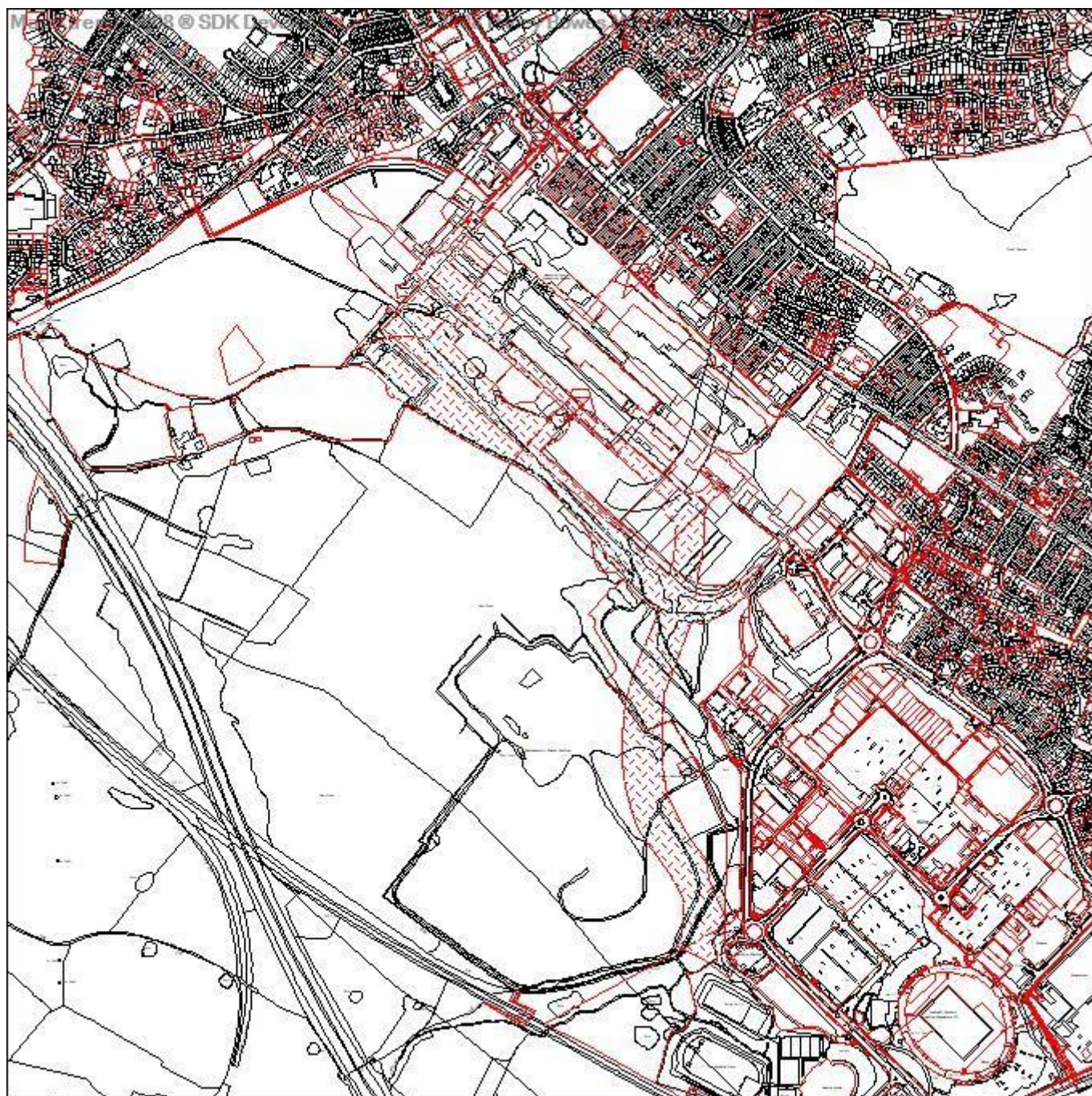


**Application number**  
**05378/19**



**Directorate of Place**  
**Development Management Section**

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**Bolton**  
**Council**

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**Date of Meeting: 22/08/2019**

**Application Reference: 05378/19**

**Type of Application: Full Planning Application**

**Registration Date: 25/01/2019**

**Decision Due By: 25/04/2019**

**Responsible Officer: Helen Williams**

**Location: FORMER HORWICH LOCO WORKS SITE, ASPINALL WAY,  
HORWICH, BOLTON, BL6 6DW**

**Proposal:** FULL APPLICATION FOR SITE PREPARATION AND INFRASTRUCTURE WORKS COMPRISING THE RE-ROUTING AND CONSTRUCTION OF THE PREVIOUSLY APPROVED ASPINALL WAY ACCESS ROAD; CREATION OF OPEN SPACE ZONE A; DEMOLITION OF EXISTING ON-SITE STRUCTURES AND ASSOCIATED LAND REMODELLING, REMEDIATION, LANDSCAPING, PEDESTRIAN/CYCLE CONNECTIONS, ECOLOGICAL MITIGATION AND DRAINAGE WORKS.

**Ward: Horwich and Blackrod**

**Applicant: Alpha Investments (NW) Ltd and Bolton Metropolitan Borough Council**

**Agent : GVA How Planning**

## **Officers Report**

**Recommendation: Approve subject to conditions**

### **Executive Summary**

- \* This application is before Committee as the Council is joint-applicant.
- \* Permission has previously been granted on the former Horwich Loco Works site for its comprehensive mixed-use redevelopment, including up to 1,700 houses. This permission (91352/14) granted full planning permission for four accesses into the development and gave outline approval for a link road and a large area of public open space (Open Space Zone A).
- \* This application now before Committee seeks permission for an amendment to the approved point of access off Aspinall Way (so that it is now further to the south and connects to the existing roundabout by the Police Station) and an amendment to the internal route of the link road, so that it can accommodate existing site levels, avoid the asbestos tip to the east, and achieve the required widths and gradients (following further site investigations by the applicant).
- \* Full details of Open Space Zone A, to include a "parkland" landscape and play area, are also proposed.
- \* The main third party objections to the proposal have been to the proposed demolition of the Erecting and Repair Shop (a heritage building sited within the Horwich Locomotive Works Conservation Area) (see Representation and Consultation Annex). This building is proposed to be demolished to accommodate the proposed new route of the link road.
- \* The applicant has provided justification as to why they are unable to propose the route of the link road elsewhere within the site (see paras. 45-47).
- \* Historic England had originally objected to the proposal (para. 44) but following the submission

of the applicant's justification they withdraw their formal objection (see para. 48). Historic England instead advise that the LPA should determine the proposal against paragraph 196 of NPPF, to balance whether the public benefits associated with the proposal outweigh the "less than substantial harm" the loss of the building would have on the conservation area.

- \* Officers' planning balance with regard to NPPF paragraph 196 is to be found at paras. 49-53. Officers consider that the public benefits do outweigh the harm.
- \* The impact of the proposed link road and an access onto Aspinall Way was fully assessed during the determination of hybrid application 92352/14. It is not considered that the amendment to the point of access onto Aspinall Way would affect the operational capacity of the local highway network or highway safety (see paras. 57-65).
- \* Land contamination would be suitably remediated prior to construction of the road and open space (paras. 68-78).
- \* It is considered, subject to the recommended conditions, that the proposal would safeguard and enhance the biodiversity of the area and would continue to protect the adjoining Red Moss SSSI and SBI (see paras. 79-93).
- \* There are no drainage, residential amenity or visual amenity objections to the proposal (see paras. 94-114).
- \* The proposed link road is considered to be vital infrastructure required to unlock the development site (particularly the delivery of houses on site) and to deliver the objectives of the Core Strategy (focus new housing in the existing urban area, rather than sites within other protected open land and Green Belt).
- \* The link road is to be funded by the Government's Housing Infrastructure Fund (HIF), with this being conditional to the approval of this application. This fund was set up especially to accelerate much needed housing delivery on marginal viability sites (such as this site) where the viability gap is needed to be bridged through the leveraging of external funding.
- \* The proposed development is considered to constitute sustainable development and therefore paragraph 11 of the NPPF should be engaged, which advises that development should be approved without delay. Members are recommended to approve this application subject to the recommended conditions.

## **Proposal**

1. Full planning permission is sought for:

- i. the construction of the link road (spine road) through the proposed Rivington Chase development, along with associated land remodelling, remediation, landscaping, pedestrian and cycle connections, ecological mitigation works, drainage works and the demolition of the main erecting and repair shop building to accommodate the proposed route of the road;
- ii. the creation of Open Space Zone A.

### **i. Link Road**

2. Hybrid planning approval 91352/14 for the Rivington Chase development at the former Horwich Loco Works site granted full planning permission for four accesses into the development; the "Rivington House" access and the "Armstrong's" access (both off Chorley New Road), access off Aspinall Way and access off Crown Lane. A link road was proposed at that time, illustrated within the masterplan for the Rivington Chase development, as linking the Rivington House access to Aspinall Way (to the south of Futura Park), with its route going through the centre of the Heritage Core part of the development.
3. This new full planning application is now required as the proposed location of the access onto Aspinall Way has been altered from that approved under application 91352/14. The access onto Aspinall Way is now proposed further south than previously approved, at the roundabout that connects Burnden Way, Aspinall Way, the Linkway and Horwich Police Station. This alteration in



the route enables the road to be accommodated solely within Council and Alpha (the joint applicants') owned land or land secured by a lease to the Council, thereby avoiding third party land or uncertainty over delivery. Access from the Rivington House access is still as previously approved.

4. The internal route of the link road has also been amended from that originally envisaged, with the road now being proposed to run to the south of the Heritage Core rather than through the centre of the Heritage Core. The route of the road has been amended to accommodate existing site levels and to avoid the asbestos tip to the east of the site, and to ensure that the road is deliverable and able to achieve the required widths and gradients. The alteration of the route now requires the demolition of one of the Heritage Core buildings (the Erecting and Repair Shop), which was previously identified for retention.
5. The road would be single carriageway, would have a speed limit of 30mph and would be a total of 1540 metres in length. A segregated walkway (2 metres wide) and a segregated cycleway (1.5 metres wide) would be provided on both sides of the carriageway. Associated landscaping and lighting would also be located on either side of the carriageway.
6. The link road will be the first entirely new road in Greater Manchester that would provide Transport for Greater Manchester's "Bee Network" facilities (segregated cycle ways on key routes).
7. Two roundabouts are proposed along the route, which would allow for future connection to the approved "Armstrong's" access onto Chorley New Road and to Crown Lane/Station Road (through the Network Rail owned part of the site). Alterations would also be made to the existing Aspinall Way (Police Station) roundabout to facilitate the site access.
8. The proposed link road would form the primary access road for the Rivington Chase development. It would also carry all construction traffic into the development and facilitate the existing Loco Works Industrial Estate-related traffic in the interim. As it is proposed that preparation and construction works would commence from the southern access point (Aspinall Way), working inwards, heavy goods vehicles would access the site via Aspinall Way rather than Chorley New Road.
9. Works associated with site preparation and the construction of the link road include:
  - \* Extensive earthworks, which include the stabilisation of poorly compacted peat and clay deposits to the road's southern extent and the construction of a new embankment to connect the lower lying land in the area neighbouring Futura Park to the upper plateau area (where most of the existing buildings on site are currently located);
  - \* Remediation works along the route of the road;
  - \* The provision of an oversized pipe/drain below the road, to carry runoff from future development plots to the SuDS attenuation pond proposed within Open Space Zone A, then to an outfall point in the tributary of Pearl Brook downstream of the site. A separate surface water drainage system within the road would deal with highway runoff;
  - \* Tree and vegetation clearance to facilitate the link road and site preparation work.
10. It is anticipated that site preparation and construction will take place in a single phase and completed within a year of commencement.
11. The link road is being brought forward with the benefit of funding secured through the Housing Infrastructure Fund (HIF), which is a Government capital grant programme to help secure the



delivery of much needed housing. Should this planning application be granted it is anticipated that the construction of the road would begin in April.

#### ii. Open Space Zone A

12. The hybrid/outline permission for Rivington Chase (91352/14) included two areas of open space for the development (Open Space Zones A and B). This application includes the detailed provision of Open Space Zone A. Open Space Zone A is an extensive recreational amenity zone, which will be sited to the south of the proposed link road (horizontal part of the road) and to the south of the plateau.
13. This area will have a "parkland" landscape and will consist of a mixture of grassland, surface water attenuation, tree planting and a play area.
14. The zone will also provide a buffer zone and barrier to access between the Rivington Chase development and the adjacent Site of Scientific Interest (SSSI) and Site of Biological Importance (SBI), Red Moss.
15. An Environmental Statement has been submitted within this application (this is an EIA application).

