVEL.

Scale 1 : 100

NOTES:

Copyright in all documents and drawings prepared by the designer and/or architect shall remain the property of the designer and architect. The drawings and documents shall be used solely for the project and shall not be reproduced, copied, or otherwise used for any other project without the written consent of the designer and architect.

DO NOT SCALE OFF THIS DRAWING.

All dimensions to be checked on site and any discrepancies to be referred to the architectural designer before proceeding.

ALL VIEWS TO BE CHECKED ON SITE.

An action to the specification shall be referred to the architect and designer prior to proceeding.

All component sizes and references to be checked prior to ordering of materials and designation 1/2 inch nominal surface water or full of existing storm on site are to be checked/confirmed by the building official. All other dimensions and materials to be confirmed by the building official. Any additional connections are to be made, as able to be made, to meet the design intent and to meet the minimum requirements of the applicable code. Additional capacity and the type of fitting must be provided.

All relevant boundary positions to be checked prior to proceed.

<p>Proposed First floor extension on side of property with dormer loft conversion at 7a Bolton Rd, Bolton</p>	<p>Dwg No. 01</p> <p>Revision 0</p>
<p>Proposed Floor Plans & Elevations</p> <p>Scale: 1 : 100 (As Dwg size)</p> <p>Date: Apr 2016</p> <p>FP Architecture & Planning email: bachplanings@gmail.com tel: 07763560011</p>	

**Application number
96462/16**



**Development & Regeneration Dept
Development Management Section**

Town Hall, Bolton, Lancashire, BL1 1RU
Telephone (01204) 333 333

**Bolton
Council**

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Date of Meeting: 30/06/2016

Application Reference: 96462/16

Type of Application: Full Planning Application
Registration Date: 03/05/2016
Decision Due By: 27/06/2016
Responsible Officer: Paul Bridge

Location: LAND AT DEALEY ROAD, BOLTON

Proposal: CHANGE OF USE OF LAND TO FORM CAR PARK PROVIDING 16 SPACES INCLUDING NEW VEHICULAR ACCESS, 2M HIGH WELDMESH FENCING TO PERIMETER AND LANDSCAPING.

Ward: Heaton and Lostock

Applicant: Wigan Road Education and Community Centre
Agent : RA Design & Project Management Ltd

Officers Report

Recommendation: Approve subject to conditions

Proposal

Planning permission is sought for the change of use of land to form a car park providing 16 spaces including new vehicular access from Dealey Road, 2m high weldmesh fencing to perimeter and landscaping. The car park would provide additional car parking for the Wigan Road Education and Community Centre located at 396 Wigan Road.

The proposal would result in the loss of an area of woodland approximately 499m² in size.

Site Characteristics

The application site is located on the eastern side of Dealey Road and is roughly rectangular in shape. The site forms part of a small area of woodland which is covered by Tree Preservation Order 956. Directly adjoining the site to the east is a residential bungalow, whilst to the south is car and van sales yard. To the north is the remainder of the woodland (which is on a lower level) beyond which are the residential dwellings along Junction Road.

Policy

National Planning Policy Framework (NPPF)

Core Strategy policies:

CG1 Cleaner and Greener, CG3 The Built Environment, CG4 Compatible Uses, OA4 West Bolton.

SPD General Design Principles

Analysis

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- * principle of development
- * impact on the visual amenity
- * impact on residential amenity
- * impact on the highway
- * impact on trees
- * impact on ecology and biodiversity

Principle of the Development

The site forms part of an area of woodland and would provide additional car parking for the Wigan Road Education and Community Centre located at 396 Wigan Road. The proposed development would improve the parking provision in accordance with the relevant policies within the adopted Core Strategy National Planning Policy Framework. The principle of the proposal is therefore considered acceptable.

Impact on Visual Amenity

Core Strategy policy CG3 seeks to ensure that development proposals contribute to good urban design, conserve and enhance local distinctiveness and have regard to the overall built character and landscape quality of the area. Furthermore, development should be compatible with the surrounding area, in terms of scale, massing, grain, form, architecture, street enclosure, local materials and landscape treatment.

The proposed car park would be constructed using tarmacadam and would result in the loss of a small area of woodland which currently contributes to the visual amenity of the area. Whilst the loss of this section of woodland is not entirely desirable, it is considered that the need for additional car parking for the centre has to be given considerable weight. In addition, the woodland area to the north of the site would remain and soft landscaping would be provided around the perimeter of the site which will soften the appearance and visually screen the proposed car park. A 2m high green weldmesh fence would also be constructed around the perimeter of the site, the design of which is considered acceptable.

In view of the above, it is considered that the development would not have an unacceptable visual impact on the immediate and wider surrounding area is compliant to Core Strategy Policies CG3 and OA4.

Impact on Residential Amenity

Core Strategy policy CG4 seeks to ensure that development is compatible with surrounding land uses and occupiers, protecting amenity, privacy, safety and security, and does not generate unacceptable

nuisance, odours, fumes, noise or light pollution.

The proposed car park would be used by visitors to the Wigan Road Education and Community Centre located at 396 Wigan Road. Directly adjoining the site to the east is a residential bungalow, whilst to the south is a car and van sales yard. The introduction of a car park in this location and adjacent to a residential dwelling could give rise to noise and disturbance if not controlled. The likely noise and disturbance would be that associated with customers voices when arriving or leaving and getting into and out of cars. Whilst this can give rise to nuisance if it occurs later in the evening or at night, it would generally be likely to be at a low level, intermittent and infrequent.

Notwithstanding the above, the submitted plans do not indicate how the car park would be managed and also secured when not in use/required by the Education Centre. Without a management plan the car park would be available to use 24 hours a day, seven days a week by members of the public visiting the many shops etc along Wigan Road. In order to control this conditions have been attached to restrict the hours of use of the car park to those which are attached to the Education Centre. (08:00am – 22:00pm Monday – Sunday) and also to ensure that there is secure access and egress arrangements of the car park to prevent use outside of these hours.

Subject to these conditions it is not therefore considered that the proposal would result in any significant increase in noise disturbance.

In respect of loss of privacy and overlooking the application site is located on a higher level than the adjacent residential dwelling and therefore without adequate boundary treatment the site would potentially offer views into the private amenity space of the bungalow. In order to mitigate this, a condition has been attached to ensure an adequately sized soft boundary treatment around the perimeter of the site is planted to prevent any loss of privacy and overlooking. Subject to appropriate conditions, there would be no unacceptable impact on nearby residents in terms of loss of light, outlook or privacy.

For the above reasons the proposal is considered to be acceptable in terms of the impact on residential amenity and in accordance with Policy CG4 of the Core Strategy.

Impact on the Highway

Core Strategy policies P5 and S1 promotes road safety in the design of new development.

Highways Officers were consulted on the proposal and had no objections to the proposal, subject to a number of conditions in respect of visibility splays, surface materials and drainage.

The proposal complies with policies P5 and S1 of the Bolton Core Strategy.

Impact on Trees

Policy CG1.1 of the Core Strategy states that the Council will safeguard and enhance the rural areas of the borough from development that would adversely affect its biodiversity including trees, woodland and hedgerows.

As stated above the proposal would result in the loss of a small area of woodland which is covered by a Tree Preservation Order. There will be approximately five poorly formed Willow trees, one fruit tree, one Maple tree and some small naturally regenerated Elder, Ash and Sycamore trees that will require removal. These trees are considered generally of poor form and condition and therefore their removal should not constrain the development and their removal is considered to be acceptable.

The addition of the turning head on the north-west corner of the plot will slightly infringe into the

root protection zone (RPZ) of an Ash tree that is positioned off site in the neighbouring property (not protected) and therefore this will require a no dig specification in this area. An informative has been attached to advise the applicant of this.

The originally submitted plans which indicated the replacement planting belts on the western boundary were not wide enough to accommodate the fence and tree planting. Therefore, an amended plan has been received which has narrowed the car parking spaces from 2.5m to 2.4m in width and the turning head moved westward by a metre. This would enable tree landscaping to be provided in this area.

The Council's Trees and Woodland Officer was consulted on the proposal and has no objection to the loss of this section of the woodland.

A condition has been attached requesting a more detailed landscape scheme to be agreed prior to commencement of development.

The proposal complies with CG1.1 of the Core Strategy.

Impact on Ecology and Biodiversity

The proposed site is presently lowland mixed broadleaf woodland which is a UK Biodiversity Action Plan (BAP), Greater Manchester BAP and Bolton BAP habitat. It is also a habitat of principal importance for the purpose of conserving biodiversity, as listed in Section 41 of the Natural Environment & Rural Communities Act 2006 which confers duty upon local authorities by Section 40 of the Act to consider its protection for biodiversity.

All wild birds, their nests and eggs are protected from disturbance by Section 1 of the Wildlife & Countryside Act 1981 (as amended). Therefore a condition has been attached to ensure that the felling of the trees and clearance of vegetation is undertaken outside the bird nesting season or not without the woodland being declared free of nesting birds by a suitable qualified specialist.

The proposal, subject to conditions it considered to be in accordance with policy CG1 of the Core Strategy.

Conclusion

The proposals are considered to be acceptable in terms of siting and design, subject to conditions and comply with Core Strategy policies CG1, CG3, CG4, S1, P5 and OA4.

Members are recommended to approve the application.

Representation and Consultation Annex

Representations

Five letters have been received in response to the planning application publicity and who have raised the following concerns:-

- loss of woodland and green space
- volume of traffic
- noise
- loss of privacy

- drainage
- the car park will be unmanned and will be available to use for various members of the public visiting shops along Wigan Road
- loss of habitat and space for nesting birds

The issues above have been addressed in the main body of the report.

The decision to approve the application for the Education Centre was wrong based on misleading information and therefore this application should be refused. *Response:- The application for the Education Centre is a not a matter before the Local Planning Authority on this occasion. This current application is to be judged entirely on its own planning merits.*

A transport impact assessment should be requested for this application. *Response:- A Transport Assessment and Travel Plan are not required for development of this scale.*

The applicants spaces are shown larger than standard and therefore more than 16 vehicles will be able to park within the site. *Response:- Should the application be approved, the proposal would have to be constructed entirely in accordance with the approved plans.*

The car park should be available for all the community. *Response:- The car park/application site is in private ownership and the potential end users would be a decision for the owners/ operator of the car park.*

Hulton Lane car park are other car parks are in close proximity to the Education Centre which could be used. *Response:- Whilst there are other public car parks within the immediate and wider surrounding area, the Local Planning Authority has to judge this application on entirely on its own planning merits.*

Anti social behaviour. *Response:- There is no evidence to suggest that a proposal of this nature will result in anti social behaviour.*

The site is located close to a conservation area:- *Response:- Due to the distance and built form between the application site and the conservation area, the Local Planning Authority do not consider that this proposal would have any effect on the character and appearance of the conservation area.*

Article 8 of the Human Rights. *Response:- Due regard has been given to the provisions of the Human Rights Act, and in particular Article 1 of the First Protocol and Article 8, namely the right to the peaceful enjoyment of property and the right to respect for private and family life. The concerns expressed through the letters of representation are acknowledged and the neighbours' rights have been carefully assessed against Article 8 of the Act but, arriving at a recommendation on this application, those rights have been balanced against the rights of the applicant under Article 1, Protocol 1 and the wider planning aims as expressed through the planning policies for the area.*

Consultations

Advice was sought from the following consultees: Highways ,Tree and Woodland Manager, Commercial Services, Greenspace Management and Environmental Health.

Planning History

94104/15 - Change of use of land to form car park providing 52 no. parking spaces including boundary fencing and access gate. – withdrawn, July 2015

Recommendation: Approve subject to conditions

Recommended Conditions and/or Reasons

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason

Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. No development shall take place until a strategy of surface water drainage for the site using sustainable drainage methods and which includes details of how water quality will be improved, and how existing surface water discharge rates reduced, has been submitted to and approved in writing by the Local Planning Authority. The approved strategy shall be implemented prior to first occupation or use of the development hereby approved unless alternative timescales have been agreed in writing as part of the strategy.

Reason: To ensure a satisfactory method of surface water disposal to reduce the risk of flooding elsewhere

3. Development shall not commence until a scheme for the eradication of Japanese knotweed has been submitted to and approved in writing by the Local Planning Authority. This shall include a timetable for implementation. Should there be a delay of more than one year between the approval of the scheme and its implementation or the commencement of development then a new site survey and, if necessary, further remedial measures shall be submitted for the further approval of the Local Planning Authority. The scheme shall be carried out as approved and retained thereafter.

Reason

To ensure the safe development of the site and eradication of an invasive species and to comply with policy CG1 of Bolton's Core Strategy.

4. Notwithstanding the approved plans and prior to the development hereby approved being first occupied or brought into use the means of vehicular access to the site from Dealey Road shall be constructed to a minimum width of 4.5 metres with 5 metres radii.

Reason

In the interests of highway safety and in order to comply with policies S1 and P5 of Bolton's Core Strategy.

5. Trees and shrubs shall be planted around the perimeter of the site in accordance with a landscape scheme to be submitted and approved in writing by the Local Planning Authority prior to the development being first brought into use. The approved scheme shall be implemented in full and carried out before the car park is brought into use. Any trees and shrubs that die or are removed within five years of planting shall be replaced in the next available planting season with others of similar size and species.

Reason

To reflect and soften the setting of the development within the landscape.

6. Notwithstanding the approved plans and before the development is occupied a scheme detailing the secure access and egress arrangements of the approved car park shall be submitted to and improved in writing by the Local Planning Authority. The secure access and egress arrangements shall thereafter, be constructed and available at all times the development is occupied and maintained in accordance with the approved details.

Reason: - In the interests residential amenity.

7. Prior to the development hereby approved being first brought into use a visibility splay measuring 2.4 metres by 43 metres at the back of the footway shall be provided at the junction from Dealey Road , and subsequently remain free of all obstructions between the height of 1.05 metres (as measured above carriageway level).

Reason

To ensure traffic leaving the site has adequate visibility onto the footway.

8. Prior to the development hereby approved being first occupied or brought into use, a scheme detailing how parts of the site to be used by vehicles are to be laid out, constructed, surfaced, drained and lit shall be submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be implemented in full and thereafter made available for the use of vehicles at all times the development is in use.

Reason

To encourage drivers to make use of the parking and circulation area(s) provided.

9. No tree or shrub clearance should take place between the months of March and August inclusive.

Reason

The site has the potential to support breeding birds. It is an offence under the Wildlife and Countryside Act 1981 (as amended) to disturb birds whilst they are breeding.

10. The development shall be screened by the planting of a suitable hedge and/or bushes around the perimeter of the site the length and location of which shall first be agreed in writing by the local planning authority. The screen planting shall be allowed to grow to a height of between 2.5 metres and 3.5metres and shall be retained thereafter at the stated height. Any trees/shrubs which are removed, die, become severely damaged or diseased within five years of their planting shall be replaced in the next planting season with trees/shrubs of similar size and species to those originally required to be planted unless the Local Planning Authority gives written consent to any variation.

Reason: To ensure the character of the area is not adversely affected

11. The car park hereby approved shall be not be available to use outside the following hours:-

08:00am - 22:00pm - Mondays to Sunday

Reason

To safeguard the living conditions of residents and the amenity and character of the area with regard

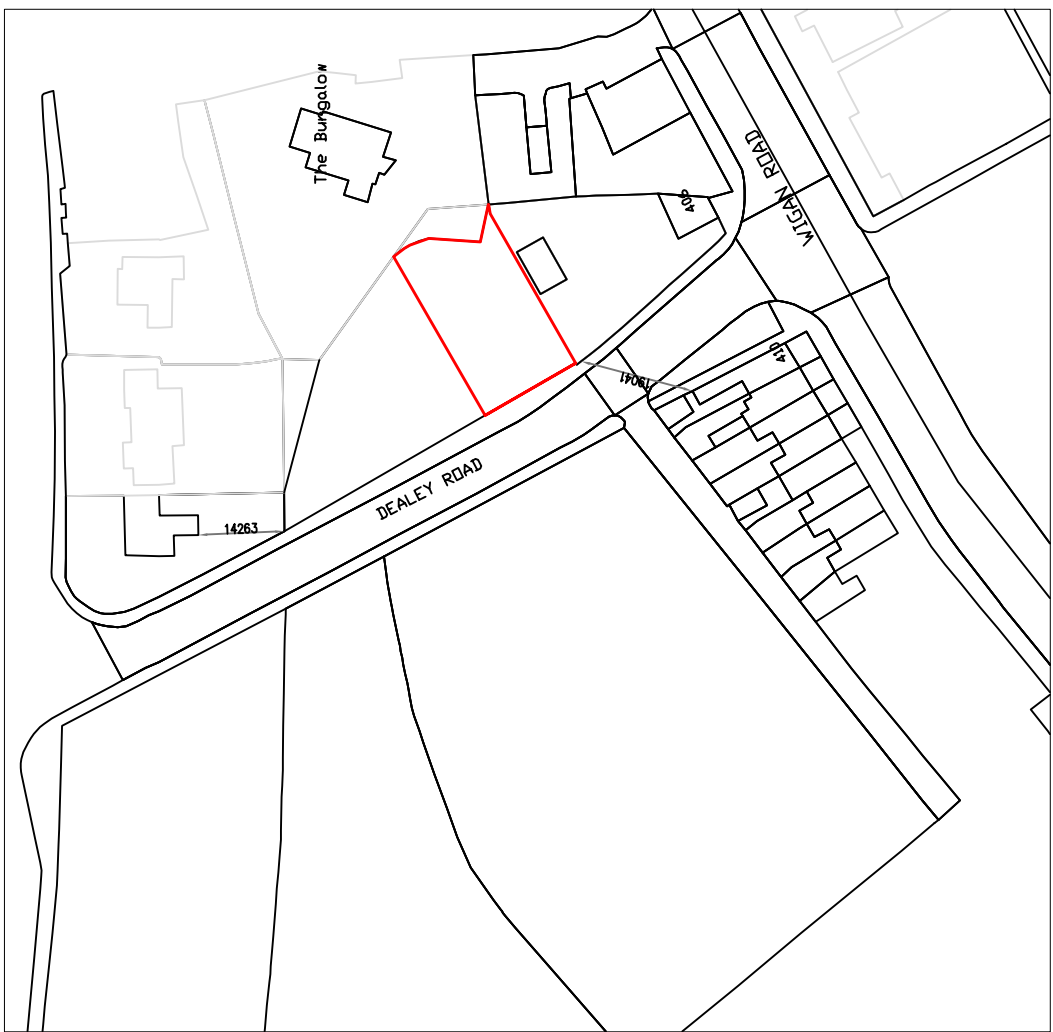
to noise and/or disturbance.