#### **Site Characteristics**

16. The application site encompasses the parts of the former Horwich Loco Works where the link road and Open Space Zone A are proposed. The former Loco Works site has a hybrid planning permission for the erection of up to 1,700 dwellings along with retail, employment and leisure development, public open space, accesses into the site and cycle and pedestrian connections (under application 91352/14).
17. The site is now referred to as "Rivington Chase" (the name officially given to the redevelopment). The first phase of Rivington Chase is currently under construction (112 dwellings being built by house builder Bellway Homes).
18. The former Loco Works site is allocated as a comprehensively developed mixed-use site within Bolton's Core Strategy. The Rivington Chase development extends to approximately 76.6 hectares and comprises previously development land and areas of naturally regenerated trees and scrub.
19. The proposed route of the road would connect to the "Rivington House" access (the access onto Chorley New Road, which runs past Rivington House and has recently been improved as part of the Bellway development), would run horizontally along the southern edge of the plateau area of the former works (where the majority of the buildings on the site are/were located) and then diagonally down to the Aspinall Way/Burnden Way roundabout (through woodland and grass land, which has been designated as employment land within the hybrid approval).
20. The majority of the Locomotive Works plateau is allocated as a conservation area (the Horwich Locomotive Works Conservation Area).
21. Along the south western edge of the plateau is a steep drop towards Red Moss and the adjacent low areas. The proposed Open Space Zone A is located here and this area currently comprises an area of dense vegetation and scrub. To the south of the proposed attenuation pond is an existing reservoir.
22. To the south and south east of the proposed Open Space is Red Moss Site of Special Scientific Interest (SSSI) and associated Site of Biological Importance (SBI) designations. The

southernmost extent of the moss adjoins a former Greater Manchester Waste Authority landfill site, also known as Red Moss, which has been capped and landscaped.

23. The Aspinall Way/Burnden Way roundabout at the southern corner of the application (where the link road will join up to the existing highway network) has four spurs, with one servicing Horwich Police Station.

### **Policy**

#### **24. Bolton's Development Plan:**

25. Core Strategy Policies: P5 Transport and Accessibility; S1 Safe Bolton; CG1 Cleaner and Greener Bolton; CG2 Sustainable Design and Construction; CG3 The Built Environment; CG4 Compatible Uses; M1 and M2 Horwich Loco Works; OA1 Horwich and Blackrod.

\* Allocations Plan Policies: P7AP Strategic Route Network; P8AP Public Rights of Way

\* Greater Manchester Joint Minerals and Waste Plans

\* Other material planning considerations:

26. National Planning Policy Framework (NPPF): 2. Achieving sustainable development; 4. Decision-making; 5. Delivering a sufficient supply of homes; 6. Building a strong, competitive economy; 8. Promoting healthy and safe communities; 9. Promoting sustainable transport; 11. Making effective use of land; 12. Achieving well-designed places; 14. Meeting the challenge of climate change, flooding and coastal change; 15. Conserving and enhancing the natural environment; 16. Conserving and enhancing the historic environment.

\* Supplementary Planning Documents: The Former Horwich Loco Works; Accessibility, Transport and Road Safety; General Design Principles.

\* Horwich Locomotive Works Conservation Area Management Plan

### **Analysis**

\* Section 38 of the Planning and Compulsory Purchase Act 2004 requires applications to be determined in accordance with policies in the Development Plan unless material considerations indicate otherwise. Applications which are not in accordance with Development Plan policies should be refused unless material considerations justify granting permission. Similarly, proposals which accord with Development Plan policies should be approved unless there are material considerations which would justify a refusal of permission. It is therefore necessary to decide whether this proposal is in accordance with the Development Plan and then take account of other material considerations.

#### **27. The main impacts of the proposal are:-**

#### **28. impact on the comprehensive redevelopment of the former Horwich Loco Works site**

- \* impact on the significance of the Horwich Locomotive Works Conservation Area
- \* impact on archaeology
- \* impact on the local highway network and highway safety
- \* impact on public rights of way
- \* impact on land contamination/ground conditions
- \* impact on ecology and nature conservation
- \* impact on drainage and flood risk

- \* impact on residential amenity
- \* impact on the character and appearance of the area

#### Impact on the Comprehensive Redevelopment of the Former Horwich Loco Works Site

- \* The former Horwich Loco Works site is allocated within the Core Strategy as a comprehensive mixed use site that will make a significant contribution to the employment and housing land requirements of Bolton. Policies M1 and M2 of the Core Strategy specifically concern the redevelopment of Horwich Loco Works, and the Former Horwich Loco Works Supplementary Planning Document (SPD) provides further details to the policies in the Core Strategy. Policy M1 of the Core Strategy states that Horwich Loco Works is a strategic site that will be developed for a sustainable mixed-use community primarily for employment and housing. Policy M2 sets out principles that the development of the Loco Works site will be guided by.

29. Members will be aware that the Council cannot currently demonstrate a five-year supply of deliverable housing land. Members will also be aware that the Council is therefore very much reliant on the comprehensive redevelopment of the former Loco Works site to help deliver a significant element of the Council's housing supply. The Loco Works is the Council's only comprehensive strategic housing site within the development plan.

30. It is the applicant's position that the comprehensive regeneration and delivery of the Rivington Chase development is entirely dependent upon the delivery of proposed spine road. The spine road would help to facilitate accelerated delivery of the development, unlocking the wider site for redevelopment: parcels of land would be able to 'plug into' the main road. The alternative is that the key infrastructure would be delivered incrementally as individual parcels of land are developed and capital receipts generate Section 106 contributions. The development of the site would be protracted owing to its marginal viability, but perhaps more importantly a key piece of local highways infrastructure would not be capable of being delivered up front in the manner that is currently being proposed.

31. The necessity and importance of delivering the spine road has indeed been acknowledged by Homes England, who have awarded the Council a grant (under the Government's Housing Infrastructure Fund (HIF)) to pay for the link road. This Government fund was set up especially to accelerate much needed housing delivery on marginal viability sites (such as this site) where the viability gap can only be bridged through the leveraging of external funding.

32. It is therefore considered that the delivery of the link road is vital to meeting the strategic housing objective of the Core Strategy (Objective 15), that is to focus new housing in the existing urban area (rather than sites within other protected open land and Green Belt). The delivery of the road would also support the Government's objective (paragraph 59 of the NPPF) to significantly boost the supply of homes without unnecessary delay.

#### Impact on the Significance of the Horwich Locomotive Works Conservation Area

33. Policy CG3.4 of the Core Strategy states that the Council will conserve and enhance the heritage significance of heritage assets and heritage areas, recognising the importance of sites, area and buildings of archaeological, historic, cultural and architectural interest and their settings. Policy M2 states that the development of the Loco Works (8) should reflect the historic importance of the Horwich Loco Works.

34. Paragraph 184 of the NPPF states that heritage assets are an irreplaceable resource, and should be conserved in a manner appropriate to their significance, so that they can be enjoyed for their contribution to the quality of life of existing and future generations. Paragraph 192 states that, in determining applications, local planning authorities should take into account of:



35. the desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation;
  - a. the positive contribution that conservation of heritage assets can make to sustainable communities including their economic vitality; and
  - b. the desirability of new development making a positive contribution to local character and distinctiveness.
- c. Paragraph 193 of the NPPF states that when considering the impact of a proposed development on the significance of a designated heritage asset, great weight should be given to the asset's conservation (and the more important the asset, the greater the weight should be). This is irrespective of whether any potential harm amounts to substantial harm, total loss or less than substantial harm to its significance.
36. Section 72(1) of the Planning (Listed Building and Conservation Areas) Act 1990 requires special attention to be paid to the desirability of preserving or enhancing the character or appearance of conservation areas in the determination of planning applications.
37. The northern part of the former Loco Works site (the plateau area where the majority of buildings are located) is designated as Horwich Locomotive Works Conservation Area. This area was designated as a conservation area in February 2016. The conservation area is the "heritage asset" (as defined within the NPPF), not the individual buildings within it.
38. Under the original site wide hybrid planning permission for the Rivington Chase redevelopment (91352/14) the Council granted consent for the demolition of all but four buildings within the conservation area. The buildings proposed to be retained ("key buildings") were Rivington House, the Stores Building, Millwrights Shop and Pattern Makers, and the Erecting and Repair Shop. These buildings were proposed to form the "Heritage Core" within the Rivington Chase development.
39. Under Schedule 3 Section 9 of the Section 106 Agreement for hybrid approval 91352/14 a provision was made for the applicant to submit a Conservation Report to the Council (two years after the permission) for the buildings within the Heritage Core. This report was to contain information including the current state of the buildings, details of any existing or proposed use, details of any funding or public grants that are available for undertaking works to the buildings, details of any work the applicant has undertaken or intends to take, and an audit of the viability of retaining each of the buildings so that they can be brought back into use or continued to be used. The Agreement stated in the event that the audit of viability of retaining each of the building identifies that public funding or grants, or other sources of private sector funding, are available and that the buildings' re-use or continued use would be viable then the applicant should seek to attribute such funding in the following order of priority: 1) Rivington House and the Stores Building, 2) Millwrights Shop, Pattern Makers and the Erecting Shop. The agreement continues that in the event that at least two consecutive Conservation Reports confirmed that it was not viable to bring back into use a building and/or buildings then subject to obtaining all necessary consents the applicant may proceed to demolish that building and thereafter that building would no longer be subject to the obligations within Section 9 of the Agreement. The applicant did submit two consecutive Conservation Reports which found that the Erecting and Repair Shop would not be viable to bring back into use, and these were confirmed by an independent viability consultant.
40. The original planning permission for Rivington Chase granted four accesses into the development (full permission) and envisaged a link road (illustrated within the masterplan) linking the Rivington House access to Aspinall Way (to the south of Futura Park), with its route going

through the centre of the Heritage Core part of the development (outline permission, therefore the detailed route was not approved within the plateaux area). The road was originally proposed to the north of the Erecting and Repair Shop, and was to run between this building and Millwrights Shop and Pattern Makers.

41. At the time the site wide permission for Rivington Chase was granted, it was acknowledged that substantial additional work would need to be undertaken on a variety of complex technical issues to facilitate the comprehensive regeneration of the site (as indicated by the 46 planning conditions requesting additional information that were attached to the hybrid permission). Prior to the submission of this current planning application a major technical study was undertaken to identify the most appropriate route for the link route, taking into account the physical and technical challenges of delivering a high quality vehicular, pedestrian and cycle connection through the site, which is integrated with and facilitates the provision of, a large area of publicly accessible open space (Open Space Zone A).
42. The link road has also been designed to a higher standard (that is, wider) than was previously assumed within the masterplan. The road now needs to accommodate appropriate carriageway widths, visibility splays, cycle lanes (to provide Transport for Greater Manchester's "Bee Network" of segregated cycle ways on key routes), footpaths and to respect junction geometry to assure appropriate highway safety and to ensure that the road works safely and efficiently with both traffic and pedestrian movements.
43. Given the combination of these factors the link road is now proposed to run to the south of the Heritage Core and provide a link via a roundabout to the Network Rail / Morris Homes land (current application). Unfortunately this would result in the loss of the Erecting and Repair Shop, as this building would be 'clipped' by the proposed roundabout.
44. During the initial consultation period for this application Historic England raised a formal objection to the loss of the Erecting and Repair Shop.
45. Following this objection the applicant has provided further justification for the proposed route of the link road, which has been forwarded to Historic England for their further comment. The applicant has illustrated that two alternative routes for the road were considered prior to the submission of this application. Plans showing these two considered alternative routes are attached to this report.
46. The first alternative route, through the centre of the Heritage Core as envisaged within the masterplan, has had to be discounted for the following reasons:
  47. As stated above, the link road is now to be constructed to a higher standard (wider) than previously assumed.
  48. The 'T' junction that was illustratively shown on the masterplan (to connect the Rivington Chase access to the horizontal part of the link road) is now not an appropriate solution. The only way the link road can accommodate the modelled traffic loading and provide a safe cyclist and pedestrian environment, whilst allowing for through traffic access to the Network Rail/Morris Home development (06232/19), would be through the provision of a new roundabout. The size of this is dictated by highway safety and engineering requirements, and the size of the roundabout shown on the submitted plans is the minimum it could be.
    - \* The proposed roundabout in this location would also impinge on the location of the Erecting and Repair Shop, resulting in its demolition.
    - \* The location of the wide road through the centre of the Heritage Core would dominate this area and would be considered to be inappropriate urban design.
- \* The second alternative route considered, to the south of the Erecting and Repair Shop, has had

to be discounted for the following reasons:

- \* Locating the roundabout to the south of the building would be a very complex engineering operation, which has therefore been deemed unacceptable. This is owing to the severe drop off in levels between the historic works plateau and the land to the south.
- 49. The alignment requirement would remove the ability to achieve a large surface water drainage feature which is a key component of the link road infrastructure (hence the reasoning for the road application to be directly associated with the proposed Open Space Zone A, for drainage reasons). The drainage feature location ultimately ensures that the hydrology and hydrogeology between the site and the adjoining SSSI/SBI remains in balance. The hydrology balance between the SSSI/SBI and the works plateau is a very precise one and it is this pattern of water ingress to the ecological designations which needs to be maintained through this detailed design feature in this location, upstream of the reservoir: the proposed SUDs features needs to be proposed where it is proposed.
- \* Moving the roundabout and associated road alignment further to the north west would result in a substandard road layout with the link road running too close in terms of traffic and visibility to the Erecting and Repair Shop. The resulting roundabout would not be acceptable in terms of highways safety.
- \* Routing the road on a more southerly track would require an even more complex geoenvironmental solution owing to the known presence of contamination and significant level differences and poor ground conditions generally. It has been determined to be technically unfeasible. A high level appraisal of the cost implications of this has been undertaken and it is estimated that it would add circa £1.5m to the overall road construction budget, which is unviable.
- \* Following the submission of this information Historic England have withdrawn their formal objection to the proposal. They acknowledge that the required specification for the link road (to meet highway safety and engineering requirements) and the constraints imposed by geology, hydrology, topography, the adjacent SSSI/SBI and contamination greatly limit the possible route for the link road. Historic England therefore state that the local planning authority should carry out the exercise required by paragraph 196 of the NPPF, and weigh the public benefits of the construction of the link road and the regeneration which its construction would enable against the high level of harm which the loss of the Erecting and Repair Shop would cause to the Loco Works Conservation Area and its character. Historic England confirm that they would not object to the demolition of the building if the local planning authority's weighing exercise leads to the conclusion that its loss is necessary to deliver the public benefits which regeneration of the area would provide, including the potential repair and reuse of the remaining buildings of the Heritage Core.
- \* Paragraph 196 of the NPPF states that where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal including, where appropriate, securing its optimum viable use.
- 50. It is considered that the public benefits of the proposal include (but are not limited to):
  - 51. The comprehensive regeneration and delivery of the Rivington Chase development is dependent upon the delivery of proposed spine road.
  - 52. The spine road would help to facilitate accelerated delivery of the development, unlocking the wider site for redevelopment: parcels of land would be able to 'plug into' the main road rather than having to waiting for trigger points for road connections within the various zones of the site.
  - \* The delivery of the road would help accelerate the provision of a range of new homes, that would directly meet the housing needs of Horwich, the wider Borough and tackle the national



shortage of new homes including the delivery of affordable homes.

- \* The delivery of the road would help accelerate the provision of new employment land and floorspace which will permit local businesses to expand and attract new investors into the Horwich area.
  - \* A very significant financial investment directly into Horwich and the local area which could generate circa 1,500 full time jobs on the site and 320 jobs during construction and the associated supply chain.
  - \* A spend of upwards of £80m per annum by the new population on local goods and services which in itself would lead to a further 200 indirect jobs.
  - \* Significant economic enhancement in the local area and tackling local issues if unemployment and worklessness.
  - \* Provision of major benefits to health and wellbeing of the local (and new) population by opening up to the public around 20 hectares of open space (which is/previously inaccessible).
  - \* The delivery of the road would help accelerate the redevelopment of the Armstrong's part of the site, which would remove visual amenity, noise, air and odour quality issues.
  - \* Remediation of brownfield land and dealing with the various legacy issues associated with the site's industrial past.
  - \* The proposed road would also carry all construction traffic into the development and facilitate the existing Loco Works Industrial Estate-related traffic in the interim. As it is proposed that preparation and construction works would commence from the southern access point (Aspinall Way), working inwards, heavy goods vehicles would access the site via Aspinall Way rather than Chorley New Road, and therefore away from neighbouring residents.
  - \* The link road would accommodate cycle and pedestrian ways, providing better connections and sustainable travel between Horwich and Middlebrook and the wider cycle networks.
- \* The submitted Conservation Reports have justified that it would not be viable to bring the Erecting and Repair Shop back into use.
- \* Historic England has recommended, should the application be approved, that the advice of Greater Manchester Archaeological Advisory Service (GMAAS) is sought on the specification for a scheme of building recording, which should be carried out on the Erecting and Repair Shop before its demolition. This would be secured by a pre-commencement condition and would be in accordance with paragraph 199 of the NPPF which states that local planning authorities should require developers to record and advance understanding of the significance of any heritage assets to be lost (wholly or in part) in a manner proportionate to their importance and the impact, and to make this evidence (and any archive generated) publicly accessible. The condition can also require an assessment and agreement from the Local Planning Authority as to what elements of the building can be re-used or recycled either on the Loco Works site or elsewhere (eg East Lancs Railway have expressed an interest in re-using elements of the building).
- \* Whilst it is acknowledged that the permanent loss of the Erecting and Repair Shop would be damaging to the Horwich Loco Works Conservation Area and its character, and would be highly regrettable, Historic England consider that it would lead to less than substantial harm to the significance of the conservation area (meaning paragraph 196 of the NPPF is applicable). Officers agree with the applicant that there are no other viable alternative routes for the proposed link road and consider that the public benefits of the proposal (as listed above) would outweigh the less than substantial harm to Horwich Loco Conservation Area.

#### Impact on Archaeological

53. Policy CG3.4 of the Core Strategy states that the Council will conserve and enhance the heritage significance of heritage assets and heritage areas, recognising the importance of sites, area and buildings of archaeological, historic, cultural and architectural interest and their settings.

54. GMAAS has commented that ground investigations have demonstrated that the proposed new road will cut across areas of deep peat, especially on the eastern side as it runs towards Aspinall Way. They have stated that there is potential to disturb peat which may contain organic materials from the prehistoric and Roman periods as has been found historically elsewhere in Red Moss. GMAAS therefore recommend that an archaeological watching brief be undertaken during the construction of the groundworks for the road, to monitor the archaeological potential of the material disturbed and to record deposits and finds of interest that come to light. They comment if particularly sensitive/significant archaeological remains are exposed than an identified contingency fund should be accessed to allow an archaeology team to undertake a detailed investigation and to allow for conservation and scientific analyses. This watching brief is therefore suggested via a planning condition.

55. It is considered, subject to the recommended condition, that the proposed development would comply with the archaeological part of Core Strategy Policy CG3.4.

#### Impact on the Local Highway Network and Highway Safety

56. Policy P5 of the Core Strategy states that the Council will ensure that developments take into account [amongst other things] accessibility by different types of transport, prioritising pedestrians, cyclists, public transport users over motorised vehicle users, the design of developments to enable accessibility by public transport, and freight movement for industrial and storage uses. Policy S1.2 states that the Council will promote road safety in the design of new development.

57. Policy M2 of the Core Strategy states that the development of the Loco Works site will be guided by the following principles [amongst others] in order to ensure that it is economically, socially and environmentally sustainable:

58. It must ensure that additional traffic generated does not result in serious inconvenience or danger on the public highway.

59. It must provide transport links between Horwich town centre, Middlebrook and the site.

60. It must be well served by public transport, and make effective provision for cycling and walking.

\* Paragraph 109 of the NPPF states that development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe.

\* Hybrid planning approval 91352/14 for the Rivington Chase development at the former Horwich Loco Works site granted full planning permission for four accesses into the development; the "Rivington House" access and the "Armstrong's" access (both off Chorley New Road), access off Aspinall Way and access off Crown Lane. A link road was proposed at that time, illustrated within the masterplan for the Rivington Chase development, as linking the Rivington House access to Aspinall Way (to the south of Futura Park), with its route going through the centre of the Heritage Core part of the development. The traffic and highway implications associated with the development were therefore fully assessed during the determination of the hybrid application and the necessary mitigation measures (improvements to the existing highway network) were also approved at that time.

\* This new full planning application is now required as the proposed location of the access onto Aspinall Way has been altered from that approved under application 91352/14. The access onto Aspinall Way is now proposed further south than previously approved, at the roundabout that connects Burnden Way, Aspinall Way, the Linkway and Horwich Police Station. This alteration in

the route enables the road to be accommodated solely within Council and Alpha (the joint applicants') owned land, thereby avoiding third party land. Access from the Rivington House access is still as previously approved.

61. The internal route of the link road has also been amended from that originally envisaged, with the road now being proposed to run to the south of the Heritage Core rather than through the centre of the Heritage Core. The route of the road has been amended to accommodate existing site levels and to avoid the asbestos tip to the east of the site, and to ensure that the road is deliverable and able to achieve the required widths and gradients.
62. The local highway authority has been involved in the design of the proposed link road, in order to ensure that it meets the Council's standards for adoption and requirements in terms of pedestrian accessibility and cycling provision.
63. The proposed development (the proposed link road and Open Space Zone A) will not increase traffic flows from the development site, from those already fully assessed under the hybrid application. The distribution of trips on to the surrounding highway will be identical to the previous application with one exception, that being the trips that would have accessed Aspinall Way via the consented signalised junction would now instead do so via the existing roundabout further south on Aspinall Way. It has however been found within the applicant's Transport Assessment (TA) that given there is no increase in trip generation, no change in the wider distribution of trips, and only a minor change in the broad location of the access road then the impact on the wider highway network would be negligible. The operational assessment in the TA shows that there is sufficient spare capacity in the AM and PM peak hours to accommodate the development as amended.
64. It is considered that the proposed development would continue not to have an unacceptable impact on highway safety or a harmful residual cumulative impact on the surrounding road network (subject to the previously approved mitigation measures) and therefore complies with Policies P5, S1.2, M2.1, M2.2 and M2.3 of the Core Strategy.

#### Impact on Public Rights of Way

65. Policy P8AP of the Allocations Plan states that the Council will permit development proposals affecting public rights of way, provided that the integrity of the right of way is retained.
66. The proposed alignment of the link road would impact on a number of public rights of way that run through the site, as the envisaged link road in the masterplan would have. Public Rights of Way Blackrod 010, Blackrod 011, Blackrod 015, Horwich 184, Horwich 108, Horwich 106 and Horwich 179 lead through and around the application site. Any required diversions of these would need to be by a separate legal order (Local Highways Authority).

#### Impact on Land Contamination/Ground Conditions

67. Policy CG4.3 of the Core Strategy states that development proposals on land that is (or is suspected to be) affected by contamination or ground instability must include an assessment of the extent of the issues and any possible risks. Development will only be permitted where land is, or is made, suitable for the proposed use.
68. Paragraph 178 of the NPPF states that planning decision should ensure that: a) a site is suitable for its proposed use taking account of ground conditions and any risks arising from land instability and contamination. This includes risks arising from natural hazards or former activities such as mining, and any proposals for mitigation including land remediation (as well as potential impacts on the natural environment arising from that remediation); b) after remediation, as a



minimum, land should not be capable of being determined as contaminated land under Part IIA of the Environmental Protection Act 1990; and c) adequate site investigation, information, prepared by a competent person, is available to inform these assessments.

69. Paragraph 179 of the NPPF states where a site is affected by contamination or land stability issues, responsibility for securing a safe development rests with the developer and/or landowner.

70. The former Loco Works site has been subject to a long standing history of heavy industrial activity, which is well known to have impacted on land quality. The nature of this contamination, as well as its implications, has been assessed via desk and site investigations over a number of years, the results of which were used to inform both the previous planning approvals for the site and this latest proposal. Updated investigations have occurred for the parts of the site proposed for the route of the link road and Open Space Zone A.

*71. Proposed link road*

72. Starting at the existing roundabout junction with Aspinall Way/Burnden Way, the proposed road would initially pass across an area of low-lying greenfield land with ground conditions here being known to contain relatively poorly compacted saturated peat soils.

73. The road then would cross into the boundary of the former Red Moss domestic landfill, cutting along the tip's northern and eastern flank. A review of this landfill has been completed as part of this proposal and this confirms that the landfill comprises of "Made Ground deposits" (including a mixture of materials from gravelly sandy clay with brick cobbles, limestone and ash, black sand and gravel containing ash, brick, clinker and glass and organic material. Soil chemical analysis of these materials indicate the presence of localised heavy metal contamination, however the concentrations are not significant to impact on the proposed development.

74. The road alignment then heads in a north-westerly direction towards the Loco Works plateau, skirting to the west of the EA registered "Ashworks" Asbestos Tip. The proposed re-routing of the link road (as now applied for rather than following the proposed route within the masterplan) has been deliberately proposed to avoid this tip. There is however localised asbestos in this area.

75. The road would then approach the works plateau towards the south eastern corner, where there would be change on level of around 10 metres. An embankment is therefore proposed within this area to link the proposed employment land zone with the upper residential development on the plateau area. The original plateau area was constructed via a cut and fill process: the current embankment face has been progressively extended by tipping operations during the life of the works. This material comprises of granular made ground deposits consisting primarily of foundry waste (sand, ash and clinker).

\* The proposed construction of the road would involve the following:

- \* Removal of all existing vegetation and topsoil along the proposed alignment.
- \* In areas of peat poorly compacted soil (particularly the southern section) the use of specialist ground improvement techniques such as dynamic replacement would be employed to stabilise the ground and facilitate the road construction. These techniques are discussed in detail within submitted Phase 2 Site Investigation report.
- \* Away from the peat and other areas of poor ground the road would be constructed using conventional means.

76. At the part of the road that would pass Red Moss landfill and the Ashworks Tip due consideration an mitigation would be required to ensure no health risks are posed to the site operatives and the general public and that no contaminants, leachate or ground gases escape into adjacent land or water courses. The clearance of vegetation from this area would facilitate the completion of supplementary ground investigations across this area (under

controlled conditions and with appropriate health and safety mitigations). This further investigation would be subject to a planning condition. As with the areas with peat, specialist ground improvement techniques would need to be employed in this area.

- \* The bulk excavation and movement of soils would be required at intervals along the length of the road to facilitate a level construction platform. The excavated soil would be re-used as engineered fill (remain within the site). Any unsuitable material would be disposed of from the site to a suitably licensed facility.

#### *Open Space Zone A*

- \* This area would be situated between the BREL Works Tip face and Red Moss SSSI. The northern and eastern portion of this proposed open space is formed by the former works tip face, extending in height between 4.9 and 6.9 metres and partially covered with vegetation. Broadly speaking the ground in this area comprised ash based made ground materials derived from tipping from the Loco Works operations. There are no specific sources of contamination that would pose an immediate risk to current users.
- \* The proposed construction works would involve the removal of all vegetation and topsoil along the tip face and in the area of the proposed attenuation features. On completion of the removal works a program of supplementary topographical and investigation surveys would be completed (as required by a planning condition) for the tip face. The face would be stabilised and re-profiled. All the proposed areas of landscaping would be formed using a 600mm clean soil capping system.