12. The development hereby permitted shall be carried out in complete accordance with the following approved plans:

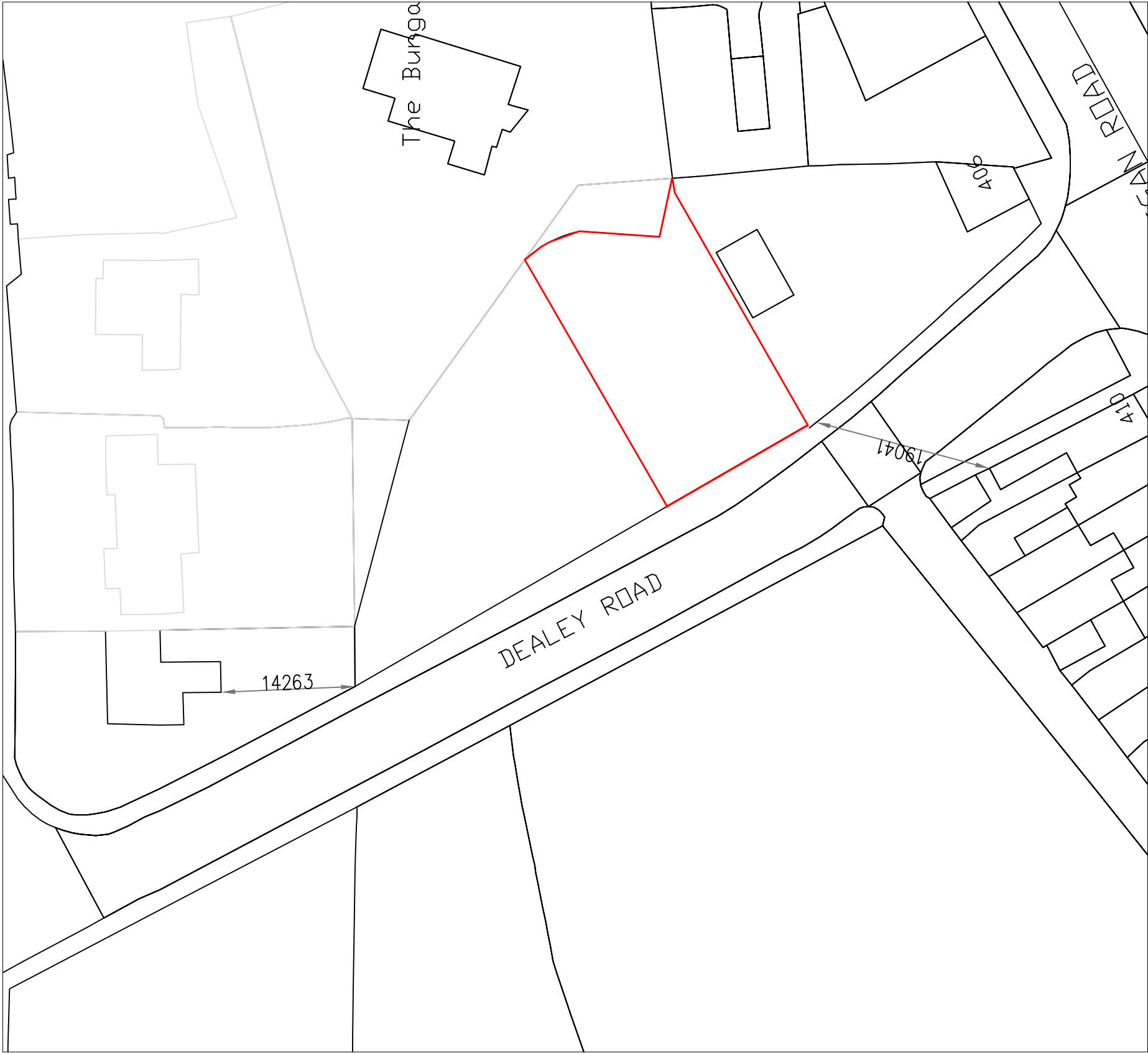
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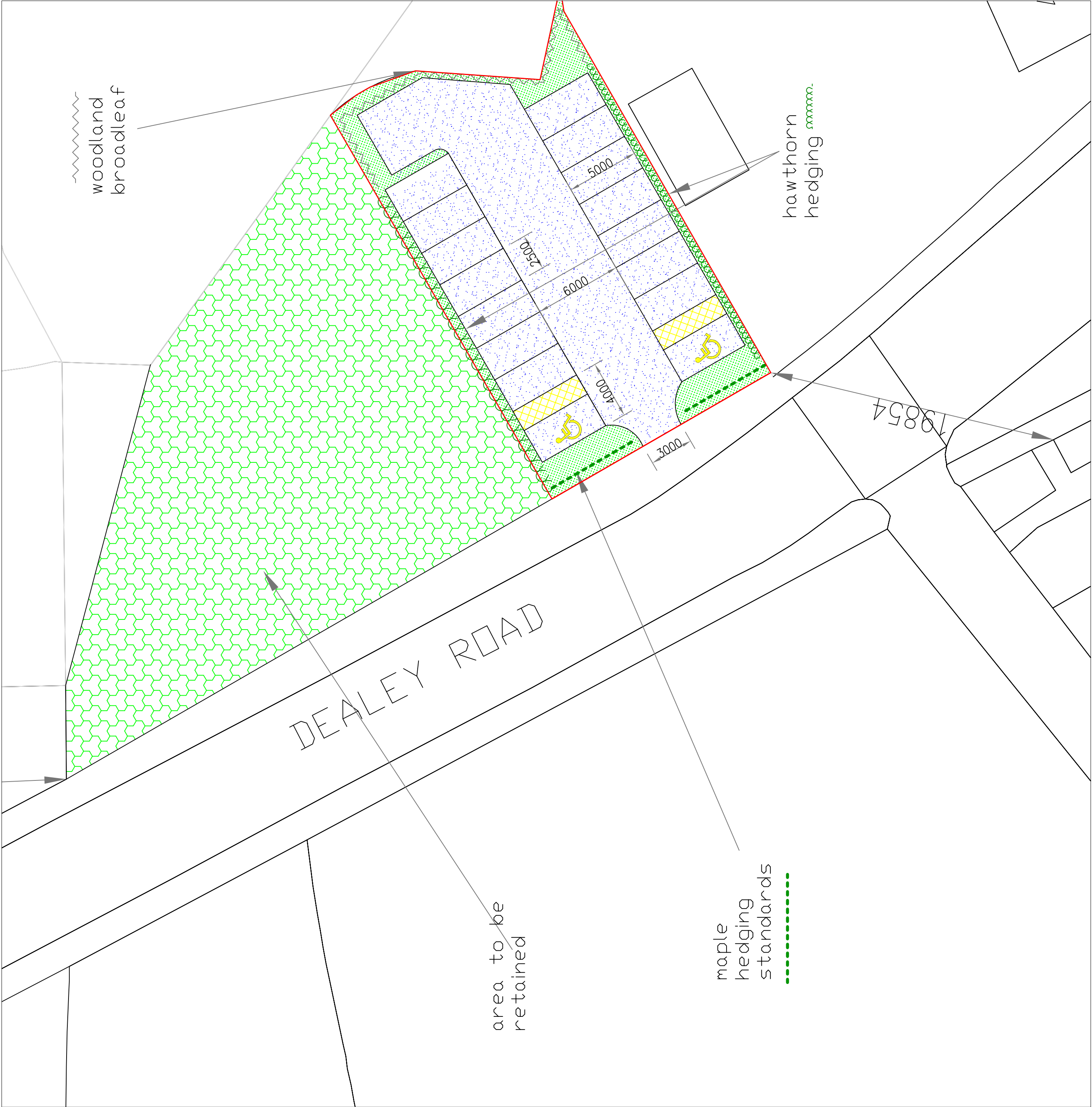
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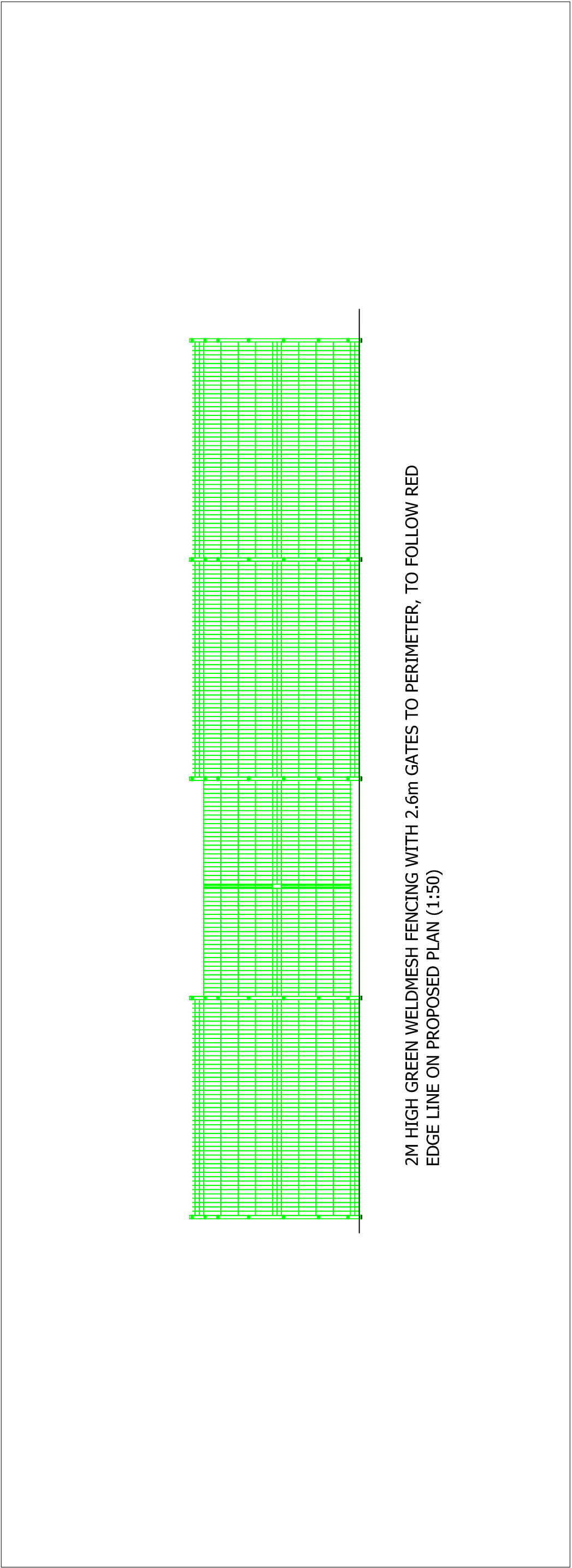
LOCATION PLAN (1:1250)
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BLOCK PLAN (1:500)

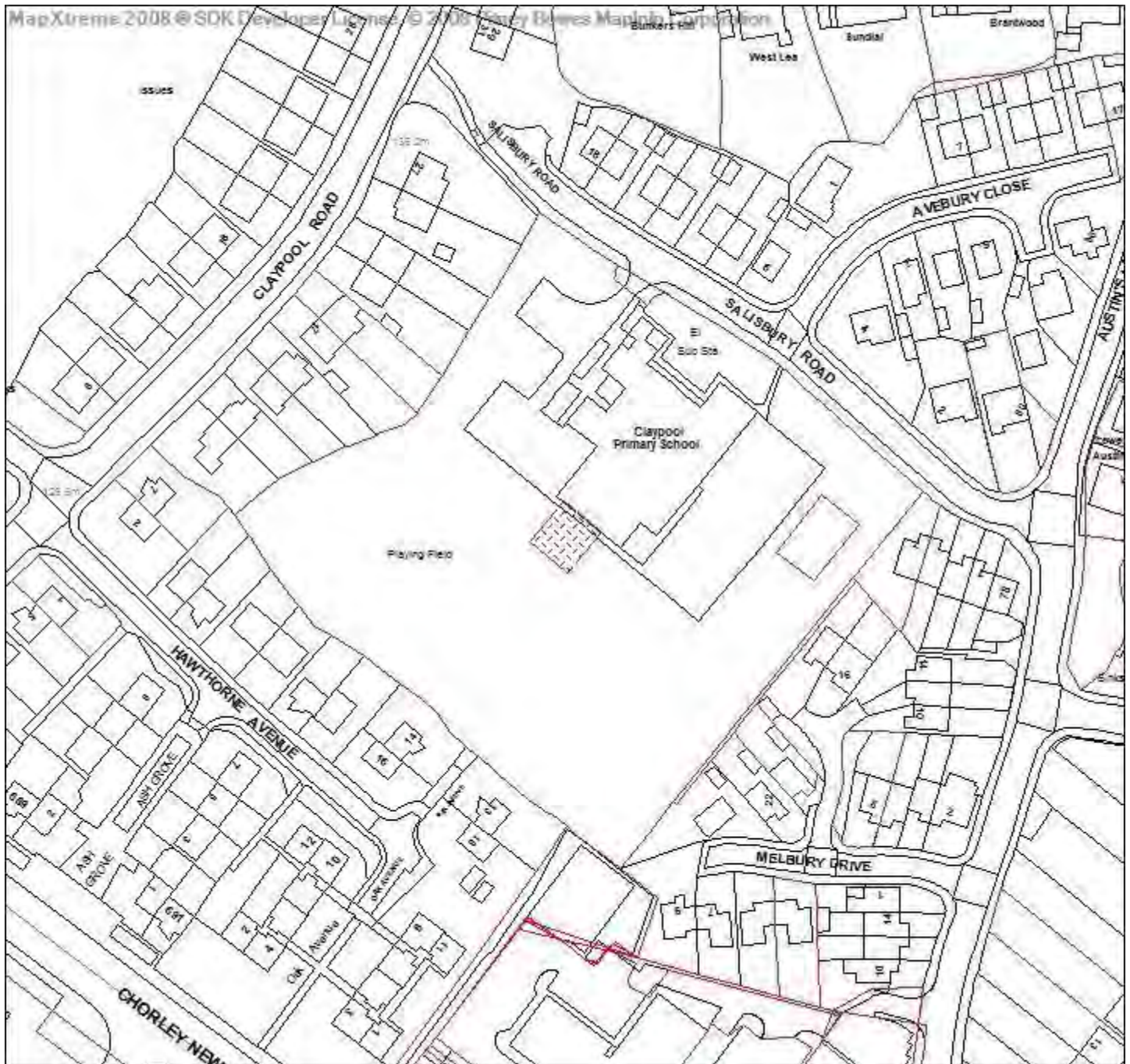


PROPOSED SITE PLAN (1:200)



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Application number 96467/16



Development & Regeneration Dept
Development Management Section

Town Hall, Bolton, Lancashire, BL1 1RU
Telephone (01204) 333 333

Bolton Council

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Date of Meeting: 30/06/2016

Application Reference: 96467/16

Type of Application: Full Planning Application
Registration Date: 03/05/2016
Decision Due By: 27/06/2016
Responsible Officer: Helen Williams

Location: CLAYPOOL PRIMARY SCHOOL, SALISBURY ROAD, HORWICH, BOLTON, BL6 6LN

Proposal: ERECTION OF SINGLE STOREY EXTENSION TO FORM ADDITIONAL CLASSROOM TOGETHER WITH SITING OF PORTABLE CABIN FOR DURATION OF PROPOSED WORKS AND USE AS TEMPORARY CLASSROOM.

Ward: Horwich North East

Applicant: Bolton Council Corporate Property
Agent : Bradshaw, Gass and Hope

Officers Report

Recommendation: Approve subject to conditions

Proposal

Permission is sought for the erection of an extension at the rear of the school to provide a new classroom. The applicant has stated that the new classroom is required owing to an increase in roll numbers at the primary school.

The proposed extension is to project 11.023 metres to the rear of the school and is to be 8.99 metres wide. It is to have a mono-pitched roof and is to be constructed from materials to match those of the existing school.

A portable building is proposed to be provided on part of the playground to provide temporary classroom accommodation for the duration of the building works. This will be removed once the new classroom has been completed.

Site Characteristics

Claypool Primary School is a single storey building with hardplay areas to its sides and a playing field to its rear (south). The school is accessed from Salisbury Road to the north, which is a cul-de-sac.

The school is located in a residential area of Horwich and is bounded on all four sides of the school grounds by residential properties.

Policy

National Planning Policy Framework (NPPF)

Core Strategy Policies: A1.2 Primary Schools; P5 Transport and Accessibility; S1 Safe Bolton; CG3 The Built Environment; CG4 Compatible Uses; OA1 Horwich and Blackrod.

SPD General Design Principles

Analysis

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- * impact on the character and appearance of the building and surrounding area
- * impact on the highway
- * impact on the amenity of neighbouring residents

Impact on the Character and Appearance of the Building and Surrounding Area

Policy CG3 of the Core Strategy states that the Council will conserve and enhance local distinctiveness, ensuring development has regard to the overall built character and landscape quality of the area, and will require development to be compatible with the surrounding area, in terms of scale, massing, grain, form, architecture, street enclosure, local materials and landscape treatment. Policy OA1 of the Core Strategy states that the Council will conserve and enhance the character of the existing landscape and physical environment, and will ensure that development in Horwich and Blackrod respects street patterns, the grain and the form of predominant architectural styles and where possible makes sympathetic use of locally distinctive materials such as stone.

The proposed extension, sited at the rear of the school, will not be visible from the road. It is considered that the scale of the extension is proportionate with the existing school building and that the extension will appear subservient to the school building. The proposed materials will match those of the existing school building.

The extension will occupy a small portion of the existing playing field at the rear of the school; as this is only a small portion it is not considered that the proposed development would affect the use of the field.

The tree sited to the east of the proposed extension (to the rear of the school) will not be affected by the developed.

It is the school's intention to site a portable building on the playground to the west of the school building to be used as temporary accommodation whilst the new classroom extension is being built. This will be removed once the new classroom has been completed. A condition is therefore suggested to ensure that the portable building is removed in full once the new classroom is ready for occupation.

The proposed siting of the portable building means that it will not be particularly visible from outside the site.

It is therefore considered that the proposed development would be compatible with the character and appearance of the existing school building and the surrounding area, compliant with Policies CG3 and OA1 of the Core Strategy.

Impact on the Highway

Policy P5 of the Core Strategy states that the Council will ensure that developments take into account [amongst other things] accessibility by different types of transport, servicing arrangements and parking. Policy S1.2 states that the Council will promote road safety in the design of new development.

The school has a small car park area to the west of the school building, which is accessed off Salisbury Road. Vehicles visiting the school will also park along Salisbury Road, which is a residential cul-de-sac to the north of the site. Representations received from residents of Salisbury Road to this application detail how busy the road becomes during school drop off and collection times, with vehicles parking on the pavements on each side of Salisbury Road.

The Council's Highways Engineers have commented that the proposed additional classroom is likely to exacerbate the existing on-street parking problems on Salisbury Road at school start and finish times.

As Officers feel that it is necessary to allow the school to expand to accommodate the increased roll numbers, it is suggested (as put forward by Cllr. Silvester in his representation to this application) that any approval be conditioned to require the school to undertake a School Travel Plan (prior to the completion of the extension) to address how they will discourage parents and staff travelling to the school by car (encouraging instead alternative forms of transport such as walking, or by car sharing).

It is considered, subject to this condition, that the proposed development would not jeopardise highway safety, compliant with Policies P5 and S1.2 of the Core Strategy.

Impact on the Amenity of Neighbouring Residents

Policy CG4 of the Core Strategy states that the Council will ensure that new development is compatible with surrounding land uses and occupiers, protecting amenity, privacy, safety and security, and does not generate unacceptable nuisance, odours, fumes, noise or light pollution.

The nearest neighbouring dwellings to the proposed classroom extension are over 50 metres away (Hawthorne Avenue) and the proposed portable building will be over 50 metres away from the nearest properties on Claypool Road. The siting of the proposed development will therefore not harm the amenity of neighbouring residents.

It is not considered that an additional classroom (increasing the number of children at the primary school by some 30 extra children) would unduly increase noise that is generated from the school during school hours, and it is considered that the suggested School Travel Plan condition should help alleviate disturbance caused by on-street parking in neighbouring residential roads.

It is therefore considered that the proposed development would not unduly harm the amenity of neighbouring residents, compliant with Policy CG4 of the Core Strategy.

Conclusion

For the reasons discussed above it is considered that the proposed classroom extension would be compatible with the character and appearance of the existing school building and the surrounding area, would not jeopardise highway safety and would not unduly harm the amenity of neighbouring residents. Members are therefore recommended to approve this application subject to the suggested conditions.

Representation and Consultation Annex

Representations

Letters:- Three letters of objection have been received from neighbouring residents of Salisbury Road. These raise the following concerns:

- * Neighbours' driveways are constantly being blocked or made hazardous due to pavement parking. An increase in traffic will make this worse;
- * Often pavement parking stops disabled access or prams passing meaning they have to go on the road;
- * Sometimes bins cannot be emptied as the bin wagon cannot access the road with parked vehicles;
- * Concerns about access for emergency vehicles;
- * An increase in traffic would be totally unacceptable, with accidents waiting to happen;
- * Could some of the greenery be used as a car park within the school confines?;
- * Potential fire hazard imposed by the temporary building as the school has been targeted in the past when temporary buildings have been in use (*officer's comment: this is not a material planning consideration and any risk of fire would instead be covered by Building Regulations*);
- * Increase in noise that the additional children will bring;
- * A new site for a new school should be sought to accommodate the ever increasing pollution of Horwich rather than adding a classroom at Claypool School.

Horwich Town Council:- made no recommendation at their meeting of 19th May 2016, but requested that Bolton Council consider the highways concerns of neighbours and that a traffic management plan be developed.

Elected Members:- Cllr Silvester has made the following comments concerning the proposal:

- * Should planning permission be granted he would like a Traffic Management Plan to be prepared by the school within 6 months of the permission, in order to promote the reduction of car journeys made by both parents and staff to the school, which would include an action plan as to how the school will discourage parents and staff from driving to the school and how this will be monitored.
- * This would reduce the carbon footprint and congestion at school opening and closing times if less car journeys were made;
- * Results following the traffic management plan should be distributed to neighbouring residents annually in the form of a school update to the local community via a newsletter.

Consultations

Advice was sought from the following consultees: Highways Engineers, Pollution Control Officers and Asset Management Unit (Children's Services).

Planning History

2.4 metre high security fencing was approved in September 2010 (84572/10).

2.4 metre high fencing, 1.2 metre high pedestrian gates and 3 metre high gates to the car park were approved in August 2009 (82395/09).

Permission was granted in August 2005 for two single storey extensions (front and rear) (71628/05).

A PE store extension, a toy store extension and new hardplay area were approved in May 2003 (63895/03).

2 metre high fencing to form a bin store and the construction of a garden feature with a seating area were approved in March 2003 (63651/03).

Permission was granted in March 2001 for a front porch, the laying out of a hardplay area and a toy store at the rear (58366/00).

An enlarged car park was granted permission in February 1999 (54094/99).

Permission was granted in January 1991 for extensions to form new classrooms/toilets and administration offices together with two new hardplay areas (38024/90).

Recommendation: Approve subject to conditions

Recommended Conditions and/or Reasons

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason

Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. Prior to the completion of the classroom extension hereby approved a School Travel Plan shall be submitted to and approved in writing by the Local Planning Authority. The School Travel Plan shall detail how members of staff and parents will be encouraged to travel to and from the school by means of transport other than the private car and how these measures will be implemented and monitored. The approved School Travel Plan shall then be implemented in full and retained thereafter.

Reason

To encourage alternative forms of transport to and from the school, following an increase in pupils at the school, and to comply with policies P5 and S1.2 of Bolton's Core Strategy.

3. The portable/temporary building (to be sited in the playground and to be used as temporary accommodation during the construction phase of the development hereby approved) shall only be used until the classroom extension hereby approved is fully ready for occupation. The use of the portable/temporary building shall be discontinued and the building fully removed from the school site once the classroom extension is first occupied.

Reason

The portable/temporary building is only proposed to be a temporary structure during the construction phase of the approved development.

4. The external surfaces of the extension hereby permitted shall be of a similar colour, texture and size of those of the existing building, and shall be retained thereafter.

Reason

To ensure the development visually reflects the existing building and to comply with policy CG3 of Bolton's Core Strategy.