#### *Mine shaft*

- \* There is a suspected mine shaft within the area of the proposed open space, to the east of the reservoir. The Coal Authority raises no objection to the proposal, but request a planning condition for further site investigation in this specific area.
- \* The Council's Pollution Control Officers and the Environment Agency have raised no concerns regarding the proposed development's impact on land contamination and ground condition.

77. It is therefore considered, subject to planning conditions requiring further site investigations of specific areas and further details of road construction to be employed in specific areas, that the land would be made suitable for its proposed end uses of a road and public open space, compliant with Policy CG4.3 of the Core Strategy.

#### Impact on Ecology and Nature Conservation

78. Policy CG1.2 of the Core Strategy states that the Council will safeguard and enhance biodiversity in the borough by protecting sites of urban biodiversity including trees, woodland and hedgerows from adverse development, and improving the quality and connectivity of wildlife corridors and habitats. Policy M2.6 states that the value of the adjoining Red Moss SSSI must be protected in the development of Horwich Loco Works.

79. Paragraphs 174 to 177 of the NPPF concern the protection and enhance of habitats and biodiversity.

80. An ecology and nature conservation assessment has been undertaken as part of the submitted Environmental Statement, which includes a desk top study, an overall site appraisal and protected species surveys for birds, bats, amphibians, reptiles, badgers, water vole and trees. In terms of habitats, there are a number of features within the wider development site, such as buildings and hard standing, meadow/pasture, unclassified grassland marshy grassland, scrub, tall ruderal, waterbodies, water courses and woodland. Invasive species of Japanese knotweed

has also been identified in several areas across the wider development site.

#### *Red Moss SSSI and SBI*

81. Red Moss Site of Special Scientific Interest (SSSI) adjoins the application site immediately to the south eastern boundary of the proposed Open Space Zone A, and extends southwards to the motorway corridor. The SSSI is designated for its vegetation community, although the citation also notes bird (wildfowl) and invertebrate (dragonflies) assemblages. The biological interest arises on the site because of the presence of vegetation with peat formation capability. The hydrology and other conditions necessary to support this vegetation community has not been irreparably damaged. The peat forming vegetation represents one of the most extensive areas compared with other peatland locations in Greater Manchester and Merseyside. Red Moss is also identified within Bolton's development plan as a Site of Biological Importance (SBI).
82. The proposed Open Space Zone A will provide a buffer zone and barrier to access between the Rivington Chase development and the adjacent Red Moss SSSI and SBI. This buffer zone has always been envisaged through the development of the proposals for Rivington Chase.
83. When initially commenting on this application Natural England raised concern that any treatment of peat under the proposed link road could likely have localised dewatering effects, as the road is within 100 to 150 metres of the SSSI. They therefore requested further information from the applicant as to the nature of the made ground and peat in that specific areas and the direct surface water link to the SSSI. Further information has been submitted by the applicant and forwarded to Natural England. Conditions are recommended for further detailed site investigation work to be undertake prior to commencement of development (to inform the detailed design of the ground improvement works where the proposed link road would cross an area of peat), further hydrogeological modelling work, and for a Construction Environmental Management Plan (CEMP) which would include continuous groundwater monitoring during the construction of the road (and in the unlikely event that significant lowering or alterations in groundwater levels is identified that could impact on the SSSI work would stop until these levels recover).
84. It is therefore considered, subject to these recommended conditions, that the proposed development would not harm Red Moss SSSI and SBI.

#### *Other Habitats*

85. The application proposal also includes the proposed demolition of the Erecting and Repair Shop (for the proposed new internal route of the link road). A bat survey has been submitted with the application. Greater Manchester Ecology Unit (GMEU) have recommended a condition that if the building is not demolished before 30th April 2020 the building should be re-assessed for bat roosting potential.
86. Owing to the requirement for existing vegetation to be removed to allow for further site investigations (contamination surveys), the construction of the link road and the formation of the new Open Space Zone A, some 3.5 hectares of primary sere willow/birch woodland would be lost. This would however be compensated for by the creation of new woodland habitat.
87. The Environment Agency has commented that a scheme should be agreed for the protection and management of a 5 metre wide buffer zone around the Nellies Clough Brook and Middle Brook waterbodies. This zone would remain free from any built development and would ensure that the development would not encroach upon the watercourses, protecting their ecological value. This buffer zone would be secured by a planning condition.

#### *Fauna*



88. The Environment Agency and GMEU recommend a condition for further details of the protection and/or mitigation of damage to populations of water vole and their habitat during the proposed construction works and once development is completed. This condition is therefore suggested.
89. GMEU also request conditions for a reasonable avoidance measure method statement for reptiles and a further survey of site (and within 30 metres of boundaries) for badger setts.

#### *New habitat*

90. New habitat would be created by the provision of Open Space Zone A, which would consist of a mixture of grassland, surface water attenuation and new tree planting. The Environment Agency has welcomed this proposal. They request a condition be attached to any approval for a landscape management plan for the open space, for the long term design objectives, management responsibilities and maintenance schedules for all the proposed landscaped areas. This would ensure the protection of habitat and secure opportunities for the enhancement of the nature conservation value of the site.
91. A section of Nellies Clough is to be taken out of the culvert (as part of the drainage strategy) at its junction with the headwall, to create a new surface channel.
92. It is therefore considered, subject to the recommended conditions from the consultees, that the proposed development would safeguard and enhance the biodiversity of the site and would continue to protect the adjoining Red Moss SSSI, compliant with Policies CG1.2 and M2.6 of the Core Strategy.

#### Impact on Drainage and Flood Risk

93. Policy CG1.5 of the Core Strategy states that the Council will reduce the risk of flooding in Bolton and other areas downstream by minimising water run-off from new development and ensuring a sequential approach is followed, concentrating new development in areas of lowest flood risk.
94. Paragraph 163 of the NPPF states when determining any planning applications, local planning authorities should ensure that flood risk is not increased elsewhere. Paragraph 165 states that major developments should incorporate sustainable drainage systems unless there is clear evidence that this would be inappropriate.
95. The majority of the site is not at any significant risk of flooding from any source. A very small area of land at the south eastern tip of the site encroaches into the Flood Zone 2 area around Middle Brook and small sections of the Middle Brook tributaries would be required to be placed into culverts to enable the link road to cross them.
96. An oversized pipe/drain is to be provided below the spine road, which is to carry runoff from future development plots to the proposed attenuation pond within Open Space Zone A; this would then continue to an outfall point in the tributary of Pearl Brook downstream of the site. A separate surface water drainage system within the road would deal with highway runoff.
97. The main sustainable drainage system (SuDS) feature for the wider development would be the proposed attenuation pond within Open Space Zone A (the primary attenuation feature). It is proposed that each development parcel within the Rivington Chase development would have their own smaller attenuation features, which would be proposed as part of the future reserved matters applications.
98. A flood risk assessment has been submitted with the application and a section on drainage and flood risk is included within the Environmental Statement.

99. The Environment Agency initially raised an objection to the proposal on flood risk grounds, as they considered there was insufficient information submitted with regard to impacts of climate change and the requirement for compensatory flood storage within the site. This further information was submitted by the applicant during the application process, which resulted in the Environment Agency withdrawing their objection in June.

100. The Environment Agency now raises no flood risk or drainage objections to the proposal, subject to a pre-commencement condition requiring further details of the compensatory storage and flood risk mitigation.

101. It is considered, subject to pre-commencement conditions requiring further details for the proposed drainage scheme, that the proposed development would minimise water run-off and would not increase flood risk elsewhere, compliant with Policy CG1.5 of the Core Strategy and the guidance contained within the NPPF.

#### Impact on Residential Amenity

102. Policy CG4.2 of the Core Strategy states that development should not generate unacceptable nuisance, odours, fumes, noise or light pollution, nor cause detrimental impacts upon water, ground or air quality.

103. Paragraph 180 of the NPPF states that planning decisions should ensure that new development is appropriate for its location taking into account the likely effects (including cumulative effects) of pollution on health, living conditions and the natural environment, as well as the potential sensitivity of the site or the wider area to impacts that could arise from the development.

104. The potential impacts of the proposed development upon the natural environment and ground condition have been discussed above.

#### *Air quality*

105. An air quality and dust assessment has been submitted with the application. This considers the construction phase for the development as well as traffic emissions once the road has been constructed. The Council's Pollution Control Officers have assessed this information and raise no objection.

#### *Noise and vibration*

106. An assessment of the potential noise and vibration impacts during both the construction and operational phase of the link road has been included within the submitted Environmental Statement. This has found that the residual effects would be negligible. The Council's Pollution Control Officers have also assessed this information and raise no objection. They also agree that a Construction Environmental Management Plan (CEMP) should be a condition of the approval.

#### *Construction traffic*

107. The proposed road would carry all construction traffic into the wider Rivington Chase development as well as facilitating the existing Loco Works Industrial Estate-related traffic in the interim. As it is proposed that preparation and construction works would commence from the southern access point (Aspinall Way), working inwards, heavy goods vehicles would access the site via Aspinall Way rather than Chorley New Road: therefore away from neighbouring residents along Chorley New Road and at the first phase of development at Rivington Chase (the Bellway Homes' development).

108. It is therefore considered that the proposed development would not unduly harm the amenity

of neighbouring (or future) residents, compliant with Policy CG4.2 of the Core Strategy.

#### Impact on the Character and Appearance of the Area

109. Policy CG3 of the Core Strategy states that the Council will conserve and enhance local distinctiveness and will require development to be compatible with the surrounding area. Policy OA1 refers specifically to developments in Horwich and Blackrod and states that the Council will conserve and enhance the character of the existing landscape and physical environment.
110. A landscape and visual assessment has been submitted as part of the Environmental Statement accompanying this application.
111. There would be a temporary negative effect on the landscape owing to the change in the land form of the site and the loss of vegetation. However, this is an unavoidable consequence of this particular development given its constraints. There will nevertheless be beneficial landscape impacts upon completion of Open Space Zone A and the wider Rivington Chase development, which the link road would assist in delivering.
112. The link road would be landscaped on either side, forming the tree-lined boulevard as envisaged within the hybrid permission for Rivington Chase.
113. It is therefore considered that there would be no conflict with Policies CG3 and OA1 of the Core Strategy.

#### **Conclusion**

114. Whilst the proposed demolition of the Erecting and Repair Shop (a key building within Horwich Locomotive Works Conservation Area) to allow for the proposed re-routing of the link road is regrettable, it has been justified by the applicant that there are no other options available and that the less than substantial harm that would be caused to the significance of the conservation area would be outweighed by the public benefits of the proposal.
115. It is not considered that the proposed new access point for the link road onto the Aspinall Way/Burnden Way roundabout (further south than previously approved) would jeopardise highway safety or have any further undue impact on the operational capacity of the local highway network than the already granted hybrid permission. The re-routed link road would be better accommodated within the site with regards to ground conditions (it would now avoid the asbestos tip) and existing gradients, and would allow the road to be accompanied by cycleways and footways (as envisaged within the hybrid permission and masterplan for the Rivington Chase development).
116. Subject to further investigatory works once vegetation has been removed, it is considered that the site would be suitably remediated to accommodate the proposed link road and Open Space Zone A.
117. The recommended planning conditions would also ensure that the proposed development of the link road and the open space would safeguard and enhance biodiversity, would not increase the risk of flooding, and would not unduly harm the amenity of neighbouring residents.
118. The comprehensive regeneration and delivery of the Rivington Chase development is considered to be dependent upon the delivery of proposed spine road. The spine road would help to facilitate accelerated delivery of the residential and employment development, unlocking the wider site for redevelopment: parcels of land would be able to 'plug into' the main road rather than having to wait for trigger points for road connections within the various zones of the site.

119. For the reasons discussed above it is considered that the proposed development would comply with Bolton's development plan policies and would constitute sustainable development as defined within the NPPF, and therefore such be granted permission without delay (NPPF paragraph 11). Members are therefore recommended to approve this application subject to the recommended conditions.

## **Representation and Consultation Annex**

### **Representations**

**Letters:-** 8 letters of objection have been received, including from Save Britain's Heritage, Horwich Heritage and Bolton and District Civic Trust. These letters raise the following concerns:

#### *Demolition of the Erecting and Repair Shop*

120. The building is part of the heritage core of buildings designated for retention and therefore there has to be an exceptional reason for its demolition;
  121. The building is not structurally unsound or in danger of collapse;
  122. A slight adjustment of the position of the western roundabout would enable the roadworks to miss this building;
  123. The applicant does not have a convincing argument for the demolition of the building;
- \* The proposed demolition would cause significant harm to the character of the Horwich Locomotive Works Conservation Area;
  - \* Repeatedly agreements have been made to protect a selection of buildings in the conservation area, whilst sacrificing the rest;
  - \* There are few examples of the original steelwork and gantries, intact within the building, remaining in the country;
  - \* The proposal is contrary to the NPPF as there is clearly no public benefit to compensate for the harm to the conservation area by the loss of the building;
  - \* The building has great potential as a dedicated modern live performing arts venue, something that Horwich is in need of.

#### *Highways concerns*

- \* Increase in traffic - will be used as a short cut to the motorway;
- \* The police station roundabout is a key node in the Middlebrook road network and experiences operational issues associated with traffic backing up along Burnden Way from the Spirit of Sport roundabout at busy times. The Aspinall Way access into Rivington Chase will be the most heavily used for the access given the linkages it provided to the M61 and A6;
- \* Priority at the police station roundabout would be given to Rivington Chase traffic, to the detriment of traffic leaving Middlebrook. The roundabout should therefore be signalled;
- \* The exit onto Chorley New Road (Rivington Chase access) should be widened to two lanes or introduce a roundabout.