5. The development hereby permitted shall be carried out in complete accordance with the following

approved plans:

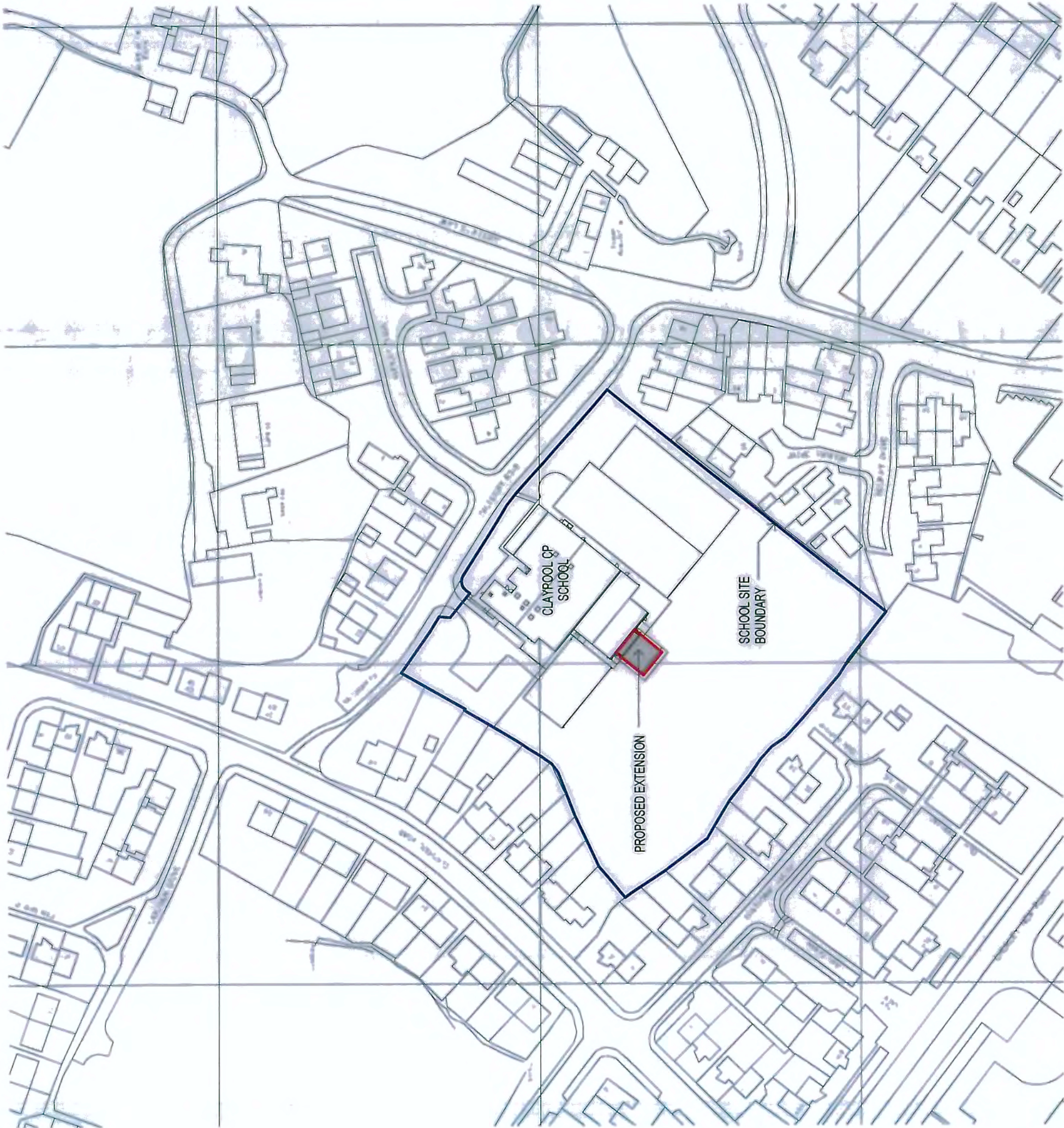
335.330.202 Rev. P2; "Proposed Site Plan"; dated 29.04.16

335.330.302 Rev. P2; "Proposed Overall Floor Plan"; dated 29.04.16 and received 14 Jun 2016

335.330.402 Rev. P2; "Proposed Elevations"; dated 29.04.16

Reason

For the avoidance of doubt and in the interests of proper planning.



status

PLANNING

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All dimensions are to be checked on site by the contractor prior to proceeding with any work.

Hazard Identification		
ref	hazard	date

BOLTON COUNCIL
DEVELOPMENT & REGISTRATION
RECEIVED
03 MAY 2015
96467/16

P2	PLANNING ISSUE	NMc	29.04.16
P1	REVISED TO SUIT POTENTIAL 2FE SCHEME	NMc	14.04.16
ref	revision	by	date



Architects
Consulting Engineers
Quantity Surveyors

21 Silverwell Street
Bolton
BL1 1PR
T: 01204 525321
email@bghbolton.co.uk
www.bghbolton.co.uk

Bradshaw Gass & Hope LLP

client	BOLTON MBC		
project	CLAYPOOL CP SCHOOL - PROPOSED CLASSROOM EXTENSION		
drawing	SITE LOCATION PLAN		
drawn by	AC	date	22.03.16
checked by	NMc	date	29.04.16
scale	1 : 1250		A3
dwg no	335.330.200	rev	P2

	P2	PLANNING ISSUE VIEWED TO BLATPOTENTIAL STE SCORE	JC	29 MAY 18
	P1	REF	JC	TABULET
			Jc	FILED

Architects
Consulting Engineers
Quantity Surveyors

21 Silverdell Street
Botolph
BL1 1 9PR
t 01204 620321
email@bgltd.co.uk
www.bgltd.co.uk

Quantity Surveyors
www.brighton.co.uk

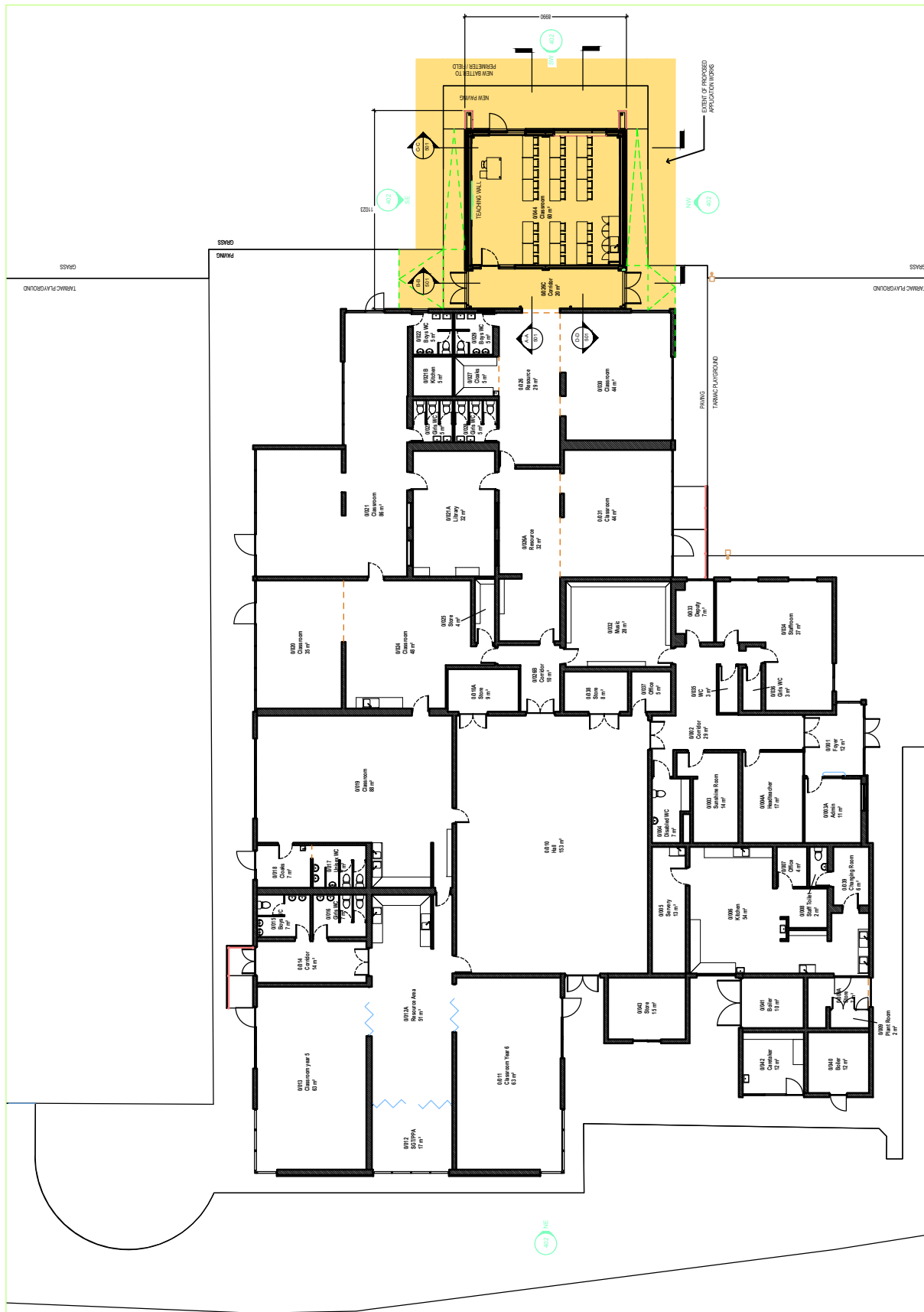
Bradshaw Gass & Hope LLP

IBC

project
CLAYPOOL CP SCHOOL - PROPOSED
CLASSROOM EXTENSION

drawing
PROPOSED OVERALL FLOOR PLAN

drawn by	AC	date	29.03.2016
checked by	NMC	date	29.04.2016
scale	1:100		A1
dwg no	335.330.302		P2



PROPOSED OVERALL FLOOR PLAN
1 : 100

MapXtreme 2008 © SDK Developer License, © 2008 Pitney Bowes MapInfo Corporation

22

478

488

490

500

463

467

469

Lee Hall Park

70.4m

Bolton Council

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Date of Meeting: 30/06/2016

Application Reference: 96470/16

Type of Application: Full Planning Application
Registration Date: 11/05/2016
Decision Due By: 05/07/2016
Responsible Officer: Alex Allen

Location: 488 LEIGH ROAD, WESTHOUGHTON, BOLTON, BL5 2JX

Proposal: CHANGE OF USE FROM SINGLE DWELLING TO TWO SELF-CONTAINED FLATS.

Ward: Westhoughton South

Applicant: Mr Gary Dooley
Agent : Harry Jackson Surveyors Ltd

Officers Report

Recommendation: Approve subject to conditions

Proposal

The applicant proposes the change of use of an existing end terraced dwelling (No. 488 Leigh Road) into two self contained flats. The ground floor flat would have a new entrance located within the gable end of the property and would provide a 2 bedroomed self-contained flat which would have separate lounge and kitchen areas. The first floor flat would have pedestrian access from the front door of the existing dwelling with stairs leading up to a one-bedroomed self-contained flat. The rear yard area would make provision for sufficient space for a bin store.

The elevations of the existing dwelling would largely remain the same under the proposed conversion with the exception of the provision of a new door way in the gable elevation, the bricking up of the existing doorway in the rear elevation of the property and the bricking up of a small ground floor window which is located in the gable elevation of the property.

Site Characteristics

The application relates to an end terraced property which is currently used as one dwellinghouse located in a row of six terraced two storey properties. The site is located within the heart of Daisy Hill village with similar terraced properties located on the western side of Leigh Road and a mix of detached (infill) houses and terraced properties located on the eastern side of Leigh Road. More recent residential development off Fenwick Close occupies land to the west.

The application site is a gable ended property with a gap of approximately 9 metres between this gable and the other gable ended property at No. 490 to the south. This gap is an unadopted road which is currently used for informal car parking by local residents. The majority of uses within the local area are residential but there are some commercial uses such as a hairdressers, vet surgery and shops in close proximity to the property. Access to the rear of properties on Leigh Road (Nos. 478 to 488) is gained from the unadopted road at the side of No. 488 / 490.

In addition, the application site is within walking distance (c. 387 metres) of Daisy Hill railway station which is located to the north off Leigh Road.

Policy

National Planning Policy Framework

Core Strategy CG3 The Built Environment, CG4 Compatible Uses, OA3 Westhoughton, SC1 Housing.

Analysis

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- * principle of residential conversion;
- * impact on the living conditions of existing / future adjoining uses;
- * impact on highway safety
- * other matters.

Principle of residential conversion

Guidance contained within NPPF and Core Strategy policy SC1 seek to ensure that Council's and their partners identify a range of housing sites for additional provision of dwellings whilst providing a mix of house types and tenures to meet the range of housing needs in the Borough. Included within the housing requirement would be additional dwellings created by changes of use/conversions and the more efficient use of vacant housing stock.

The proposal would provide one additional dwelling in a highly sustainable location. The change of use would convert an existing 3 bedroomed house to two self contained flats which would accommodate a total of 3 bedrooms.

Whilst the proposal would create one new dwelling the proposed change of use would provide for a wider mix of housing in the local area.

It is considered that the proposal would comply with Core Strategy SC1 and guidance relating to housing supply and the mix of property types contained within NPPF.

Impact on the living conditions of existing / future adjoining uses

Core Strategy policy CG4 seeks to ensure that new development proposals are compatible with surrounding land uses and occupiers, protecting amenity, privacy, safety and security.

Local residents raise a number of concerns in terms of loss of privacy and also the specification of

the conversion and whether this is likely to result in noise disturbance to adjoining neighbours.

In terms of loss of privacy the proposal would not create any additional windows in any of the elevations of the property. The windows overlooking No. 465 Leigh Road (opposite side of Leigh Road) would remain as a bedroom at first floor and change from a lounge to a bedroom at ground floor level. These rooms would remain the same distance away from properties on the east side of Leigh Road and there would be no further impacts on privacy.

In terms of the potential impact on the occupants of No. 490 Leigh Road, there are existing windows in the gable elevations of both 488 and 490 which provide windows for a number of main room windows. Whilst the distance between each window is relatively short at approximately 9 metres, these are existing windows. The current proposal would not create any additional windows. The proposal would change one of the first floor windows into a landing window which would reduce the potential overlooking to windows located within the gable of 490.

It is considered that the current proposal would not result in any loss of privacy to existing residents.

The other issues regarding the quality of the proposed conversion work would be a matter for the Building Regulations system to address.

Impact on highway safety

Core Strategy policies P5 and S1 seek to ensure that new development proposals provide sufficient car parking, servicing arrangements which promote road safety within the design of the new development.

The applicant proposes the provision of two off road car parking spaces which would be located adjacent / parallel with the existing gable of the property. The Council's maximum car parking standards as detailed within Appendix 3 of the Core Strategy state that for 1 bedroomed dwellings 1 car parking space should be provided and as for 2 bedroomed properties a maximum of 2 car parking spaces need to be provided.

Given that the standards are maximum and due to the modest nature and highly accessible location of the property the provision of 2 spaces for the development is considered to be adequate.

It is noted that the Council's Highways Engineers does not raise objection to the proposal.

Whilst at least one local resident has provided photos which show at least 6 cars parked on the land in between Nos. 488 and 490 Leigh Road this appears to be an informal arrangement. The management of this area would be a private matter between the current owners of the land and anyone else who decides to park there.

The sole consideration in this application is whether the applicant can make provision for sufficient off road car parking. It is considered that this can take place and thus the proposal complies with policy.

Other matters

Another matter has been raised by local residents:

Impact on infrastructure - the existing property is 3 bedroomed and perhaps more suited to family accommodation. The proposal would provide 3 bedrooms albeit in two flats rather than one house. It is considered that an additional dwelling in this location is unlikely to exacerbate any existing problems in terms of the capacity of existing schools, doctors and dentists. In addition, Core

Strategy policy IPC1 seeks to secure on / off site contributions for a range of matters on sites of 15 houses or more. This proposal is substantially below the threshold.

Conclusion

The proposal represents a modest change in both the appearance of the existing dwelling and the current use of the site. There would be little or no change to parking arrangements in the locality and the proposal would fully comply with policy. The concerns of residents and the Town Council are noted and in the main would be dealt with by other regimes e.g. Building Control through the Building Regulations system to achieve a conversion which does not detrimentally impact on the adjoining properties.

The proposal is recommended for approval subject to conditions.

Representation and Consultation Annex

Representations

Letters:- six objection letters have been received raising the following concerns:

- Existing levels of parking in and around the property result in congestion. The proposal will make matters worse. Photos have been provided which show 6 cars parked in the space between No. 688 and 690 Leigh Road, together with rows of cars parked along Leigh Road. Reference is also made to users of Daisy Hill railway station parking on Leigh Street during the day;
- The existing party wall between the application property and No. 486 Leigh Road are of poor quality and if the property is converted will result in potential disturbance to the occupiers of the adjoining property - ***this matter would be dealt with through the Building Regulations submission to ensure adequate sound attenuation between the two properties. This would also apply to sound attenuation between the two flats;***
- Loss of privacy - to both No. 490 and No. 465 Leigh Road;
- The area provides housing for families - how will flats and their tenants fit in? - ***the quality and behaviour of potential owner occupiers or tenants of the self contained flats is not a material consideration;***
- Tenants of the flats may bring associated problems of alcohol and substance misuse - there would be minor changes in the appearance of the property. The issue of potential problems tenants may bring with them is not a planning consideration and would not alter the character / appearance of a property;
- The proposal does not meet fire regulations - ***this again would be considered at the time the Building Regulations application is submitted and would not be something which the planning system looks at;***
- Increased stress as a result of the submission of the application (**not a material planning consideration**);
- Unsold properties in the area - ***inference that no new dwellings are required;***
- Increased infrastructure - e.g. doctors surgeries, school places etc are required to meet the increased demands;
- Potential concerns over the quality of any renovation of the property if permission is granted - ***again this is a Building Regulations matter not a material planning consideration;*** and
- Negative impact on house prices in the area - ***not a material planning consideration.***

Petitions:- one petition has been received with a total of 48 signatures from local residents (predominantly of Leigh Road) stating that they are against the proposed development. No further reasons are provided for the objection to the proposal.

Town Council:- raised no objections subject to sufficient parking spaces being provided and wall insulation is installed between No's 486 and 488 Leigh Road.

Elected Members:- no comments received.

Consultations

Advice was sought from the following consultees: Westhoughton Town Council and the Council's Highways Engineers.

Planning History

The property has no relevant planning history.

Recommendation: **Approve subject to conditions**

Recommended Conditions and/or Reasons

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason

Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. Before the approved/permitted development is first brought into use no less than 2 car parking spaces with minimum dimensions of 2.4 metres by 4.8 metres shall be marked out and provided within the curtilage of the site, in accordance with details to be submitted to and approved by the Local Planning Authority. Such spaces shall be made available for the parking of cars at all times the premises are in use.

Reason

To ensure that adequate provision is made for vehicles to be left clear of the highway and to comply with policy P5 and Appendix 3 of Bolton's Core Strategy.

3. The development hereby permitted shall be carried out in complete accordance with the following approved plans:

Ground Floor Layout, First Floor Layout, stamped received 1st June 2016
Proposed Elevations Drawing, stamped received 1st June 2016

Reason

For the avoidance of doubt and in the interests of proper planning.

96470/16

AA

Proposed Conversion of Terraced Property
at 466 Leigh Road Westborough U Bolton
B15 1XX to two self contained
Flats for Mr G Dooley

Scale 1:100

BOLTON COUNCIL
DEVELOPMENT & REGENERATION
RECEIVED

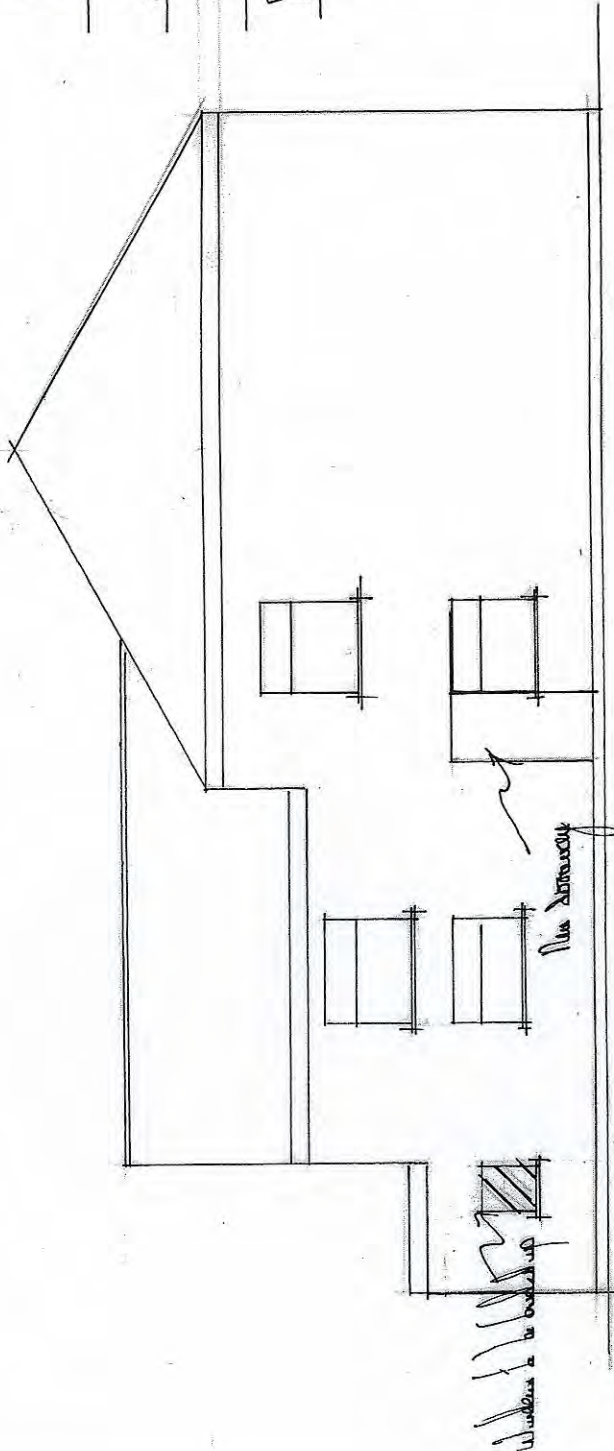
01 JUN 2016

* The Front Elevation *

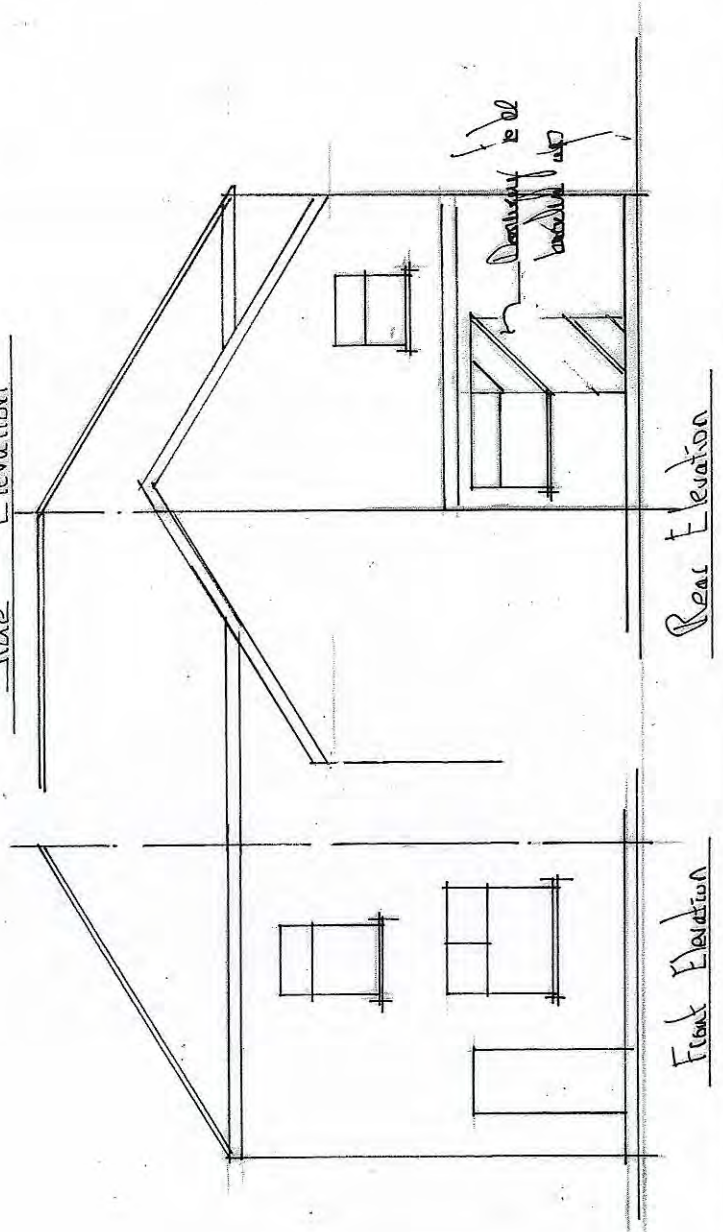
remains exactly the same as
before the works