#### *Other matters*

- \* Concern about the Ashworth's Asbestos Tip being disturbed;
- \* Loss of green space;
- \* Impact on the nearby SSSI;
- \* A link road to Mansell Way would be shorter.

2 comments have been received. These raise the following:

- \* The overall design of the scheme is commended, however believe there are areas where improvements could be made to the design for pedestrians and cyclists, including the width of the cycleway, the angle of deviation around bus stops and the design of the roundabouts;
- \* Internal swift bird bricks and hedgehog houses in boundaries should be used in the residential development (officer comment: dwellings are not being proposed within this application)

**Horwich Town Council:-** raised an objection at their meeting of 21st February 2019 owing to concerns about the demolition of the erecting and repair shop, contamination risk from asbestos and other hazardous materials, and highway safety (they suggested 20mph rather than 30mph). They

also requested that access was opened up to the proposed cycleways and walkways from Station Park (this is outside the red edge of the application and not part of the proposed link road).

**Blackrod Town Council:-** raised concerns at their meeting of 4th March 2019 regarding the existing road infrastructure and the entrances and exits, removal of the contaminated waste, and drainage and flooding (particularly in relation to Red Moss).

### **Consultations**

Advice was sought from the following consultees: Highways Engineers, Drainage Officers, Pollution Control Officers, Greenspace Officers, Tree Officers, Landscape Officers, Public Rights of Way Officer, Economic Strategy Officers, Strategic Development Unit, Corporate Property Officers, the Environment Agency, Natural England, Greater Manchester Ecology Unit, Lancashire Wildlife Trust, Historic England, Greater Manchester Archaeological Advisory Service, Ramblers Association (Bolton Group), Peak and Northern Footpaths Society, the Open Spaces Society, the Coal Authority, the National Grid, United Utilities, Greater Manchester Police and the Planning Casework Unit.

As there has been some uncertainty as to whether the red-edge of the application site encroaches onto third party land, the applicant has served notice on these adjoining landowners, on the 2 August and 9 August. Any decision on this application will not be issued until after 2 September, which allows for the 21 day notification period to lapse. If any representations are received during this notification period, these will be given due consideration prior to the issuing of any decision. It should however be noted that the route of the proposed road does not encroach onto this potential third party land.

### **Planning History**

91352/14 - Permission was granted (following a Committee approval and a referral to the Secretary of State) in September 2015 for:

- \* Part A – Full planning permission for site access, demolition of some and change of use of core heritage buildings for A1 retail and D2 leisure (both up to 2,500 sqm) plus A2, A3, A4, A5, B1, C1 and D1 uses including car parking, diversions to public rights of way, land remodelling/site remediation, together with related associated works.
- \* Part B – Outline application for residential development (C3) up to 1,700 dwellings, A1 retail (up to 2,500 sqm), employment zone of B1 and B2 uses and up to 2,700 sqm of ancillary D1 uses, children's play area, recreational open space and landscaping together with ecological mitigation areas, attenuation features and new cycle/pedestrian connections (Means of access details only).

92214/14 - Part A: Full planning application for site access, demolition of some buildings and change of use of one building to a flexible mix of commercial and community uses (A1/D2/A2/A3/A4/A5 and D1 uses including car parking), 28 apartments, and associated works. Part B: Outline application for residential development (C3) for up to 130 dwellings, recreational open space and landscaping and new pedestrian and cycle routes. This planning application was for part of the Former Horwich Loco Works site (part of the Heritage Core area). The application was deferred at the Planning Committee meeting in March 2015 (before planning application 91352/14 was approved) to enable the applicant to enter into a collaboration agreement with the then Horwich Vision. The applicant (Hong Kong Racing) is now a member of the collaboration group with the other four landowners of the Loco Works site. The application was reheard at Planning Committee in February 2018, where the decision was delegated to the Director for the signing of the Section 106 Agreement.

01373/17 – The first reserved matters application for the erection of 112 dwellings at Rivington Chase (Bellway Homes) was approved by Committee in January 2018. These dwellings are currently being built.

04445/18 – Erection of 2 detached dwellings (on the approved arrival green for the Bellway development) – refused under delegated powers in December 2018 and subsequently dismissed at appeal in June 2019.

*Applications currently being determined/to be presented at a later date to Committee*

06232/19 – Reserved matters application pursuant to outline application 91352/14 for the erection of 402 dwellings (applicants Morris Homes and Network Rail).

06233/19 – Creation of a new access to Station Road and associated works to serve the former Horwich Loco Works Strategic Site (this road would connect to the proposed link road, to join the Rivington House access to Station Road, and to provide access to the proposed Network Rail/Morris Homes development).

**Recommendation:            Approve subject to conditions**

**Recommended Conditions and/or Reasons**

1.     The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason

Required to be imposed pursuant to Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2.     No development (excluding site clearance, drainage investigations, demolition, or works relating to site investigation) shall commence until a remediation strategy to deal with the risks associated with contamination of the site has been submitted to, and approved in writing by, the Local Planning Authority. This strategy will include the following components:

\* A preliminary risk assessment which has identified:

- all previous uses;
- potential contaminants associated with those uses;
- a conceptual model of the site indicating sources, pathways and receptors; and
- potentially unacceptable risks arising from contamination at the site.

\* A site investigation scheme, based on (i) to provide information for a detailed assessment of the risk to all receptors that may be affected, including those off site.

iii. The results of the site investigation and the detailed risk assessment referred to in (ii) and, based on these, an options appraisal and remediation strategy giving full details of the remediation measures required and how they are to be undertaken.

iv. A verification plan providing details of the data that will be collected in order to demonstrate that the works set out in the remediation strategy in (iii) are complete and identifying any requirements for longer-term monitoring of pollutant linkages, maintenance and arrangements for contingency action. Any changes to these components require the written consent of the local planning authority. The scheme shall be implemented as approved.

Reason

To safeguard the amenity of the future occupants of the development and to comply with Core Strategy policy CG4.

3.     Prior to the commencement of the Open Space Zone A proposals (excluding site clearance, drainage investigations, demolition, or works relating to site investigation and ground remediation/stabilisation) the following shall be submitted to and approved in writing by the local planning authority:
  - \* Undertaking of the scheme of intrusive site investigations, designed by a competent person and



adequate to properly assess the location and condition of mine entry: 363410- 001 and establish the risks posed to the development from this mining feature;

- \* The submission of a report of findings arising from the intrusive site investigations and any remedial works and/or mitigation measures considered necessary; and
- i. Implementation of the remedial works and/or mitigation measures.

Any approved remedial works and/or mitigation measure shall be implemented in full and retained thereafter.

Reason

In the interests of public safety and in order to ensure that the proposed development is safe and stable, in accordance with the NPPF and policy CG4 of Bolton's Core Strategy.

4. No development (excluding site clearance, drainage investigations, demolition, or works relating to site investigation and remediation) shall commence until a scheme to ensure appropriate compensatory storage and flood risk mitigation has been submitted to, and approved in writing by, the local planning authority. The scheme must:

- ii. Be informed by a new detailed hydraulic modelling study of the Middle Brook Catchment. The model study should be based on detailed channel survey of the Middle Brook tributaries and the results of the hydraulic modelling study should be used to refine the proposed flood mitigation scheme measures for the site. The scheme should confirm the locations for compensatory storage in the approximate volume of 3,119m<sup>3</sup>.
- Detail proposals that will ensure the floodplain is maintained during the entire construction period of the development.
- Conserve and where possible enhance the biodiversity value of the site, promoting the creation of wetlands wherever possible. Suitable planting of the compensation area should form part of the scheme. Planting within the compensation area should comprise native species, preferably of local origin, and bearing in mind the increased frequency and duration of flooding, whilst maintaining the overall storage capacity.

The scheme shall be fully implemented and subsequently maintained, in accordance with the scheme's timing/phasing arrangements, or within any other period as may subsequently be agreed, in writing, by the local planning authority.

Reason

To prevent and or reduce the risk of flooding and ensure appropriate means of storage/disposal of surface water from the site, and to comply with policy CG1.5 of Bolton's Core Strategy.

5. No development (excluding site clearance, drainage investigations, demolition, or works relating to site investigation) shall commence until full details of the proposed link road drainage design and surface water attenuation feature in Open Space Zone A, have been submitted to and approved in writing by the Local Planning Authority. The scheme shall be in accordance with the principles set out in the approved outline surface water drainage strategy (Appendix 11.3a of the Environmental Statement Addendum, May 2019).

The development shall thereafter be carried out in accordance with the approved details and timescales.

Reason

To ensure that the proposed new attenuation ponds are developed in a way that contributes to the nature conservation value of the site in accordance with policy CG1 of Bolton's Core Strategy.

6. No development (excluding site clearance, drainage investigations, demolition, or works relating to site investigation) shall commence, until a method statement to protect retained services (including the foul sewer network) has been submitted to the Local Planning Authority and approved in writing. The method statement shall detail the measures to protect the existing on-site utility assets during:

- the site investigation work;
- the construction and decommissioning phases; and
- the future day to day operation and maintenance of the scheme.

This must include proposals for reinforcements of any crossing points to ensure any assets are protected from heavy loads during and after construction.

Reason

To ensure a satisfactory form of development and to afford appropriate protection of infrastructure that crosses the site.

7. No development (excluding site clearance, drainage investigations, demolition, or works relating to site investigation and remediation) shall commence until an Ecological Construction Environment Management Plan (eCEMP) for the onsite ecological enhancements has been submitted to and approved in writing by the Local Planning Authority. The eCEMP should include all the necessary on site protection and / or mitigation measures as set out in paragraphs 8.15 to 8.28 of the submitted Environmental Statement Addendum (Avison Young, May 2019). The development shall thereafter be carried out in accordance with the approved eCEMP.

Reason

To ensure necessary on site ecological mitigation measures and management regimes are implemented in accordance with the Environmental Statement and the overarching principles of the National Planning Policy Framework, and in accordance with policy CG1.2 of Bolton's Core Strategy.

8. No development shall take place until a scheme for the provision and management of a 5 metre wide buffer zone alongside the open watercourses shall be submitted to and agreed in writing by the local planning authority. Thereafter the development shall be carried out in accordance with the approved scheme and any subsequent amendments shall be agreed in writing with the local planning authority. The buffer zone scheme shall be free from built development including lighting and formal landscaping. The scheme shall include:
  - plans showing the detailed extent and layout of the buffer zone, outlined in masterplan (Drwg. 1010-646A, and indicative drainage strategy drawings DR008A)
  - be preferably no less than 5m from bank top of existing waterbodies feeding from Red Moss or Nellies Clough waterbodies.
  - details clearly demonstrating all land remodelling and remediation works associated with road construction or required flood compensation requirements within and adjoining riparian buffer zone
  - details how the buffer zone will be protected during development construction
  - details of any new watercourse crossing design that maintains ecological connectivity and minimises crossing length.
  - details of any required watercourse diversion,
  - details of any proposed new footpaths, fencing, lighting along riparian corridors
  - details of any landscape reinstatement for waterbodies affected by construction activities.

Reason

In order to prevent development encroaching on watercourses/wetlands and harming their ecological value in accordance with policy CG1 of the Bolton Council LDF Core Strategy and the NPPF.

9. No development shall take place until a detailed method statement for removing or the long-term management / control of Japanese knotweed and Himalayan balsam on the site shall be submitted to and approved in writing by the local planning authority. The method statement shall include measures that will be used to prevent the spread of Japanese knotweed and Himalayan balsam during any operations e.g. mowing, strimming or soil movement. It shall also contain measures to ensure that any soils brought to the site are free of the seeds / root / stem of any invasive plant covered under the Wildlife and Countryside Act 1981, as amended. Development shall proceed in accordance with the approved method statement.

Reason

To ensure the safe development of the site and eradication of an invasive species and to comply with policy CG1 of Bolton's Core Strategy.

10. Prior to any earthworks taking place a reasonable avoidance measures method statement for reptiles shall be supplied to and agreed in writing by the LPA. The method statement will include details of any:
- Precautionary surveys;
  - Methodology for removal of vegetation;
  - Timing of works;
  - Phasing of works;
  - Clerking of works;
  - Details of how arisings and materials on site will be stored and;
  - Protocols in the event that common lizard are found.

Reason

To safeguard the habitats of protected species and to comply with policy CG1 of Bolton's Core Strategy.

11. Prior to commencement of earthworks a survey of the site and within 30 metres of the boundaries for badger setts will occur and the findings supplied to and agreed in writing by the local planning authority.

Reason

To safeguard the habitats of protected species and to comply with policy CG1 of Bolton's Core Strategy.

12. No development shall take place until a plan detailing the protection and necessary mitigation measures for populations of water vole and their associated habitat is submitted and approved by the Council. Any change to operational, including management responsibilities shall be submitted to and approved in writing by the local planning authority. The water vole protection plan shall be carried out in accordance with a timetable for implementation as approved. The scheme shall include the following elements:
- details of design and construction processes that minimise or avoid protected species impacts and adhere to the Water Vole conservation handbook.
  - details of appropriate mitigation where construction impacts cannot be avoided.
  - details of new SuDS drainage from new access road to waterbodies.

Reason

To safeguard the habitats of protected species and to comply with policy CG1 of Bolton's Core Strategy.

13. No demolition of the Erecting and Repair Shop shall commence unless and until the applicant or their agents or successors in title has secured the implementation of a programme of archaeological works. The works are to be undertaken in accordance with a Written Scheme of Investigation (WSI) submitted to and approved in writing by the local planning authority. The WSI shall cover the following:
1. A phased programme and methodology of investigation and recording to include:
    - historic building survey (Historic England Level 3)
    - an archaeological watching brief with contingency
  2. A programme for post-investigation assessment to include:
    - analysis of the site investigation records and finds
    - production of final reports on the significance of the archaeological and historical interest

represented

- recommendations for the reuse or recycling of elements of the building that possess historical interest, either on site in the form of public art, public realm works or as part of a Heritage Trail, or off site in an appropriate location such as a local heritage railway organisation or similar

3. Deposition of the final reports with the Greater Manchester Historic Environment Record

4. Dissemination of the results commensurate with their significance

5. Provision for archive deposition of the report and records of the site investigation.

6. Nomination of a competent person or persons/organisation to undertake the works set out within the approved WSI.

7. Implementation of any approved recommendations for reuse or recycling of elements of the building that possess historical interest.

Reason

In accordance with NPPF Section 16, Paragraph 199 - To record and advance understanding of heritage assets impacted on by the development and to make information about the archaeological heritage interest publicly accessible.

14. No development affecting the Nellies Clough Brook culvert (excluding site clearance, demolition, ground works or works relating to site investigation and remediation) shall commence until a scheme for works to and / or modification of the culvert are submitted to and approved in writing by the Local Planning Authority in consultation with the Environment Agency. This shall include details of the line, depth, size and condition of the culvert, details of any required remedial works to replace, divert or open up the culvert, and timescales for its implementation.

Reason

To reduce the risk of flooding by ensuring the satisfactory storage of/disposal of surface water from the site and to ensure ease of access to the watercourse, in accordance with policy CG1 of the Bolton's Core Strategy.

15. Prior to the commencement of works in relation to any drainage culvert reconstruction, a comprehensive program of surveys of the existing drainage network and a scheme for abandonment, (if required) should be submitted to, and approved in writing by the Local Planning Authority.

The development shall thereafter be carried out in accordance with the approved details.

Reason

To reduce the risk of flooding, in accordance with policy CG1 of the Bolton's Core Strategy.

16. No development within Open Space Zone A (excluding site clearance, drainage investigations, demolition, or works relating to site investigation and remediation) shall commence until a landscape management plan, including long term design objectives, management responsibilities and maintenance schedules, shall be submitted to and approved in writing by the local planning authority. The landscape management plan shall be carried out as approved and any subsequent variations shall be agreed in writing by the local planning authority. The scheme shall include the following elements:
- details of maintenance regimes
  - details of any new habitat created on site including de-culverting of sections of Nellies Clough, diverted sections of Middle Brook, and potential flood compensation wetlands areas
  - details of treatment of site boundaries and/or buffers around water bodies
  - details of management responsibilities
  - details as to how retained semi natural greenspaces will be maintained over the longer term and named body responsible for management plus production of detailed ecological management plan.

Reason

To ensure the protection of wildlife and supporting habitat on site and secure opportunities for the enhancement of the nature conservation value on the site in accordance with policy CG1 of the Bolton's Core Strategy and the NPPF.

17. No part of the development which adjoins the boundaries of the designated Red Moss Site of Special Scientific Interest (SSSI) or Site of Biological Interest (SBI) shall take place until, a detailed scheme for the provision and management of an ecological/greenspace buffer zone has been submitted to and approved in writing by the Local Planning Authority. Thereafter the development shall be carried out in accordance with the approved scheme and any subsequent amendments shall be agreed in writing with the Local Planning Authority. The buffer zone shall be free from built development including lighting and formal landscaping. The scheme shall include:
- detailed plans showing the extent and layout of the buffer zone including a number of cross sections through the ecological buffer to adjoining new development.
  - details demonstrating all land remodelling or remediation works within or adjoining buffer zone.
  - details of retained or enhanced semi-natural or BAP habitats in buffer zone.
  - details of any surface water diversions, if sought.
  - details demonstrating how the buffer zone will be protected during development
  - details of any proposed footpaths, fencing and lighting.

Reason

In order to prevent development encroaching on watercourses/wetlands and harming their ecological value in accordance with policy CG1 of the Bolton's Core Strategy and section 11 of the NPPF.

18. No part of the new road infrastructure which would affect an existing public right of way shall be constructed until a strategy for the temporary and/or permanent diversion of the public rights of way affected by this part of the development has been submitted to and approved in writing by the Local Planning Authority. The Strategy shall include plans showing the existing and proposed diverted/amended routes of the public rights of way as necessary; details of the design and materials and details of any temporary or permanent road crossings where necessary. The Aspinall Way access road shall not be brought into use until the strategy has been carried out in accordance with the approved details.

Reason

In the interests of highway safety and public access in accordance with policies S1, P3 and CG4 of Bolton Council's LDF Adopted Core Strategy and Allocations Plan Policy P8AP.

19. If, during the course of development, contamination not previously identified is found to be present, no further works shall be undertaken in the affected area and the contamination shall be reported to the Local Planning Authority as soon as reasonably practicable (but within a maximum of 5 days from the find). Prior to further works being carried out in the identified area, a further assessment shall be made and appropriate action agreed with the Local Planning Authority.

Reason

To safeguard the amenity of the future occupants of the development and to comply with Core Strategy policy CG4.

20. If the demolition hereby approved (Dwg No. 1010-702) does not commence before 30 April 2020, the building will be reassessed for bat roosting potential and the findings supplied to and agreed in writing by the Local Planning Authority.

Reason

To safeguard the habitats of protected species and to comply with policy CG1 of Bolton's Core Strategy.

21. An archaeological watching brief shall be undertaken during road construction groundworks to monitor the archaeological potential of the material disturbed and to record deposits and finds of interest that come to light. If particularly sensitive/significant archaeological remains are exposed then an identified contingency fund should be accessed to allow an archaeology team to undertake a detailed investigation and to allow for conservation and scientific analyses.

Reason

There is potential to disturb peat which might contain organic materials from the prehistoric and Roman periods as has been found elsewhere in Red Moss, and to comply with paragraph 199 of the NPPF.

22. The development hereby permitted shall be carried out in complete accordance with the following approved plans:

**Site Location Plan (Dwg Ref: 1010-700 G)**  
**Cross Section 1 (Dwg No. 1010-616 B)**  
**Cross Section 2 (Dwg No. 1010-617 B)**  
**Demolition Plan (Dwg No. 1010-702)**  
**PROW Plan (Dwg No. 1010-703 A)**  
**Road General Arrangement Overview and Key Plan (Dwg No. 757-010-001 Rev B)**  
**Road General arrangement Sheet 1 of 3 (Dwg No. 757-010-002 Rev A)**  
**Road General arrangement Sheet 2 of 3 (Dwg No. 757-010-003 Rev B)**  
**Road General arrangement Sheet 3 of 3 (Dwg No. 757-010-004 Rev A)**  
**Rivington House Access Roundabout (Dwg No. 757-010-005)**  
**Armstrong Access Roundabout (Dwg No. 757-010-006 Rev A)**  
**Aspinall Way Roundabout (Dwg No. 757-010-007)**  
**Road Vertical Alignment Sheet 1 of 5 (Dwg No. 757-010-008)**  
**Road Vertical Alignment Sheet 2 of 5 (Dwg No. 757-010-009)**  
**Road Vertical Alignment Sheet 3 of 5 (Dwg No. 757-010-010)**  
**Road Vertical Alignment Sheet 4 of 5 (Dwg No. 757-010-011)**  
**Road Vertical Alignment Sheet 5 of 5 (Dwg No. 757-010-012)**  
**Street Lighting Layout Sheet 1 of 3 (Dwg No. 757-010-013)**  
**Street Lighting Layout Sheet 2 of 3 (Dwg No. 757-010-014)**  
**Street Lighting Layout Sheet 3 of 3 (Dwg No. 757-010-015)**  
**Landscape Key Plan (Dwg No. 1010-646)**  
**Link Road Landscape General Arrangement Sheet 1 of 13 (Dwg No. 1010-600 B)**  
**Link Road Landscape General Arrangement Sheet 2 of 13 (Dwg No. 1010-601 A)**  
**Link Road Landscape General Arrangement Sheet 3 of 13 (Dwg No. 1010-602 A)**  
**Link Road Landscape General Arrangement Sheet 4 of 13 (Dwg No. 1010-603 A)**  
**Link Road Landscape General Arrangement Sheet 5 of 13 (Dwg No. 1010-604 A)**  
**Link Road Landscape General Arrangement Sheet 6 of 13 (Dwg No. 1010-605 A)**  
**Link Road Landscape General Arrangement Sheet 7 of 13 (Dwg No. 1010-606 B)**  
**Link Road Landscape General Arrangement Sheet 8 of 13 (Dwg No. 1010-607 A)**  
**Link Road Landscape General Arrangement Sheet 9 of 13 (Dwg No. 1010-608 A)**  
**Link Road Landscape General Arrangement Sheet 10 of 13 (Dwg No. 1010-609 A)**  
**Link Road Landscape General Arrangement Sheet 11 of 13 (Dwg No. 1010-610 A)**  
**Link Road Landscape General Arrangement Sheet 12 of 13 (Dwg No. 1010-611 A)**  
**Link Road Landscape General Arrangement Sheet 13 of 13 (Dwg No. 1010-612 A)**  
**Link Road Landscape Planting Plan Arrangement Sheet 1 of 13 (Dwg No. 1010-630 A)**  
**Link Road Landscape Planting Plan Arrangement Sheet 2 of 13 (Dwg No. 1010-631 A)**  
**Link Road Landscape Planting Plan Arrangement Sheet 3 of 13 (Dwg No. 1010-632 A)**  
**Link Road Landscape Planting Plan Arrangement Sheet 4 of 13 (Dwg No. 1010-633 A)**  
**Link Road Landscape Planting Plan Arrangement Sheet 5 of 13 (Dwg No. 1010-634 A)**  
**Link Road Landscape Planting Plan Arrangement Sheet 6 of 13 (Dwg No. 1010-635 A)**  
**Link Road Landscape Planting Plan Arrangement Sheet 7 of 13 (Dwg No. 1010-636 B)**  
**Link Road Landscape Planting Plan Arrangement Sheet 8 of 13 (Dwg No. 1010-637 A)**  
**Link Road Landscape Planting Plan Arrangement Sheet 9 of 13 (Dwg No. 1010-638 A)**  
**Link Road Landscape Planting Plan Arrangement Sheet 10 of 13 (Dwg No. 1010-639 A)**  
**Link Road Landscape Planting Plan Arrangement Sheet 11 of 13 (Dwg No. 1010-640 A)**  
**Link Road Landscape Planting Plan Arrangement Sheet 12 of 13 (Dwg No. 1010-641 A)**  
**Link Road Landscape Planting Plan Arrangement Sheet 13 of 13 (Dwg No. 1010-642 A)**  
**Tree Pit Detail- Grassed Area (Dwg No. 1010-627)**  
**Tree Pit Detail- Wildflower Area (Dwg No. 1010-628)**  
**Open Space Landscape General Arrangement Sheet 1 of 2 (Dwg No. 1010-620 H)**  
**Open Space Landscape General Arrangement Sheet 2 of 2 (Dwg No. 1010-621 J)**  
**Open Space Landscape Planting Plan Sheet 1 of 2 (Dwg No. 1010-625 C)**  
**Open Space Landscape Planting Plan Sheet 1 of 2 (Dwg No. 1010-626 D)**  
**Tree Pit Detail- Soft Areas (Dwg No. 1010-629)**  
**No Dig Footpath (Dwg No. 1010-614 A)**

**Entrance Features (Dwg No. 1010-618 A)**  
**Viewing Platform (Dwg No. 1010-619)**  
**Stone Wall and Cycle Chicane Details (Dwg No. 1010-649)**  
**Play area (Dwg No. 1010-650 A)**  
**Environmental Impact Assessment, Volumes 1-3, Avison Young, January 2019**  
**Environmental Impact Assessment Addendum, Volumes 1-3, Avison Young, May 2019**

Reason

For the avoidance of doubt and in the interests of proper planning.