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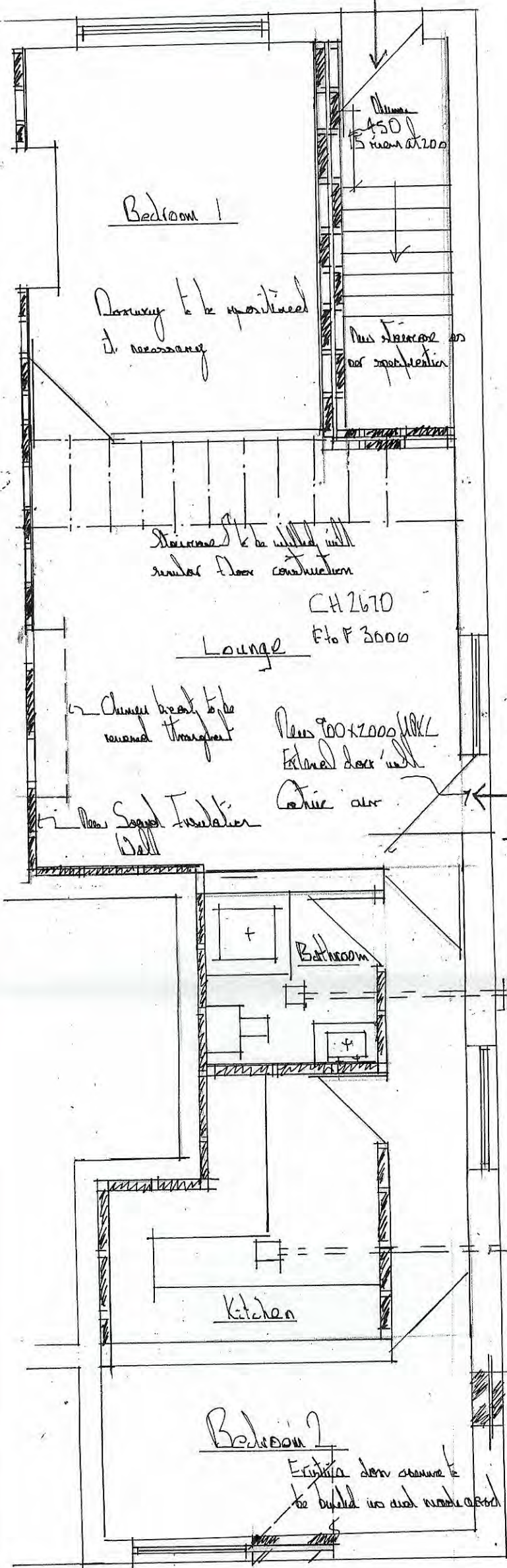
Side Elevation



Front Elevation

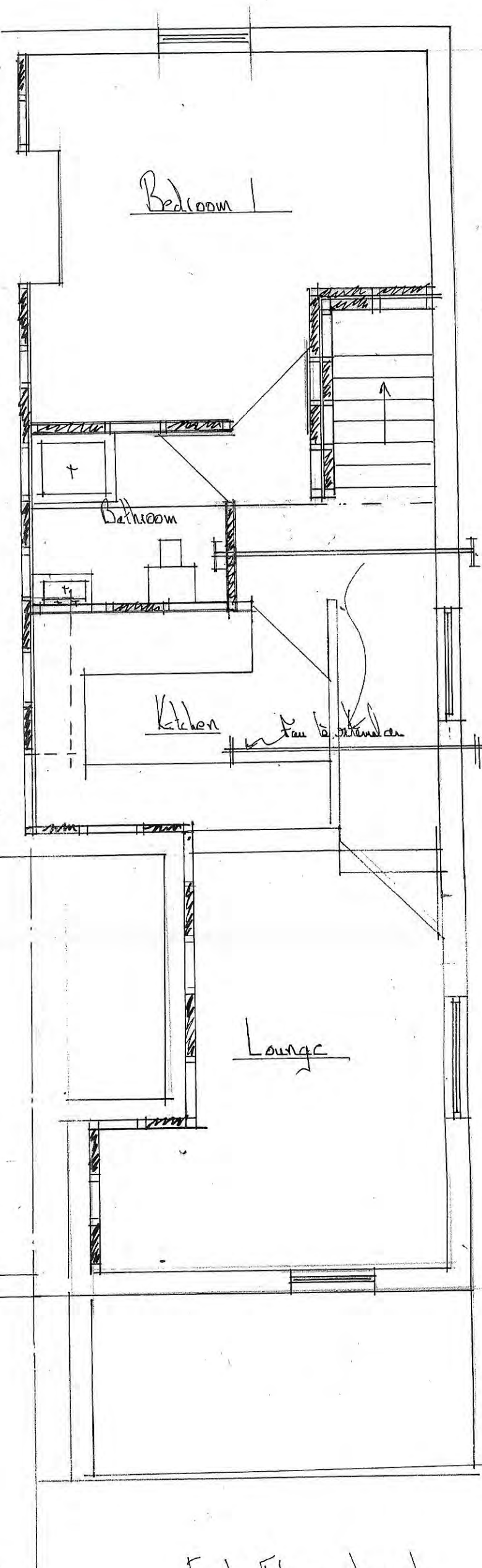
Rear Elevation

Flat 2 Entrance



Ground Floor Layout

Scale 1:50



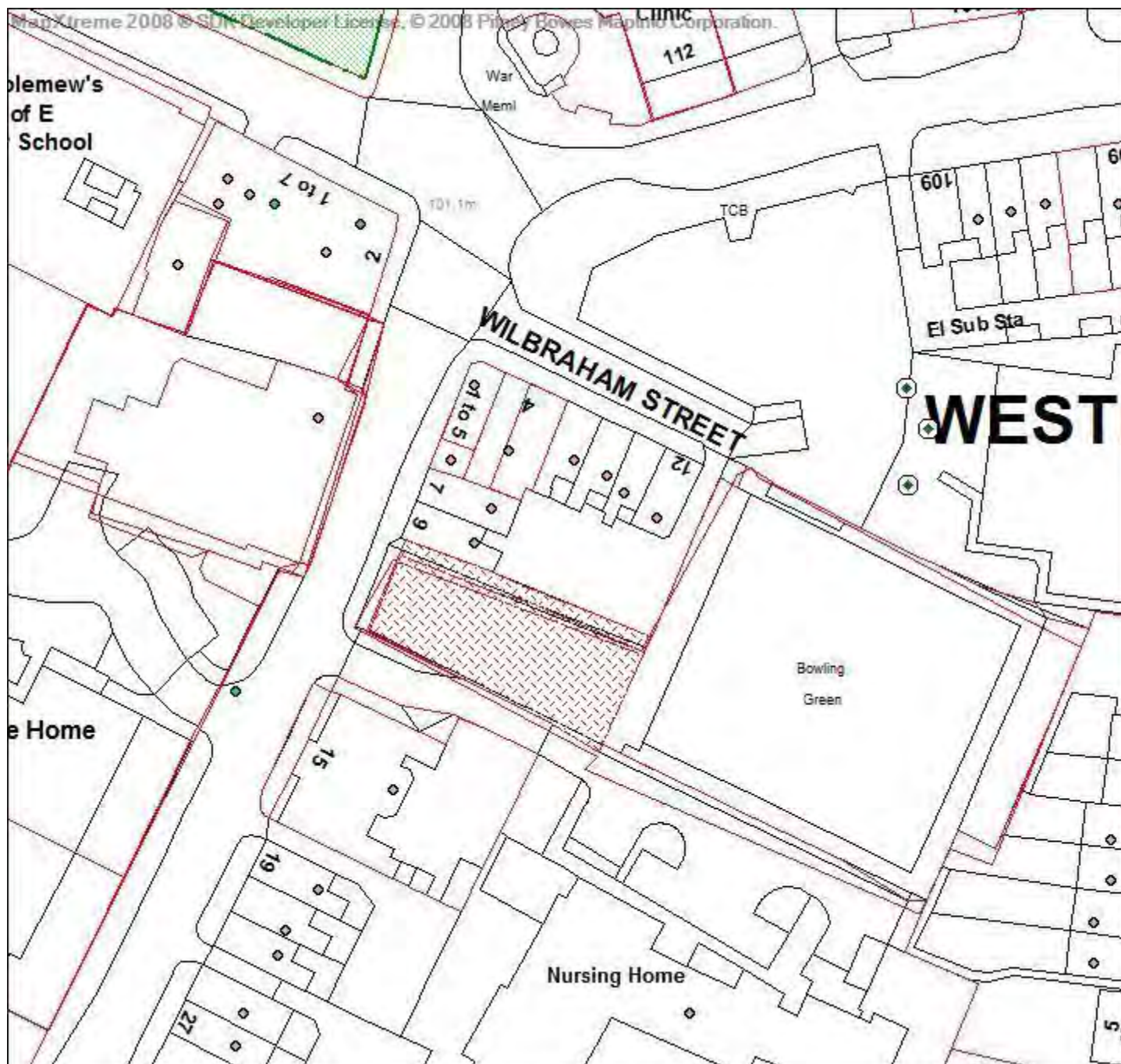
First Floor Layout

191 m² at 225
= 3150

BOLTON COUNCIL
DEVELOPMENT & REGENERATION
RECEIVED
01 JUN 2016

Conversion of Terraced Property at 488 Leigh Rd Westhoughton N. Bolton
BL5 2JX to two self contained flats for Mr. G Dooley

Application number 96482/16



Development & Regeneration Dept
Development Management Section

Town Hall, Bolton, Lancashire, BL1 1RU
Telephone (01204) 333 333

Bolton Council

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Date of Meeting: 30/06/2016

Application Reference: 96482/16

Type of Application: Full Planning Application
Registration Date: 09/05/2016
Decision Due By: 03/07/2016
Responsible Officer: Alex Allen

Location: LAND ADJACENT 9 WIGAN ROAD, WESTHOUGHTON, BOLTON, BL5 3RD

Proposal: ERECTION OF 7No APARTMENTS.

Ward: Westhoughton South

Applicant: Hearle Homes Ltd
Agent : Grosvenor Architectural Design Ltd

Officers Report

Recommendation: Approve subject to conditions

Proposal

The applicant proposes the erection of a part 3 storey and part single storey new build property which would accommodate 7 one bed roomed apartments. The proposed 3 storey element would be 8.3 metres to the eaves of the new build and 11.2 metres to the apex / ridge of the proposed roof. The design / appearance and materials used within the development would complement those used within the recently rebuilt Red Lion Public House.

The originally submitted plans provide for the provision of 3 off road car parking spaces together with a bin store and cycles store and ancillary landscaping. The applicant has agreed to amend to proposal to add another car parking space by the removal of the proposed cycle store.

Site Characteristics

The application relates to a rectangular shaped site located within Westhoughton town centre. Existing two storey terraced properties occupy a row of properties to the north which wrap around onto Wilbraham Street. Westhoughton Bowling Club occupy land to the east whilst a Childrens nursery (No. 15 Wigan Road) and sheltered accommodation occupy land/properties to the south / south east.

The newly rebuilt Red Lion public house now providing assisted living accommodation (part two / part three storey) is located on the opposite (West) side of Wigan Road.

Policy

National Planning Policy Framework

Core Strategy CG3 The Built Environment, CG4 Compatible Uses, OA3 Westhoughton P5 Accessibility, S1 Safe.

Supplementary Planning Documents - General Design Principles.

Analysis

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- * principle of residential development;
- * impact on the character and appearance of the area;
- * impact on the living conditions of existing / future adjoining uses;
- * impact on highway safety
- * other matters.

Principle of residential development

Guidance contained within NPPF and Core Strategy policy SC1 seek to ensure that Council's and their partners identify a range of housing sites for additional provision of dwellings whilst providing a mix of house types and tenures to meet the range of housing needs in the Borough.

The proposal would provide seven additional dwellings in a highly sustainable location.

It is considered that the proposal would comply with Core Strategy SC1 and guidance relating to housing supply and the mix of property types contained within NPPF.

Impact on the character and appearance of the area

Core Strategy policies CG3 and OA3 seek to ensure that new development proposals conserve and enhance local distinctiveness and which requires development to be compatible with the surrounding area, in terms of scale, massing, grain, architecture, street enclosure and local materials. Core Strategy policy CG3.4 also seeks to ensure development conserves and enhances the heritage significance of heritage assets and heritage areas.

The proposal is located just outside the southern part of the Westhoughton Conservation Area. However, the application site is stated as an area of opportunity to enhance the setting of the Conservation Area. At the time of writing of the Conservation Area Appraisal it was commented that:

'...has a detrimental effect on its setting. It is roughly surfaced, with weed growth around the perimeter together with concrete posts for a post and panel fence. At present it is used for parking and storing bins and is unsightly. Its appearance and the setting of the Conservation Area would be greatly improved by the removal of the fence posts, provision of a bin store, resurfacing the parking area and tree and shrub planting around the perimeter.'

Whilst the proposal would provide a 3 storey building rather than provide upgraded hard and soft landscaping, it is considered that the development has the potential to provide another high quality development adjacent to the Conservation Area. The three storey scale of the property would not be dissimilar in height than the adjoining properties and would be viewed in the context of the redeveloped Red Lion public house which has been rebuilt successfully to further enhance the Conservation Area. The rebuilt Red Lion is part two and part three storey.

It is considered that the proposal would enhance the setting of the Conservation Area and also enhance the character and appearance of the application site.

It is considered that the proposal complies with policy.

Impact on the living conditions of existing / future adjoining uses

Core Strategy policy CG4 seeks to ensure that new development proposals are compatible with surrounding land uses and occupiers, protecting amenity, privacy, safety and security.

Local residents raise a number of concerns in terms of loss of privacy and also the specification of the conversion and whether this is likely to result in noise disturbance to adjoining neighbours.

In terms of loss of privacy the proposal would create new habitable main room windows facing the recently rebuilt Red Lion public house. The interface distances between the two properties would be below the recommended distances outlined within the General Design Principle SPD. However, given the town centre location of the site and the interfaces achieved between existing properties it is considered that the relationship between the two is acceptable and is something which also enhances urban design especially in a Conservation Area creating a feeling of enclosure.

The main body of the building would be sited in line with the building line of Wigan Road with only the single storey element projecting beyond the building line. Whilst a number of residents have raised concerns over loss of light due to the siting of the main part of the house being compatible with existing properties on Wigan Road it is not considered that the proposal would result in any significant loss of light. The single storey element would also not impact on existing/future residents.

There are a number of windows which overlook the application site in the gable of No. 9 Wigan Road. It is considered that these windows are non main windows and the proposed development would not be detrimental to the outlook of the owner/occupier. Furthermore, it is noted that the applicant states that No. 9 Wigan Road is within their control and they will be infilling the existing windows which overlook the proposed development site.

In terms of the adjacent nursery, there are windows which overlook the application site. However, none of these windows are principal/main room windows.

It is considered that the current proposal would not result in any loss of privacy or amenity to existing residents or adjoining uses.

Impact on highway safety

Core Strategy policies P5 and S1 seek to ensure that new development proposals provide sufficient car parking, servicing arrangements which promote road safety within the design of the new development.

The applicant originally proposed the provision of three off road car parking spaces which would be located at the rear of the property. adjacent / parallel with the existing gable of the property. The Council's maximum car parking standards as detailed within Appendix 3 of the Core Strategy state that for 1 bedroomed dwellings 1 car parking space should be provided. The applicant has agreed to

provide a total of 4 off road spaces by removing the proposed cycle store. This would still provide a bin store for the occupants of the apartments.

Given that the standards are maximum and the availability of parking elsewhere within Westhoughton Town Centre the provision of 4 spaces for the development is considered to be adequate.

It is noted that the Council's Highways Engineers does not raise objection to the proposal.

A number of residents have commented that the land is currently used for informal parking by a range of users including people dropping off at the adjacent nursery, bowling club members, visitors to the wider town centre, visitors to the sheltered accommodation at Thurstons and residents of Wilbraham Street and Wigan Road. However, as the current applicant is the owner of the land people park on this land at their discretion and appear to have no legal right over parking vehicles on the land.

In addition, a number of residents of Thurstons have raised concerns over the potential impact on access to their accommodation. It is noted that the development would not reduce the width of the access to Thurstons. Construction traffic / workers vehicles would be required by law not to block access. This issue lies outside the remit of the planning system.

The sole consideration in this application is whether the applicant can make provision for sufficient off road car parking. It is considered that this can take place and thus the proposal complies with policy.

Conclusion

The proposal would improve an underused site which has the potential through this development to regenerate the local area by the provision of a new building which complements the scale and design of recently built developments in the area. Whilst the proposal potentially under provides for car parking for the proposed use it is considered that the location of the site within Westhoughton town centre with the prevalence of numerous free car parking areas would offset this under provision.

The proposal complies with policy and is recommended for approval subject to conditions.

Representation and Consultation Annex

Representations

Letters:- six letters of objection have been received raise the following concerns:

- the land is currently used for parking for a number of the adjoining uses (i) day nursery, (ii) cafe, (iii) Thurstons and (iv) bowling club. The proposal would remove this car parking;
- currently cars park on the approach / road to Thurstons. The proposal will intensify this problem;
- the land is not derelict but actively used - ***this is correct, although the site frontage is poorly maintained;***
- the adjoining Bowling Club have a retained access right over the application site - ***this is a private matter between the Bowling Club and the owners of the land to resolve. Planning permission does not override rights over land.***

Petitions:- no petitions received.

Town Council:- raise objection to the proposal as it represents overdevelopment, would have a detrimental impact on the street scene and on access to Thurstons. The proposal would also provide insufficient car parking.

Elected Members:- no comments received.

Consultations

Advice was sought from the following consultees: the Council's Pollution Control officers, Highways Engineers.

Planning History

No planning history.

Recommendation: **Approve subject to conditions**

Recommended Conditions and/or Reasons

1. The development hereby permitted shall be begun before the expiration of one year from the date of this permission.

Reason

Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. Before the approved development is commenced details of the existing and proposed ground levels within the site and on adjoining land including spot heights, cross sections and finished floor levels of all buildings and structures shall be submitted to and approved in writing by the Local Planning Authority. The approved details shall be implemented in full and retained thereafter.

Reason

To safeguard the visual appearance and or character of the area.

3. Prior to the commencement of the development:-

- A Site Investigation report shall be submitted to and approved in writing by the Local

Planning Authority. The investigation shall address the nature, degree and distribution of land contamination on site and shall include an identification and assessment of the risk to receptors focusing primarily on risks to human health, property and/or the wider environment; and

- The details of any proposed remedial works shall be submitted to, and approved in writing by the Local Planning Authority. The approved remedial works shall be incorporated into the development during the course of construction and completed prior to occupation of the development or the development being first brought into use; and

Prior to first use/occupation of the development hereby approved:

- A Verification Report shall be submitted to, and approved in writing by, the Local Planning Authority. The Verification Report shall validate that all remedial works undertaken on site were completed in accordance with those agreed by the LPA.

Reason

To safeguard the amenity of the future occupants of the development and to comply with Core Strategy policy CG4.

4. Prior to the commencement of development the details (samples if required) of the type and colour of materials to be used for the external walls and roof to be agreed with the Local Planning Authority. The approved materials shall be implemented in full thereafter.

Reason

To ensure the development reflects local distinctiveness and to comply with policy CG3 of Bolton's Core Strategy.

5. Prior to the commencement of development a detailed specification for all doors and windows shall be submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be implemented in full before the development is first occupied or brought into use and retained thereafter.

Reason

To ensure the development reflects local distinctiveness.

6. Before the development is first brought into use a detailed scheme shall be submitted to and approved by the Local Planning Authority showing the design, location and size of a facility to store refuse, waste materials and cycles. The approved scheme shall be implemented in full prior to the development hereby approved being brought into use and retained thereafter.

Reason

To ensure satisfactory provision is made for the storage of waste materials.

7. Trees and shrubs shall be planted on the site in accordance with a landscape scheme to be submitted and approved in writing by the Local Planning Authority prior to the development being first brought into use. The approved scheme shall be implemented in full and carried out within 6 months of the occupation of any of the buildings or the completion of the development, whichever is the sooner, or in accordance with phasing details included as part of the scheme and subsequently approved by the Local Planning Authority. Any trees and shrubs that die or are removed within five years of planting shall be replaced in the next available planting season with others of similar size and species.

Reason

To reflect and soften the setting of the development within the landscape.

8. Before the approved/permitted development is first brought into use no less than 3 car parking spaces with minimum dimensions of 2.4 metres by 4.8 metres shall be marked out and provided within the curtilage of the site, in accordance with Drawing No. 159.01.P1. Such spaces shall be made available for the parking of cars at all times the premises are in use.

Reason

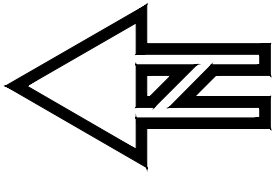
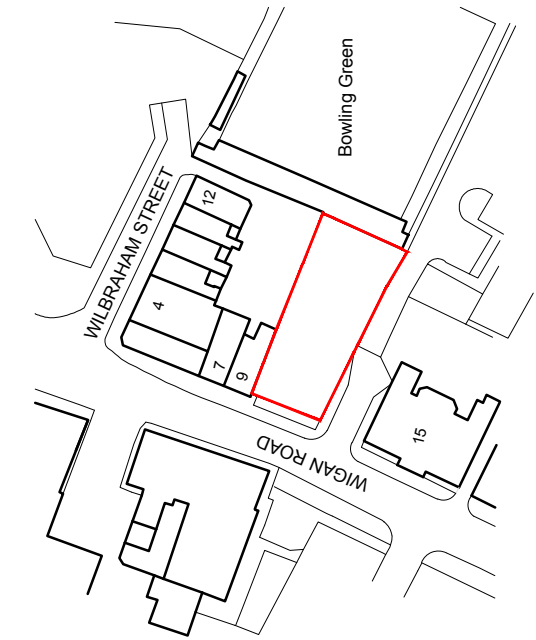
To ensure that adequate provision is made for vehicles to be left clear of the highway and to comply with policy P5 and Appendix 3 of Bolton's Core Strategy.