[illegible]

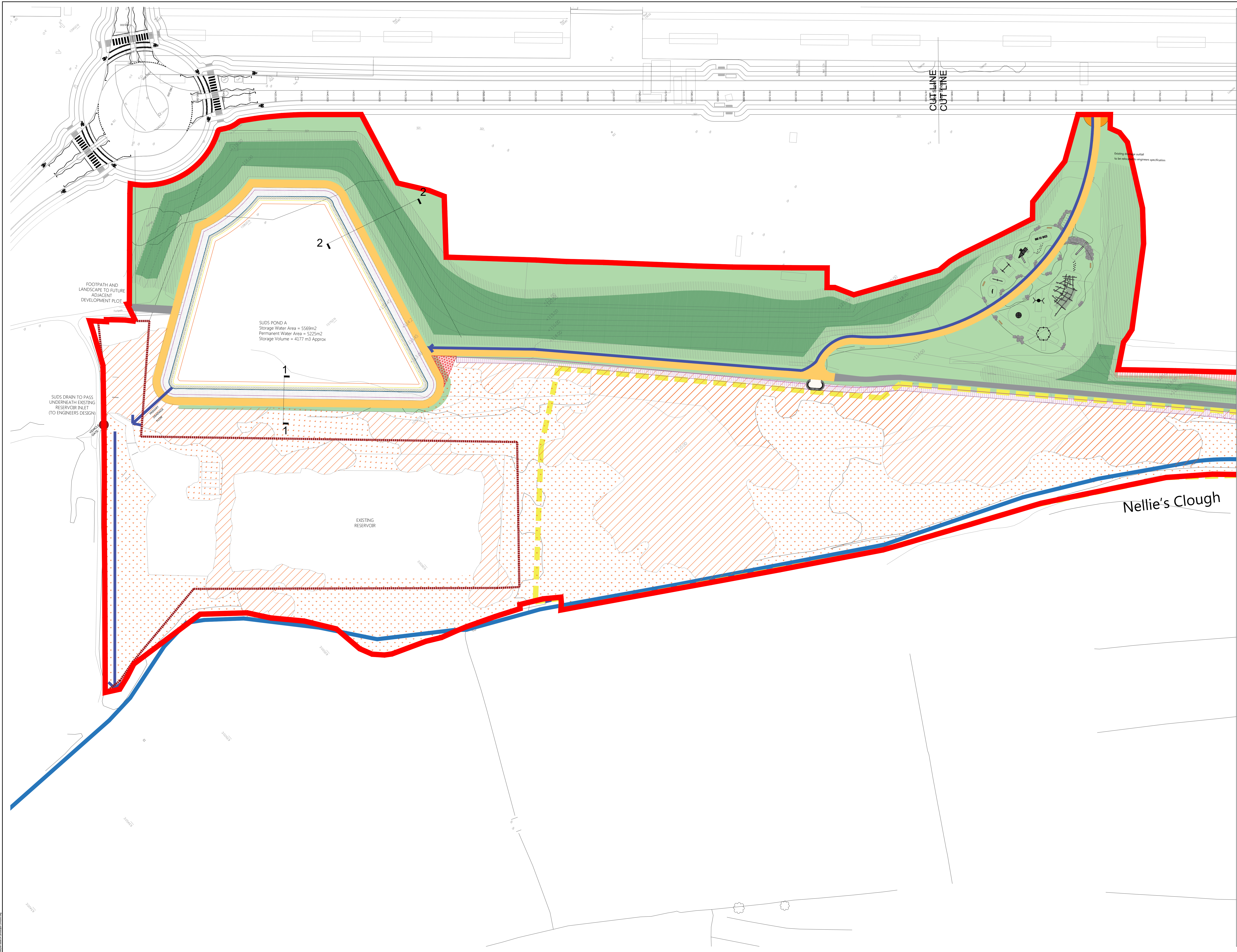












- Key
- Open Space Zone A Boundary
  - Proposed contour at 1 metre intervals
  - Proposed SUDS pond storage water level
  - Proposed SUDS pond permanent water level
  - Proposed SUDS pond lowest pond contour
  - Proposed SUDS pond access track also used as footpath/cycleway (3m wide) to engineers specification
  - Proposed footpath/cycleway, self-binding gravel finish (2 metres wide) refer to detail on drawing 1010-614
  - Existing trees/shrubs retained
  - Levels to be refined after detailed slope and stability measures TBC after site investigation to engineers specification
  - Existing grassland retained
  - Proposed woodland edge planting
  - Proposed Woodland Core planting
  - Proposed wildflower seed mix for woodland and general areas
  - Proposed wildflower seed mix for damp areas
  - Proposed floating mix
  - Proposed grass reinforcement to facilitate vehicular maintenance access to SUDS Pond To Engineer's specification
  - Drainage Flows to Engineer's specification
  - Proposed 2.4m high chain link fence Colour - Green Ref - Jackson Ltd - 304100
  - Proposed entrance feature Details on drawing number 1010-618
  - Prickly planting mix to prevent access into SBI
  - Nellie's Clough Buffer Zone Boundary

NOTE

Areas to be planted with woodland mixes to receive a minimum of 300mm sub soil covered with 300mm topsoil giving a minimum depth of soil coverage of 600mm. The depth of sub soil is to be increased to a minimum of 500mm over asbestos tips giving a minimum depth of soil coverage of 800mm.

Areas to receive wildflower seed mix only to receive a minimum of 450mm sub soil covered with 150mm topsoil giving a minimum depth of soil coverage of 600mm. The depth of sub soil is to be increased to a minimum of 650mm over asbestos tips giving a minimum depth of soil coverage of 800mm.

All topsoil to comply with BS 3882:2007 'Specification for topsoil and requirements for use'. Topsoil to be spread across seeded and woodland areas to be to classification 'specific purpose topsoil, low fertility'. Each tree and shrub to be planted with 0.1m3 topsoil of classification 'multipurpose grade'.

All sub and top soils to be freely draining.

Soils inside the access tracks to the SUDS ponds to be specified by engineers.

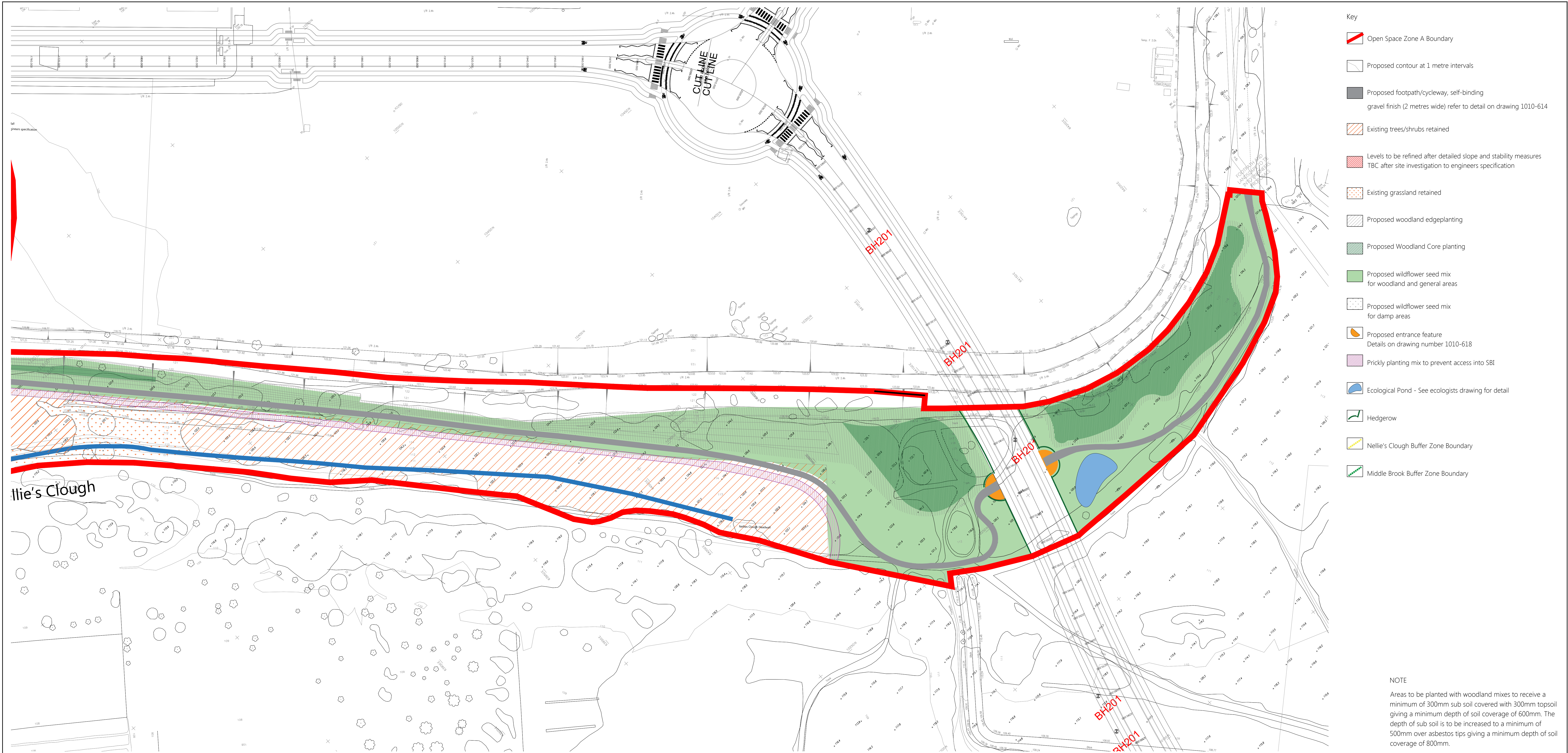
All placed soils to be to open space standards.

Where no contours are shown existing contours are to be retained

Engineers to confirm: extent and methodologies of the remediation measures; design of SUDS ponds including access tracks; structural stability/reinforcement of slopes including top and sub soils; depth of top and sub soils; extent of tree/ vegetation removal

CONTINUED ON DRAWING 1010-621F





- Key
- Open Space Zone A Boundary
  - Proposed contour at 1 metre intervals
  - Proposed footpath/cycleway, self-binding gravel finish (2 metres wide) refer to detail on drawing 1010-614
  - Existing trees/shrubs retained
  - Levels to be refined after detailed slope and stability measures TBC after site investigation to engineers specification
  - Existing grassland retained
  - Proposed woodland edgeplanting
  - Proposed Woodland Core planting
  - Proposed wildflower seed mix for woodland and general areas
  - Proposed wildflower seed mix for damp areas
  - Proposed entrance feature Details on drawing number 1010-618
  - Prickly planting mix to prevent access into SBI
  - Ecological Pond - See ecologists drawing for detail
  - Hedgerow
  - Nellie's Clough Buffer Zone Boundary
  - Middle Brook Buffer Zone Boundary

NOTE

Areas to be planted with woodland mixes to receive a minimum of 300mm sub soil covered with 300mm topsoil giving a minimum depth of soil coverage of 600mm. The depth of sub soil is to be increased to a minimum of 500mm over asbestos tips giving a minimum depth of soil coverage of 800mm.

Areas to receive wildflower seed mix only to receive a minimum of 450mm sub soil covered with 150mm topsoil giving a minimum depth of soil coverage of 600mm. The depth of sub soil is to be increased to a minimum of 650mm over asbestos tips giving a minimum depth of soil coverage of 800mm.

All topsoil to comply with BS 3882:2007 'Specification for topsoil and requirements for use'. Topsoil to be spread across seeded and woodland areas to be to classification 'specific purpose topsoil, low fertility'. Each tree and shrub to be planted with 0.1m<sup>3</sup> topsoil of classification 'multipurpose grade'.

All sub and top soils to be freely draining.

Soils inside the access tracks to the SUDS ponds to be specified by engineers.

All placed soils to be to open space standards.

Where no contours are shown existing contours are to be retained

Engineers to confirm: extent and methodologies of the remediation measures; design of SUDS ponds including access tracks; structural stability/reinforcement of slopes including top and sub soils; depth of top and sub soils;

#### Wildflower Seed Mix for Woodland Areas

Supplier:	John Chambers or similar approved
Reference:	Heritage Woodland and Shaded Areas 80% Grass Seed Wildflower Mix PRODUCT CODE: 130JC4110-PRO
Species:	Low Maintenance Grass Seed, Agrimony, Wild Angelica, Wood Avens, Hedge Bedstraw, Nettle-leaved Bellflower, Betony, Bluebell, Red Campion, Columbine, Foxglove, Common Hemp Nettle, Meadowsweet, Garlic Mustard, Primrose, Ragged-Robin, Ramsoms, Wood Sage, Selfheal, St John's Wort, Greater Stitchwort, Tufted Vetch, Hedge Woundwort, Upright Hedge-Parsley
Sowing rate:	5g/m <sup>2</sup>

#### Wildflower Seed Mix for Damp Areas

Supplier:	Emorsgate or similar approved
Reference:	EM8
Species:	Agrostis stolonifera, Cynosurus cristatus, Anthoxanthum odoratum, Deschampsia caespitosa, Alopecurus geniculatus, Glyceria fluitans, Silene flos-cuculi, Filipendula ulmaria, Ranunculus repens, Pulicaria dysenterica, Caltha palustris, Stachys palustris, Achillea ptarmica, Lythrum salicaria
Sowing rate:	5g/m <sup>2</sup>

PLANNING DRAWING  
THIS DRAWING IS FOR PLANNING PURPOSES ONLY AND  
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client  
**Bluemantle**  
project  
**Horwich Loco Works**

drawing title  
**LANDSCAPE OPEN SPACE ZONE A  
PLANTING PLAN - SHEET 2 OF 2**

scale @ A0 1:500	1010-626 C
date 04.10.18	
drn JS chkd AW	

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Original paper size (A0) 841x1189mm





Riko Railway Train by Timberplay (Ref:4.24120)



Jumping Blocks by Jupiter Play (Ref:906110300R)



Varioset Fohr Ship by Jupiter Play (Ref:V01140050R)



Double Cradle Seat Swings by Jupiter Play (Ref:901035100R)



Riko Railway Train by Timberplay (Ref:4.24120)



Xylophone by Jupiter Play (Ref:XIL)



Channel Slide by Jupiter Play (Ref:9023380225u)



Stepping Stilts by Jupiter Play (Ref:906110400R)



Climbing Structure by Timberplay (Ref:6.51005)



Cradle Nest Swing by Timberplay (Ref:6.14520)