9. The development hereby permitted shall be carried out in complete accordance with the following approved plans:

Drawing No. 159.01.P01 - Site Location and Location Plans dated May 2016;
Drawing No. 159.01.P02 - Proposed Floor Plans dated May 2016;
Drawing No. 159.01.P03 - Proposed Elevations dated May 2016; and
Drawing No. 159.01.P04 - Proposed Streetscene dated May 2016.

Reason

For the avoidance of doubt and in the interests of proper planning.



A	20.06.16	CYCLE STORE OMITTED, ADDITIONAL CAR PARKING SPACE ADDED	
Rev	Date	Revision	
Title			
Site Layout & Location Plans			

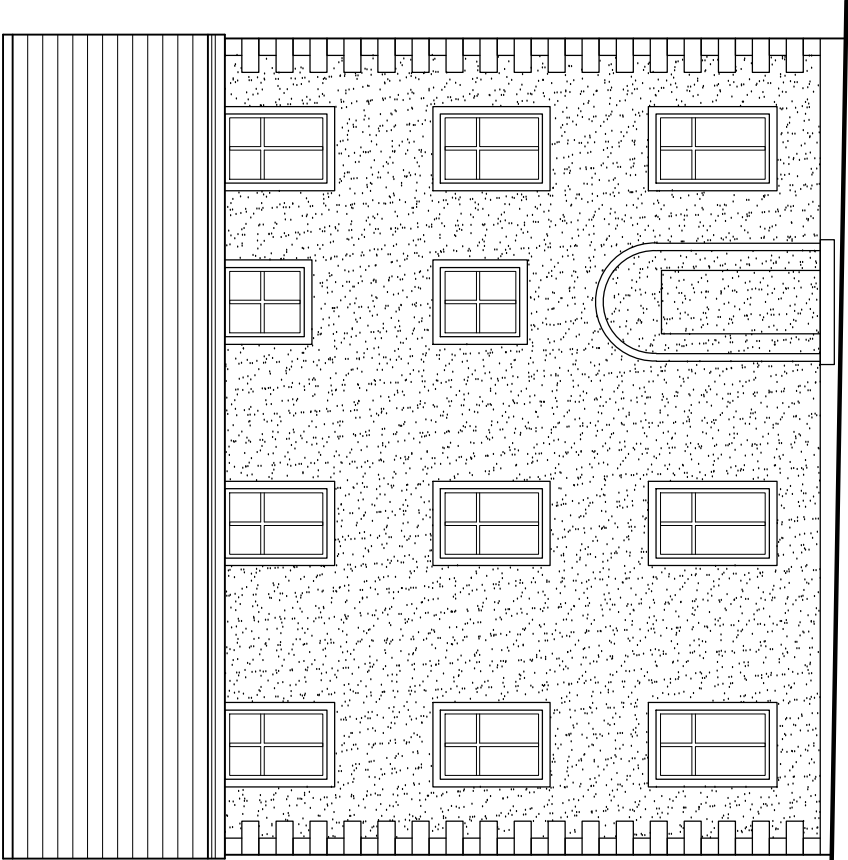
Project	Proposed 7 No. Apartments Wigan Road, Westhoughton		
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Scale	1:200, 1:1250		
Date	MAY 2016		
Dwg No	159.01.P.01		Rev A

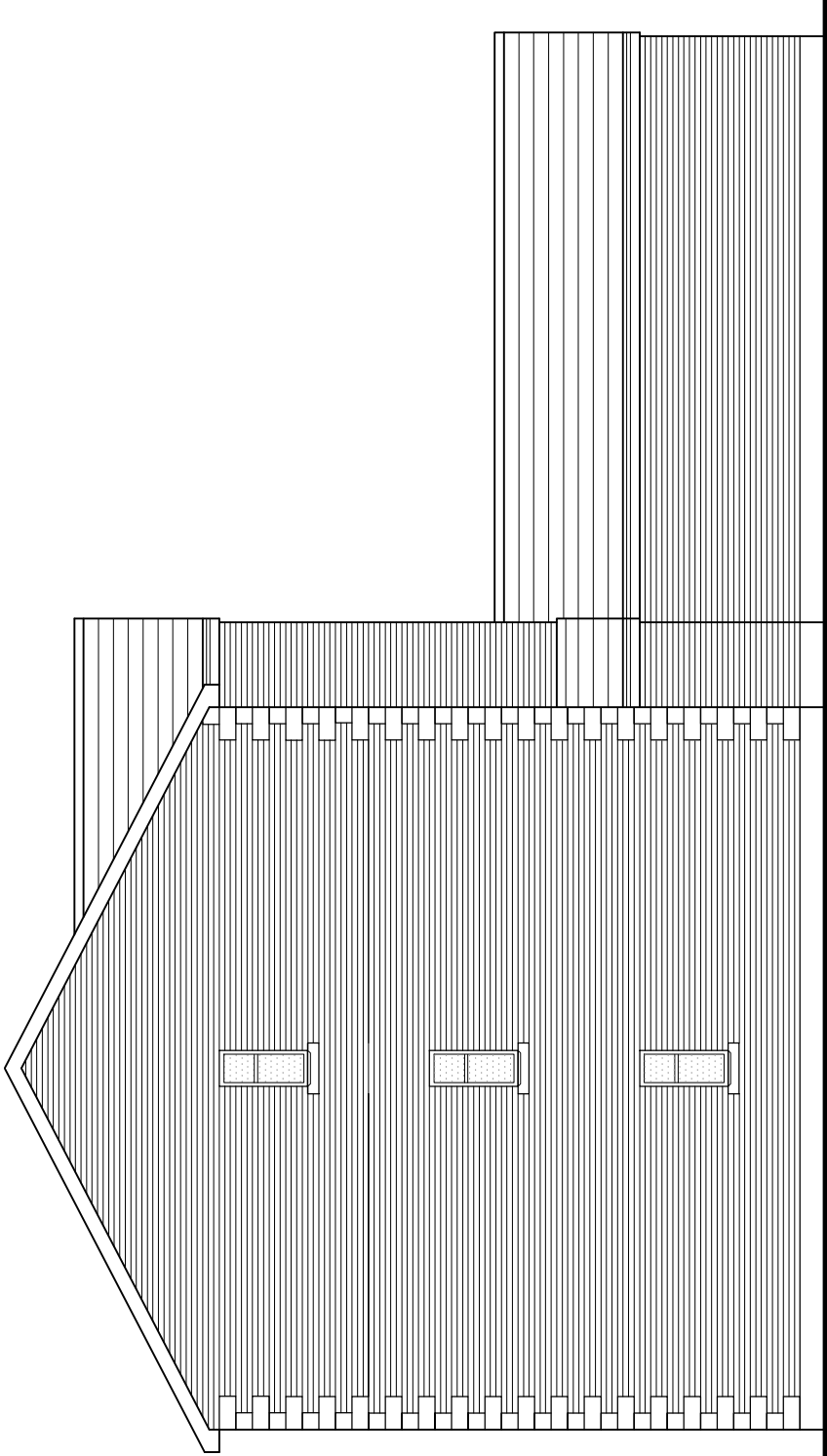
grosvenor
architectural design

Hearle House, Unit 5
East Terrace Business Park
Euxton Lane Chorley PR7 6TB

Tel : 01257 208820
Web : www.GrosvenorArchitectural.co.uk
E-Mail : enquiries@GrosvenorArchitectural.co.uk

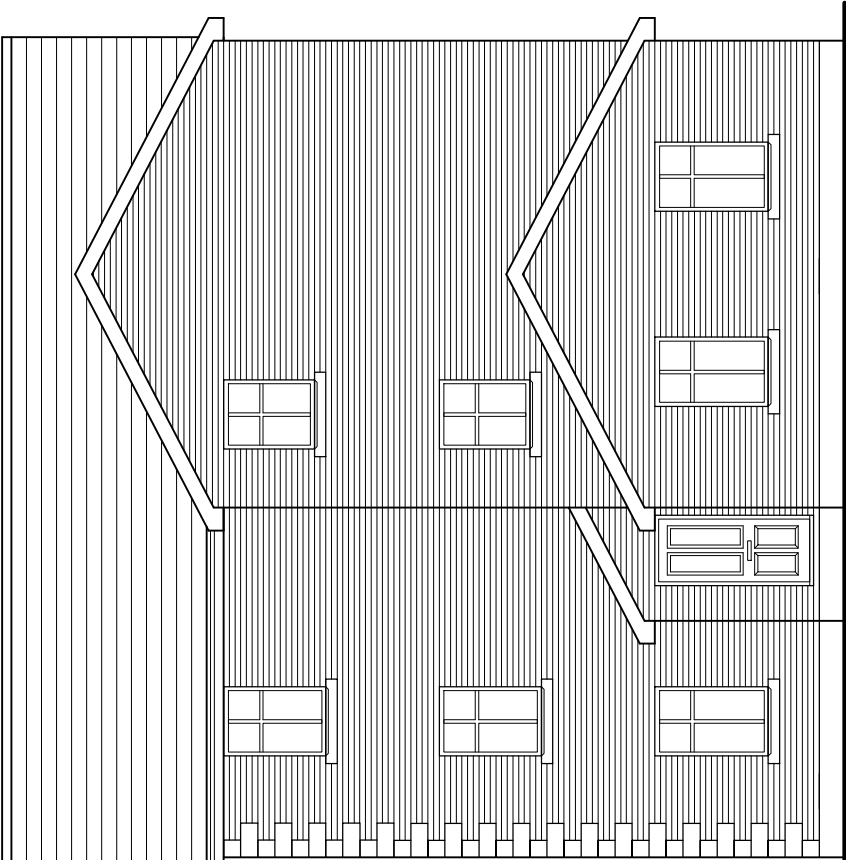


Proposed Front Elevation

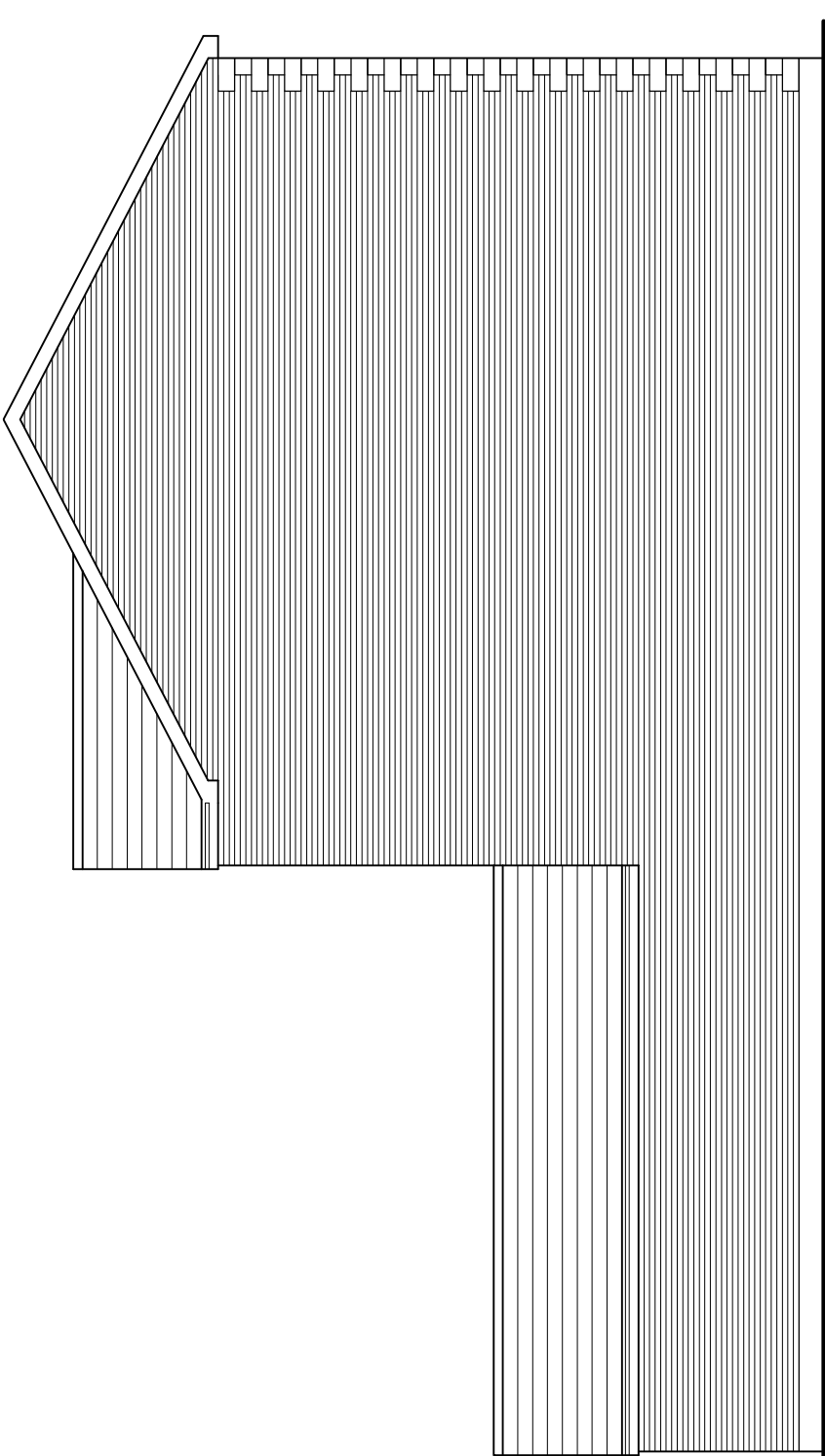


Proposed Side Elevation

Rev	Date	Revision
Title		
Proposed Elevations		



Proposed Front Elevation



Proposed Side Elevation

Project	Proposed 7 No. Apartments	
	Wigan Road, Westhoughton	

Scale	1:100	
Date	MAY 2016	
Dwg No	159.01.P.03	Rev

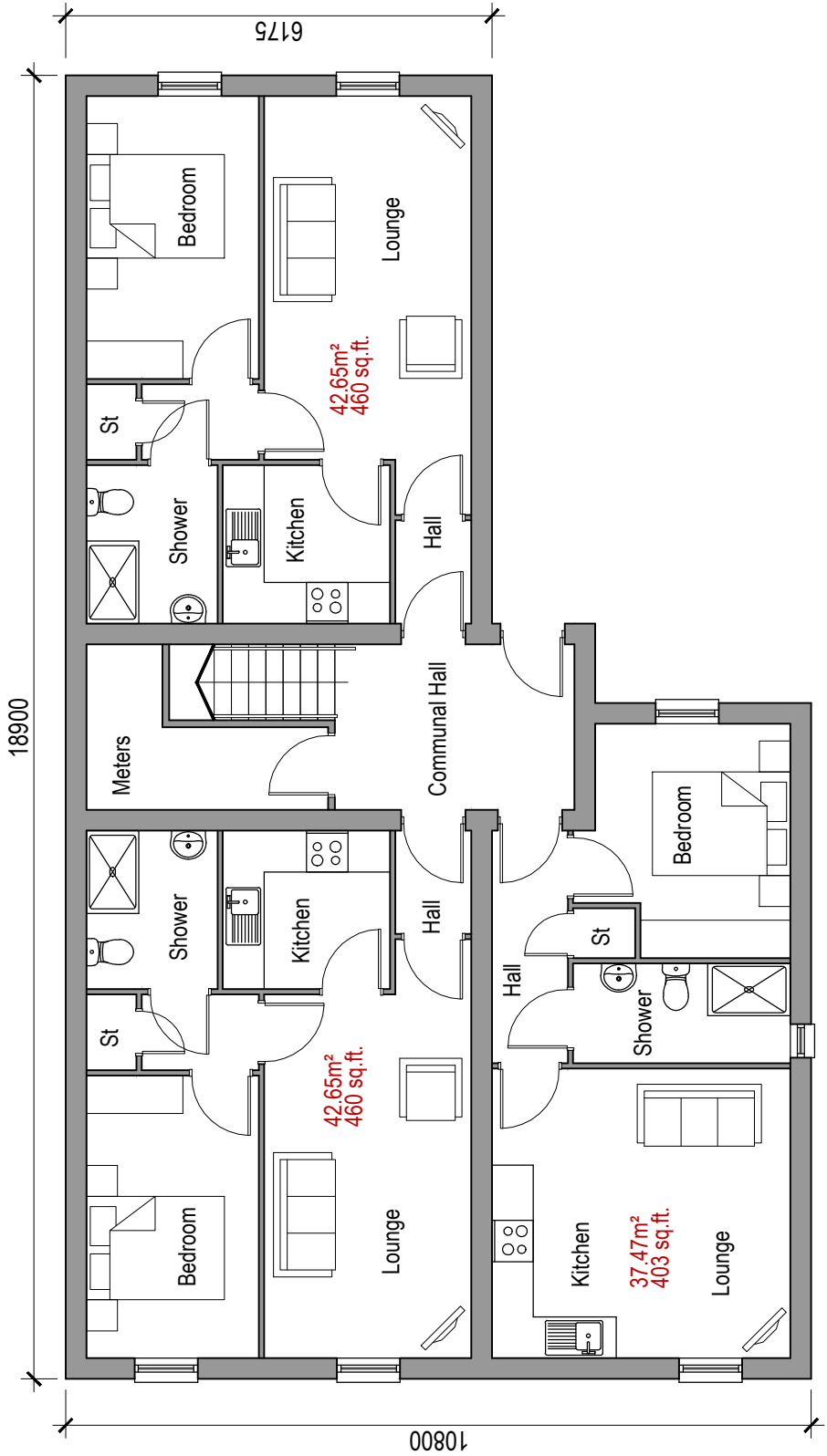
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East Terrace Business Park
Euxton Lane Chorley PR7 6TB

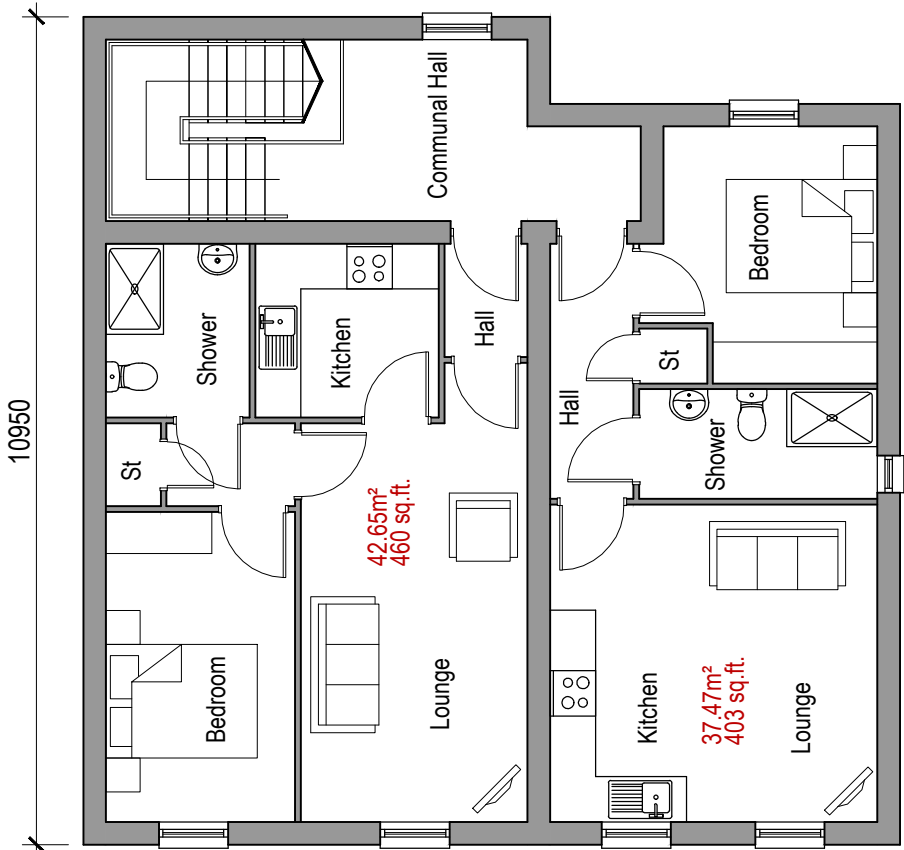
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Web : www.GrosvenorArchitectural.co.uk

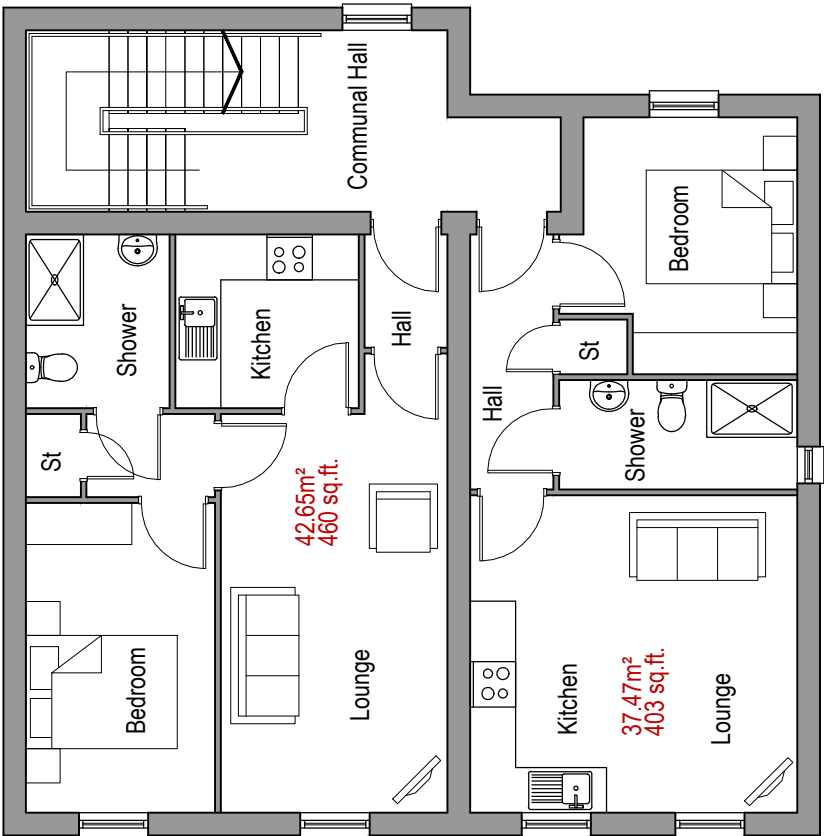
E.Mail : enquiries@GrosvenorArchitectural.co.uk



Proposed Ground Floor Plan



Proposed Second Floor Plan



Proposed First Floor Plan

Rev	Date	Revision
Title		
Proposed Floor Plans		

Project	Proposed 7 No. Apartments	
	Wigan Road, Westhoughton	

Scale	1:100	
Date	MAY 2016	
Dwg No	159.01.P.02	Rev

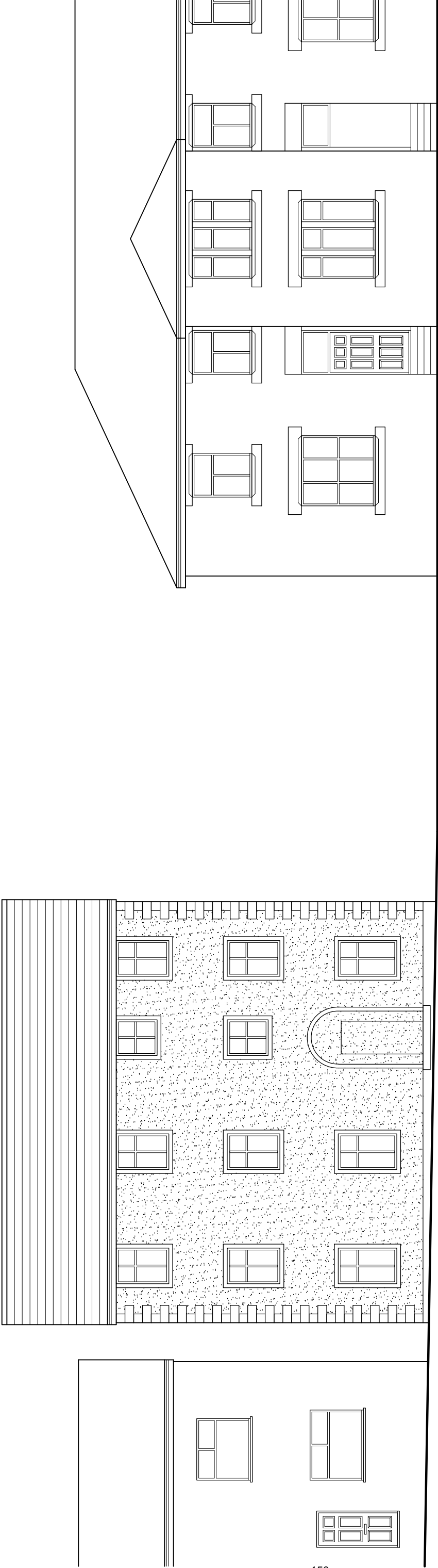
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Project

Proposed 7 No. Apartments
Wigan Road, Westhoughton

Rev	Date	Revision	
Title			
Proposed Streetscene			

Scale	1:100		
Date	MAY 2016		
Drg No		Rev	
159.01.P.04			

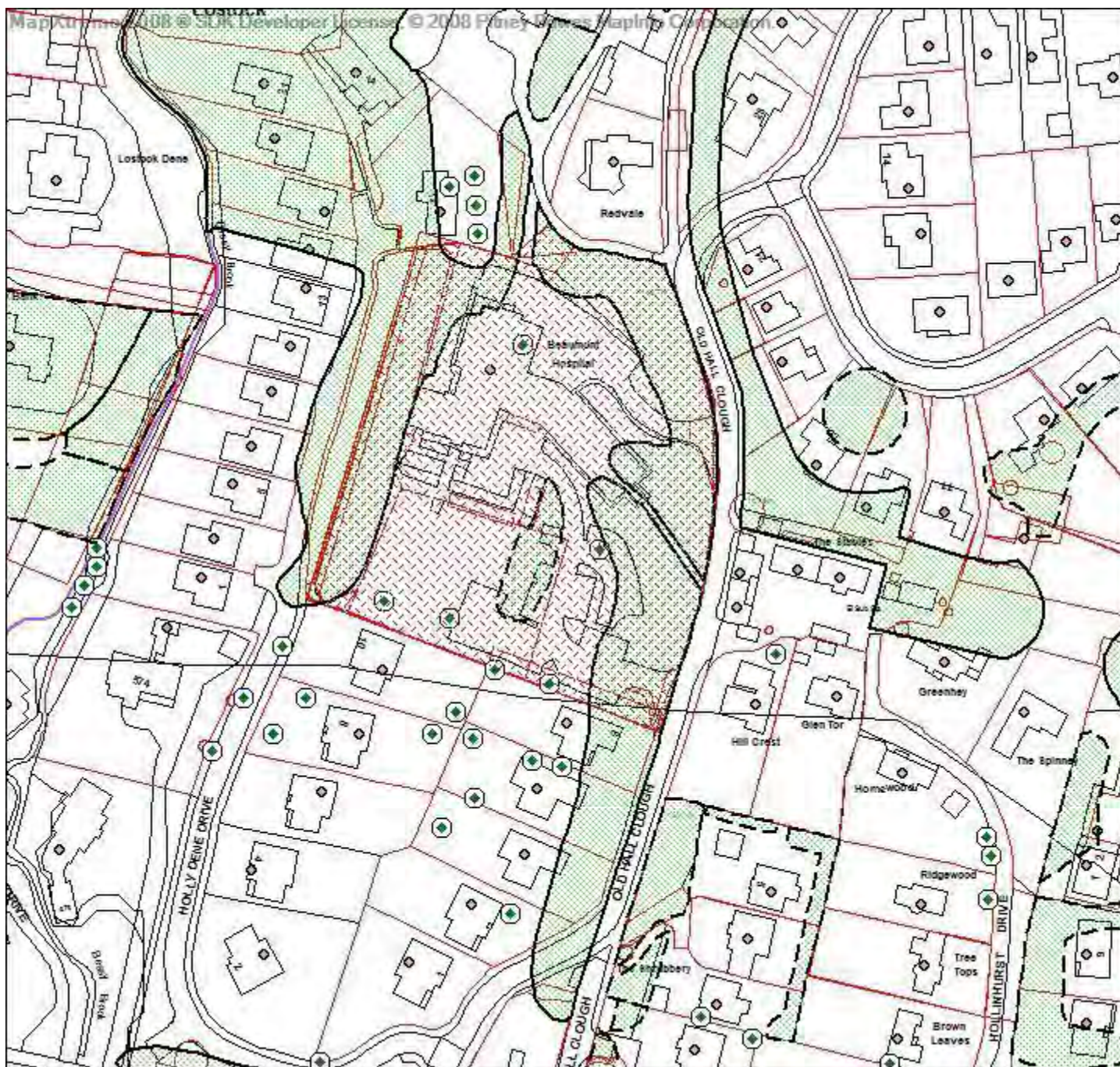
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architectural design

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Euxton Lane Chorley PR7 6TB

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Web : www.GrosvenorArchitectural.co.uk
E.Mail : enquiries@GrosvenorArchitectural.co.uk

Application number 96488/16



Development & Regeneration Dept
Development Management Section

Town Hall, Bolton, Lancashire, BL1 1RU
Telephone (01204) 333 333

Bolton Council

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Date of Meeting: 30/06/2016

Application Reference: 96488/16

Type of Application: Full Planning Application

Registration Date: 10/05/2016

Decision Due By: 04/07/2016

**Responsible
Officer: Kara Hamer**

**Location: BEAUMONT HOSPITAL, OLD HALL CLOUGH, LOSTOCK,
BOLTON, BL6 4LA**

**Proposal: ERECTION OF CHILLER COMPOUND TOGETHER WITH
ACOUSTIC FENCE, QUENCH PIPE AND EXTERNAL
ALTERATIONS TO NORTH ELEVATION TO ACCOMMODATE
NEW MRI MACHINE.**

Ward: Heaton and Lostock

Applicant: BMI Healthcare Limited

Agent : Walsingham Planning Limited

Officers Report

Recommendation: Approve subject to conditions

Proposal

The Beaumont Hospital seek permission for the erection of a free standing chiller and compound together with acoustic fence, quench pipe and external alterations to the rear (north) elevation of the main hospital building to accommodate a new MRI machine.

The proposed chiller compound will be located within part of the existing car parking area and will not result in the loss of car parking spaces. Alterations to the north elevation of the hospital building comprise blocking up two windows.

Currently a mobile MRI machine visits the hospital site twice per week. This facility results in the temporary loss of 6 - 8 car parking spaces within the lower car park.

Site Characteristics

Beaumont Hospital is a small private hospital located within a built up suburban residential area with access via Old Hall Clough. Many trees including TPOs surround the site and the locality which adds much to its character.

Policy

National Planning Policy Framework 2012

Core Strategy Policies CG1.2 Safeguard and Enhance Urban Biodiversity; CG3.2 Conserve and Enhance Local Distinctiveness; CG3.3 Scale, Massing, Grain and Form; CG4.1 Compatible Uses; OA4.4 Conserve and Enhance Physical Character of West Bolton; OA4.5 Massing and Materials of

Analysis

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- * impact on the Character and Appearance of the Area and Amenity of Neighbouring Residents
- * impact on Protected Trees

Impact on the Character and Appearance of the Area and Amenity of Neighbouring Residents

The NPPF in paragraph 56 states that The Government attaches great importance to the design of the built environment. Good design is a key aspect of sustainable development, is indivisible from good planning, and should contribute positively to making places better for people.

Core Strategy policies CG3.2 and CG3.3 require proposals to respect and enhance local distinctiveness, being compatible with the surrounding area in terms of scale, massing, grain, form, architecture and landscape treatment, including hard/soft landscaping and boundary treatment. OA4.4 aims to conserve and enhance the character of the existing physical environment, especially the conservation areas at Deane and Chorley New Road, and the historic registered Hulton Park. Policy OA4.5 ensures that development in West Bolton has particular regard to the overall density, plot sizes, massing and materials of the surroundings – these features should be retained where possible.

Core Strategy policy CG4.1 ensures that new development is compatible with surrounding land uses and occupiers, protecting amenity, privacy, safety and security.

The proposed chiller unit and quench pipe would be sited on the rear (north) elevation of the hospital building within close proximity to a small section of car parking spaces and an existing adjacent clinical waste store. The applicant submits that it is essential that the chiller and quench pipe are located within close proximity to the MRI machine.

Acoustic fencing would surround the proposed chiller measuring 3.250m x 5.3m, with part weather louvre (inlet/discharge) part solid metal external surface, providing double door entry, to be painted in an external colour to be agreed with the LPA.

The quench pipe outlet will be located on top of an existing hoist shaft and will measure 200mm in diameter with 50mm insulation and will not be higher than the highest part of the building. It is considered that the size and height of the quench pipe means that it will not be visible from ground level and due to the surrounding mature trees and vegetation is screened from neighbouring properties.

Alterations to the north elevation of the hospital building comprise blocking up two windows with brickwork to match the existing.

The north elevation of the hospital is screened by protected mature trees and vegetation and a single paved track 'Holly Hill' (leading to Hollywood Stables) separates the hospital site and its screening from the residential property at 'Redvale', the side elevation of which faces the rear (north) of the hospital and has an interface distance of approx. 35m. The nearest residential property is at no. 12 Holly Dene Drive and is sited approx 32m from the proposal, not directly overlooking the hospital site and screened by mature trees and vegetation.

A Condition will be included in an approving Decision Notice for the LPA to approve a scheme to limit noise emissions from the proposed Chiller Unit prior to development commencing. In addition, Acoustic Fencing is proposed to fully screen the Chiller Unit, painted in a colour to match the building and approved by the LPA.

The Council's Drainage Team have been consulted and have raised no objections.

The proposed works are considered to be minor in nature and are essential for the operation of the MRI machine. The proposal will be viewed against the backdrop of a small and busy hospital site and it is therefore considered that subject to Conditions, the proposal complies with policies CG3 and CG4 of Bolton's Core Strategy.

Impact on Protected Trees

The NPPF states in paragraph 109 that the planning system should contribute to and enhance the natural and local environment by protecting and enhancing valued landscapes, geological conservation interests and soils.

Core Strategy Policy CG1.2 safeguards and enhances biodiversity in the borough by protecting sites of urban biodiversity including trees, woodland and hedgerows from adverse development, and improving the quality and inter connectivity of wildlife corridors and habitats.

The Council's Trees and Woodland Officer has raised no objections. It is therefore considered that the proposal accords with Core Strategy Policy CG1.2.

Conclusion

As detailed above and subject to Conditions, the proposed chiller unit and quench pipe comply with aforementioned local and national planning policy and therefore Members are recommended to approve the application.

Representation and Consultation Annex

Representations

Letters; 2 letters have been received from nearby residents objecting to the proposal on the grounds of;

- We are concerned that the proposed chiller compound will be sited directly adjacent to our back garden. We are further concerned that there is no environmental impact assessment (noise pollution) included in the proposals. As the council is aware, our property is situated in a quiet cul-de-sac within a protected wooded area. The planning application makes note of an acoustic screening being required but provides no data regarding;
 - a) the level of decibels the chiller compound will emit
 - b) the frequency and use of the operating equipment supported by the chiller compound, in particular evenings and nights
 - c) the amount of mutation of noise afforded by the screening and in particular whether the unit is enclosed (sound proofed)
 - d) the nature of any noise, fumes, vapour arising from the quench pipe
 - e) the nature of any machine vibration, given that the surrounding area is primarily clay,shale making a highly receptive conduit to our neighbouring property.
- We presently experience a considerable degree of assorted noise emanating from Beaumont hospital and are therefore extremely concerned at the prospect of a new source of potential disturbance. It would be greatly appreciated if some indication of the level of noise likely to be generated by the proposed construction and operation of the chiller could be provided. We would request that the Planning Committee be cognisant of the fact that this is a residential area which has already been subjected to considerable disruption and inconvenience by the operation of the hospital.

Consultations

Advice was sought from the following consultees: Bolton Council's Tree and Woodland Officers, Drainage and Bridge Maintenance and The Coal Authority.

Planning History

In July 2001 temporary permission (for 5 years) was granted for the siting of portacabins (59048/01).

In 2006 the applicant applied for further temporary consent for the portacabins, splitting the proposal into two applications (73624/06 for one building and 73641/06 for two buildings (the buildings within this latest application)). Both permissions (approved in April 2006) granted temporary consent for a further 5 years.

In 2010 two applications were submitted to apply for further temporary consent for the three buildings, 85032/10 for the one building and 85033/10 for the two buildings (which are subject to this latest application). Permission was granted in November 2010 for a further temporary 3 year consent for the buildings (expiring 10th April 2013).

Permission was granted at Committee in June 2013 for the variation of condition 1 on application 85032/10 to allow the continued use of the one temporary building (89774/13). This permission gave the building further temporary permission for another two years.

Permission was granted with Conditions in July 2013 for continued use of two temporary buildings (90029/13). This permission gave the two buildings further temporary permission for another two years.

Permission was granted in March 2016 for the variation of condition 1 on approval 90029/13 to extend permission for two temporary buildings for a further three years (until 22nd July 2019) (95711/16).

Permission was refused in April 2016 for the retention of a temporary building (95858/16).

Various tree applications.

Recommendation: Approve subject to conditions

Recommended Conditions and/or Reasons

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason

Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. Before the approved development commences, a scheme shall be submitted to and approved in writing by the Local Planning Authority specifying the provision to be made to control externally generated noise emanating from the approved Chiller Unit. The approved scheme shall be implemented in full prior to the development being brought into use and retained thereafter.

Reason

To safeguard the living conditions of residents, particularly from the effects of noise and to comply with policy CG4 of Bolton's Core Strategy.

3. Notwithstanding the approved plans, the hereby approved acoustic fencing shall be painted in a colour which will be agreed in writing with the Local Planning Authority prior to installation of the acoustic fencing. The acoustic fencing shall remain so coloured thereafter.

Reason

To safeguard the character and appearance of the surrounding area and to comply with Policy CG3 of Bolton's Core Strategy.

4. The development hereby permitted shall be carried out in complete accordance with the following approved plans:

Site Location Plan (scanned to file 06/05/16)

Ground floor layout as proposed, Drwg. No 3039-P-007 A (scanned to file 06/05/16)

External Elevation 1 - as proposed, Drwg. No 3039-P-025 A (scanned to file 06/05/16)

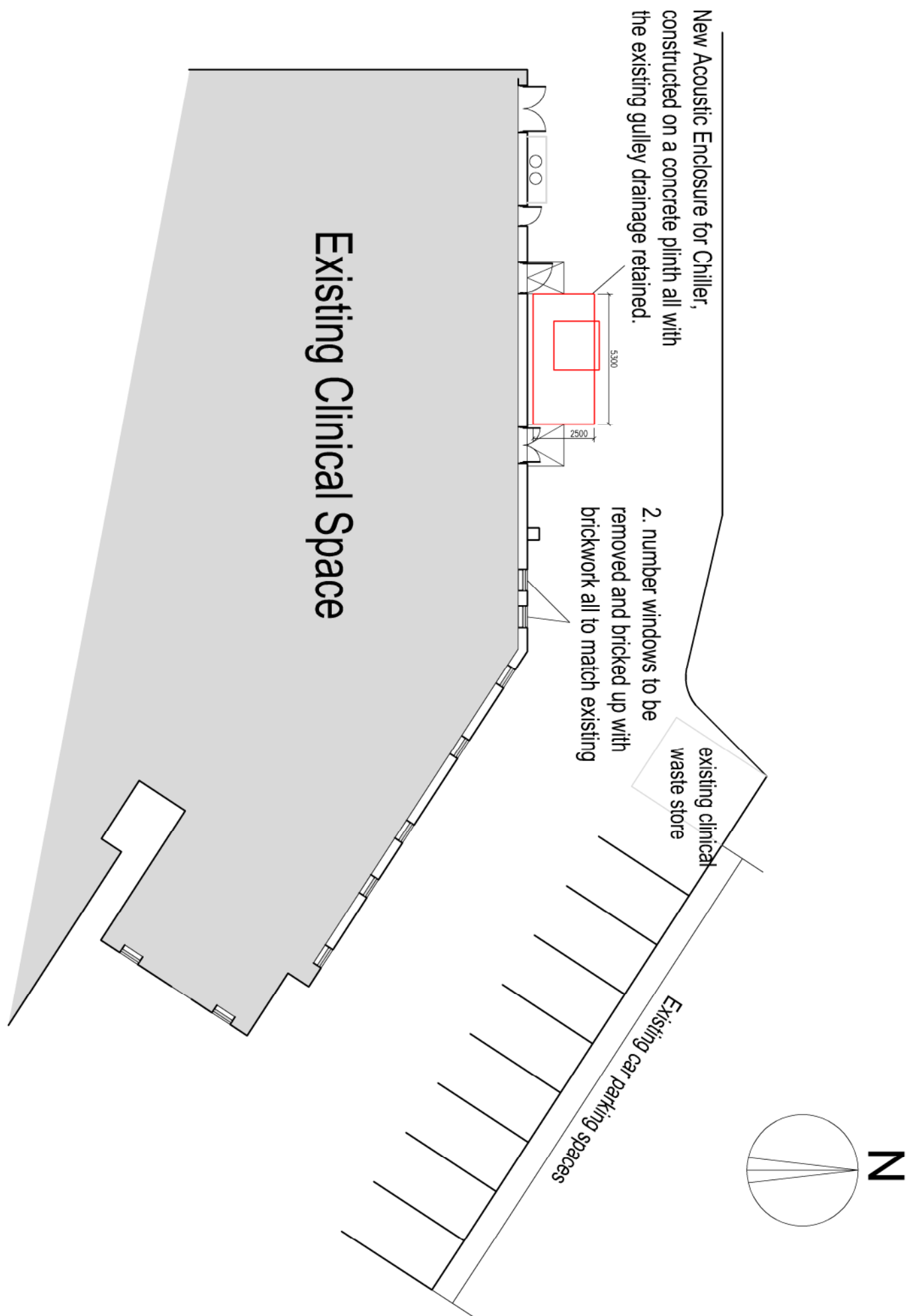
External Elevation 2 - as proposed, Drwg. No 3039-P-026 A (scanned to file 06/05/16)

External Elevation 3 - as proposed, Drwg. No 3039-P-027 A (scanned to file 10/05/16)

Acoustic Enclosure (scanned to file 06/05/16)