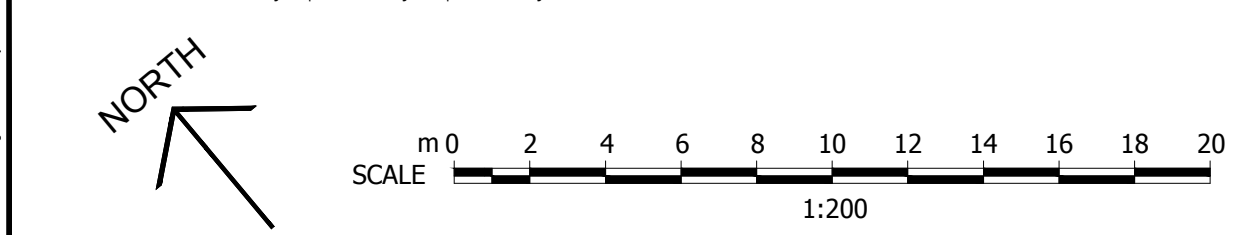
Rotating Disc by Timberplay (Ref:7.26000)



Hexagonal Swing by Timberplay (Ref:6.17020)

LEGEND

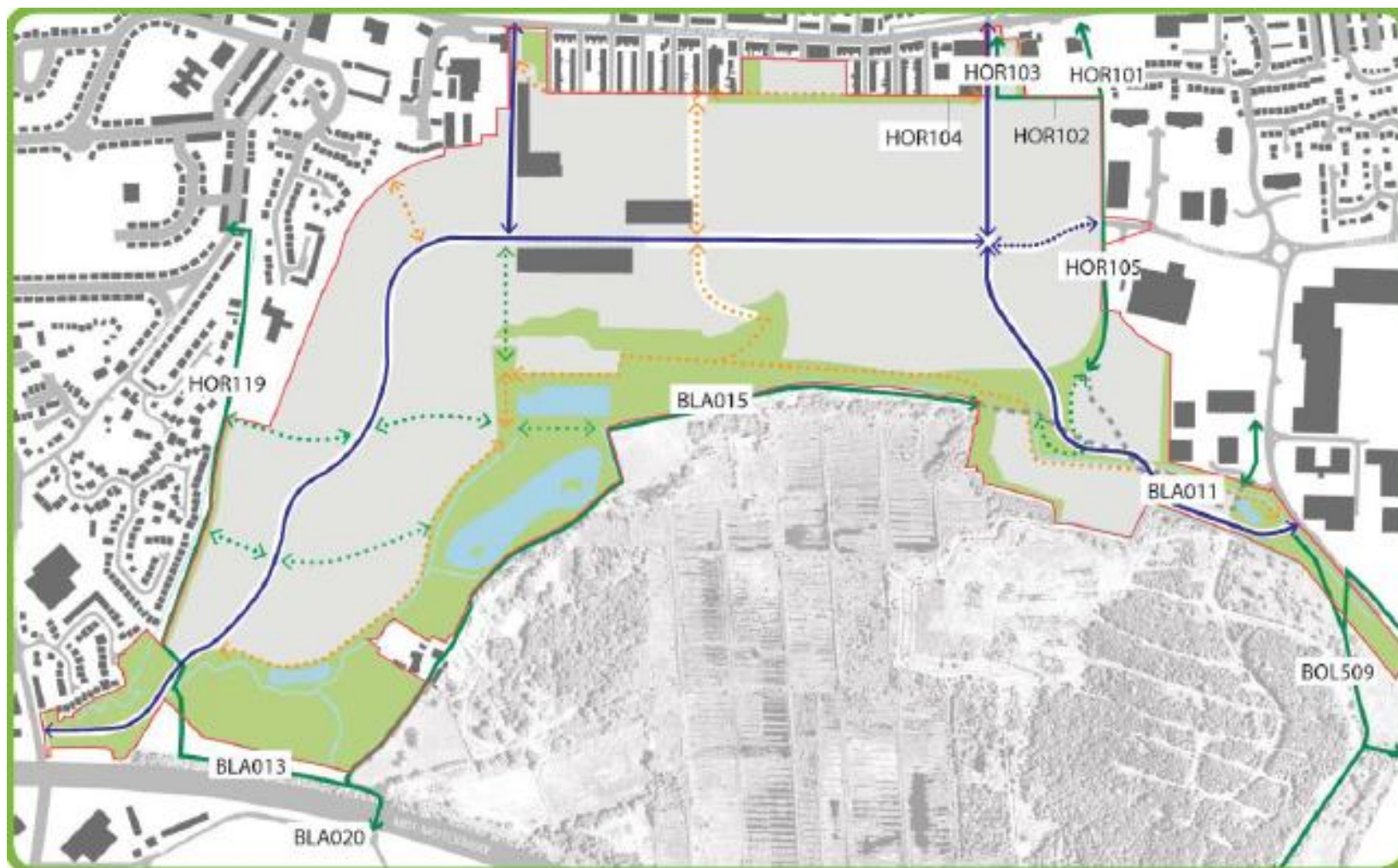
- Proposed Woodland
- Proposed 14-16 Betula Carpinus Tree Planting
- Proposed Amenity Grass Area
- Proposed Footpath
- Proposed Bark Mulch Safety Surfacing
- Proposed Boulders
- Proposed Bench
- Proposed Cycle Stand
- Proposed Litter Bin



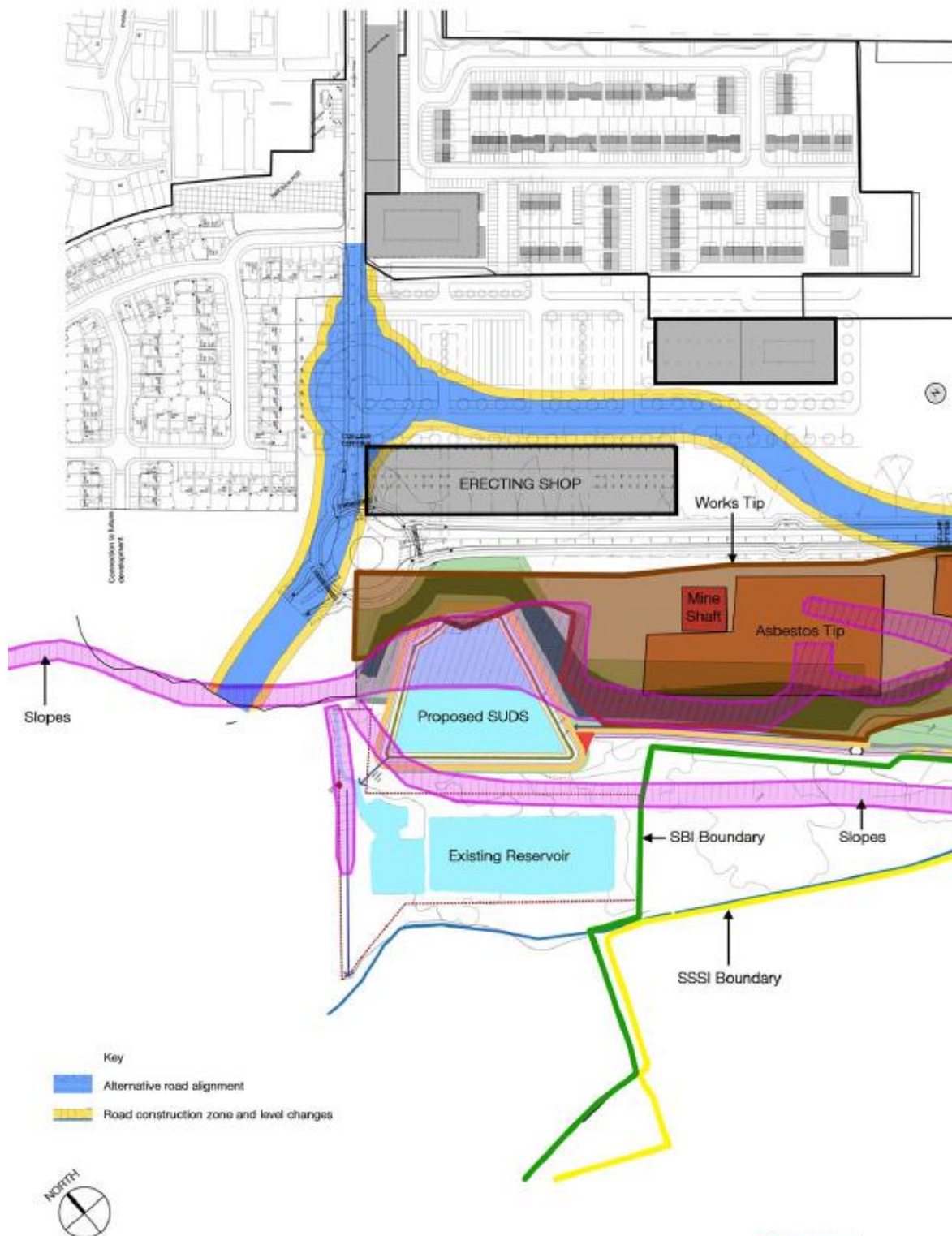
A	Tree Species Added	23.11.18	JS	KC
Rev	Description	Date	By	Chkd
REVISIONS				
purpose				
PLANNING				
Cass associates		architecture masterplanning planning landscape conservation		
Studio 204B The Tea Factory, 82 Wood Street, Liverpool, L1 4DQ T +44 (0)151 7070 0110 E info@cassassociates.co.uk W www.cassassociates.co.uk				
client Bluemantle				
project Rivington Chase				
drawing title Playground General Arrangement				
scale @ A1 1:200				1010-650 A
date 21/11/2018				
drn	PS	chk'd KC		



Route of spine road as envisaged within the masterplan/outline approval

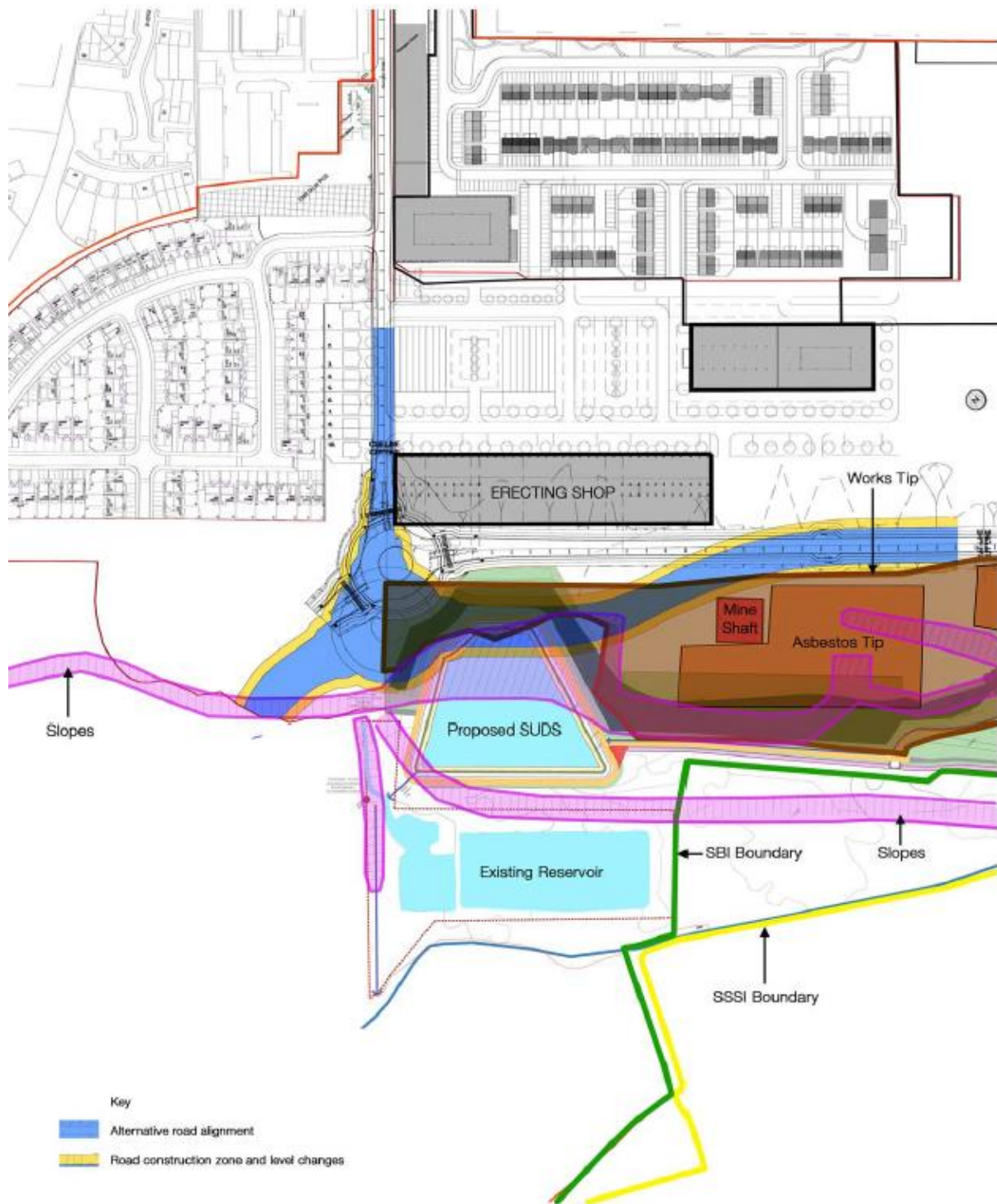






**RIVINGTON CHASE**  
**SPINE ROAD ALIGNMENT - OPTION 1**

**Cass** 1010-SK-001  
 associates JULY 2018  
 REVISION - (00.00.00)



RIVINGTON CHASE  
SPINE ROAD ALIGNMENT - OPTION 2