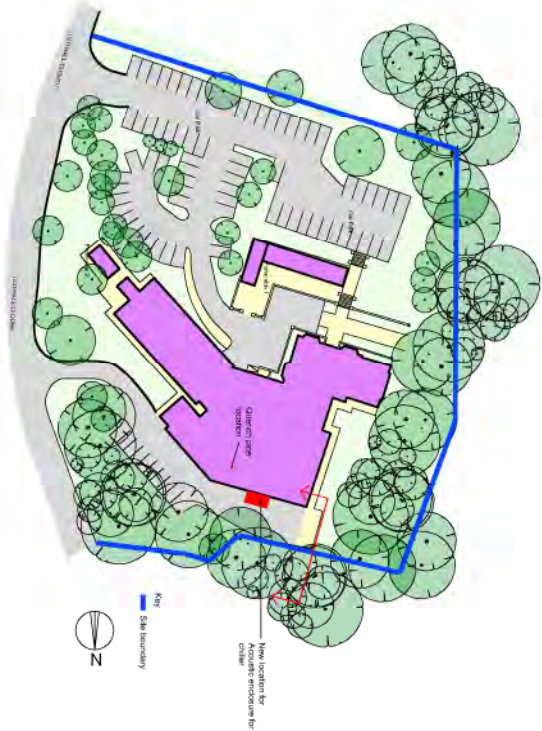
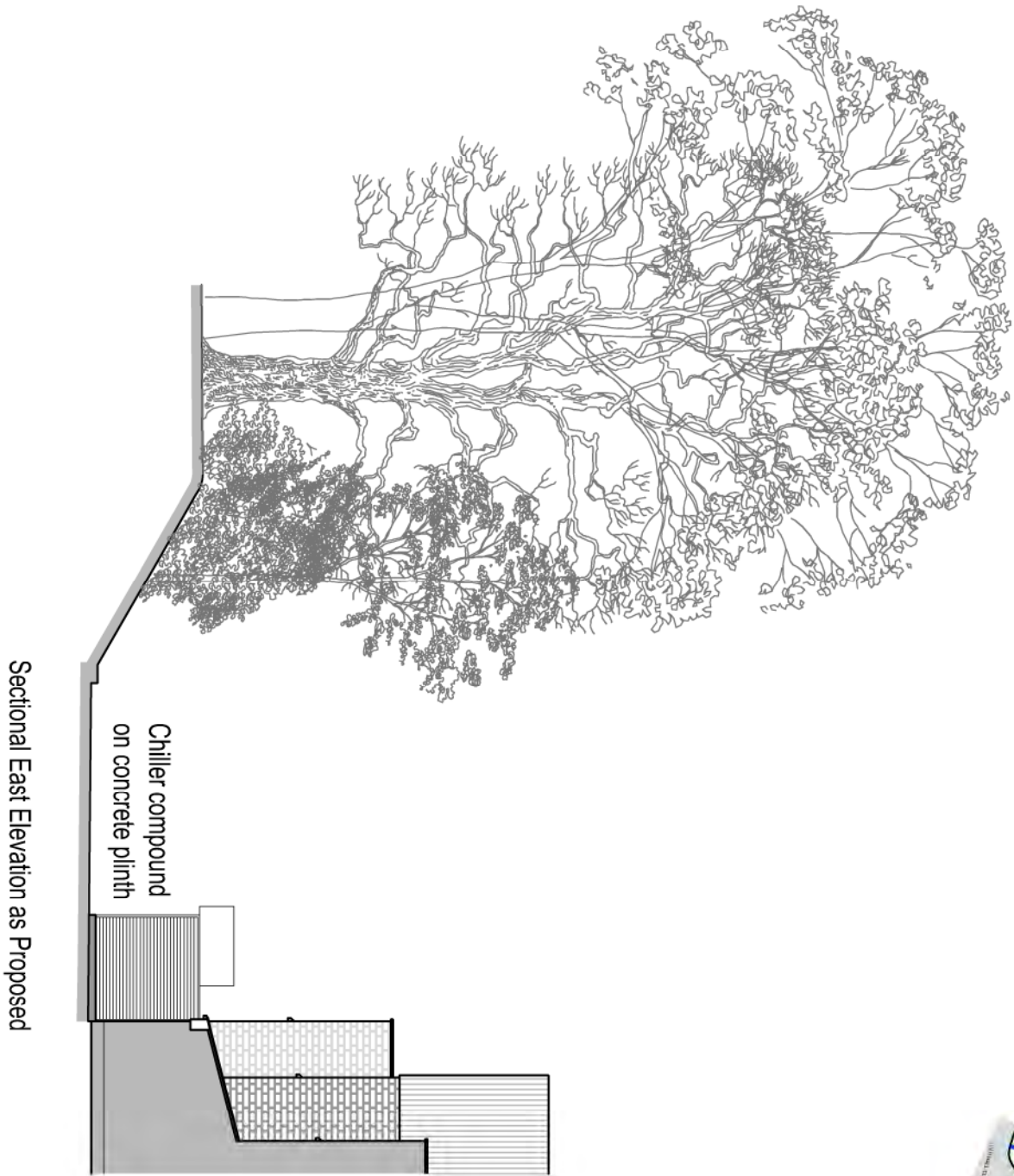
Reason

For the avoidance of doubt and in the interests of proper planning.



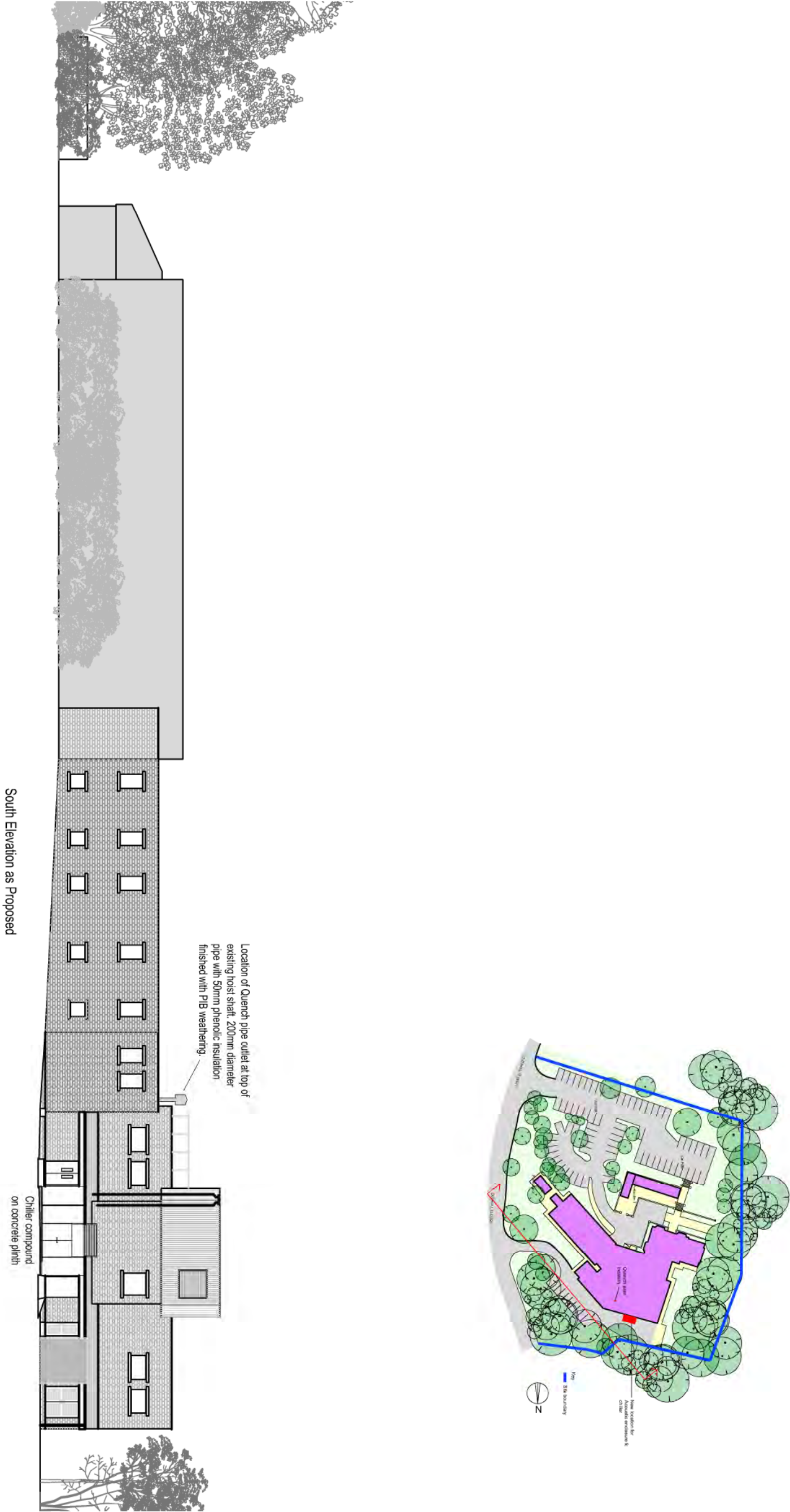
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THIS SHEET IS PART OF THE DOCUMENT SET LISTED ON SHEET C-1 AND SHOULD NOT BE SEPARATED



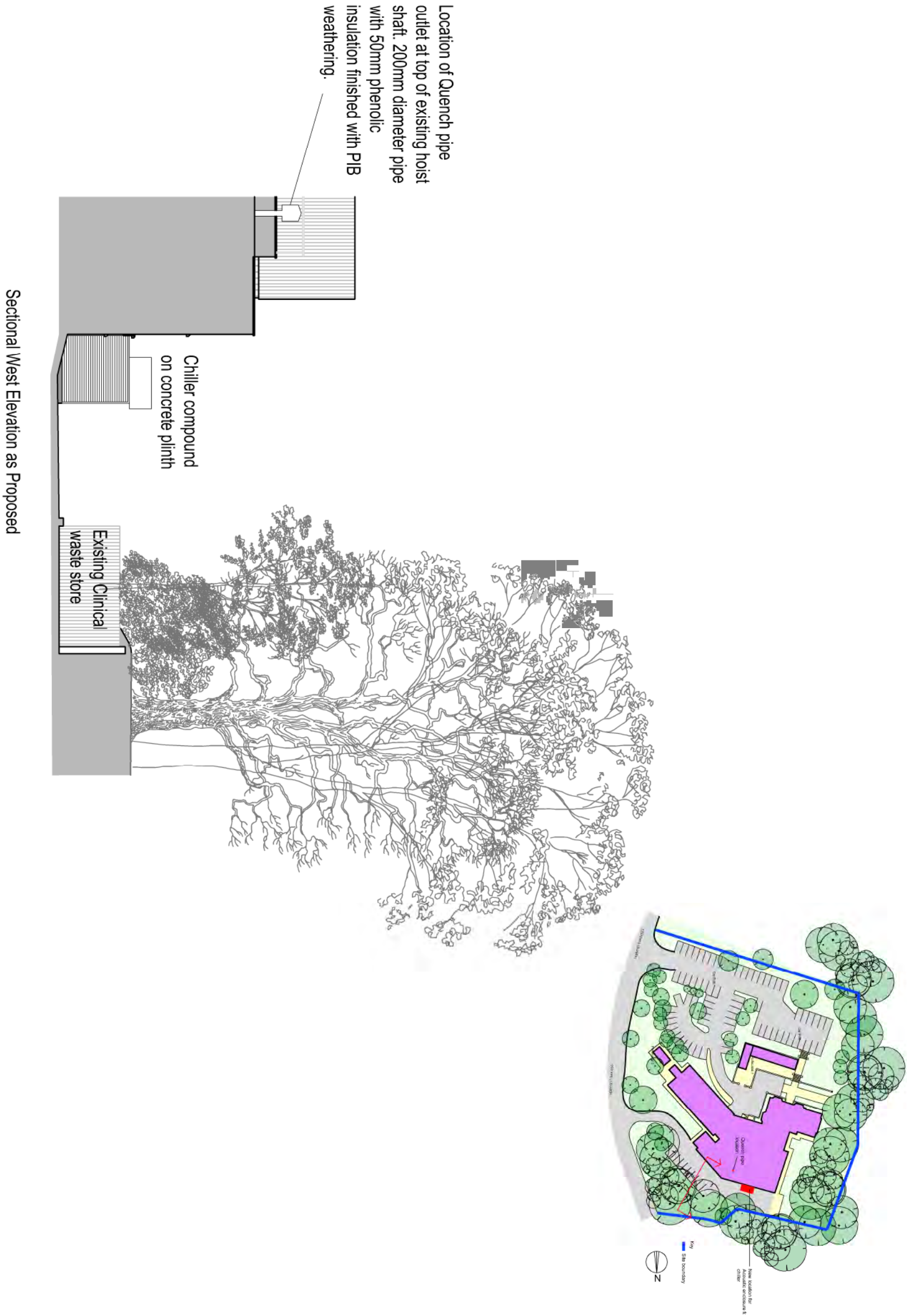
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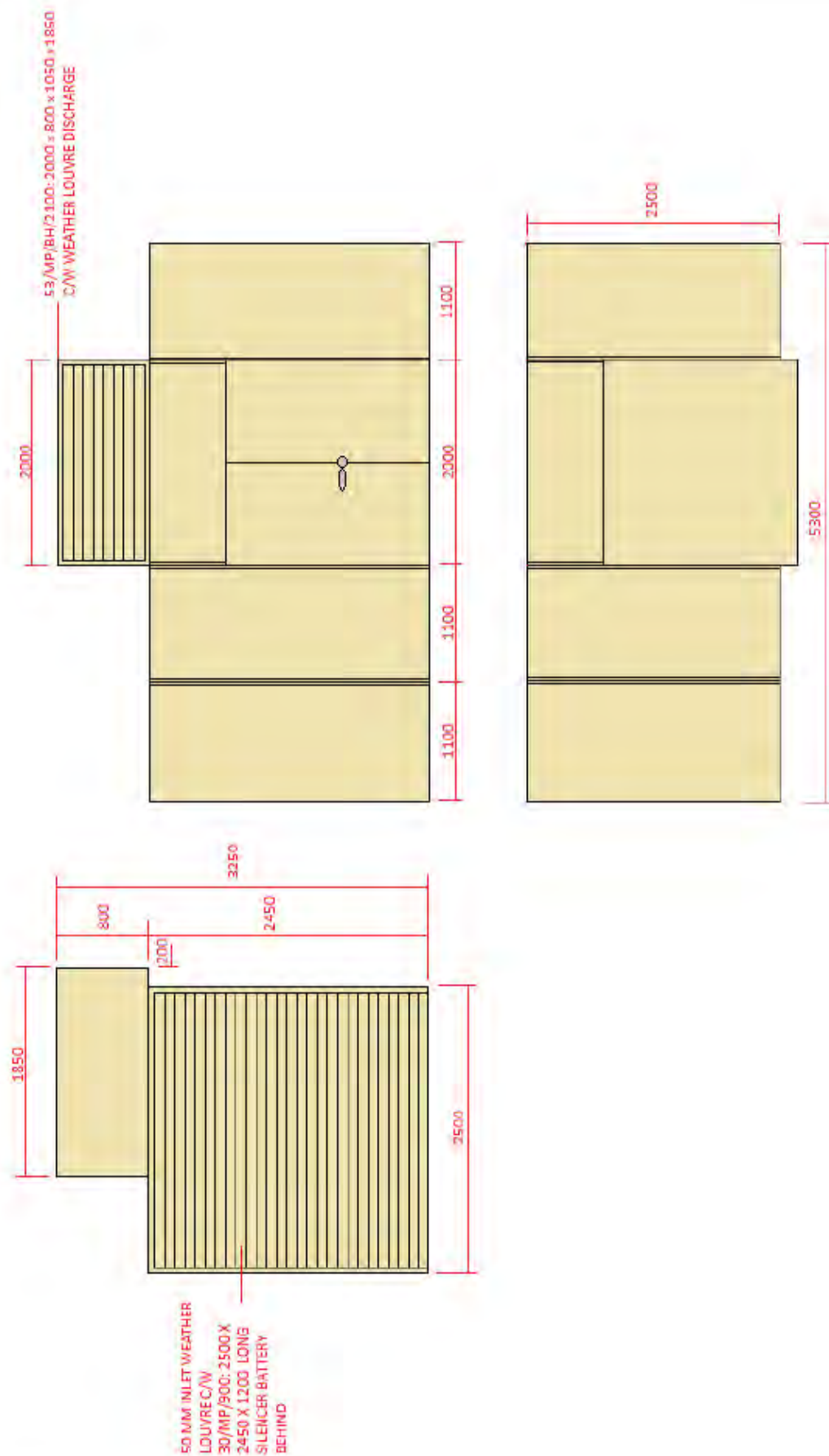


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Project PROPOSED MRI SUITE BMI, Beaumont BISHOPS WOOD, UNITED KINGDOM	Sheet Subject EXTERNAL ELEVATION 3- AS PROPOSED	Drawing 3039- P-027 A	A3: 1: 150 Sheet 1/1
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Town Hall, Bolton, Lancashire, BL1 1RU
Telephone (01204) 333 333

Bolton Council

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Date of Meeting: 30/06/2016

Application Reference: 96511/16

Type of Application: Full Planning Application
Registration Date: 11/05/2016
Decision Due By: 05/07/2016
Responsible Officer: Jeanette Isherwood

Location: 170 CAMPBELL STREET, FARNWORTH, BOLTON, BL4 7HW

Proposal: RETENTION OF COLD STORE

Ward: Farnworth

Applicant: Mr Aziz
Agent : RA Design & Project Management Ltd

Officers Report

Recommendation: Approve subject to conditions

Proposal

The application proposes the retention of a walk in cold store to the rear of the property at 170 Campbell Street.

The store is sited in the rear yard of the property and is 3850 mm long, 2700mm wide and 2500 mm high and of flat roofed construction. The external surfaces of the cold store are currently clad with timber cladding.

The application was submitted as part of an enforcement complaint.

Site Characteristics

The site is situated at the corner of Campbell Street and Harper Green Road. The premises is an end property in a terraced row of three shops and accommodates a green grocer at ground floor and a flat at first floor. The property has an original two storey outrigger, handed with the adjoining house and a small single storey rear extension. Immediately adjoining is a hairdressers that has also been further extended along the party boundary at single storey level, with a small sandwich shop to the end of the row. Adjacent to 170 is a large detached property accommodating an off-licence/convenience store at 166-168 which has a large single storey extension to the side/rear abutting the side boundary nearest to 170. To the rear the property overlooks the grounds of Clifton Court a residential low rise block of flats.

Policy

National Planning Policy Framework,

Core Strategy Policies:- S1 Safe, P5 Accessibility, CG3 The Built Environment, CG4 Compatible Uses and RA2 Farnworth.

SPD General Design Principles

Analysis

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- * design and impact on the character and appearance of the surrounding area;
- * amenity of existing residents and future occupiers;

Design and Impact on the Character and Appearance of the Surrounding Area

Core Strategy policies CG3 and RA2 seek to ensure that development proposals contribute to good urban design, respect and enhance local distinctiveness, ensuring development has regard to the overall built character and landscape quality of the area.

The development for which retrospective permission is sought has been constructed and is utilised as a cold store for the shop. The store is partially clad with timber cladding, however the plans detail that the cladding will encompass the whole store. Although not an ideal material the applicant states that the cladding can be painted a colour more compatible with the existing building.

Notwithstanding the objection described below, the siting of the cold store is to the rear of the building and not considered to be sited in a highly visible location. However, to overcome this issue a condition will be added to the approval for the submission of a detailed colour scheme to be agreed and approved to minimise the impact of the store.

The proposal, subject to conditions is not thought to have any detrimental impact on the character and appearance of the area. Therefore, it is considered that the scheme would be in accordance with policy CG3 of the Core Strategy.

Impact on residential amenity

Policy CG4 of the Core Strategy has the objective of ensuring that proposal do not adversely affect the level of residential amenity neighbouring occupants can reasonably expect to enjoy.

The nearest residential properties to the rear are in excess of 9 metres from the store which is considered to be an appropriate interface as indicated in SPD - General Design Principles.

The store is sited adjacent to a large single storey extension at 168 and a single storey extension at 172. Given the relatively small scale of the proposal and that both these properties are also commercial units it is not considered that this would result in any unacceptable and significant impact and is compliant to Core Strategy Policy CG4 and SPD - General Design Principles.

Conclusion

For the reason discussed above the proposal is considered to adhere to Core Strategy policies CG3, CG4 and SPD - General Design Principles and is therefore considered to be acceptable.

Members are recommended to approve the proposal subject to conditions.

Representation and Consultation Annex

Representations

Letters:- one objector has raised the concern that in its present form the store is an eyesore and does not fit in well with its surroundings.

Consultations

Advice was sought from the following consultees: None

Planning History

Permission was granted for the retention of air conditioning unit and erection of external staircase at rear under ref 96047/16

Recommendation: Approve subject to conditions

Recommended Conditions and/or Reasons

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason

Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. Before the development hereby approved is first brought into use, a colour scheme for the external walls shall be submitted to and approved in writing by the Local Planning Authority. Thereafter the approved colour scheme shall be implemented in full and retained thereafter unless otherwise agreed with the Local Planning Authority.

Reason

To ensure the development safeguards the character and visual appearance of the locality.

3. The development hereby permitted shall be carried out in complete accordance with the following approved plans:

Proposed plans - RAD/1363/16/2 - dated January 2016

Reason

For the avoidance of doubt and in the interests of proper planning.

Application number 96597/16



Development & Regeneration Dept
Development Management Section

Town Hall, Bolton, Lancashire, BL1 1RU
Telephone (01204) 333 333

Bolton Council

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Date of Meeting: 30/06/2016

Application Reference: 96597/16

Type of Application: Full Planning Application

Registration Date: 23/05/2016

Decision Due By: 17/07/2016

Responsible Officer: Helen Williams

Location: 1049 CHORLEY NEW ROAD, LOSTOCK, BOLTON, BL6 4LN

**Proposal: CONTINUED CHANGE OF USE FROM GARAGE FORECOURT TO
HAND CAR WASH SERVICE**

Ward: Horwich North East

Applicant: Chorley New Road Car Wash Ltd

Agent : SMS Architecture

Officers Report

Recommendation: Approve subject to conditions

Proposal

Permission is sought for the continued use of the former garage forecourt as a hand car wash service.

The hand car wash was granted a temporary three year planning approval at Planning Committee in October 2012 (planning application 88511/12). This permission has now expired, and the site is currently subject of an enforcement case.

It is proposed that the car wash would continue to be open between the hours of 09:00 and 17:30 seven days a week (condition2 of temporary approval 88511/12 allowed opening between 09:00 and 18:00 hours).

The applicant has stated that the business employs five full time members of staff.

Site Characteristics

The application site is a former petrol filling station (Lostock Filling Station) that is now being used as a hand car wash service ("5 Star Hand Car Wash"). The site comprises a forecourt, a canopy over the former petrol filling area and a two storey red brick building, which was the former kiosk and staff area. There is a one way system in and out of the site onto Chorley New Road. A large existing free standing sign by the exit has been retained (with new information on it) and the canopy and front wall have been rebranded.

1049 Chorley New Road is sited immediately behind the site and has a very close relationship with the application site. This house is given as the applicant's address.

The residential properties of "Mussoorie" and "The Heathers" adjoin the site to the west and east

(respectively) and these are set back from the road. 1 Stoneacre sides on to the rear of the garage building to the north of the site. Opposite the road are also residential properties that are set some distance back from the road.

Chorley New Road is a main thoroughfare between Bolton and Horwich.

Policy

National Planning Policy Framework (NPPF)

Core Strategy Policies: P5 Transport and Accessibility; S1.2 Road Safety; CG1.5 Reduce the Risk of Flooding; CG3 The Built Environment; CG4 Compatible Uses; OA1 Horwich and Blackrod.

Analysis

Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise.

Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission.

Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission.

It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

The main impacts of the proposal are:-

- * impact on the amenity of neighbouring residents
- * impact on the character and appearance of the street scene
- * impact on the highway
- * impact on drainage

Impact on the Amenity of Neighbouring Residents

Policy CG4 of the Core Strategy states that the Council will ensure that new development is compatible with surrounding land uses and occupiers, protecting amenity, privacy, safety and security and does not generate unacceptable nuisance, odours, fumes, noise or light pollution.

There are residential properties on either side of the site ("Mussoorie" and "The Heathers") and residential properties to the rear on Stoneacre and opposite along Chorley New Road.

During the three years the car wash has had temporary permission, the Council's Pollution Control section has not received any objections from neighbouring residents. Indeed the objection received to this application does not raise any concerns regarding living conditions.

It is suggested by Officers that the hours of opening are again restricted by condition to between the hours of 09:00 and 18:00 seven days a week and that deliveries are restricted to between 09:00 and 18:00 hours Monday to Saturday (with no deliveries on Sundays and Bank Holidays).

As with the previous temporary permission it is again suggested that a condition be attached to restrict the noise rating levels of any equipment on site, to safeguard the amenity of neighbours with regard to noise.

It is therefore considered, subject to the suggested conditions, that the proposed continued use of the site as a hand car wash would not unduly harm the amenity of neighbouring residents, compliant with Policy CG4 of the Core Strategy.

Impact on the Character and Appearance of the Street Scene

Policy CG3 of the Core Strategy states that the Council will conserve and enhance local distinctiveness and require development to be compatible with the surrounding area. Policy OA1 states that the Council will, for developments in Horwich and Blackrod, conserve and enhance the character of the existing physical environment and ensure that development respects street patterns and the grain and form of predominant architectural styles.

The building, forecourt and canopy of the former petrol filling station are being reused for the hand car wash use and the large freestanding sign by the exit was erected in association with the petrol station. It is not considered that the proposed use is any more harmful to the character and appearance of the street scene than the previous petrol station.

It is noted that a storage container that was previously used and sited at the north western corner of the site has been removed.

Whilst the proposed use does not enhance the character and appearance of Chorley New Road it is considered that it would not be any more visually harmful than the previous use. It is therefore considered that the proposal complies, on balance, with Policies CG3 and OA1 of the Core Strategy.

Impact on the Highway

Policy P5 of the Core Strategy states that the Council will ensure that developments take into account [amongst other things] servicing arrangements and parking. Policy S1.2 states that the Council will promote road safety in the design of new development.

A one way system into and out of the site from Chorley New Road is in operation.

The Council's Highways Engineers are aware that vehicles visiting the hand car wash queue on Chorley New Road in contravention of the waiting restrictions on the road (double yellow lines). They also note that this results in the adjacent residential driveway (at "Mussoorie") being obstructed and the footway being blocked.

Engineers have therefore recommended that the applicant submit an enforceable management plan to demonstrate how they will prevent customers from queuing on the highway. The applicant has shown on an amended plan that there is space to the left of the site (by the entrance) for waiting visitors to park (four spaces), and that visitors will be encouraged to use this.

Engineers also advise, should Members approve this application, that the applicant be required to fund the marking out of a "Keep Clear" road marking across the entrance to the adjacent residential driveway at Mussoorie. Officers have suggested this via a condition.

Highways Engineers are also recommending that permission only again be granted for a three year temporary permission. Officers however feel that this is not necessary, and would be unreasonable if the Council was requiring the applicant to fund a "Keep Clear" road marking.

It is therefore considered, subject to a condition requiring the applicant to fund a "Keep Clear" road marking to the front of the driveway at Mussoorie, that the proposal would not jeopardise highway safety and would be compliant with Policies P5 and S1.2 of the Core Strategy.

Impact on Drainage

Policy CG1.5 of the Core Strategy states that the Council will reduce the risk of flooding in Bolton and other areas downstream by minimising water run-off from new development.

The Council's Drainage Officers raise no objection and there have been no complaints received regarding water on Chorley New Road from the hand car wash.

It is therefore considered that the proposal complies with Policy CG1.5 of the Core Strategy.

Conclusion

For the reasons discussed above it is considered, subject to the recommended conditions, that the proposed continued use of the application site for a hand car wash would not unduly harm the amenity of neighbouring residents, would not be detrimental to the character and appearance of the street scene, would not jeopardise highway safety and would not lead to flooding of Chorley New Road. Members are therefore recommended to approve this application.

Representation and Consultation Annex

Representations

Letters:- One objection has been received from a neighbouring residents, which raises the following concerns:

- * The footpath is often blocked with cars being cleaned on it;
- * Further cars queue to enter the car wash from Chorley New Road, blocking the footpath;
- * Waiting cars park on the double yellow lines;
- * Pedestrians have to walk in the road to get past;
- * The parked cars add to congestion when the bus is at the bus stop on the opposite side of the road and a car is turning right;
- * Visibility is restricted when emerging from Stoneacre owing to an advertising sign that is always on the footpath - it is an accident waiting to happen (*officer's comment: this concern has been forwarded to the Council's Highways enforcement (Street Care) team*);
- * Litter from plastic sleeves, probably from air fresheners.

The objector has submitted photographs that show cars waiting on the road to be washed and vehicles being washed on the footpath to the front of the car wash.

Horwich Town Council:- comments from their meeting of 23rd June 2016 will be reported directly at the meeting.

Consultations

Advice was sought from the following consultees: Highways Engineers, Drainage Officers, Pollution Control Officers and Greater Manchester Police's Architectural Liaison Officers.

Planning History

A three year temporary consent was granted at Planning Committee in October 2012 for the change of use of the garage forecourt to a hand car wash service (88511/12). This permission therefore expired in October 2015.

Recommendation: Approve subject to conditions

Recommended Conditions and/or Reasons

1. Within 28 days of this permission full details of the highway works at Chorley New Road comprising the marking out of a "KEEP CLEAR" road marking to the front of the driveway of 1047 ("Mussoorie") Chorley New Road shall be submitted to and approved in writing by the Local Planning Authority. The approved scheme shall be funded by the applicant and shall be implemented in full and retained thereafter.

Reason

In the interests of highway safety and to comply with policy S1.2 of Bolton's Core Strategy.

2. The premises subject of this consent shall not be open for trade, including the repair of plant machinery and the movement of such machinery, outside the following hours:-

09:00 to 18:00 hours Mondays to Sundays

Reason

To safeguard the living conditions of residents and the amenity and character of the area with regard

to noise and/or disturbance, and to comply with policies CG3 and CG4 of Bolton's Core Strategy.

3. No deliveries shall be taken or dispatched from the premises outside the following hours:-

09:00 to 18:00 hours Mondays to Saturdays

No operations shall take place on Sundays or Bank Holidays.

Reason

To safeguard the living conditions of residents and the amenity and character of the area with regard to noise and/or disturbance, and to comply with policies CG3 and CG4 of Bolton's Core Strategy.

4. All fixed plant and equipment requires to be designed to prevent noise disturbance to local residential property and should be designed to give a rating level, as defined in BS4142:1997, 5dB below the night time LAF90(5 min) or the daytime LAF90(1 hour) which ever is the most appropriate, as measured 4 metres from the nearest residential properties. The monitoring information and the acoustic calculations together with the proposed specifications of the fixed plant and equipment and any sound attenuation proposed should be submitted to the Local Planning Authority for approval prior to the commencement of installation.

Reason

To minimise the impact of noise on the general and residential amenity, and to comply with policy CG4 of the Core Strategy.

5. The development hereby permitted shall be carried out in complete accordance with the following approved plans:

00; "Site Location & Proposals"; received 14th June 2016

Reason

For the avoidance of doubt and in the interests of proper planning.



Site Plan
Scale 1:200 @ A3

Site Area - 685 SqM/ 7,375 SqFt

Application Boundary



Site Location
Scale 1:1250 @ A3

NOTES:

No scaling or dimensions other than those figured are to be taken from this drawing. All dimensions to be checked on site

This drawing is for use at the named location and for Planning Permission purposes only and as such should not be used at any other location or for any other purposes

Project Location:

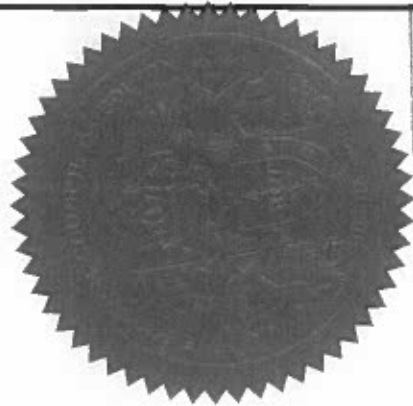
Former Lostock Petrol Station
1049 Chorley New Road
Bolton
BL6 4LN

Drawing:

00 Site Location & Proposals

Scale:	Size:	Date:
Varies	A3	05/16

THE COMMON SEAL OF THE BOROUGH COUNCIL OF
BOLTON WAS HERETO AFFIXED ON THE MAKING OF
THE BOLTON (NISA, 417 CROMPTON WAY) TREE
PRESERVATION ORDER 2016 ON THE 5TH MAY 2016
IN THE PRESENCE OF :-



SEALING NO.
19256

Aileen Gorman
AUTHORISED SEALING
OFFICER

Bolton (Nisa, 417 Crompton Way) Tree Preservation Order 2016

DC23.960
drawn: SC
May 2016
Scale : 1250



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**Bolton
Council**

A1  **Area of Trees**

**Department of Place
Development Management Section**

**Third Floor, Town Hall,
Bolton, Lancashire BL1 1RU**
Email: planning.control@bolton.gov.uk
Telephone: 01204 336000
www.bolton.gov.uk



Bolton (Nisa, 417 Crompton Way) Tree Preservation Order 2016

The order consists of:

- A1 An area of trees and hedgerow tress consisting of Portuguese Laurel, Common Laurel, Hawthorn, Cotoneaster, Elderberry and Willow.

The above trees are situated around the periphery of the Nisa Store car park area, 417 Crompton Way, bounding 7-17 Firwood Lane and 407 Crompton Way, Bolton.

An emergency Tree Preservation Order (TPO) was placed on the trees (detailed above) in response to concerns from residents on Firwood Lane that works had begun to remove some of the trees within the hedgerow.

An objection has been received to the making of the order from the landowner on the following grounds: -

- The landowners had contacted the Council last year and were told that there was no protection order and they could be cut. We also contacted planning this year, before we started to cut them and were told the same thing;
- There is a need to re-tarmac the car par, which will require some of the bushes clearing to tarmac up to the kerbing;
- There is a security light which is covered by the bushes. This is impacting on the security of the carpark/premises;

Two letters of support have been received from residents on Firwood Lane. The points they raise are as follows: -

- The trees are an important landscape feature;
- They are a valuable amenity for both local residents and for users of the supermarket;
- The trees provide a habitat for a wide variety of birds and other wildlife.

The area order category is intended for short-term protection in an emergency and may not be capable of providing appropriate long-term protection. The Order will protect only those trees standing at the time it was made, so it may over time become difficult to be certain which trees are protected. Authorities are advised to only use this category as a temporary measure until they can fully assess and reclassify the trees in the area.
(Ref: Planning practice guidance).

Further to the above, and to the making of Bolton (Nisa, 417 Crompton Way) Tree Preservation Order 2016 (Emergency order) it is worthy to note that a revised schedule has been drawn-up by the Tree and Woodland Section which omits the Common Laurel and would allow the Landowners to carry out the resurfacing works to the car park: -

The revised schedule is as follows –

<i>Ref on Map</i>	<i>Description of trees in Group</i>	<i>Situation</i>
G1	1x Hawthorn tree 1x Cotoneaster tree	A linear shaped group of trees situated in the south west corner of the site adjacent to the gable wall of the building.
G2	23x Portuguese Laurel trees 5x Hawthorn trees 1x Holly	A linear shaped group of trees situated on the southern boundary of the site adjacent to the car parking area.

Bolton (Nisa, 417 Crompton Way) Tree Preservation Order 2016

	2x Rowan trees 2x Whitebeam trees	
--	--------------------------------------	--

BURY ROAD

86.9m
G.P.

466 476 488

490 500

BACK BURY ROAD SOUTH

2

16

OSMUND AVENUE

26

4

20

22

32

27

The Common Seal of the Borough Council of Bolton was used in Affixing on the Making of the Bolton (Land Off Osmund Avenue) Tree Preservation Order 2016 on the 17th May 2016 in the Presence of:-



Authorised Sealing
Officer

SEALING NO.
19269

Bolton (Land Off Osmund Avenue) Tree Preservation Order 2016

DC23.961
drawn: SC
May 2016
Scale : 500

G1  Group of Trees



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Department of Place
Development Management Section

Third Floor, Town Hall,
Bolton, Lancashire BL1 1RU
Email: planning.control@bolton.gov.uk
Telephone: 01204 336000
www.bolton.gov.uk

**Bolton
Council**

Bolton (Osmund Avenue) Tree Preservation Order 2016

The order consists of: =

- G1 Consisting of 18x Cypress trees in a linear group on southern boundary with Osmund Avenue, Bolton
- G2 Consisting of 16x Cypress trees in a linear group south of the boundary with 26 Oakenbottom Road, Bolton.

Interest has been shown recently in installing an access gate to a strip of land adjacent to Osmund Avenue and a Tree Preservation Order was placed on the trees in response as it was considered that they had a significant amenity value to the locality.

An objection has been received to the order from the landowner on the following grounds: -

First part

- Dispute how significant the trees are given that they make up a hedge of Cypress trees, specifically *Cupressus x leylandii*;
- Cypress trees have very little value to wildlife (Woodland Trust);
- **'Amenity value' has been defined as a 'useful or desirable feature'. Given that these trees do not make up part of the local woodland and are not a native species which would enhance the woodland or residential area, I see the value as minimal;**
- The order makes no reference to the methodology used to determine value of the trees, amenity or otherwise;
- **'Amenity' is not defined in law so authorities need to make judgements when dealing whether it is in its power to make an order';**
- 'Orders should be used to protect selected trees and woodlands if their removal would have a significant negative impact on the local environment and its enjoyment by the public. Before authorities make or confirm an order they should be able to show that protection will bring a reasonable degree of public benefit in the present or future (NPPF)'

Second part

- As the landowner affected by these trees, I am unable to access my property as there is no gated entrance. To enable a gate to be installed it would mean the removal of (approx.) six trees adjacent to Osmund Avenue, leaving the majority of trees intact.
- This would have little impact on the hedge line and the local environment and would not detract from the enjoyment of the adjoining native woodland.
- I have previously received an email from the Tree and Woodland Officer, confirming that the row of Cypress trees in question are not protected by virtue of the TPO.
- **I would argue why these trees are suddenly 'an important feature of the landscape and contribute significantly to the amenities of the area' despite not being covered by any TPO order in the past.**

Further to the above, seven letters of support have been received from residents on Osmund Avenue. The points they raise are as follows: -

- The street is at present a kind of nature tunnel as it feeds into Leverhulme Country Park. We have a number of regular wildlife in the area, from deer, owls and bats, along with a variety of birds and squirrels. Any interference to this could have a negative impact;

Bolton (Osmund Avenue) Tree Preservation Order 2016

- The trees in question screen a piece of land that is nothing more than a scrap yard and dumping ground;
- The street would become a less desirable place to live and if we wanted to move the impact of such an eyesore would have a serious and detrimental effect on the value and make the properties practically unsaleable;
- There are other trees opposite our home that have had a TPO on them for many years, this has led to wildlife to come/nest in this area;
- The trees subject to this order should be made permanent to encourage the wildlife to remain;
- There is a covenant of the land that states that the land should remain unbuilt upon.
- The trees provide a positive impact within the area, providing the residents with a pleasant view.
- There are birds nesting within these trees;
- There is no gain in these trees being felled and this would have a knock on effect to the appearance of the street as these trees start the wildlife corridor.

The Common Seal of the Borough Council of Bolton was here to Affixed on the Margin of the Bolton (Hough Fold Way, Bolton) Tree Preservation Order 2016 on the 11th April, 2016 in the Presence of:-



Alan Gorman
 AUTHORIZED SEALING
 OFFICER

SEALING
 19234

Bolton (Hough Fold Way, Bolton) Tree Preservation Order 2016

DC23. 958
 drawn: SC
 April 2016
 Scale : 1250

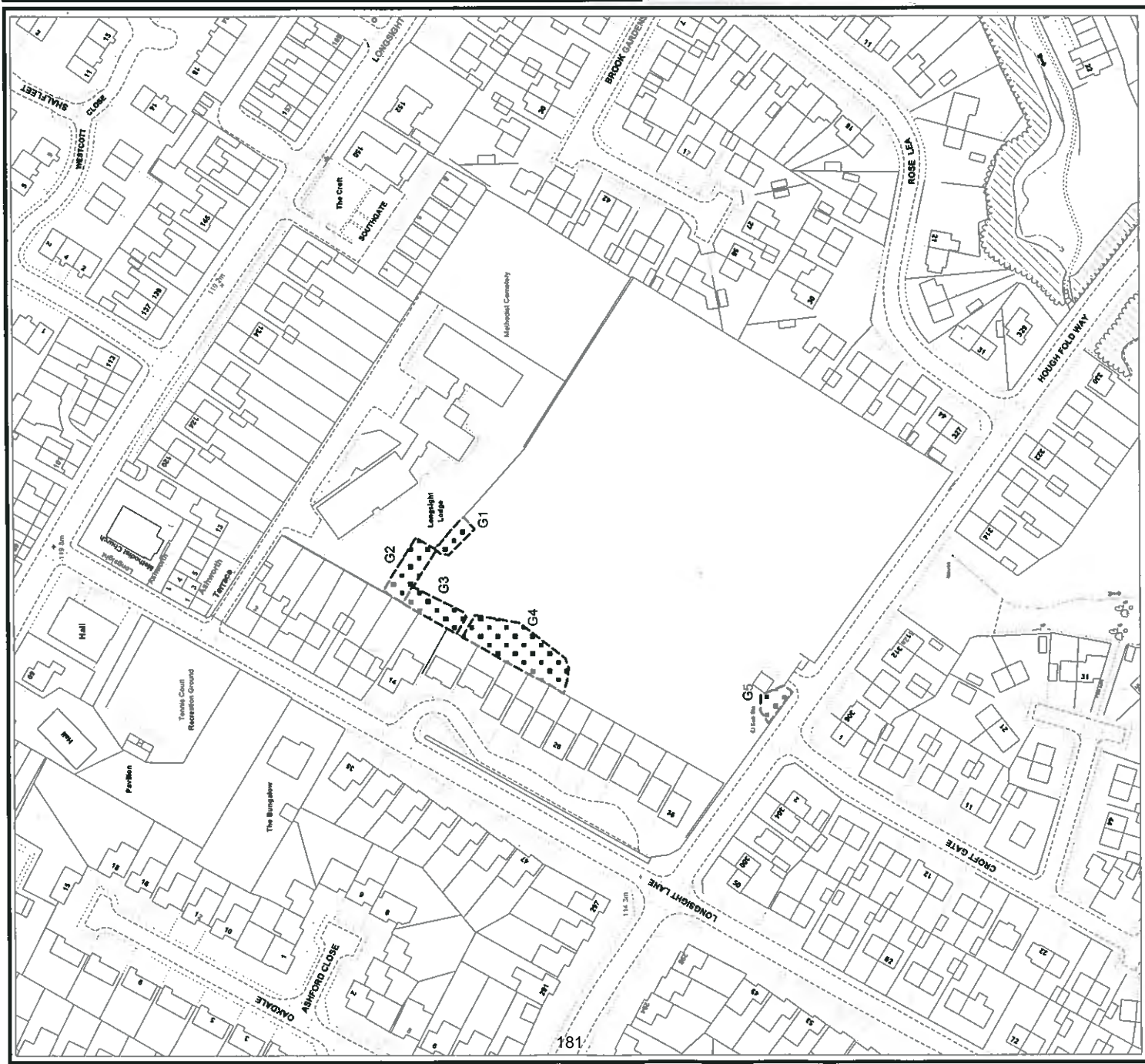
 Groups of Trees

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Bolton DC23:958 (Hough Fold Way) TPO 2016 Objections

Cathy & Terry Morris
Date: 20/05/16

- RE: Hough Fold Way Bolton Preservation Order 2016
The second issue is regarding the Hough Fold Way Bolton Tree Preservation Order 2016 that we received. There are no existing trees behind our property; however, if there is a tree **preservation order on trees to be planted behind our property (this isn't clear).**

Response: any additional tree planting within the TPO group are not automatically protected by the TPO. The protected trees are only those within the tree specification schedule within the order. Any landscaping requirements/ conditions are a separate issue.

- RE: Application to trees to be planted pursuant to a condition, then we would wish to air an objection. As I have previously mentioned we have strong concerns relating to loss of light to both our garden and to our property as the living room is on the rear of our property and overlooks the garden. Therefore if trees are planted and branches overhang our garden we feel it should be appropriate to prune and trim them without having to ask permission to do so.

Response: As stated above any new trees planted would not be protected by the order and therefore should any pruning to newly planted trees be required this would not require the Councils authorisation through the planning process.

M Dracup Estates Dept. (Corporate Property)
Date: 23rd May 2016

RE: Exclusion of G4 (consisting of 7x Birch trees) of the Bolton DC23:958 (Hough Fold Way) TPO 2016

- Restricts the ability of the residents at 18-24 Longsight Lane to extend their gardens

Response: Birch trees are generally light canopies trees that could be incorporated into back gardens. The Council would actively support good tree management of protected trees should this be a later requirement.

- Possible that the residents will not want to proceed with the areas of land if the TPO is confirmed.

*Response: I think it right to raise the point that if we were to remove one group from the order simply to facilitate the disposal of some of the land, then this may have an impact on the **Planning Authority's future credibility in relation to making TPOs.***

We only recommend TPOs on trees which merit such a designation and are worthy of protection, this applies irrespective of who we are selling the land to. It is unfortunate that on this occasion there were different potential buyers involved and that Officers were not aware of this when we were discussing the trees to be protected on the site with the developer.

- This would effectively leave the Council with an area of land that is not accessible to manage.

Bolton DC23:958 (Hough Fold Way) TPO 2016 Objections

Response: This issue seems to have arisen due to a failure in the process for disposals or an issue that just didn't come to light that the Tree Section officers were unaware of the sale of this strip of land.